



AGENDA

CALGARY PLANNING COMMISSION

**TO BE HELD 2018 JANUARY 25 AT 1:00 PM
IN COUNCIL CHAMBERS**

- 1. Confirmation of Agenda**
- 2. Confirmation of Minutes of Previous Meeting**
- 3. Consent Agenda**
- 4. Consideration of Tabled/Referred Items**
- 5. Consideration of Planning Items**
- 6. Consideration of Miscellaneous Items**
- 7. Adjournment**

NOTE:
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



CALGARY
PLANNING
COMMISSION

A CENTURY OF VISION 1911 - 2011

CONSENT AGENDA

ITEM NO.: 3.01

Jillian Geen

COMMUNITY:

SOUTHVIEW (WARD 9)

FILE NUMBER:

LOC2017-0337

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

2530 - 35 Street SE

APPLICANT:

Calgary Aging in Place Co-Operative

OWNER:

Jill Moreton
Richard Tew

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.02

Sabrina Brar

COMMUNITY:

RIVERBEND (WARD 12)

FILE NUMBER:

LOC2017-0331

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

103 Riverstone Close SE

APPLICANT:

Muhammad Khan

OWNER:

Muhammad Khan
Shahida Perveen

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.03 Breanne Harder
COMMUNITY: PENBROOKE MEADOWS (WARD 9)
FILE NUMBER: LOC2017-0348
PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One Dwelling (R-C1s) District
MUNICIPAL ADDRESS: 112 Penmeadows Close SE
APPLICANT: Valerie Stephenson
OWNER: Valerie Stephenson
Rob Stephenson
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 3.04 Madeleine Krizan
COMMUNITY: EVERGREEN (WARD 13)
FILE NUMBER: LOC2017-0355
PROPOSED REDESIGNATION: From: Residential – One Dwelling (R-1) District
To: Residential – One Dwelling (R-1s) District
MUNICIPAL ADDRESS: 6 Everhollow Green SW
APPLICANT: Muhammad Mumtaz Alam
OWNER: Muhammad Mumtaz Alam
Yasmeen Akhtar
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 3.05 Ezra Wasser

COMMUNITY: HAYSBORO (WARD 11)

FILE NUMBER: LOC2017-0350

PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One Dwelling (R-C1s) District

MUNICIPAL ADDRESS: 24 Harmon Place SW

APPLICANT: Harmon Holdings Ltd

OWNER: Harmon Holdings Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 3.06 Ezra Wasser

COMMUNITY: HAYSBORO (WARD 11)

FILE NUMBER: LOC2017-0351

PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One Dwelling (R-C1s) District

MUNICIPAL ADDRESS: 20 Harmon Place SW

APPLICANT: Harmon Holdings Ltd

OWNER: Harmon Holdings Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 3.07 Fraser McLeod

COMMUNITY: SOUTHWOOD (WARD 11)

FILE NUMBER: LOC2017-0321

PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One Dwelling (R-C1s) District

MUNICIPAL ADDRESS: 45 Snowdon Crescent SW

APPLICANT: Janey Chupik

OWNER: Pulse Renovations Ltd

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 3.08 Giyan Brenkman

COMMUNITY: SILVER SPRINGS (WARD 1)

FILE NUMBER: LOC2017-0328

PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1) District
To: Residential – Contextual One Dwelling (R-C1s) District

MUNICIPAL ADDRESS: 7716 Silver Springs Road NW

APPLICANT: Angel Ann Marsh

OWNER: Angel Ann Marsh
Christopher John Marsh

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 3.09

Giyana Brenkman

COMMUNITY:

HAWKWOOD (WARD 2)

FILE NUMBER:

LOC2017-0327

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

375 Hawkland Circle NW

APPLICANT:

Vahideh Nejat

OWNER:

Vahideh Nejat
Hamid Hadad Razavi

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.10

Jennifer Cardiff

COMMUNITY:

THORNCLIFFE (WARD 4)

FILE NUMBER:

LOC2017-0357

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

6324 Thorncliffe Drive NW

APPLICANT:

Phillip Pattison

OWNER:

Phillip Pattison
Jennifer Pattison

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.11

Jillian Geen

COMMUNITY:

CHARLESWOOD (WARD 4)

FILE NUMBER:

LOC2017-0362

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

2428 Chicoutimi Drive NW

APPLICANT:

Michael McWilliam

OWNER:

Michael McWilliam
Lindsay McWilliam

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.12

Giyen Brenkman

COMMUNITY:

COLLINGWOOD (WARD 4)

FILE NUMBER:

LOC2017-0322

PROPOSED REDESIGNATION:

From: Residential – Contextual One Dwelling (R-C1)
District

To: Residential – Contextual One Dwelling (R-C1s)
District

MUNICIPAL ADDRESS:

3216 Carol Drive NW

APPLICANT:

Jessica Chan

OWNER:

532723 Alberta Ltd (Jessica Chan)

ADMINISTRATION RECOMMENDATION:

APPROVAL

ITEM NO.: 3.13 Giyan Brenkman
COMMUNITY: TARADALE (WARD 5)
FILE NUMBER: LOC2017-0324
PROPOSED REDESIGNATION: From: Residential – One Dwelling (R-1) District
To: Residential – One Dwelling (R-1s) District
MUNICIPAL ADDRESS: 121 Taralea Green NE
APPLICANT: Amarjit Tambar
OWNER: Amandeep Tambar
Amarjit Tambar
ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 3.14 Giyan Brenkman
COMMUNITY: CASTLERIDGE (WARD 5)
FILE NUMBER: LOC2017-0317
PROPOSED REDESIGNATION: From: Residential – Contextual One Dwelling (R-C1)
District
To: Residential – Contextual One Dwelling (R-C1s)
District
MUNICIPAL ADDRESS: 192 Castleglen Way NE
APPLICANT: 1742705 Alberta Ltd (Bhagwant Singh)
OWNER: 1742705 Alberta Ltd (Bhagwant Singh)
ADMINISTRATION RECOMMENDATION: APPROVAL

TABLED/REFERRED ITEMS

ITEM NO.: 4.01 Martin Beck
(Referred back to Administration on 2017 December 14)

COMMUNITY: SADDLE RIDGE (WARD 5)

FILE NUMBER: LOC2016-0196(OP)

PROPOSED OUTLINE PLAN: Subdivision of 1.39 hectares ± (3.43 acres ±)

MUNICIPAL ADDRESS: 6819 – 89 Avenue NE

APPLICANT: Civil Engineering Solutions

OWNER: 1976722 Alberta Ltd (Sada Investments Ltd)

ADMINISTRATION RECOMMENDATION: **APPROVAL**

INDEX OF PLANNING ITEMS

ITEM NO.: 5.01 Gareth Webster

COMMUNITY: MEDICINE HILL (WARD 6)

FILE NUMBER: DP2017-2343

PROPOSED DEVELOPMENT: New: Liquor Store, Outdoor Cafe, Fitness Centre, Retail and Consumer Service, Restaurant: Food Service Only - Medium, Restaurant: Licensed - Medium, Restaurant: Licensed - Large (4 Buildings)

MUNICIPAL ADDRESS: 2200 Na'a Drive SW
8395 Canada Olympic Drive SW

APPLICANT: B&A Planning Group

OWNER: Plateau Village Properties Inc

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.02 Shane Gagnon

COMMUNITY: SADDLE RIDGE (WARD 5)

FILE NUMBER: DP2017-1042

PROPOSED DEVELOPMENT: New: Liquor Store, Medical Clinic, Outdoor Café, Restaurant: Food Service Only – Small, Fitness Centre, Convenience Food Store, Supermarket, Restaurant: Licensed – Medium, Restaurant: Food Service Only – Medium, Drinking Establishment – Medium, Retail and Consumer Service, Child Care Service, Office, Restaurant: Licensed – Small (8 Buildings)

MUNICIPAL ADDRESS: 9320 – 52 Street NE

APPLICANT: Abugov Kaspar

OWNER: Genstar Titleco Limited

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.03 Steve Jones

COMMUNITY: SUNNYSIDE (WARD 7)

FILE NUMBER: LOC2016-0079

PROPOSED REDESIGNATION: From: DC Direct Control District
To: Multi-Residential – Contextual Grade-Oriented (M-CG) District

MUNICIPAL ADDRESS: 802 – 2 Avenue NW

APPLICANT: O2 Planning and Design

OWNER: Rob Froese

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 5.04 Fraser McLeod

COMMUNITY: SAGE HILL (WARD 2)

FILE NUMBER: LOC2017-0296

PROPOSED REDESIGNATION: From: Multi-Residential – Low Profile (M-1d60) District
To: Commercial – Neighbourhood 2 (C-N2) District

MUNICIPAL ADDRESS: 45 Sage Meadows Circle NW

APPLICANT: B&A Planning Group

OWNER: Genesis Land Development Corporation

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 5.05 Ryan Hall

COMMUNITY: ALTADORE (WARD 8)

FILE NUMBER: LOC2017-0290

PROPOSED POLICY AMENDMENT: Amendment to the South Calgary/Altadore Area
Redevelopment Plan

PROPOSED REDESIGNATION: From: Residential-Contextual Two Dwelling (R-C2)
District
To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS: 3822 - 18 Street SW

APPLICANT: Willix Developments

OWNER: Willix Developments Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.06 Ryan Hall

COMMUNITY: ALTADORE (WARD 8)

FILE NUMBER: LOC2017-0291

PROPOSED POLICY AMENDMENT: Amendment to the South Calgary/Altadore Area
Redevelopment Plan

PROPOSED REDESIGNATION: From: Residential-Contextual Two Dwelling (R-C2)
District
To: Residential – Grade-Oriented Infill (R-CG)
District

MUNICIPAL ADDRESS: 3823 - 17 Street SW

APPLICANT: Willix Developments

OWNER: Willix Developments Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.07 Felix Ochieng

COMMUNITY: BELTLINE (WARD 11)

FILE NUMBER: LOC2017-0223

PROPOSED POLICY AMENDMENT: Amendment to the Beltline Area Redevelopment Plan

PROPOSED REDESIGNATION: From: Centre City Multi-Residential High Rise District (CC-MH)
To: Centre City Multi-Residential High Rise Support Commercial District (CC-MHX)

MUNICIPAL ADDRESS: 111 – 14 Avenue SE

APPLICANT: CityTrend

OWNER: Spring Creek Development Corporation
1376743 Alberta Ltd (Jay C.P. Damen, Jane M. Hoffman)

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 5.08 Calvin Chan

COMMUNITY: GLENBROOK (WARD 6)

FILE NUMBER: LOC2017-0295

PROPOSED REDESIGNATION: From: Residential - Contextual Two Dwelling (R-C2) District
To: Residential - Grade-Oriented Infill (R-CG) District

MUNICIPAL ADDRESS: 3340 - 41 Street SW

APPLICANT: Maria Livaditis

OWNER: Maria Livaditis
Billy Koutselas

ADMINISTRATION RECOMMENDATION: APPROVAL

ITEM NO.: 5.09

Dino Civitarese
(related to Item 5.10)

COMMUNITY: WEST SPRINGS (WARD 6)

FILE NUMBER: LOC2017-0213

PROPOSED POLICY AMENDMENT: Amendment to the West Springs Area Structure Plan

PROPOSED REDESIGNATION: From: DC Direct Control District

To: Residential – One Dwelling (R-1s) District and
Special Purpose – School, Park and Community
Reserve (S-SPR) District

MUNICIPAL ADDRESS: 7750 - 11 Avenue SW

APPLICANT: WSP Canada

OWNER: Richard Press
Jennifer Press

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.10

Dino Civitarese
(related to Item 5.09)

COMMUNITY: WEST SPRINGS (WARD 6)

FILE NUMBER: LOC2017-0213(OP)

PROPOSED OUTLINE PLAN: Subdivision of 1.93 hectares ± (4.77 acres ±)

MUNICIPAL ADDRESS: 7750 - 11 Avenue SW

APPLICANT: WSP Canada

OWNER: Richard Press
Jennifer Press

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.11 Lisette Burga Ghersi

COMMUNITY: KINGSLAND (WARD 11)

FILE NUMBER: DP2016-2458

PROPOSED DEVELOPMENT: New: Multi-Residential Development, Live Work Units (1 Building), Sign – Class B (Fascia Sign)

MUNICIPAL ADDRESS: 617, 623, 627, 631, 635, 639, 643, 703, 707, 711 and 715 - 69 Avenue SW

APPLICANT: Sturgess Architecture

OWNER: James T F Lee
Jean Libin
Janl Developments Ltd

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.12 Christine Leung
(related to Item 5.13)

COMMUNITY: RED CARPET (WARD 9)

FILE NUMBER: LOC2015-0085

PROPOSED CLOSURE: 0.04 hectares ± (0.09 acres ±) of road adjacent to 825, 841, 901 – 68 Street SE

PROPOSED REDESIGNATION: From: Special Purpose – Future Urban Development (S-FUD) and Undesignated Road Right-of-Way

To: Multi-Residential – Contextual Grade-Oriented (M-CGd60) District, Multi-Residential – High Density Low Rise (M-H1h18d155) District, and Commercial – Corridor 1 (C-COR1f3.0h16) District

MUNICIPAL ADDRESS: 809, 825, 841 and 901 – 68 Street SE

APPLICANT: Casola Koppe

OWNER: Lansdowne Equity Ventures Ltd
The City of Calgary

ADMINISTRATION RECOMMENDATION: **APPROVAL**

ITEM NO.: 5.13

Christine Leung
(related to Item 5.12)

COMMUNITY:

RED CARPET (WARD 9)

FILE NUMBER:

LOC2015-0085(OP)

PROPOSED OUTLINE PLAN:

Subdivision of 11.37 hectares ± (28.09 acres ±)

MUNICIPAL ADDRESS:

809, 825, 841 and 901 – 68 Street SE

APPLICANT:

Casola Koppe

OWNER:

Lansdowne Equity Ventures Ltd
The City of Calgary

ADMINISTRATION RECOMMENDATION:

APPROVAL

A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument mounted on a tripod. The surveyor is looking through the instrument. The entire image is rendered in a dark grey color with white outlines.

Calgary Planning Commission 2018 January 25

Item 3.01

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

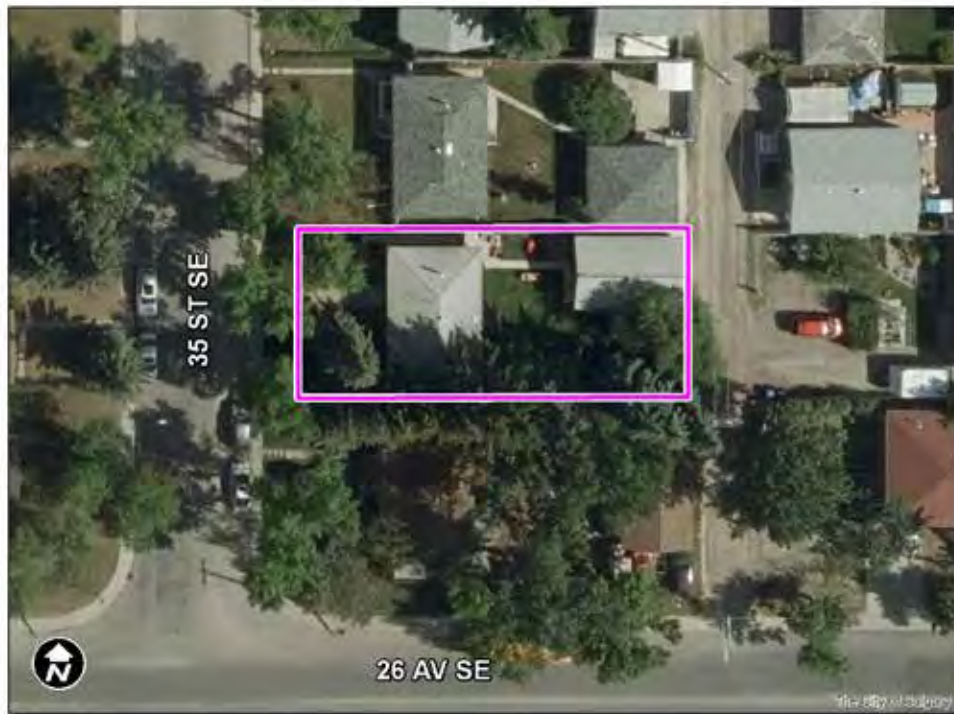
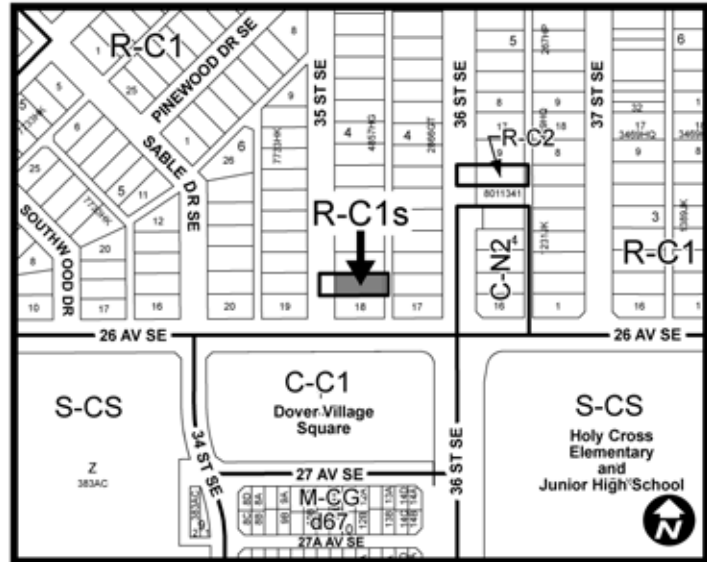
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

LOCATION MAPS



LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.15 acres \pm) located at 2530 - 35 Street SE (Plan 4857HG, Block 4, Lot 19) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

Applicant:

Calgary Aging in Place Co-Operative

Landowner:

Jill Moreton
Richard Tew

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Southview, the site is approximately 16 metres by 36 metres in size and is developed with a one-storey single detached dwelling and a detached two-car garage that is accessed from the rear lane. Surrounding development consists of low density residential R-C1 development to the north, east, south, and west of the site.

According to data from The City of Calgary Census, the following table identifies Southview's peak population and year, current 2017 population, and the population amount and percentage difference between the peak and current populations.

| Southview | |
|------------------------------------|---------|
| Peak Population Year | 1970 |
| Peak Population | 3,464 |
| 2017 Current Population | 1,841 |
| Difference in Population (Number) | - 1,623 |
| Difference in Population (Percent) | - 47% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the Development Permit process. A Development Permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a Building Permit would be required.

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Southview.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from 35 Street SE and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within an approximately 100 metre walking distance on 26 Avenue SE, and a bus stop location within an approximately 200 metre walking distance on 36 Street SE. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the Development Permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Southview Community Association. Follow-up communication was sent to confirm receipt of the circulation application. At the time of submission, no comments were received.

Citizen Comments

Administration received one (1) letter signed by five (5) residents in support of the application. The residents represent the four (4) adjacent homes (north, east, south and west).

No specific reasons are stated for the support.

Public Meetings


No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
SOUTHVIEW (WARD 9)
35 STREET SE NORTH OF 26 AVENUE SE

MAP 9E

APPENDIX I

APPLICANT'S SUBMISSION

| | |
|--|--|
|  | <p>Land Use Redesignation Applicant's Submission Secondary Suites PL 1264 (R2017-09)</p> |
| <p>This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments must be limited to the area designated on this page to ensure it will fit the space requirements of the report.</p> | |
| <p>1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary? The added living quarters/suite will only be a small density increase in keeping with a fully occupied residential home using existing infrastructure and space. The living quarters/suite will allow a senior couple to remain in their home with the support of a live-in caregiver or by adding income to support home care. The couple will be able to maintain a high quality of life and independence in their own community and reduced costs associated with institutional care on the health care infrastructure of the city.</p> | |
| <p>2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response? Once we have a file number to proceed we will engage with our neighbours, Community Association and Ward Councillor and provide details of the response.</p> | |
| <p>3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel? Parking is available for 2-3 vehicles directly in front of the property and there is off-street parking in a detached garage at the rear with alley access.</p> | |
| <p>4) Are there any potential negative impacts of this development that you are aware of? We see no negative impacts to this development as this is a diverse, mixed use area with plenty of parking.</p> | |
| <p>NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans</p> | |

APPENDIX II

IMPORTANT TERMS

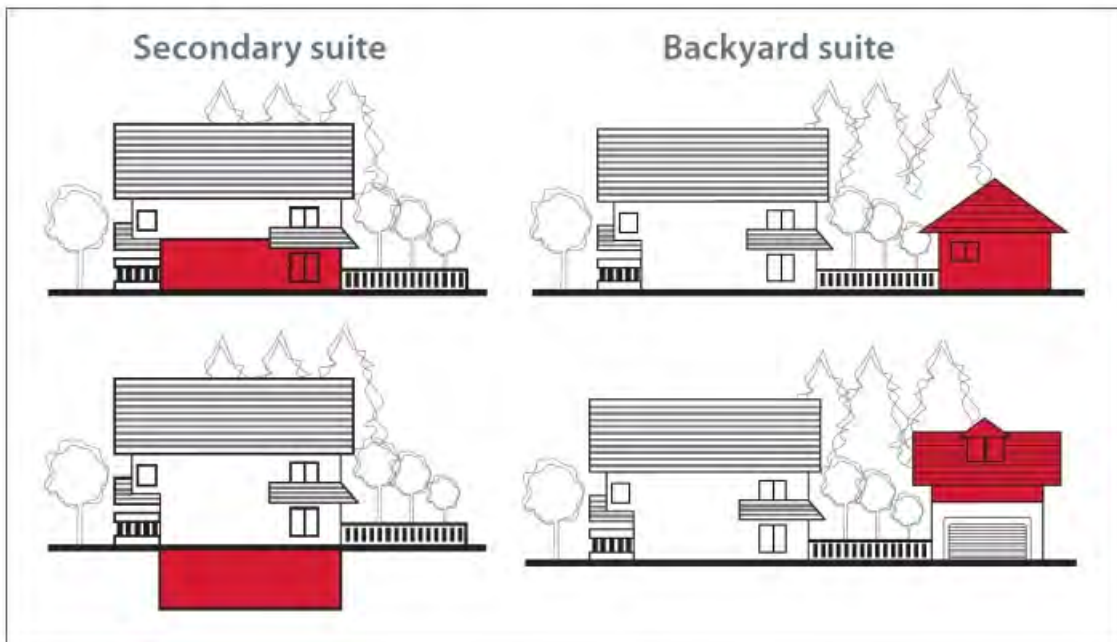
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is a large, complex piece of equipment with a circular level and a vertical staff. The entire image is rendered in a dark grey color against a white background.

Calgary Planning Commission 2018 January 25

Item 3.02

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

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2018 January 25

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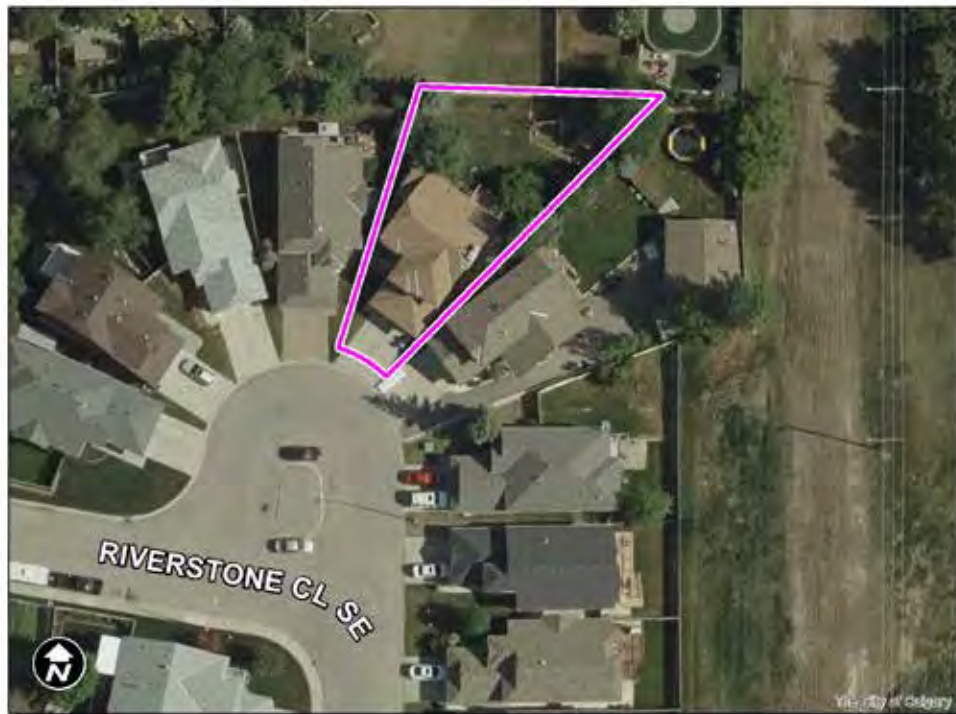
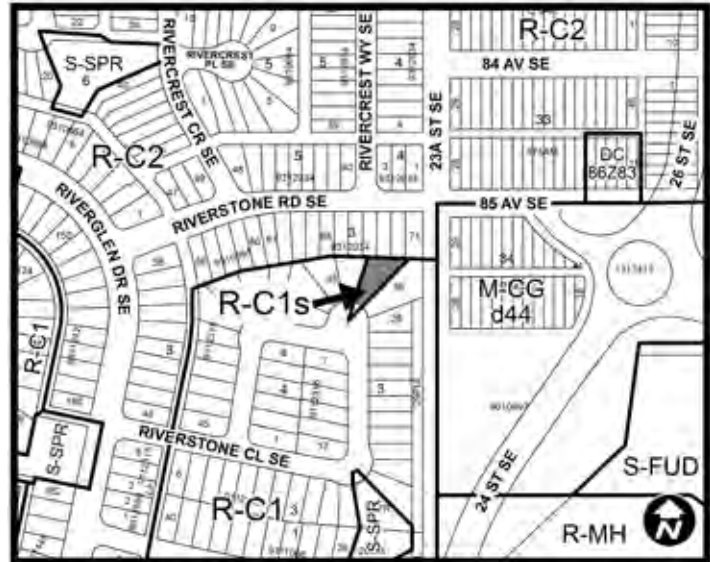
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LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

LOCATION MAPS



LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.07 hectares \pm (0.14 acres \pm) located at 103 Riverstone Close SE (Plan 9112315, Block 3, Lot 31) from Residential – Contextual One Dwelling (R-C1) to Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

Applicant:

Muhammad Khan

Landowner:

Muhammad Khan
Shahida Perveen

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Riverbend, the site is approximately 18 metres by 42 metres in size and is developed with a two-storey single detached dwelling two-car attached garage that is accessed from Riverstone Close SE. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Riverbend's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Riverbend | |
|------------------------------------|--------|
| Peak Population Year | 2002 |
| Peak Population | 10,773 |
| 2017 Current Population | 9,338 |
| Difference in Population (Number) | -1,435 |
| Difference in Population (Percent) | -13% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Riverbend.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Riverstone Close SE and there is no rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 300 metre walking distance of the site on Riverglen Drive SE. On-street parking adjacent to the site is (unregulated through the Calgary Parking Authority’s residential parking permit system).

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

The Riverbend Community Association provided a letter of no objection, but did have comments on secondary suite applications. The Community Association also had concerns regarding parking. The letter is attached in APPENDIX II.

Citizen Comments

Administration received 1 (one) letter in opposition to the application.

Reasons stated for opposition are summarized as follows:

- Parking concerns. The cul-de-sac is overparked and the current residents of the dwelling already have 4 vehicles parked on site. A day home also operates in the home and this adds to the parking issues of this cul-de-sac.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

My son or my daughter can live or I can rent
in future. City can get tax as well. Neighbours are
other community people can stay in case of any emergency.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

I am the member of Community Association. my relations
are very good with neighbours, we help to each other.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

I have double garage so that I can provide
a spot for secondary suite.

- 4) Are there any potential negative impacts of this development that you are aware of?

No

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

APPENDIX II

LETTERS SUBMITTED

From: RCA_President [<mailto:president@riverbendcommunity.ca>]
Sent: Wednesday, November 15, 2017 10:54 AM
To: CPAG Circ <CPAGCirc@calgary.ca>
Cc: Burga Ghersi, Lisette <Lisette.BurgaGheresi@calgary.ca>; Community Liaison Ward 12 <CAWard12@calgary.ca>
Subject: [EXT] RE: LOC2017-0331 - Circulation package

Good Morning,

Thank you so much for passing on the attached information.

First, let me say, we don't necessarily wish to object, but, do have some comments we would like to pass on.

The comments are not necessarily specific application (although there are some that are specific) - but any application for Secondary Suites.

Typically, the City looks only at one thing – does it meet any/all building requirements?

But from a Community point of view, we look at a much wider range of issues that may impact other residents, or the community as a whole.

- **It should be stated that as a community, we are generally in favor of the concept of Secondary Suites, if done within certain conditions.**

What we are not necessarily in favor of blanket approvals of Secondary Suite, as it opens the door too wide for any kind of development that may or may not be in keeping with the spirit of what a Secondary Suite was intended to do.

- In this case, looking at the application, it seems to be “open ended”.

As the applicant has noted in their response – the applicant's son or daughter can live here, or they can rent it out.

Though not a requirement from a City license point of view, this is one of the things we like to see. Although not explicitly stated, it is assumed that the primary home owner intends to stay with the residence.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

We feel that this is important in any application.

The reason is that by definition, a Secondary Suite is just that – a second suite. So, in order to have a Secondary Suite, you should have a Primary Suite which we feel should typically be the home owner. The whole idea behind a Secondary Suite is to provide a means for the home owner a means of supplementing their income due to any number of circumstances.

Given the economy, and ever looming layoffs, having a means to supplement income during tough times is a good thing, or to assist an aging parent or elder person keep the home they live in.

What it is intended for, at least our way of thinking, is when the Primary occupant leaves the residence, and then rents out both areas, the Primary and Secondary as two separate suites as then, what you have created is a de-factor duplex, and not a secondary suite as you now have two Secondary Suites since the Primary home owner no longer lives at the residence.

We have heard the City say that they cannot mandate that the home owner live at the residence, but, what the City can do is add a set of financial incentives to applicants to make it more attractive if they do live there. As the saying goes – money talks, and if a homeowner can get a break on fees attached to having a Secondary Suite, then they usually will as it helps things.

- **As far as Question 2** - the City asked how the individual engaged their neighbors. I am not sure that the response addressed the question.

So, it is not clear that the neighbors know. Good relations is not the same as saying they have been advised. What we find sometimes is that when they find out later – they tend to get upset.

Secondly, what we have learned from people is that in many cases, people assume that just because they live in a Community, they are automatically members of the Community Association. But, this is not always true. In fact, it is really only true in a Community like MacKenzie Towne (as one example) where residents pay a fee as part of their property tax to be members of what is called a Resident Association.

When we talk to most people, they have no idea that there is a difference between a Community Association (where membership is voluntary) and a Resident Association (where membership is mandatory).

- **As far as Question 3** – on the issue of parking. The response provided is great and typically is enough to meet the City's needs. But, this again goes back to the Community. The response assumes that there are only two cars involved. But, what happens if there are

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

more – for example a husband, wife, or three teens going to school, and each with their own car?

Secondly, the location of the property appears to be at the end of a cul-de-sac, so, if additional cars were introduced, it is not clear where they will park, except around the island in the middle.



Thirdly, the applicant noted that they “can”, but, it is not clear if they will. I am not saying they will not – just that it is not clear.

If they do not, that would mean increased parking on the island, which may or may not interfere with other residents using it for the same purpose, or even possibly garbage pick-up.

- **Legal Suite** – we also think the applicant should be commended for trying to follow the process, as we know that the City still has an issue with people creating illegal suites and not telling the City.
- **Secondary Suite Process** – at this time, we also know that City Council has yet to adopt a set of rules that is acceptable to a majority of Councilors, and the City of Calgary as a whole. Having said that, having worked with our new Councilor in the past on this, we know he has/had a great multi-bullet plan for Secondary Suites.

With the election of the new Council, we expect that the topic of Secondary Suites will again be arising. Our hope and desire is that a good framework is put in place for dealing with these and the concerns we have with them. Having said that, we know that our Councilor, Shane Keating has been doing all he can to resolve the lack of a framework in a way that makes sense for all concerned, and we are a big supporter of his efforts.

**LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE**

MAP 24S

As such, part of what we would like to see is a system of licensing so that applications can be reviewed on an ongoing basis like a home-basis business to address any problems that arise.

As part of the Secondary Suite debate that took place in 2015, we also felt that the Community Standards by-law should be reviewed sections added to address new problems that may not have been envisioned.

FINAL COMMENT(S):

- Looking at the application, we do not necessarily see any reason to object, but as noted, it would be nice to clarify some of the feedback, and at some point, for the City to address the issues around Secondary Suites once and for all to provide a workable framework for all.

LAND USE AMENDMENT
RIVERBEND (WARD 12)
SOUTH OF RIVERSTONE ROAD SE AND EAST OF 24 STREET
SE

MAP 24S

APPENDIX III

IMPORTANT TERMS

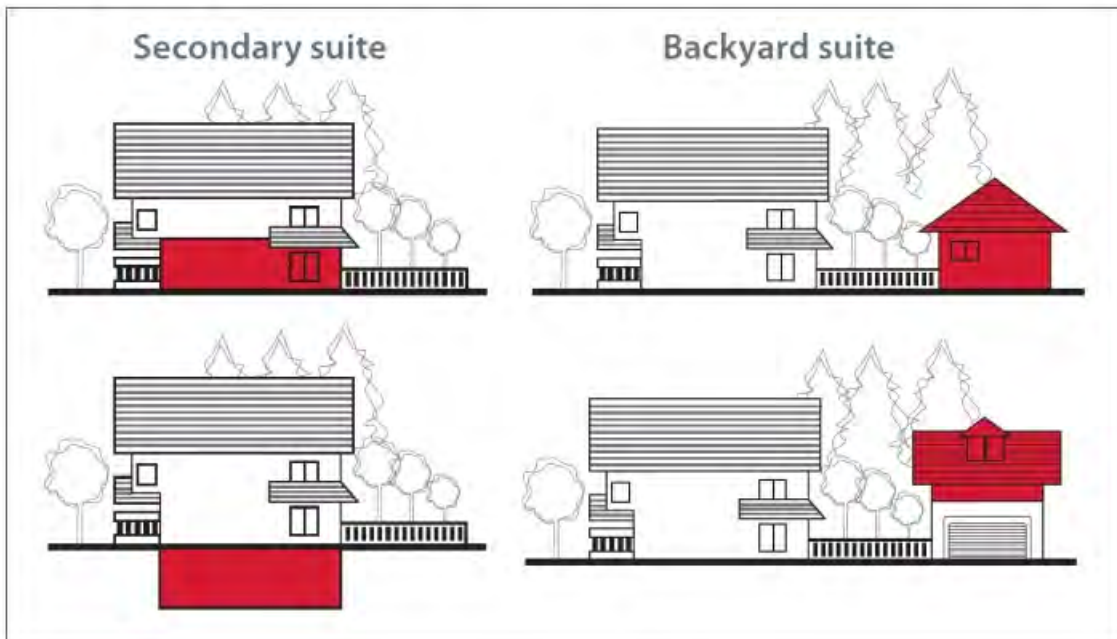
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a pencil in their right hand. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

Calgary Planning Commission 2018 January 25

Item 3.03

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. There is an existing suite located on the parcel and the application was submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

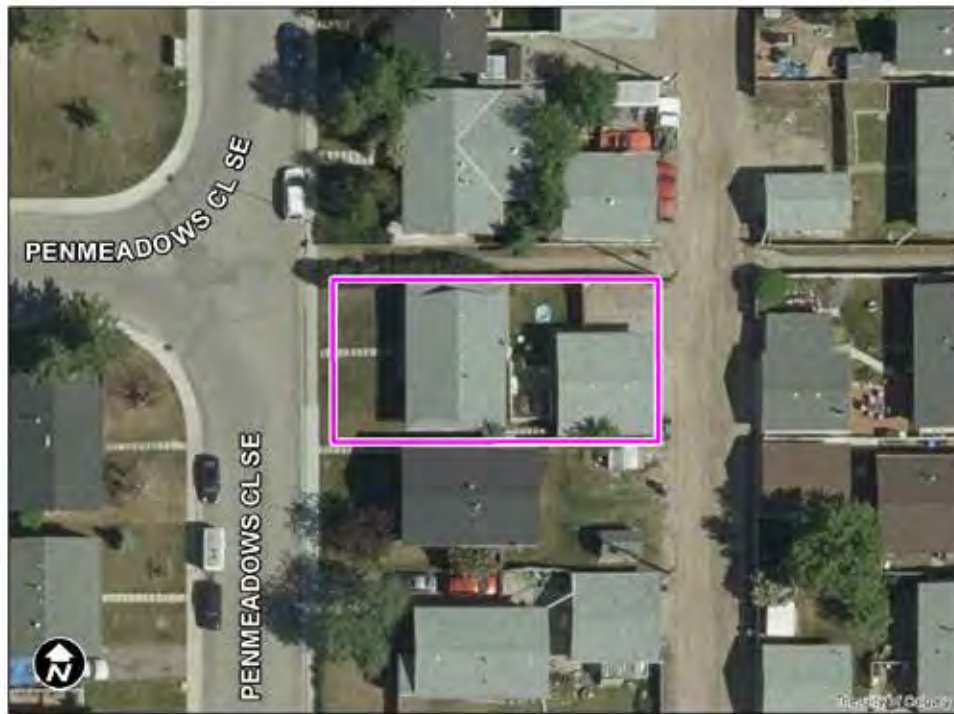
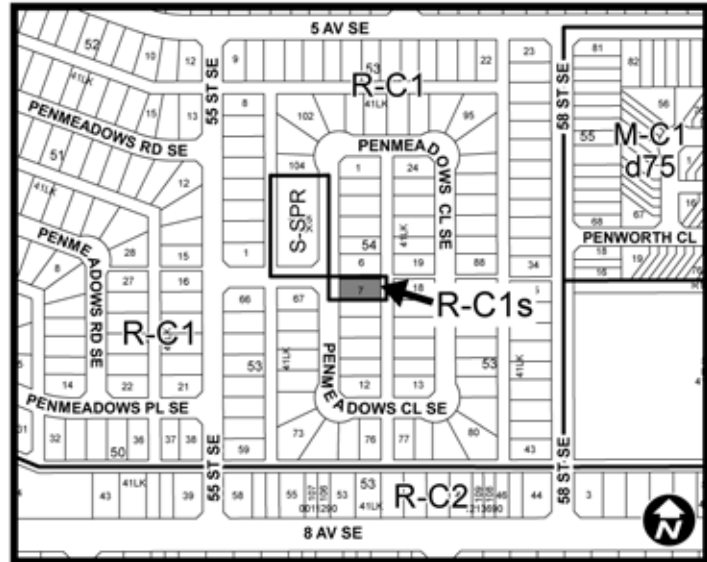
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

LOCATION MAPS



LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.12 acres \pm) located at 112 Penmeadows Close SE (Plan 41LK, Block 54, Lot 7) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

Applicant:

Valerie Stephenson

Landowner:

Valerie Stephenson
Rob Stephenson

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Penbrooke Meadows the site is approximately 15 metres by 30 metres in size and is developed with a single-storey single detached dwelling with a two-car garage that is accessed from rear lane and a two car gravel parking pad that is accessed from the rear lane. Residential uses exist to the north, east, south, and west of the site. A tot lot list located to the northwest of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Penbrooke Meadows' peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Penbrooke Meadows | |
|------------------------------------|--------|
| Peak Population Year | 1973 |
| Peak Population | 10,031 |
| 2017 Current Population | 8,324 |
| Difference in Population (Number) | -1,707 |
| Difference in Population (Percent) | -17% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

**LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE**

MAP 14E

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the Municipal Development Plan (MDP). While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Pembroke Meadows.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Penmeadows Close SE and the rear lane. The area is served by Calgary Transit bus service with a bus stop located within an approximately 400 metre walking distance of the site on 5 Avenue SE. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Penbrooke Meadows Community Association.

Citizen Comments

Administration did not receive any responses from citizens.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

The suite will help subsidize the mortgage payment for the owners and also provide reasonably priced housing for people on fixed incomes, creating sustainability for the community bringing an influx of people and funds to local businesses, thereby helping the economy, as well as increasing property values, which in turn increases property taxes collected by the City of Calgary.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

We talked to a few neighbors who said they have no problem with the idea of legalizing our suite. Also said they have not had any issues with any people that have lived in our house in the past, and wished us best of luck with our endeavor.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There is rear RV Parking which provides off-street parking for two vehicles as well as on-street parking for two vehicles in the front

- 4) Are there any potential negative impacts of this development that you are aware of?

no, none that we are aware of.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
PENBROOKE MEADOWS (WARD 9)
PENMEADOWS CLOSE SE AND 55 STREET SE

MAP 14E

APPENDIX III

IMPORTANT TERMS

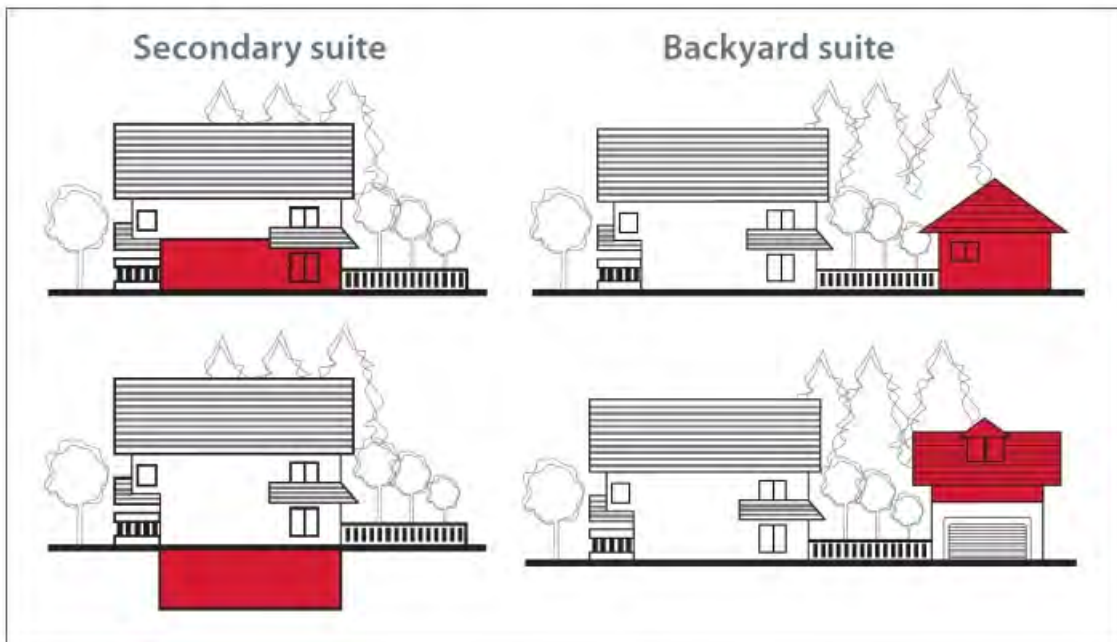
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a staff in their right hand. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

Calgary Planning Commission 2018 January 25

Item 3.04

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – One Dwelling (R-1) District to a Residential – One Dwelling (R-1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

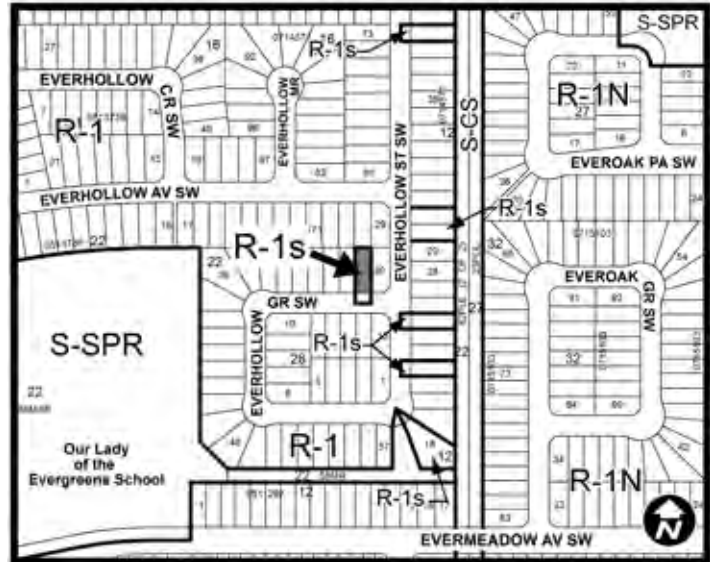
REASON(S) FOR RECOMMENDATION:

The proposed R-1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

LOCATION MAPS



LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.04 hectares \pm (0.09 acres \pm) located at 6 Everhollow Green SW (Plan 0811084, Block 22, Lot 31) from Residential – One Dwelling (R-1) District **to** Residential – One Dwelling (R-1s) District.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

Applicant:

Muhammad Mumtaz Alam

Landowner:

Muhammad Mumtaz Alam
Yasmeen Akhtar

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-1 setting in the community of Evergreen, the site is approximately 12 metres by 33 metres in size and is developed with a 2-storey single detached dwelling with an attached two-car garage that is accessed from Everhollow Green SW. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Evergreen's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Evergreen | |
|------------------------------------|--------|
| Peak Population Year | 2015 |
| Peak Population | 21,700 |
| 2017 Current Population | 21,439 |
| Difference in Population (Number) | - 261 |
| Difference in Population (Percent) | - 1% |

LAND USE DISTRICTS

The proposed R-1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential; Developing; Planned Greenfield with Area Structure Plan (ASP)” area as identified on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and Housing Diversity and Choice policies (subsection 2.3.1).

Midnapore III Community Plan (1997)

The site is located within a “Neighbourhood Area” as identified on the Land Use Concept Map (Map 2) in the *Midnapore III Community Plan*. While the community plan makes no specific reference to the site, this land use proposal is consistent with Neighbourhood Area policies (section 1.6) that encourage a range of housing types to support a variety of lifestyles and income levels.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Everhollow Green SW and there is no rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 600 metre walking distance of the site on Eversidge Drive SW. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available on Everhollow Green SW and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

ENVIRONMENTAL ISSUES

No environmental issues have been identified. An Environmental Site Assessment was not required.

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Evergreen Community Association.

Citizen Comments

Administration received two (2) letters in opposition to the application.

Reasons stated for opposition are summarized as follows:

- Limited / no parking available on street due to prevalence of front driveways;
- Not being engaged by applicant;
- Increase in prowling; and
- Decrease in property value.

Administration has considered the planning related concerns as follows:

- The subject site has the ability to provide the minimum required parking stalls (two) for the dwelling and secondary suite on site, as there is an existing two-car garage.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

APPENDIX I

APPLICANT'S SUBMISSION

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

I can develop legal secondary suites per city regulations and it will develop under city rules and regulations. I have two sons so later upper and lower level will be used by sons.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

A mail contacted with my neighbors and they don't have any issue

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

I have double garage parking, also driveway parking, and my neighboring street have a lot parking so parking is never issue in my neighboring street / community.

- 4) Are there any potential negative impacts of this development that you are aware of?

There is no negative impacts.

LAND USE AMENDMENT
EVERGREEN (WARD 13)
EVERHOLLOW GREEN SW WEST OF EVERHOLLOW STREET
SW

MAP 31SS

APPENDIX II

IMPORTANT TERMS

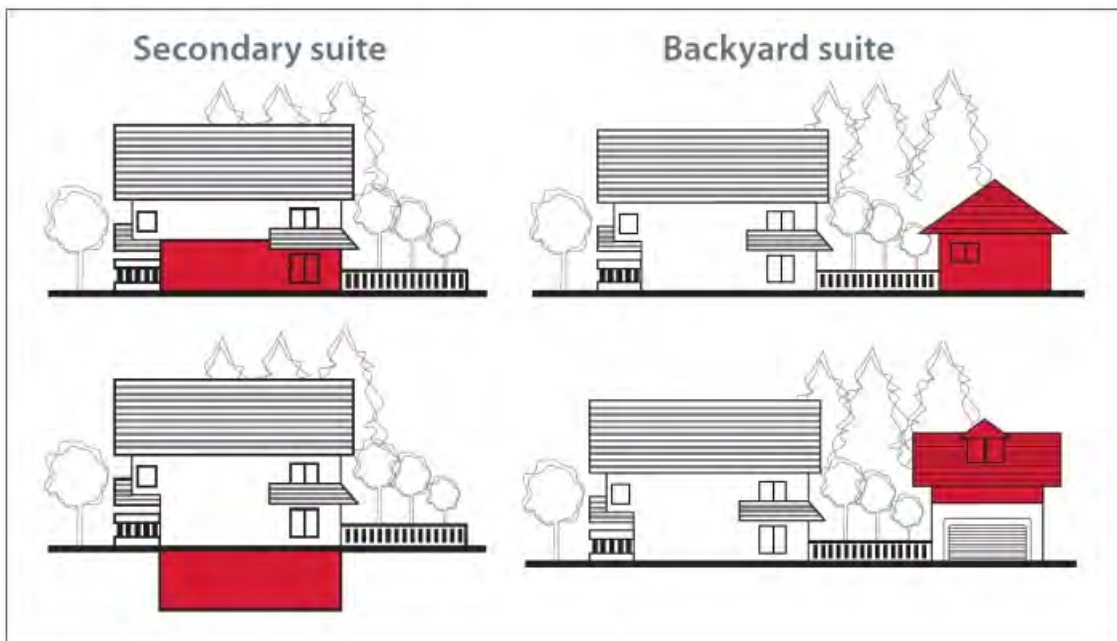
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a staff vertically. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

Calgary Planning Commission 2018 January 25

Item 3.05

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

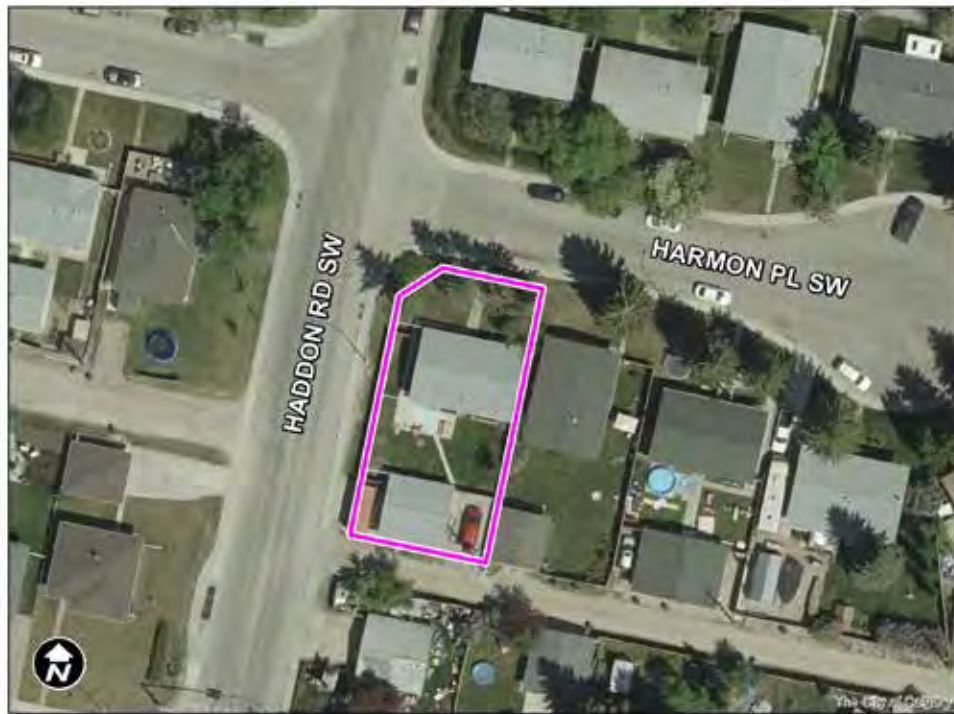
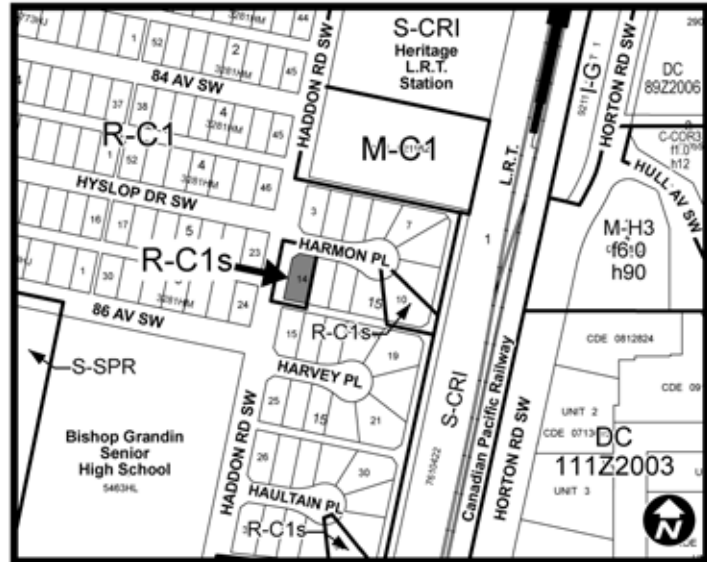
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

LOCATION MAPS



LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.15 acres \pm) located at 24 Harmon Place SW (Plan 3184JK, Block 15, Lot 14) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

Applicant:

Harmon Holdings Ltd

Landowner:

Harmon Holdings Ltd

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Haysboro, the site is approximately 35 metres by 17 metres in size and is developed with a one-storey single detached dwelling, a detached one-car garage that is accessed from Haddon Road SW, and a one-car parking pad that is accessed from the rear lane. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census,, the following table identifies Haysboro's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Haysboro | |
|------------------------------------|-------|
| Peak Population Year | 1968 |
| Peak Population | 8,044 |
| 2017 Current Population | 7,076 |
| Difference in Population (Number) | -968 |
| Difference in Population (Percent) | -12% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Haysboro.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Haddon Road SW, Harmon Place SW, and the rear lane. The area is served by Calgary Transit light rail transit, with a light rail transit station located within approximately 450 metre walking distance of the site at Heritage Station. On-street parking adjacent to the site is regulated through the Calgary Parking Authority’s residential parking permit system.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Haysboro Community Association.

Citizen Comments

Administration received three (3) letters written by the applicant signed in support of the application via the applicant. Reasons for support were not included in the letters.

Administration received four (4) letters in opposition to the application.

Reasons stated for opposition are summarized as follows:

- A different property owned by the same owner was previously redesignated from R-C1 to R-C1s;
- Lack of on-street parking;
- Owner owning multiple properties on Harmon place SW;
- The vehicle access to the two-car garage on the property is blocked by a shipping container;
- Amount of traffic;
- Uncertainty regarding property values; and
- Waste, recycling, and compost bins taking up motor vehicle parking stalls.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission

Secondary Suites

PL 1264 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

For me: Increased rental revenue in an easier to manage property
For the City and Neighborhood: Increased density near an LRT Station

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

I have not spoken to the community but will.
I have spoken to most of the owners and residents on Harmon Place and heard no objections

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There are two uncovered stalls off the alley and a heated double garage.

- 4) Are there any potential negative impacts of this development that you are aware of?

No. The increased density will not affect the number of people living on the property

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

APPENDIX III

IMPORTANT TERMS

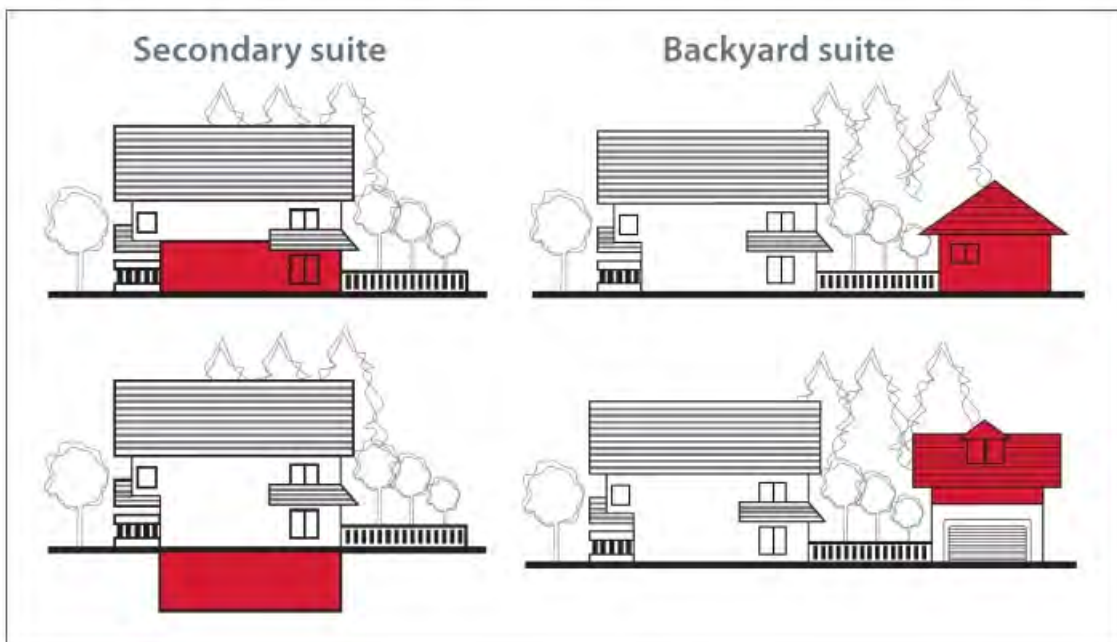
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



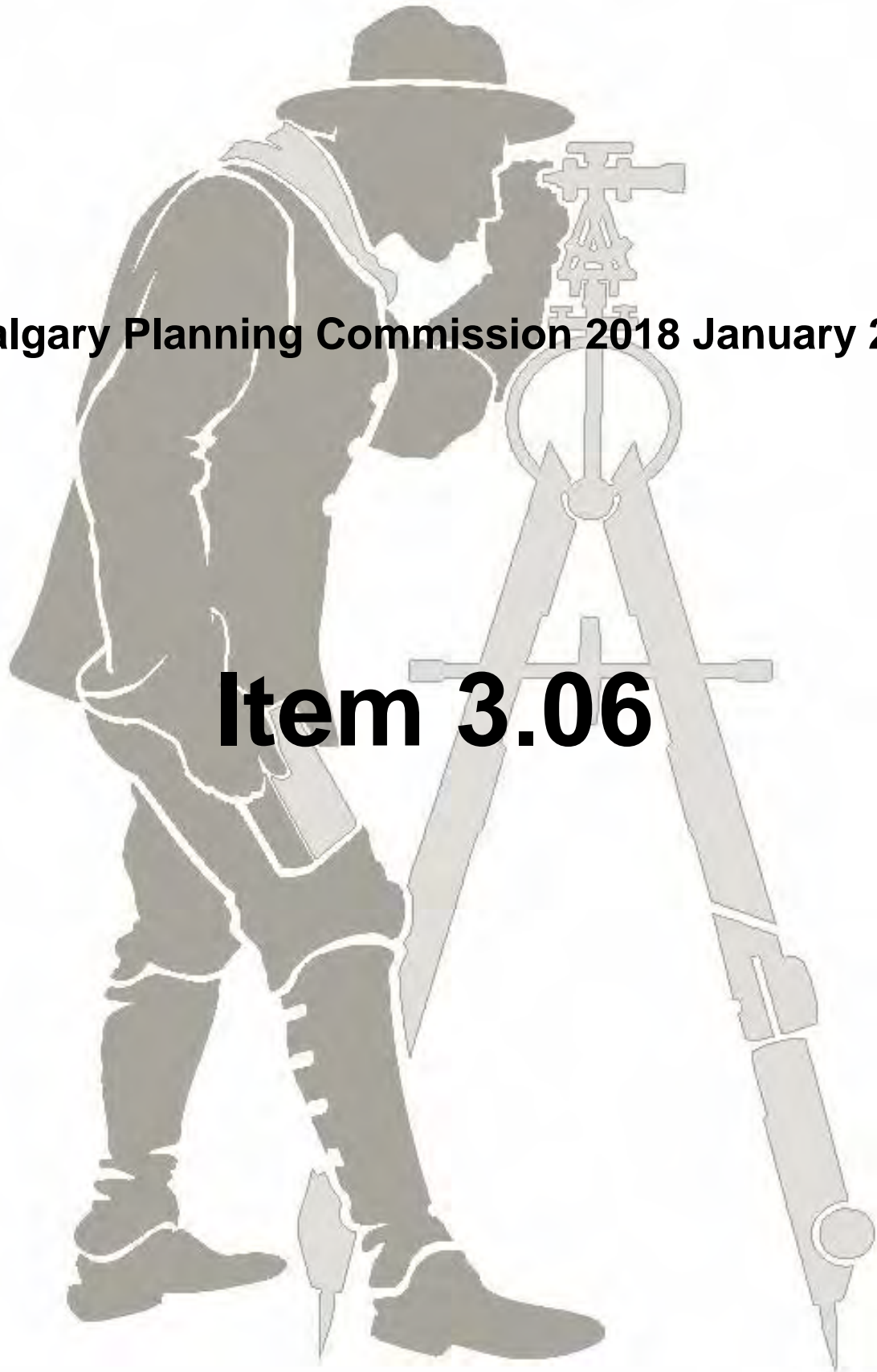
Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



Calgary Planning Commission 2018 January 25

Item 3.06



LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

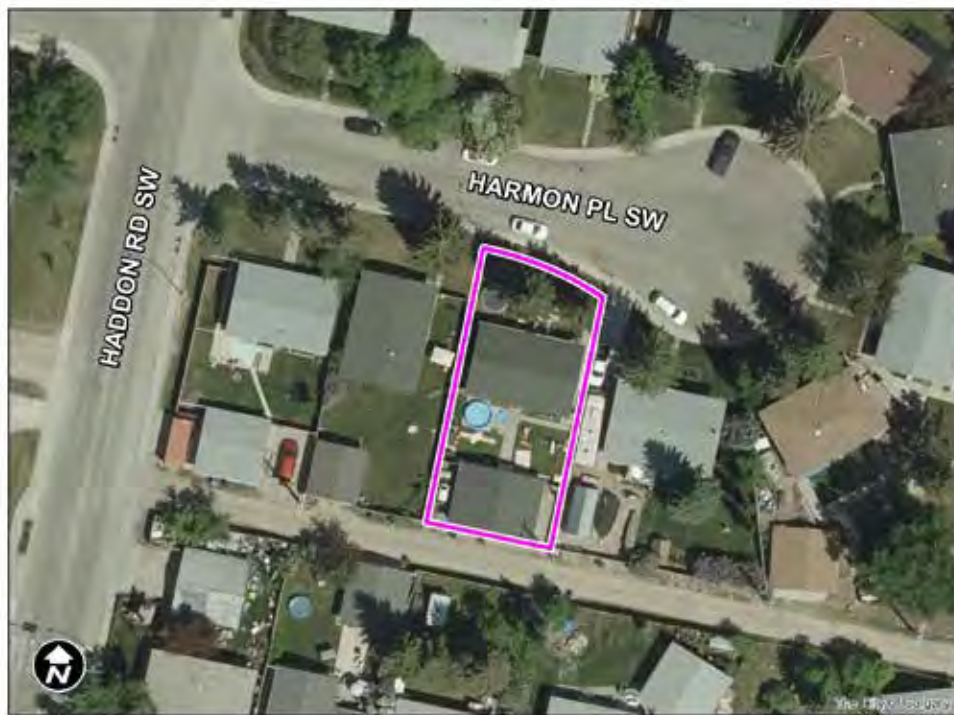
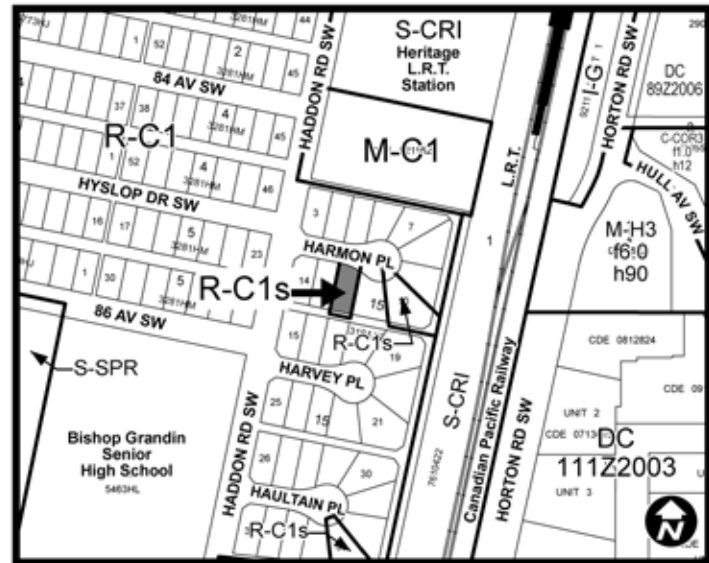
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

LOCATION MAPS



LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.13 acres ±) located at 20 Harmon Place SW (Plan 3184JK, Block 15, Lot 12) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

Applicant:

Harmon Holdings Ltd

Landowner:

Harmon Holdings Ltd

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Haysboro, the site is approximately 34 metres by 16 metres in size and is developed with a one-storey single detached dwelling a detached two-car garage that is accessed from the rear lane, and a two (2) one-car parking pad that are accessed from the rear lane. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Civic Census, the following table identifies Haysboro's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Haysboro | |
|------------------------------------|-------|
| Peak Population Year | 1968 |
| Peak Population | 8,044 |
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| Difference in Population (Number) | -968 |
| Difference in Population (Percent) | -12% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

**LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW**

MAP 21S

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

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There is no local area plan for Haysboro.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Harmon Place SW and the rear lane. The area is served by Calgary Transit light rail transit, with a light rail transit station located within approximately 450 metre walking distance of the site at Heritage Station. On-street parking adjacent to the site is regulated through the Calgary Parking Authority’s residential parking permit system.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Haysboro Community Association.

Citizen Comments

Administration received three (3) letters written by the applicant signed in support of the application via the applicant. Reasons for support were not included in the letters.

Administration received four (4) letters in opposition to the application.

Reasons stated for opposition are summarized as follows:

- A different property owned by the same owner was previously redesignated from R-C1 to R-C1s;
- Lack of on-street parking;
- Owner owning multiple properties on Harmon place SW;
- Uncertainty regarding property values and property taxes;
- Neighbourhood would not be family-oriented if the application is approved;
- Property maintenance levels by renters;
- Opposition to renters; and
- Amount of traffic.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission

Secondary Suites

PL 1264 (R2017-09)

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- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

For me: Increased rental revenue in an easier to manage building
For City & Community: Increased density near an LRT station

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

I have not spoken to the community but will.
I have spoken to most of the owners and residents on
Harmon Place and heard no objections

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There are two uncovered stalls off the alley and a
double heated garage

- 4) Are there any potential negative impacts of this development that you are aware of?

No. The increased density will not affect the number of
people living on the property.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

LAND USE AMENDMENT
HAYSBORO (WARD 11)
HADDON ROAD SW AND HARMON PLACE SW

MAP 21S

APPENDIX III

IMPORTANT TERMS

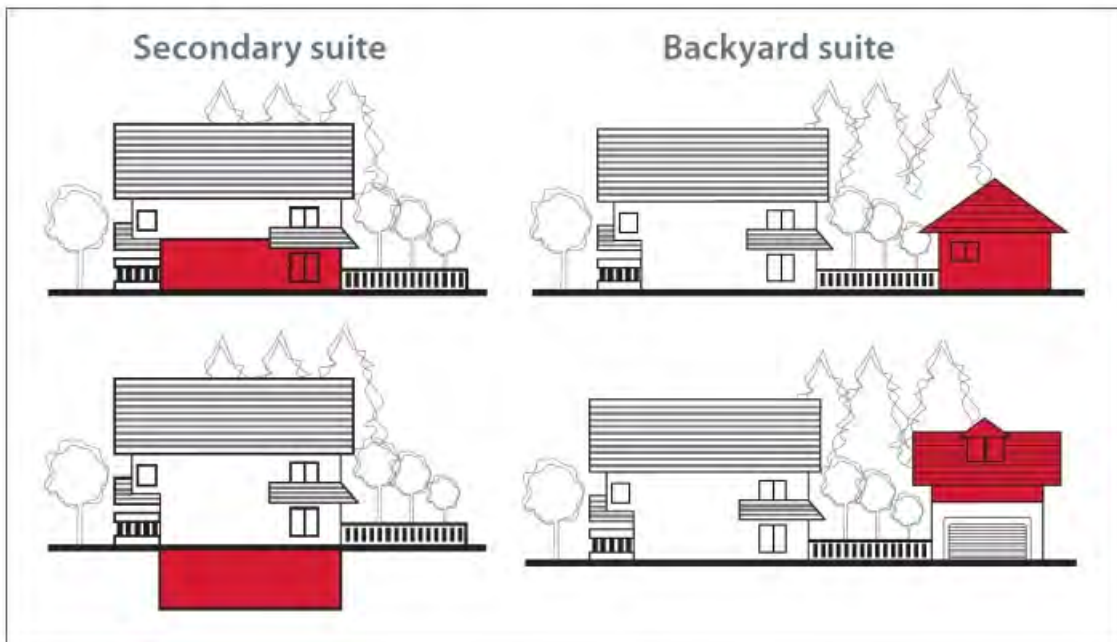
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms

Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

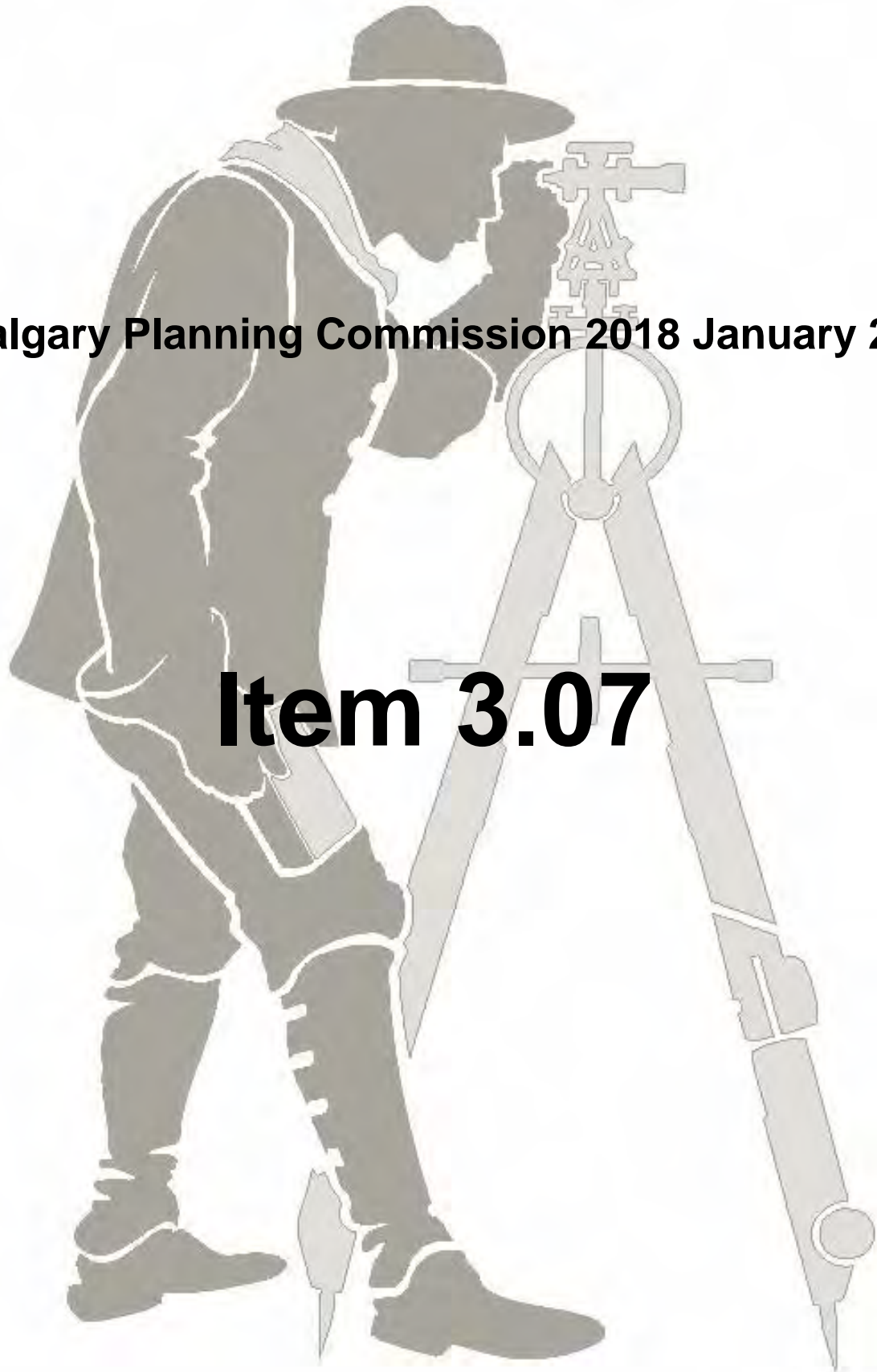


Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



Calgary Planning Commission 2018 January 25

Item 3.07



LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

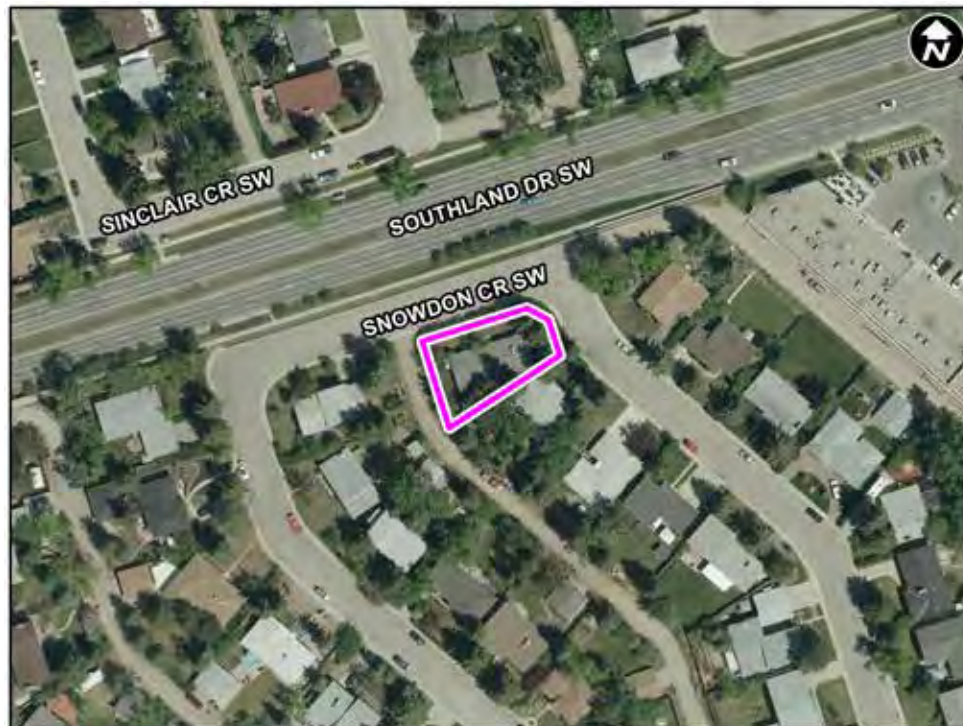
REASON(S) FOR RECOMMENDATION:

The proposed R-C1 district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

LOCATION MAPS



LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.15 acres \pm) located at 45 Snowdon Crescent SW (Plan 403HT, Block 3, Lot 24) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

Applicant:

Janey Chupik

Landowner:

Pulse Renovations Ltd

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Southwood, the site is approximately 19 metres by 34 metres in size and is developed with a one-storey single detached dwelling, and a one-car parking pad that is accessed from Snowdon Crescent SW. Single detached dwellings exist to the east, and south of the site. A rear lane and single detached dwellings exist to the west of the site. Southland Drive SW exists to the north of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Southwood's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Southwood | |
|------------------------------------|--------|
| Peak Population Year | 1978 |
| Peak Population | 8,101 |
| 2017 Current Population | 6,214 |
| Difference in Population (Number) | -1,887 |
| Difference in Population (Percent) | -23% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Southwood.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Snowdon Crescent SW and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 200 metre walking distance of the site on Southland Drive SW. On-street parking adjacent to the site on Snowdon Crescent SW is unregulated through the Calgary Parking Authority’s residential parking permit system.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration received a response of no position to the application from the Southwood Community Association. The email correspondence is attached in APPENDIX II of this report.

Citizen Comments

Administration received one (1) letter in support of the application.

Reasons stated for support are summarized as follows:

- Southwood has an ageing population and this type of application would allow Calgarians of all ages, abilities, and incomes to reside safely in the community;
- Southwood has experienced a decline of approximately 23% of its population since peak, and has ample capacity to growth;
- The subject site is within walking distance of local commercial, several bus rapid transit stops, and the Southland CTrain Station;
- Nearby commercial retail would benefit from additional population in the community;
- The lot dimensions of the subject site as well as its laneway access can accommodate additional parking on site;
- Snowdon Crescent has a surplus of available parking and traffic is minimal;
- A basement suite would not affect the character of the neighbourhood and would likely not result in major alterations to the exterior of the house; and
- The subject site is a corner lot and is a relatively private site due to location and landscaping.

Administration receive two (2) letters of opposition to the application.

Reasons stated for opposition are summarized as follows:

- Increased density and its implications on local traffic;
- Application may set precedent for other secondary suite applications;
- Negative impacts to parking for existing residents; and

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

- Desire to keep the single detached dwelling character of the neighbourhood.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

APPENDIX I

APPLICANT'S SUBMISSION

October 26, 2017

Hello!

I am so very excited to be starting a new company in Calgary called Pulse Renovations. My main goal is to give back to a community of people that require modifications to homes due to a disability or simply aging. My goal is to focus on Aging in Place and Able in Place.

I found this property after a long search and my hopes are to create a secondary suite with its own entrance in the basement as well as redefining the upstairs from a three bedroom to a two bedroom with two master suites. The basement suite could be used for a caregiver or as a rental. The upstairs is being renovated with wider halls, doors, anti-slip flooring, updated energy efficient heating, superior indoor air quality, add compliant plumbing, better led lighting and so on.

I am hoping this property will be a place of comfort for a person in need of some additional services and help.

I have also partnered with some companies in the city that are also involved in Aging in Place and offer care in a variety of ways to these people. In speaking with these partners, they are so excited about the opportunity that people are able to stay in a smaller environment instead of going to a larger facility.

This property has plenty of space in the backyard to have two different spaces as well we would be looking to add an additional driveway for the secondary suite off the lane.

It is my hope that this will be one of many opportunities to renovate a space to allow an individual to have privacy while keeping and maintaining their dignity.

Due to the Aging Population the reality is that we need places like this and I feel like I need to get a head start in providing a space.

This is a vision I have and I hope that the City of Calgary will be on board to support our community together one or two families/people at a time.

Thank you for your time!

Janey Chupik

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

APPENDIX II

LETTERS SUBMITTED

From: [Becky Poschmann](#)
To: [McLeod, Fraser A.](#)
Subject: [EXT] Re: 45 Snowdon CR SW, LOC2017-0321 - Southwood Community Association Response
Date: Monday, November 27, 2017 9:46:54 AM

Hi Fraser,

Thank you for this reminder. We have been busy in Southwood with a number of land use applications.

For this application in particular, LOC2017-0321, the Southwood Community Association does not take a position.

I would like to add that the concept for this application is great and I hope to see many more throughout the City.

Regards,
Becky Poschmann
Director of Development

LAND USE AMENDMENT
SOUTHWOOD (WARD 11)
SOUTH OF SOUTHLAND DRIVE SW AND WEST OF ELBOW
DRIVE SW

MAP 16S

APPENDIX III

IMPORTANT TERMS

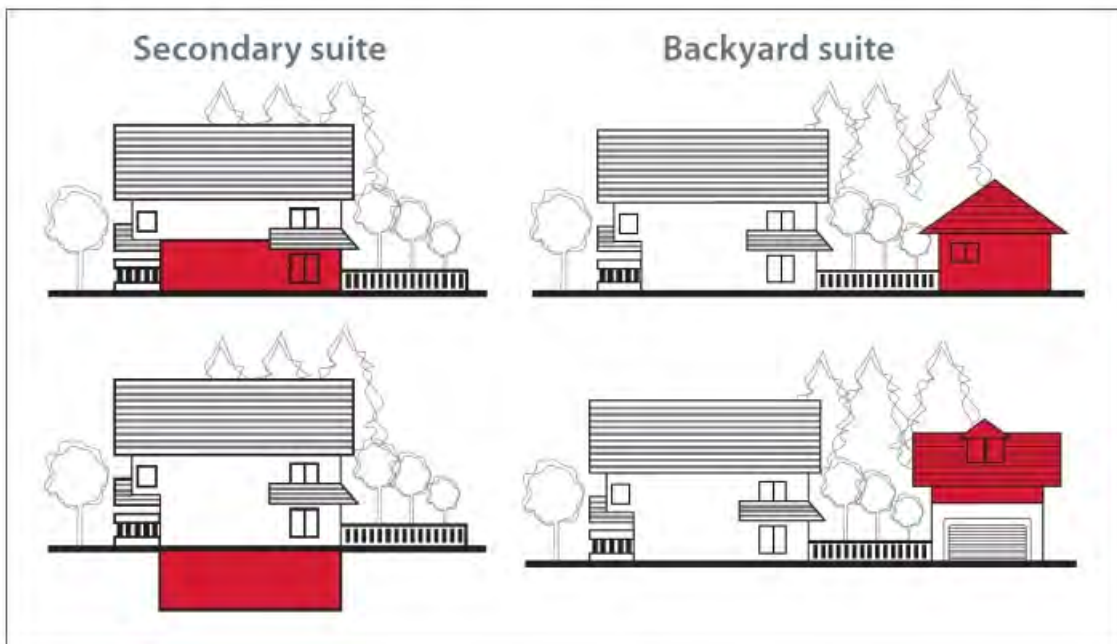
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a staff in their right hand. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

Calgary Planning Commission 2018 January 25

Item 3.08

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing one-storey single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

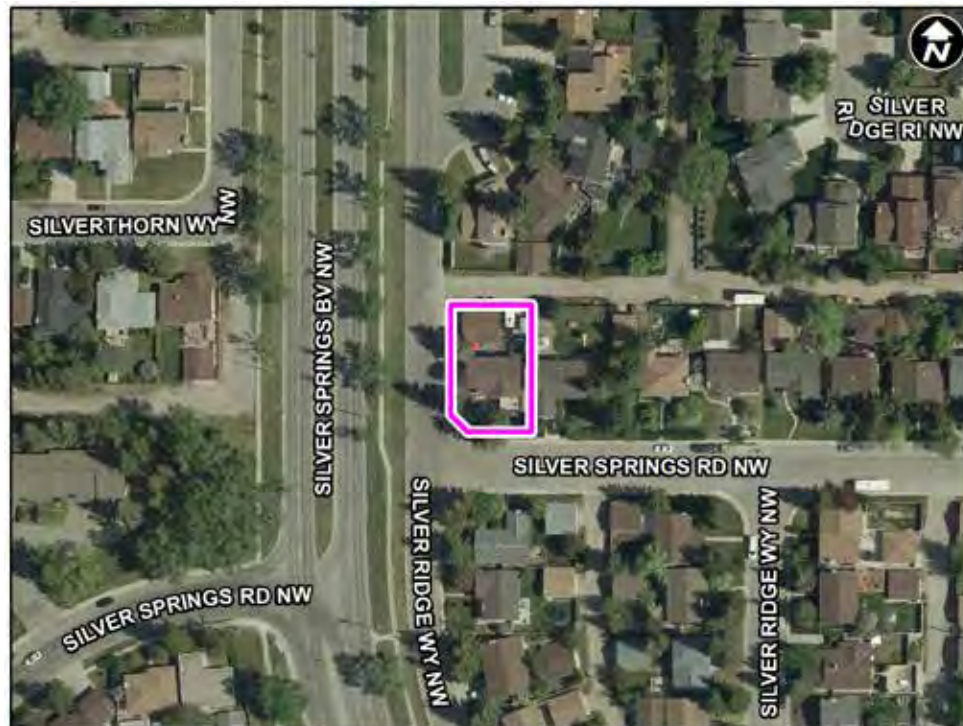
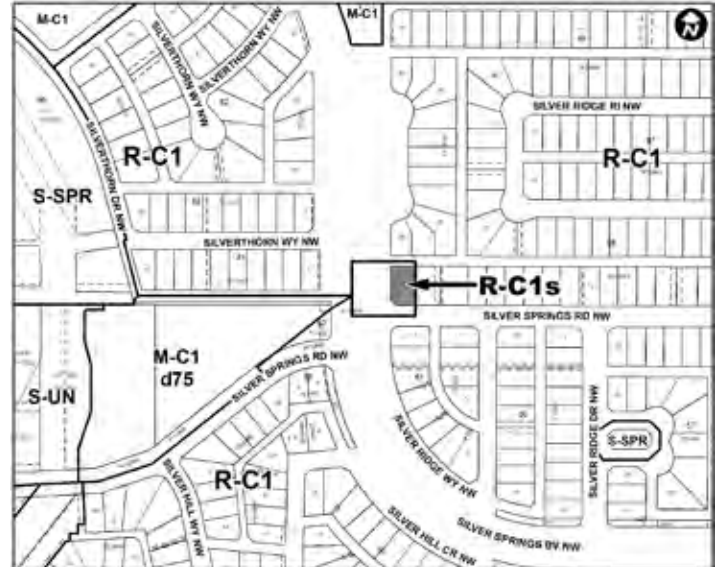
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

LOCATION MAPS



LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.07 hectares \pm (0.17 acres \pm) located at 7716 Silver Springs Road NW (Plan 7610683, Block 96, Lot 19) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

Applicant:

Angel Ann Marsh

Landowner:

Angel Ann Marsh
Christopher John Marsh

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Silver Springs, the site is approximately 17 metres by 33 metres in size and is developed with a one-storey single detached dwelling. On-site parking consists of the following:

- a detached two-car garage that is accessed from Silver Ridge Way NW;
- an attached one-car garage that is accessed from Silver Ridge Way NW; and,
- and a two-car parking pad that is accessed from the rear lane.

According to data from The City of Calgary 2017 Civic Census, the following table identifies Silver Springs' peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Silver Springs | |
|------------------------------------|-------|
| Peak Population Year | 2017 |
| Peak Population | 6,907 |
| 2017 Current Population | 6,907 |
| Difference in Population (Number) | 0 |
| Difference in Population (Percent) | 0% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for the community of Silver Springs.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Silver Springs Road NW and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 100 metre walking distance of the site on Silver Springs Blvd NW. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Silver Springs Community Association.

Citizen Comments

One letter of support was received from an adjacent neighbour as well as one letter against the proposed redesignation.

The stated reason for opposition are the creation of parking issues in the area.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

We would like my parents to move in. I have an autoimmune disease and my mom and dad would be a great help to me with my kids. My father also has had heart issues and it is time for them to downsize and retire. This will help Calgary by decreasing Calgary's footprint and my property taxes will increase so Calgary will benefit that way. The community benefits by getting two vibrant individuals who can help take care of the neighborhood kids and help love to volunteer and be engaged.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

I only have a neighbor beside me and behind me. The new neighbors behind me have not moved in yet. The house was just sold so I have not had that opportunity. I am on a corner with no neighbors to the west. I have spoken to the neighbors on the east and they are in support of the suite.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

We have 6 parking spaces on the lot. The two in the front drive will be designated for the secondary suite.

- 4) Are there any potential negative impacts of this development that you are aware of?

None.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
SILVER SPRINGS (WARD 1)
NE CORNER OF SILVER SPRINGS RD NW AND SILVER RIDGE
WAY NW

MAP 10NW

APPENDIX II

IMPORTANT TERMS

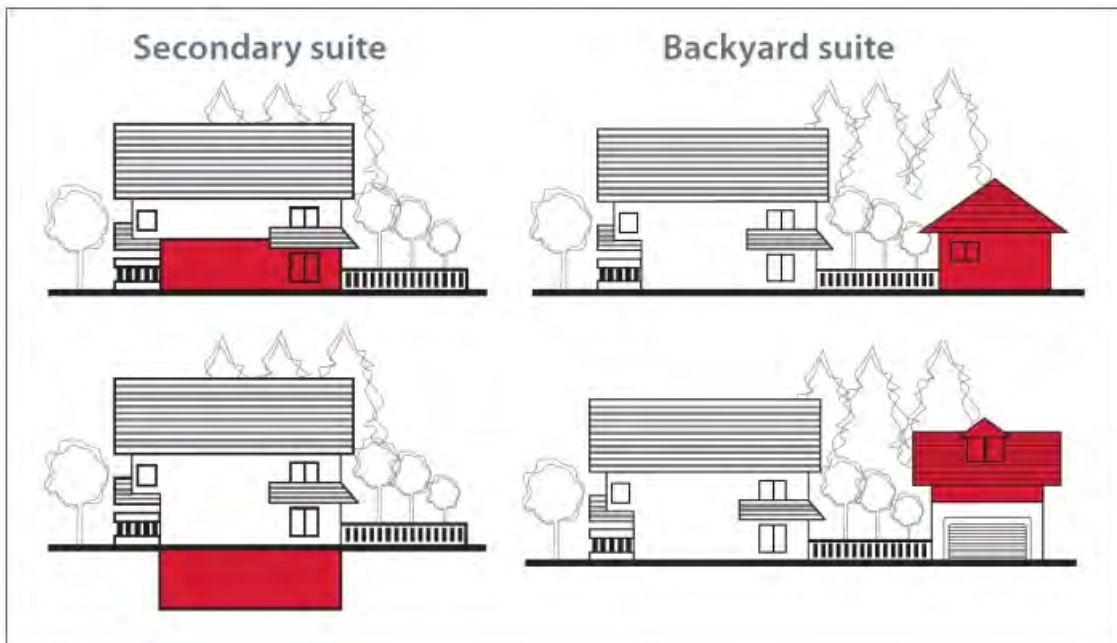
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is a large, complex piece of equipment with a circular level and a vertical axis. The surveyor has a tool in a case on his hip.

Calgary Planning Commission 2018 January 25

Item 3.09

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing two-storey single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and Crowchild Phase 3 Area Structure Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

LOCATION MAPS



LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.11 acres \pm) located at 375 Hawkland Circle NW (Plan 9010590, Block 9, Lot 8) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

Applicant:

Vahideh Nejat

Landowner:

Vahideh Nejat
Hamid Hadad Razavi

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Hawkwood, the site is approximately 14 metres by 33 metres in size and is developed with a double storey single detached dwelling with an attached two-car garage that is accessed from Hawkland Circle NW. Surrounding development consists of low-density residential R-C1 development to the north, east, south, and west of the site. Hawkland Circle Playground is located one parcel away to the east of the site.

According to data from The City of Calgary 2017 Civic Census, the following table identifies Hawkwood's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Hawkwood | |
|------------------------------------|--------|
| Peak Population Year | 1998 |
| Peak Population | 11,165 |
| 2017 Current Population | 9,512 |
| Difference in Population (Number) | -1,653 |
| Difference in Population (Percent) | -15% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

**LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW**

MAP 14NW

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

Crowchild Phase 3 Area Structure Plan (1979)

The site is within the Residential Cell “C” area of the Crowchild Phase 3 Area Structure Plan (ASP). Although the ASP does not mention secondary suites, the document outlines that residential development should allow for the opportunity for unique design and planning innovation.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Hawkland Circle NW and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 350 metre walking distance of the site on Arbour Ridge Way NW. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

The Hawkwood Community Association provided general comments on secondary suites which can be summarized as follows:

- Homeowners may not reside on the property resulting in two rental dwellings on one lot which can lead to poor maintenance; and,
- Parking – with inadequate on-site parking, congestion may become a problem.

Noting that this does not appear to be a problem on the subject site at the moment, the house may be sold in the future and may result in the abovementioned issues.

Citizen Comments

Administration did not receive any responses from citizens.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

close families can leave together and have their privacy and independently.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

mostly email and some newsletters.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

in front of the building or we can provide extra parking on back yard (there is enough space)

- 4) Are there any potential negative impacts of this development that you are aware of?

No, I don't think so.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

To whom it may concern:

I am writing to inform you, Hamid Haded Razavi and I would like to renovate and make our basement or secondary suite.

Usually, we invite our parents and some close families from our backhome country and they are welcome to stay in our home for several months.

As a result, we think that having a secondary suite is very helpful and essential, so our families can stay in our basement and they can have their privacy and live at peace.

Yours,

Vahideh Nejat

Nejat

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

APPENDIX II

LETTERS

Following are the Hawkwood Community Association comments regarding the above development permit for a secondary suite:

Our concerns are basically two issues with secondary suites: 1) In many cases, the homeowner does not reside in the home which leads to two renters. This can lead to poor maintenance both in the summer and winter which affects the general appearance of the property, and 2) Parking....if there is not adequate off-street parking, this has the potential for more congestion with multiple vehicles. Hawkwood already has some problems with multiple vehicles (both operational and non-operational, commercial and personal) which is a concern.

It does not appear from the request that there would be a problem at this time, however, if the home were sold, then things can change as there would already be a secondary suite in place.

This is our standard response to requests of this nature.

Donna Chapman
Planning and Development
Hawkwood Community Association

LAND USE AMENDMENT
HAWKWOOD (WARD 2)
EAST OF NOSE HILL DRIVE NW NORTH OF HAWKSTONE
DRIVE NW

MAP 14NW

APPENDIX III

IMPORTANT TERMS

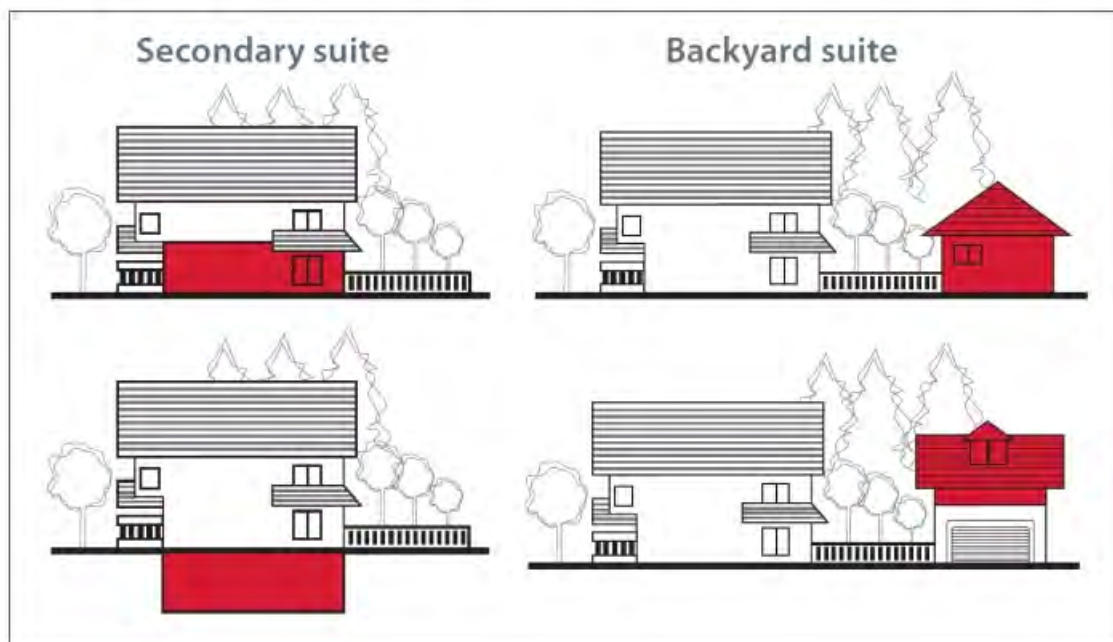
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A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is mounted on a tripod and has a circular level and a telescope. The surveyor has a tool case on his hip and a pencil in his hand.

Calgary Planning Commission 2018 January 25

Item 3.10

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration's knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

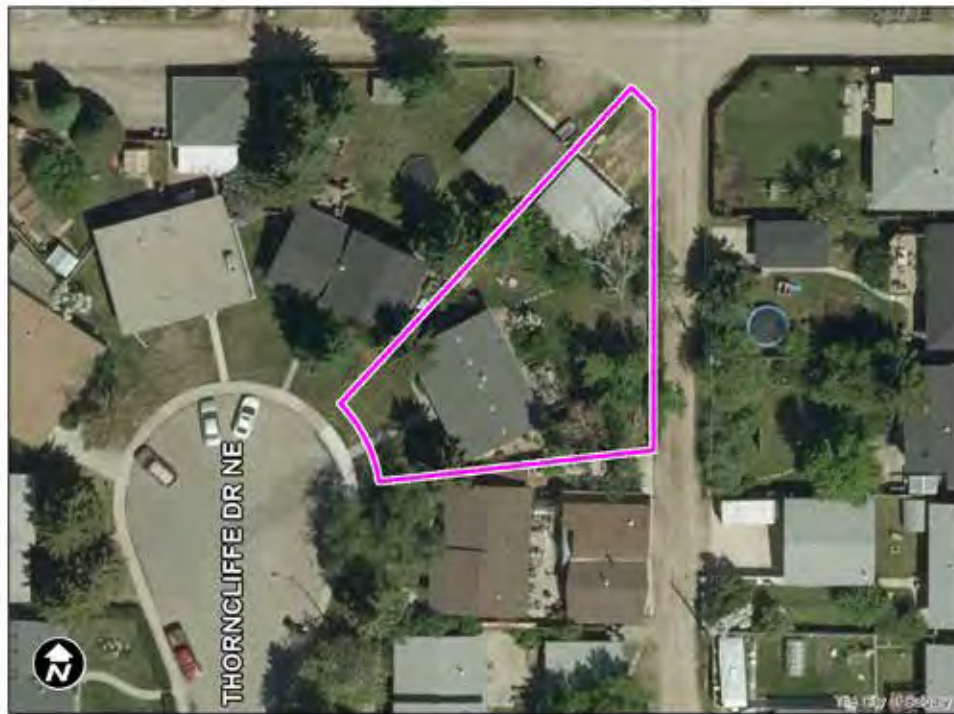
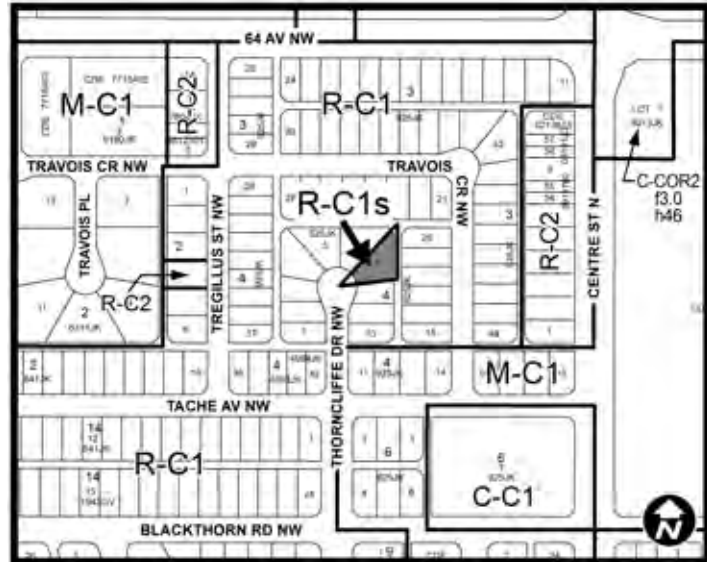
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

LOCATION MAPS



LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.08 hectares ± (0.19 acres ±) located at 6324 Thorncliffe Drive NW (Plan 925JK, Block 4, Lot 7) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

Applicant:

Phillip Pattison

Landowner:

Phillip Pattison
Jennifer Pattison

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Thorncliffe, this pie-shaped site is approximately 25 metres by 34 metres in size and is developed with a bi-level single detached dwelling and a two-car detached garage that is accessed from the rear lane. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Thorncliffe's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Thorncliffe | |
|------------------------------------|--------|
| Peak Population Year | 1977 |
| Peak Population | 11,379 |
| 2017 Current Population | 8,474 |
| Difference in Population (Number) | -2,905 |
| Difference in Population (Percent) | -26% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Thorncliffe.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Thorncliffe Drive and the rear lane. The area is served by Calgary Transit Primary Transit Network bus service with a bus stop location within approximately 300 metre walking distance of the site on Centre Street N. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration received a letter in support of the application from the Thorncliffe/Greenview Community Association (APPENDIX II).

Citizen Comments

Administration did not receive any responses from citizens.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission

Secondary Suites

PL 1284 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

For me, provide space for my work shop and allow indoor parking, at the same time, Rental income would help subsidize the cost.
For community and city, provide more budget rental availability 2 blocks from future C train station in a mature neighborhood close to downtown

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?
What was the response?

I've spoken to neighbors that I know, haven't done much of this

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There will be parking for all in the new building as well as street parking out front ie, culdesac unwritten rule in the culdesac is 2 vehicles/house

- 4) Are there any potential negative impacts of this development that you are aware of?

construction can be noisy

LAND USE AMENDMENT
THORNCLIFFE (WARD 4)
THORNCLIFFE DRIVE NW WEST OF TRAVOIS CRESCENT NW

MAP 3N

APPENDIX II
LETTERS SUBMITTED

Action Items

The Thorncliffe/Greenview Community Association fully supports LOC2017-0357.

Thanks
Marvin Quashnick
TGCA

APPENDIX II

IMPORTANT TERMS

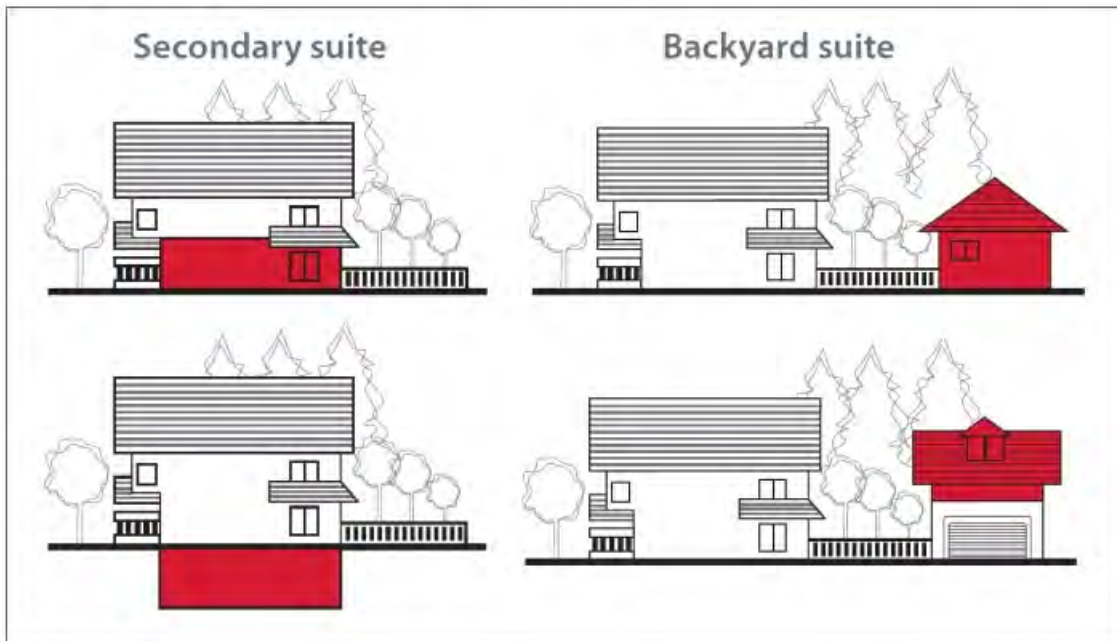
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is a large, complex piece of equipment with a circular level and a vertical axis. The background is a light, textured pattern.

Calgary Planning Commission 2018 January 25

Item 3.11

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

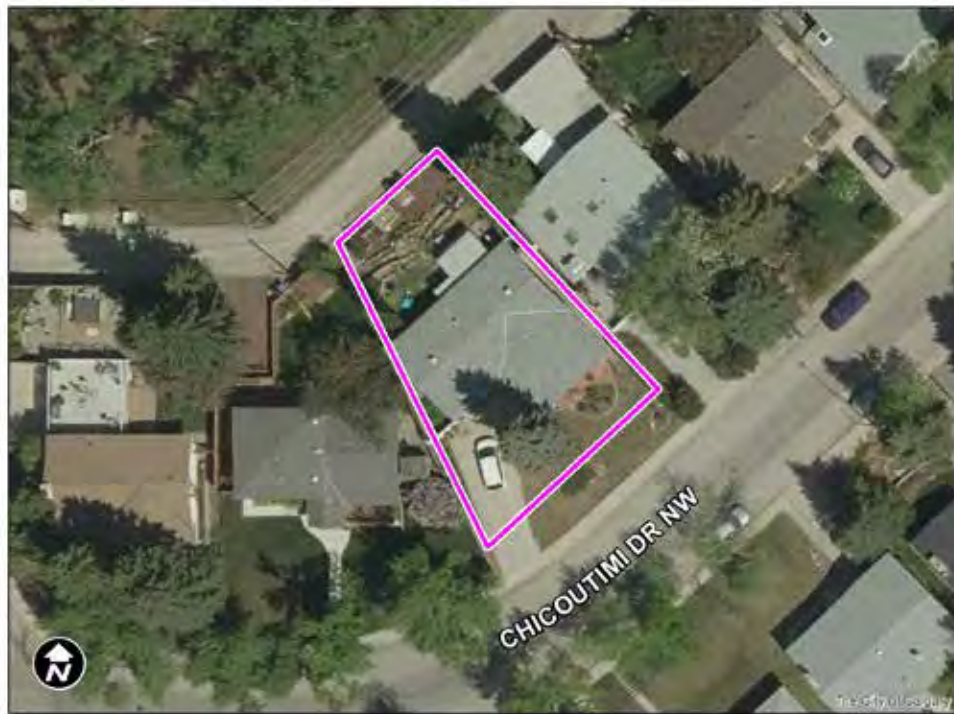
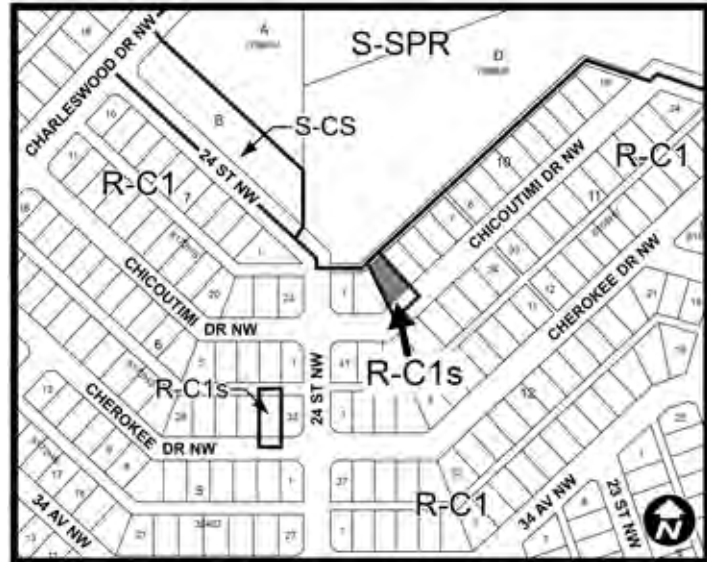
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

LOCATION MAPS



LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares \pm (0.13 acres \pm) located at 2428 Chicoutimi Drive NW (Plan 6108HV, Block 10, Lots 2 and 3) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

Applicant:

Michael McWilliam

Landowner:

Michael McWilliam
Lindsay McWilliam

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Charleswood, the site is approximately 17 metres by 31 metres in size and is developed with a one-storey single detached dwelling with an attached two-car garage that is accessed from Chicoutimi Drive NW. Surrounding development consists of low density residential R-C1 development to the east, south, and west of the site. To the north is S-SPR land with a Junior High School and sports fields.

According to data from The City of Calgary 2017 Census, the following table identifies Charleswood's peak population and year, 2017 population, and the population amount and percentage difference between the peak and current populations.

| Charleswood | |
|------------------------------------|--------|
| Peak Population Year | 1969 |
| Peak Population | 9,822 |
| 2017 Current Population | 5,798 |
| Difference in Population (Number) | -4,024 |
| Difference in Population (Percent) | -41% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the Development Permit process. A Development Permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Charleswood.

TRANSPORTATION NETWORKS

Access to the site is available from Chicoutimi Drive NW and the rear lane. Direct vehicular access is via a front driveway from Chicoutimi Drive NW to an attached garage. On-street parking adjacent to the site is regulated through the Calgary Parking Authority’s residential parking permit system. The area is served by Calgary Transit bus service with a bus stop location within an approximately 350 metre walking distance of the site on Charleswood Drive NW.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the Development Permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration received a letter of 'no objection' to the application from the Triwood Planning Committee, acting on behalf of the Triwood Community Association (APPENDIX II). The Community Association supports secondary suite development with the caveats that the owner is a resident of the property, the parking requirements can be met, and the suite is contained within the home (not backyard suite).

Reasons stated for no objection of the application are summarized as follows:

- allows the owners and their family to reside in one home.

Citizen Comments

Administration received one (1) letter in support of the application and one (1) in opposition.

Reasons stated for support are summarized as follows:

- expands use of older homes
- secondary suites are seen as a way to rejuvenate older communities while maintaining character of the neighbourhood.

Reasons stated for opposition are summarized as follows:

- wants area to remain R-C1, as that is the reason for purchasing a home in the area
- concerns of negative impacts on property values
- concern of traffic and parking issues
- quality of life concerns due to potential rental property and no mechanism to restrict or determine owner-occupied tenure.

Public Meetings



No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

APPENDIX I

APPLICANT'S SUBMISSION

| | |
|---|--|
|   | <p>Land Use Redesignation Applicant's Submission Secondary Suites PL 1264 (R2017-09)</p> |
| <p>This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments must be limited to the area designated on this page to ensure it will fit the space requirements of the report.</p> | |
| <p>1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary? Welcome to Chicoutimi Dr.! Our parents live down the street, but no longer need the space of their current home. The benefits of the redesignation to us, and our extended family, is that it allows our parents to age-in-place, without the maintenance and costs of owning an entire single family dwelling in their neighbourhood (Charleswood). For my husband and I, it allows us the ease of supporting our parents as they age - but in the more immediate future, it allows them to support us with our small children and mortgage. For the community, it allows us to hopefully set a new precedent of creating secondary suites legally. We are aware of at least three secondary (basement style) suites within the one-block radius of our home, but to the best of our knowledge none are legal or registered (unless they opted out of the map listing). Maybe we can be the start of turning that trend around. The potential is huge for the city, be it adding another affordable housing option in the future, or simply another safe and legal suite.</p> <p>2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response? -As of completing this form, five of our surrounding neighbours had been contacted, as well as our Community Association, either in person or by phone, and are in support of our ideas and plans (this includes the two adjacent properties that would be most impacted) -As new members of ward 4 (previously 7), we have contacted Councillor Chu, who also has offered us advice and support as we navigate this process -All conversations, to date, have been in support of having a legal suite - as the community and neighbours are well aware of the realities and prevalence of illegal suites in our community -Neighbours, understandably, expressed a strong preference for owner-occupied suites instead of absentee landlords - we were able to reassure them that our intent with this process is to create a very long-term living arrangement for our family</p> <p>3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel? -If our application for land use redesignation is successful, we will be subsequently applying to construct a rear garage, which would allow for more parking than necessary than the current, or even projected future, requirements of our home. The plan is that this would allow for parking, as well as storage, specific to the suite -If, for reasons unseen, this was not approved, we currently are able to park three vehicles within our existing front-drive garage and extended driveway...as a one-car family, looking to carshare with our own parents (they spend much of the year at a cottage in Ontario), we can therefore already provide the required parking within our parcel (although we understand that's not the intent of the question - more just wanted to offer reassurance regardless to the approval of any future building permit).</p> <p>4) Are there any potential negative impacts of this development that you are aware of? I suppose that depends on perspective - we view a small increase in density as a positive thing (as opposed to developing other natural areas in the future), however others may disagree, preferring a more traditional 'one-family per house' approach</p> | |
| <p>NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans</p> | |

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

APPENDIX II
COMMUNITY ASSOCIATION SUBMISSION

Re LOC2017-0362. 2428 Chicoutimi Drive NW.

The owners at 2428 Chicoutimi Drive NW met with the Triwood Planning Committee (TPC) to discuss their ideas for a proposed new development which will include a secondary suite. The Triwood Planning Committee acting for the Triwood Community Association supports the development of safe, legal secondary suites in our community with the following caveats:

- 1. The owner(s) will be a resident of the subject property.*
- 2. The parking requirements for RC-1s be followed without resorting to accessing parking over the sidewalk.*
- 3. The creation of a legal suite be done within the envelop of the house. The TPC does not support detached backyard suites, nor suites over detached or attached garages.*

Because a secondary suite will assist the owners and their family to reside in their new development at 2428 Chicoutimi Drive NW, the TPC would have no objections to the proposed land use amendment from RC-1 to RC-1s.

*Gordon Alger
Triwood Planning Committee*

LAND USE AMENDMENT
CHARLESWOOD (WARD 4)
CHICOUTIMI DRIVE NW EAST OF 24 STREET NW

MAP 32C

APPENDIX III
IMPORTANT TERMS

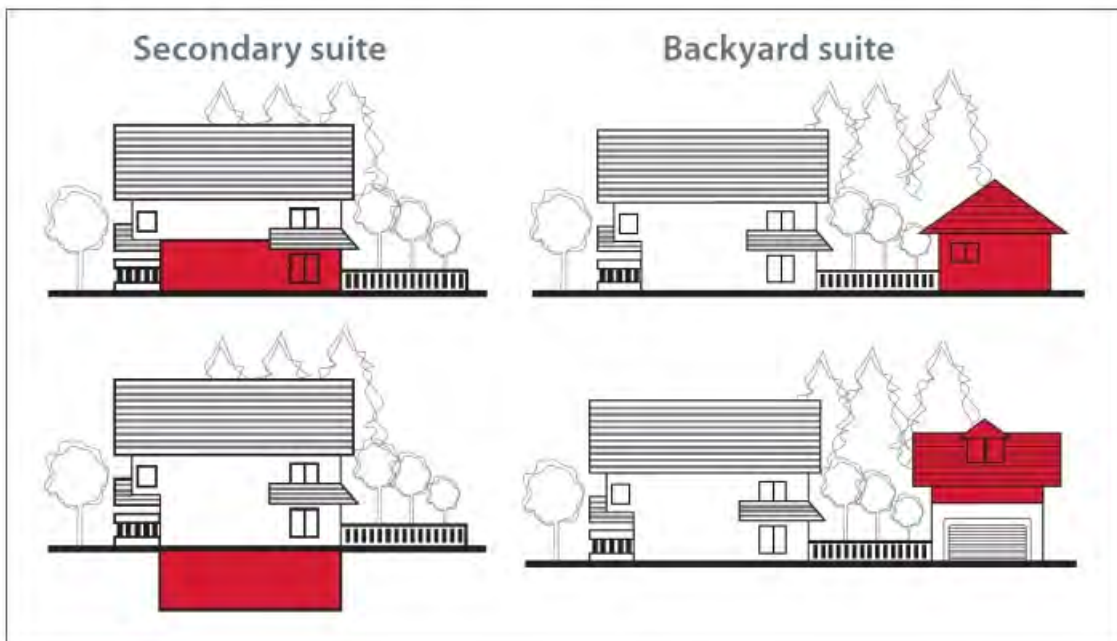
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms

Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.



Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a large transit instrument. The surveyor is looking through the instrument's eyepiece. The instrument is a classic surveying tool with a circular level and a vertical staff.

Calgary Planning Commission 2018 January 25

Item 3.12

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is an existing suite located on the parcel and the application was submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

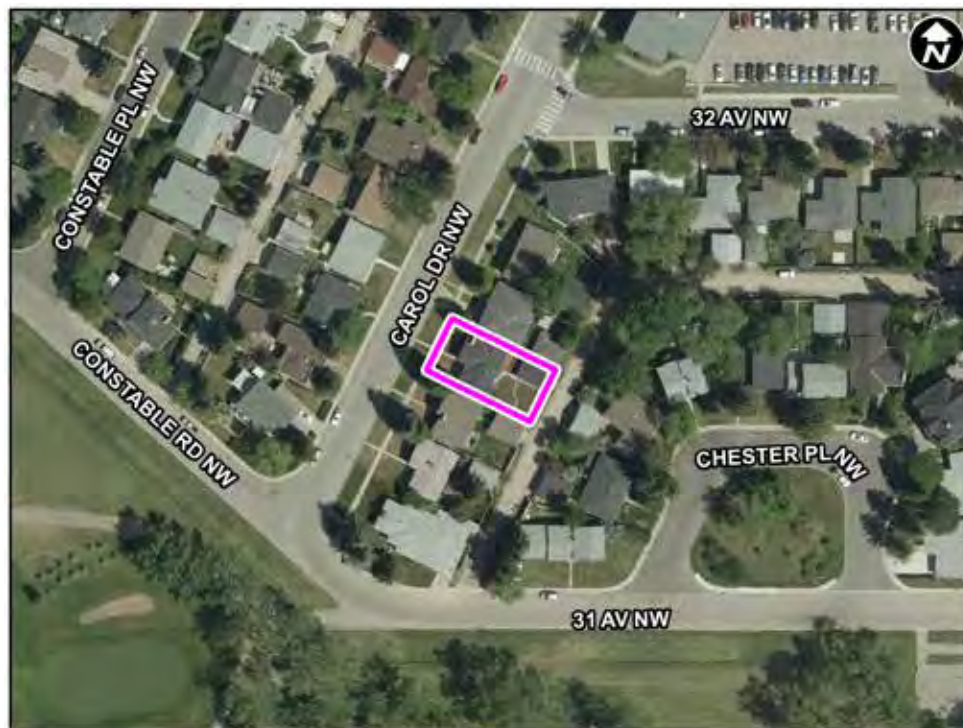
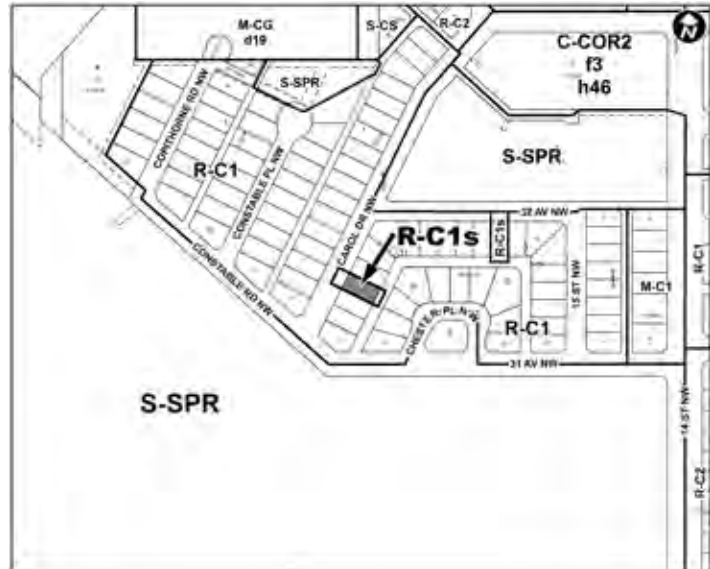
REASON(S) FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

LOCATION MAPS



LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares ± (0.11 acres ±) located at 3216 Carol Drive NW (Plan 3844HS Block 2, Lot 4) from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

Applicant:

Jessica Chan

Landowner:

532723 Alberta Ltd (Jessica Chan)

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Collingwood, the site is approximately 15 metres by 30 metres in size and is developed with a one-storey single detached dwelling, and a detached two-car garage that is accessed from the rear lane. Surrounding development consists of low-density residential R-C1 development to the north, east, south, and west of the site. The Confederation Park Golf Course is situated in close proximity to the south of the subject site.

According to data from The City of Calgary 2017 Census, the following table identifies Collingwood's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Collingwood / Charleswood | |
|------------------------------------|--------|
| Peak Population Year | 1969 |
| Peak Population | 9,822 |
| 2017 Current Population | 5,798 |
| Difference in Population (Number) | -4,024 |
| Difference in Population (Percent) | -41% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for the community of Collingwood.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Carol Drive NW and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 400 metres walking distance of the site on Northmount Drive NW. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

PUBLIC ENGAGEMENT

Community Association Comments

Administration received a letter in opposition to the application from the Triwood Community Association (APPENDIX II).

Reasons stated for opposition are summarized as follows:

- The Owner does not live on site and the home is an investment property;
- A similar land use was refused by Council on the same street;
- Parking is a potential issue; and
- Increased density will lead to increased traffic.

Citizen Comments

Administration received one letter in opposition to the application.

Reasons stated for opposition are summarized as follows:

- Impact on property values and community atmosphere;
- The proposed suite would likely be rented by students potentially leading to social issues;
- Parking issues; and
- The owner does not live in the existing house.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission

Secondary Suites

(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

The original owner had lived in this house since the house was new till early 2017. The suite was developed since 1970/1980 (cannot verify the exact year). It was developed for the sake of renting to student since this property is close to University of Calgary. He was an older fellow and just wanted to have a student living in the basement as a companion to him. It is a one bedroom suite with full bathroom, kitchen and a big Living Room. It provides good rental suite for someone like student or single person needs an affordable suite to rent.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

I did not approach the Community Association. As far as I know this is Inner City neighbourhood, there are many suites in the basement in this area.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There is a single garage at the back of the house. There are lots of street parking on this street. Tenant may have one car that can park right in front of this house.

- 4) Are there any potential negative impacts of this development that you are aware of?

The suite has no negative impact to the neighbourhood at all. It is a Bungalow with total of 3 bedrooms, and only total of 3 adults are living in the whole house. Having the suite has no impact to anyone on this block. With the City growing, a house with a suite is to help the single person or student who can rent something affordable, and close to Transit and Downtown.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
COLLINGWOOD (WARD 4)
WEST OF 14 STREET NW AND SOUTH OF NORTHMOUNT DRIVE NW

MAP 29C

APPENDIX II

LETTERS SUBMITTED

Giyana Brenkman
File manager
City of Calgary
Planning Department

Re: LOC2017-0322, 3216 Carol Drive NW

The Triwood Planning Committee (TPC) objects to this land use re-designation from RC-1 to RC-1s for the following reasons:

- The TPC objects to an investor-owned RC-1 property being turned into a de facto up/down duplex. The social and community benefits that can occur when a secondary suite permits an owner-resident to remain in their home are not a factor here. This land use amendment application from RC-1 to RC-1s seeks to enhance the economics of a rental investment. This is not what our community sees as beneficial to the inherent right that the nearby residents have to enjoy the density and use that the RC-1 land designation allows.
- Although the property has an existing illegal suite, it does not follow that a land use amendment will make everything all right. What has changed is that the owner is no longer a resident. Now an investor seeks to take advantage of the suite's existence at the expense of the best interests of the nearby residents and the wider community.
- A land use amendment application was denied by council for the property at 3208 Carol Drive NW, just a few doors away. Most of the objections that held for council's refusal to amend the land use to RC-1s for 3208 Carol Drive would also hold for the subject property.
- The applicant seems to be unaware of the parking requirements for RC-1s (2 off-street parking stalls). The applicant in their accompanying letter suggests that on street parking would be satisfactory. It would appear that the applicant has no consideration for the impact that increased density will have on the nearby residents.
- Access onto and off of Northmount Drive via Carol Drive NW is very difficult weekdays during the morning and evening commute. Permitting increased density in this area will only add to this access problem and add further to the commuting frustrations experienced by the residents of Carol Drive.

The TPC did meet with two residents that lived close to the subject property.
The TPC did not meet with the applicant.

In summary the Triwood Planning Committee strongly objects to this land use amendment and would urge the Development Authority to deny this application.

Prepared by

Gordon Alger
Triwood Planning Committee

G. Brenkman

APPENDIX III

IMPORTANT TERMS

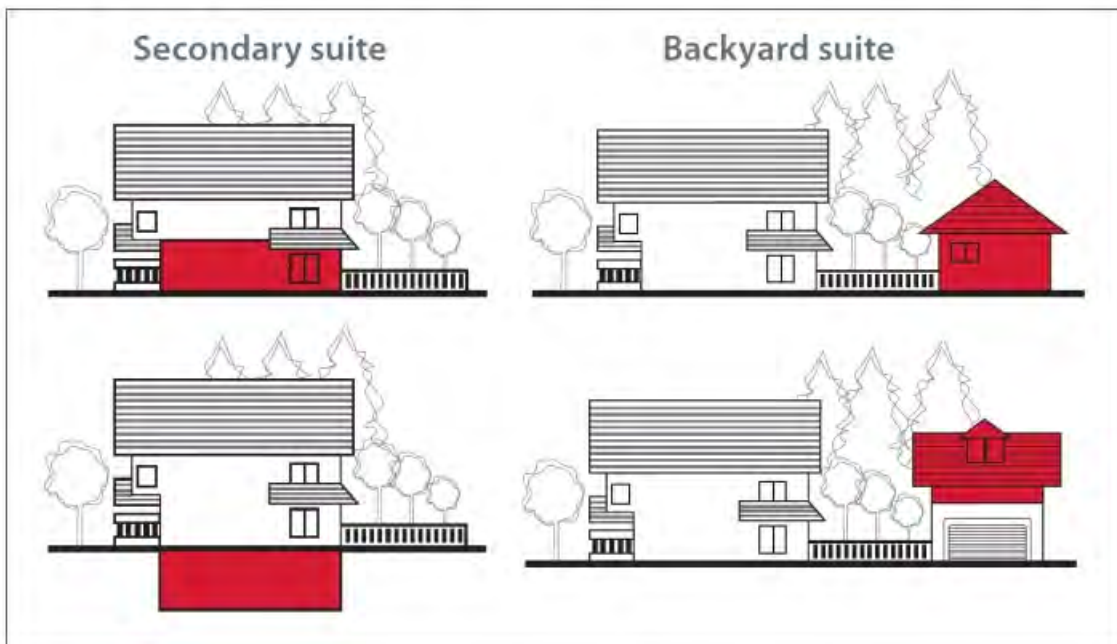
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms

Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.



Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a pencil in their right hand. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

Calgary Planning Commission 2018 January 25

Item 3.13

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – One Dwelling (R-1) District to a Residential – One Dwelling (R-1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

REASONS FOR RECOMMENDATION:

The proposed R-1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and Saddle Ridge Area Structure Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

LOCATION MAPS



LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.14 acres \pm) located at 121 Taralea Green NE (Plan 0213602, Block 3, Lot 30) from Residential – One Dwelling (R-1) District to Residential – One Dwelling (R-1s) District.

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

Applicant:

Amarjit Tambar

Landowner:

Amandeep Tambar
Amarjit Tambar

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-1 setting in the community of Taradale, the site is approximately 14 metres by 39 metres in size and is developed with a two-storey single detached dwelling with a two-car attached garage that is accessed from Taralea Green NE, and a two-car parking pad that is accessed from the rear lane. Surrounding development consists of low-density residential R-1 and R-1N development to the north, south, and west of the site. An elementary school site is situated in close proximity to the east of the subject site.

According to data from The City of Calgary 2017 Census, the following table identifies Taradale's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| Taradale | |
|------------------------------------|--------|
| Peak Population Year | 2015 |
| Peak Population | 19,223 |
| 2017 Current Population | 18,925 |
| Difference in Population (Number) | -298 |
| Difference in Population (Percent) | -2% |

LAND USE DISTRICTS

The proposed R-1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential; Developing; Planned Greenfield with Area Structure Plan (ASP)” area as identified on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and Housing Diversity and Choice policies (subsection 2.3.1).

Saddle Ridge Area Structure Plan (1984)

The subject property is located within Cell B of the Saddle Ridge ASP and is identified as Residential land use on Map 6: Land Use Plan. The ASP makes no mention of secondary suites but state that residential areas are intended for residential and associated land uses.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Taralea Green NE and the rear lane. The area is served by Calgary Transit primary transit network bus service with a bus stop location within approximately 300 metres walking distance of the site on Taracove Road NE. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Taradale Community Association.

Citizen Comments

Administration did not receive any responses from citizens.

Public Meetings

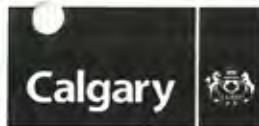
No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
TARADALE (WARD 5)
NORTEAST CORNER OF TARALEA GREEN NE

MAP 11NE

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments must be limited to the area designated on this page to ensure it will fit the space requirements of the report.

1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

- Invest in house to increase income so as to pay down mortgage.
- Will enable my extended family to live near by.
- Likely to increase the value of the property.
- Will be a safe secondary suite that will meet all requirements under Alberta safety code Act.

2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

- Have met the neighbours and informed about our intention, to change zoning of our property from R1 to R1S, and our intention to renovate basement suite to have it passed by city authorities as a safe living suite. They did not seem to have any objection to it.

3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

- AN ATTACHED Double garage provides 2 parkings for 2 vehicles
2 more on the driveway, 2 Rear gravel parking, and being
Giner lot there are more parking spaces.

4) Are there any potential negative impacts of this development that you are aware of?

NONE, That we are aware of.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

APPENDIX II
IMPORTANT TERMS

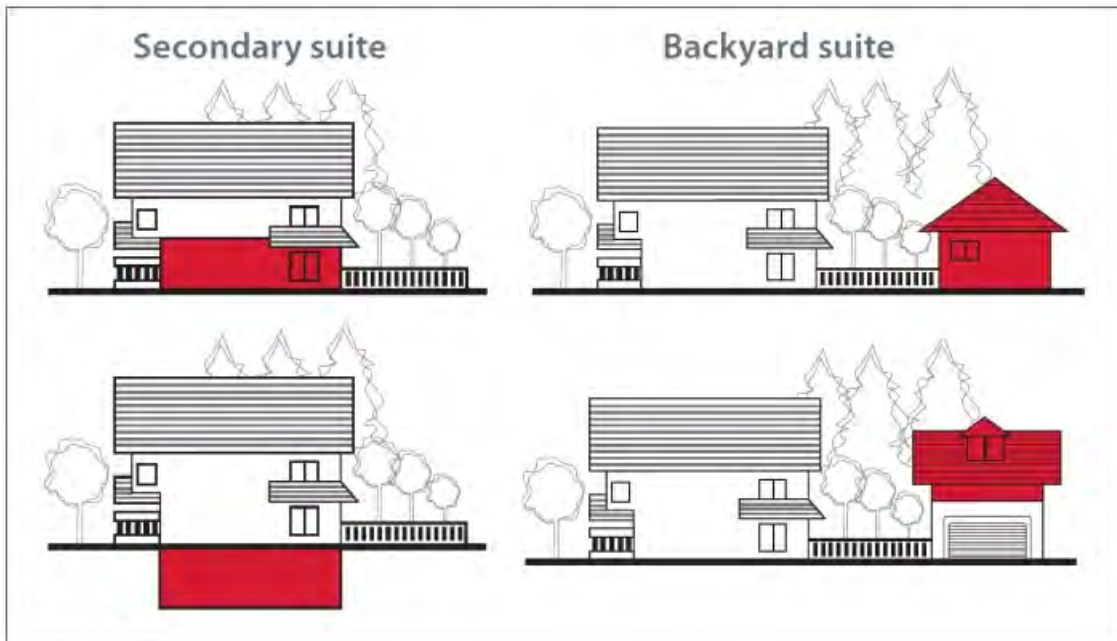
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



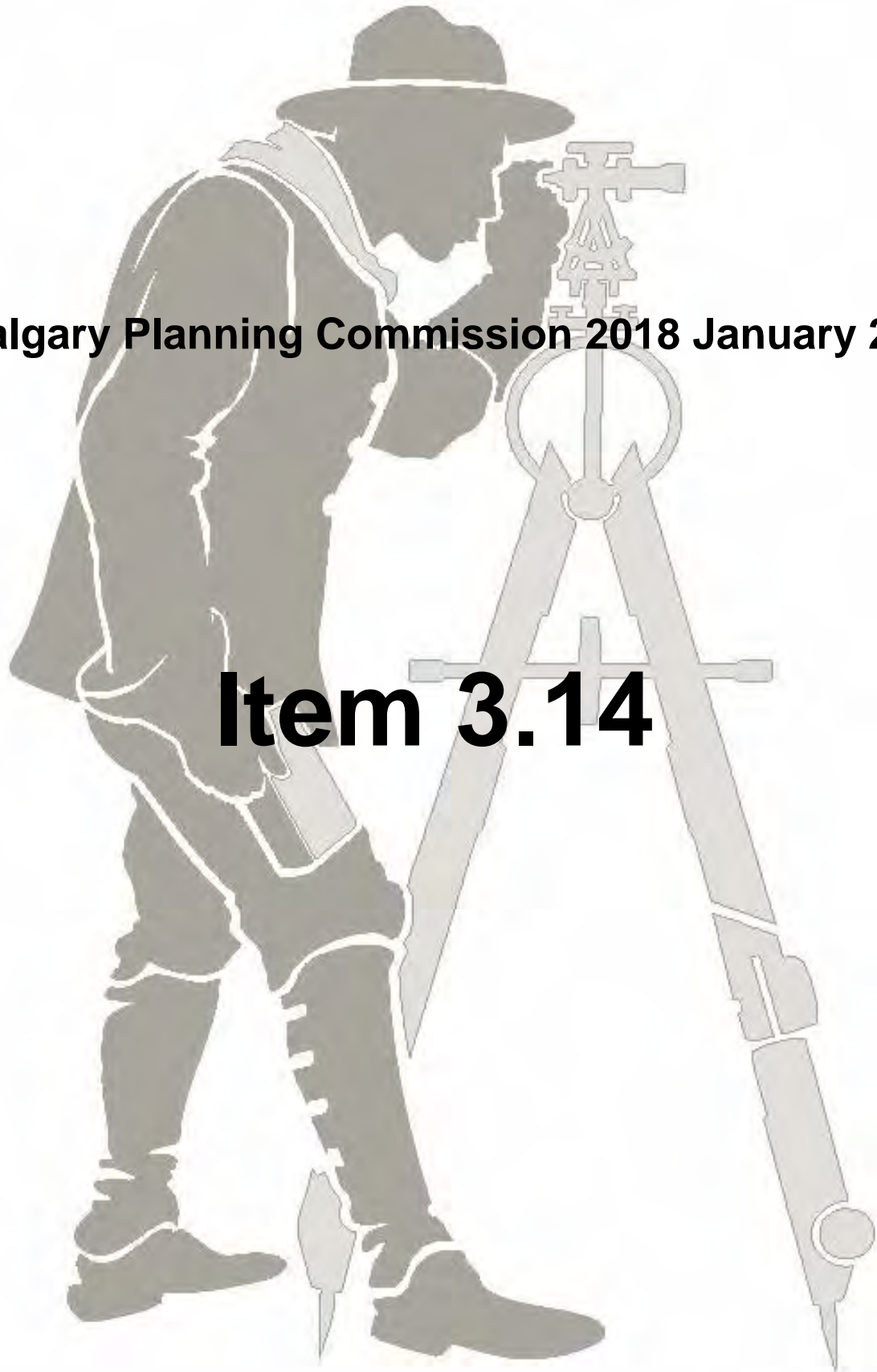
Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



Calgary Planning Commission 2018 January 25

Item 3.14



LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint

PREVIOUS COUNCIL DIRECTION

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

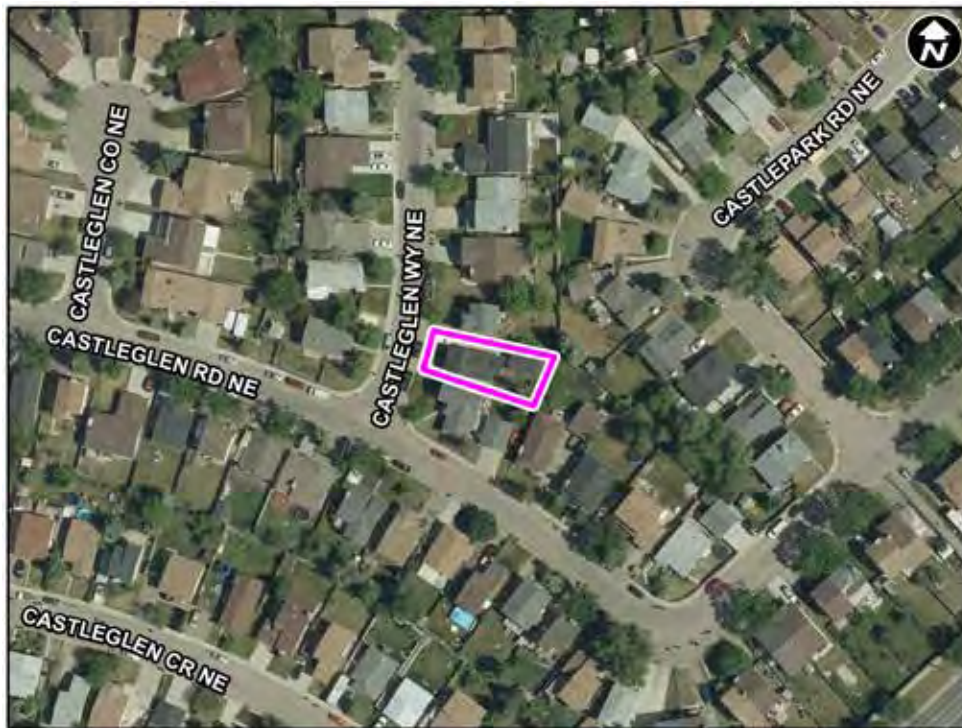
REASONS FOR RECOMMENDATION:

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

LOCATION MAPS



LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.04 hectares ± (0.09 acres ±) located at 192 Castleglen Way NE (Plan 7911471, Block 11, Lot 59) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

Applicant:

Bhagwant Singh

Landowner:

1742705 Alberta Ltd (Bhagwant Singh)

PLANNING EVALUATION

SITE CONTEXT

Located in a low density residential R-C1 setting in the community of Castleridge, the site is approximately 12 metres by 32 metres in size and is developed with a one-storey single detached dwelling and a detached one-car garage that is accessed from Castleglen Way NE. Surrounding development consists of low-density residential to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Castleridge's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

| CASTLERIDGE | |
|------------------------------------|-------|
| Peak Population Year | 2015 |
| Peak Population | 6,475 |
| 2017 Current Population | 6,181 |
| Difference in Population (Number) | -294 |
| Difference in Population (Percent) | -5% |

LAND USE DISTRICTS

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for the community of Castleridge.

Airport Vicinity Protection Area (AVPA)

The parcel is located within the AVPA boundary, however, it is located outside of all Noise Exposure Forecast Contours. The applicant is responsible for ensuring uses are compatible with the Airport Vicinity Protection Area Regulation. All buildings constructed and renovated on land in the Protection Area must comply with the acoustical requirements set out in the Alberta Building Code.

TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Castleglen Way NE and there is no rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 300 metre walking distance of the site on Falconridge Boulevard NE. On-street parking adjacent to the site is unregulated.

UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

PUBLIC ENGAGEMENT

Community Association Comments

Administration did not receive a response from the Falconridge / Castleridge Community Association.

Citizen Comments

Administration did not receive any responses from citizens.

Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission
Secondary Suites
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

- Invest in house to boost my income to paydown my mortgage.
- Likely to increase the value of my property.
- With a safe secondary suite that will meet all the requirements under Alberta Safety Code Act.
- will help in maximizing use of existing infrastructure in the neighbourhood.

2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

- Neighbors were met and informed about our intention to change zoning of our property from R-C1 to R-C1S and our intention to renovate basement suite to have it passed by city authorities as a safe living suite. They did not seem to have a problem with it.

3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

- A parking pad in front of garage has a parking space for 3 vehicles and a detached garage also provides an additional parking for 2 vehicles.

4) Are there any potential negative impacts of this development that you are aware of?

- Not, that I am aware of.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT
CASTLERIDGE (WARD 5)
NORTH OF CASTLEGLLEN ROAD NE AND CASTLEGLLEN WAY NE

MAP 2NE

APPENDIX II

IMPORTANT TERMS

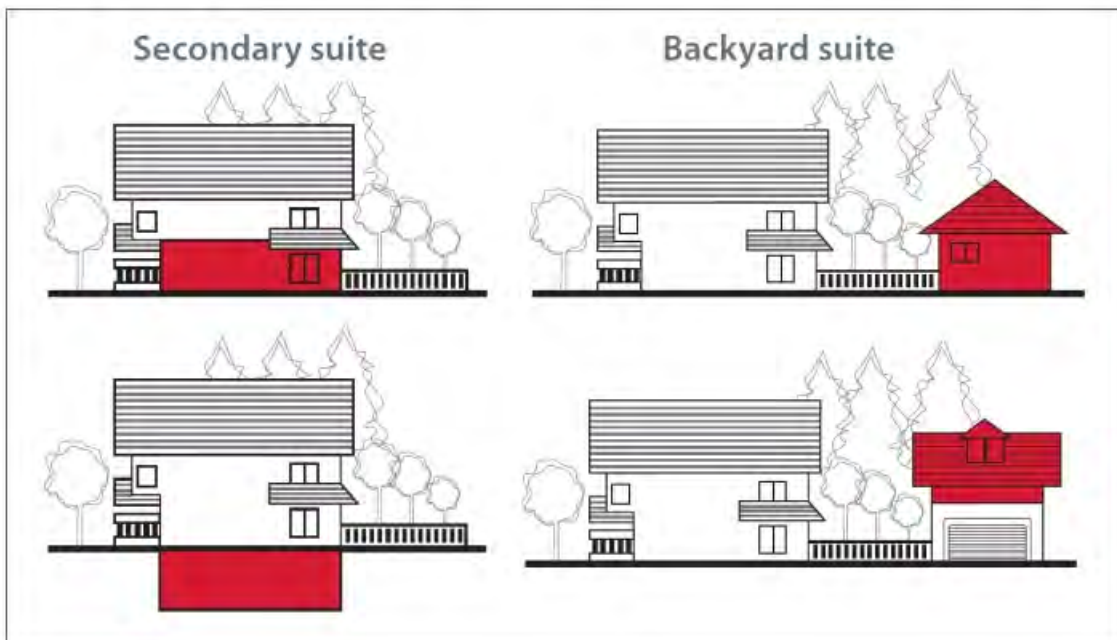
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



Secondary suite: A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

Backyard suite: A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A silhouette illustration of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is mounted on a tripod and has a circular level at the top. The surveyor has a tool case slung over his shoulder and a pencil in his hand.

Calgary Planning Commission 2018 January 25

Item 4.01

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

SUPPLEMENTARY REPORT

The following supplementary report relates to a previously considered outline plan application in the community of Saddle Ridge. This report is intended to capture the supplementary discussions, decisions and analysis that has resulted from the Calgary Planning Commission directives noted below. Additional background and supporting information can be found in the original report to Calgary Planning Commission, attached as at the end of this report.

CALGARY PLANNING COMMISSION DIRECTIVES

At the meeting held on 2017 December 14, Administration brought forward a report to Calgary Planning Commission (CPC) recommending approval of a proposed 1.39 hectare (3.43 acres) outline plan in the community of Saddle Ridge. The subject site is located east of Saddlecrest Boulevard NE and directly south of 89 Avenue NE. The site is one of eight previously subdivided estate residential lots in this part of Saddle Ridge awaiting more intensive urban development. This outline plan proposal and the associated land use application are intended to accommodate:

- a street network that allows for comprehensive development of the subject site and adjacent, currently undeveloped properties;
- approximately 19 single-detached dwellings (R-G);
- approximately 12 rowhouse units (M-G and R-G); and
- approximately 0.14 hectares of public park space (S-SPR).

At the 2017 December 14 CPC meeting, there was considerable discussion regarding a proposed public lane on the eastern edge of the plan area intended to service a block of single detached dwellings (R-G). The discussion stemmed from a letter received from the adjacent landowner to the east expressing concern about the location and extent of the proposed lane and its potential impact on the future subdivision design for their parcel. As a result of the discussions at the CPC hearing and questions about the merits of the proposed lane:

*The Calgary Planning Commission **REFERRED** the proposed Outline Plan for the subdivision of 1.39 hectares ± (3.43 acres ±) in order for:*

- *The Applicant and adjacent landowner to have further discussion; and*
- *To review the north/south lane on the east portion of the plan;*

to return to Calgary Planning Commission no later than 2018 January 25.

It is important to note that CPC recommended that Council approve the associated land use framework. The land use amendment will be considered by Council at the 2018 February 20 public hearing.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

ADMINISTRATION CONSULTATION WITH APPLICANT

Administration has engaged in telephone and email conversations with the Applicant and representatives of the adjacent landowner to further discuss the opportunities and constraints associated with the originally proposed north/south lane. As a result of these discussions, it has been agreed amongst the relevant parties that removal of the north/south lane on the east portion of the plan presents a mutually acceptable subdivision design solution that will continue to allow flexibility for future development to the east and an efficient use of land and infrastructure on the subject site. Based on this agreement and the intent of CPC's direction, the outline plan has been revised accordingly (see Figure 1)

PREVIOUS COUNCIL DIRECTION

None.

| | |
|---|------------------------|
| <p>ADMINISTRATION RECOMMENDATION(S)</p> | <p>2018 January 25</p> |
| <p>That Calgary Planning Commission APPROVE the proposed Outline Plan.</p> | |

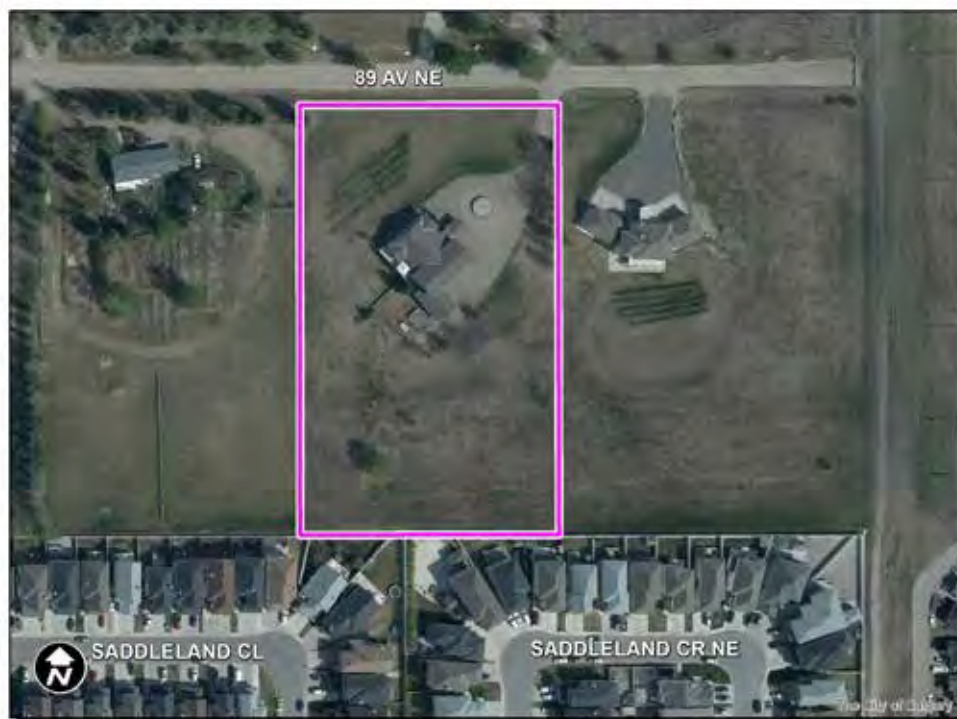
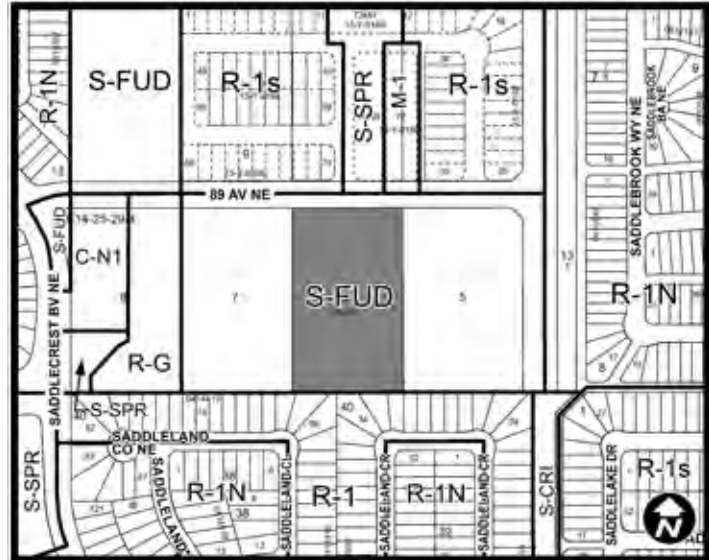
REASON(S) FOR RECOMMENDATION:

Administration has engaged in further discussions with the Applicant and the owner of the adjacent parcel to the east regarding the proposed north/south lane on the east portion of the outline plan area. The relevant parties support the removal of the north/south lane. Even with removal of the lane, there remains a mix of laned and laneless product within the subject outline plan area. The removal of the lane will continue to allow for development options for the landowner to the east. Further, the proposed land uses can be serviced without the rear lane and the proposed outline plan does not preclude a lane from being introduced in conjunction with subdivision of the adjacent parcel to the east in the future, should it be determined appropriate.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

LOCATION MAPS



ADMINISTRATION REPORT
TO CALGARY PLANNING COMMISSION
2018 JANUARY 25
2017 DECEMBER 14

ISC: PROTECTED
LOC2016-0196(OP)
Page 4 of 38

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Calgary Planning Commission **APPROVE** the proposed Outline Plan to subdivide 1.39 hectares \pm (3.43 acres \pm), with conditions (SUPPLEMENTARY APPENDIX I).

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

Applicant:

Civil Engineering Solutions

Landowner:

1976722 Alberta Ltd (Sada
Investments Ltd)

Address

6819 – 89 Avenue NE

Legal

Plan 731001, Block 6

PLANNING EVALUATION

SUBDIVISION DESIGN

The original subdivision design included a north/south lane to provide vehicular access to the eastern low density residential (R-G) block. The outline plan recommended by way of this report to remove the north/south lane in order to provide a subdivision design that addresses the concern and direction that the CPC raised and provided during the 2017 December 14 meeting (see Figure 1 below). The revised outline plan / subdivision layout does not impact the recommended approval for land use redesignation – as directed by CPC 2017 December 14.

Overall, the subdivision design remains largely unchanged and maintains its intended objectives of building upon the street pattern established by recently approved outline plan to further the west (LOC2014-0074) and providing a design that allows for a functionally independent plan that has the ability to integrate harmoniously with future adjacent subdivisions. The new subdivision design will facilitate a development pattern that makes efficient use of land and infrastructure and provides additional flexibility for the future subdivision design on the parcel to the east. Further, the plan can function from a transportation and access perspective without the proposed lane.

TRANSPORTATION NETWORKS

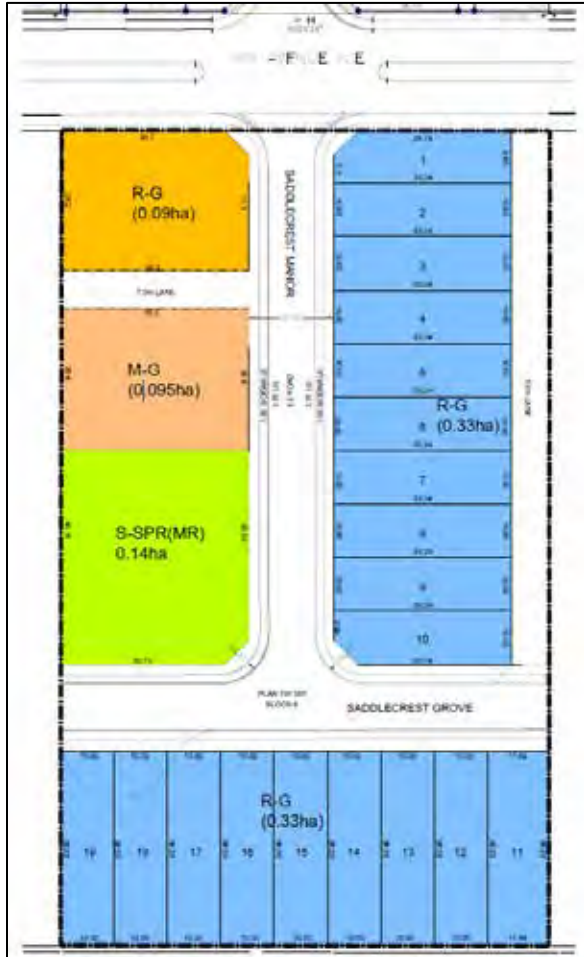
The originally proposed north/south lane is preferred but not necessary from planning and transportation network operations perspectives. Future dwellings within the eastern residential block can be effectively serviced by individual driveways from the abutting street.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

Figure 1: Original vs. Updated Subdivision Design

Original:



Updated:



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

SUPPLEMENTARY APPENDIX I

UPDATED CONDITIONS OF APPROVAL

Planning & Subdivision Services:

1. The relocation of any electrical and telecommunications installations shall be at the Developer's expense to the appropriate standards.
2. The standard City of Calgary Party Wall Agreement regarding the creation of separate parcels for all semi-detached dwellings, rowhouses, or townhouses where applicable in the R-G Land Use District areas, shall be executed and registered against the titles **concurrently with the registration of the final instrument.**
3. The existing buildings shall be removed prior to endorsement of the final instrument.

Development Engineering:

4. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
5. Off-site levies, charges and fees are applicable. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
6. Prior to endorsement of the final instrument/prior to release of the Development Permit execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
7. Make satisfactory cost sharing arrangements with West Nose Creek Development Corp. for part cost of the existing storm water wet pond constructed in Lot 2PUL in Block 4 that was paid for and/or constructed by Saddlecrest Lands Inc. under Saddle Ridge, Phase 02, DA2003-030.
8. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
 - a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

-
- b) Construct the underground utilities and surface improvements within 89 Avenue NE along the north boundary of the plan area.
 - c) Construct the roads and lanes within the boundary of the plan area.
 - d) Construct the MR within the plan area.
 - e) Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

Transportation:

- 9. **Prior to the approval of the affected tentative plan**, confirm that ROW at all intersections align with the existing or approved ROW to the north of the subject property (across 89 Avenue NE).
- 10. **Prior to the approval of the affected tentative plan**, provide three cross-sections along 89 Avenue NE – across each of the R-G (west) parcel, Saddlecrest Manor NE and the R-G (east) parcel. Ensure that the grades of the outline plan meet the approved grades of 89 Avenue NE as per the approved construction drawings for the development to the north. Since 89 Avenue NE is to be classified as a modified collector street (22.5m), the applicant will be required to dedicate any portion needed to meet the modified collector street right-of-way width (if applicable).
- 11. No direct vehicular access shall be permitted to or from 89 Avenue NE and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the affected Tentative Plan stage.
- 12. Unless otherwise approved by Transportation Planning at the Development Permit stage, the M-G site will be restricted to access via the lane only. **A restrictive covenant will be required concurrent with registration of the affected tentative plan**. Note that a Transportation Impact Assessment may be required at the Development Permit stage in support of the application.
- 13. Temporary turnarounds are required for all discontinuous links and must remain until such time that the links are made continuous. The temporary turnaround located on the southerly R-G lands must be a minimum radius of 15.25 metres.
 - a) **Prior to approval of the first tentative plan**, confirm design of temporary turnarounds (including drafting of any defined access agreements) to the satisfaction of the Director, Transportation Planning.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

-
- b) **Concurrent with registration of the affected tentative plan** register any access easements required to facilitate temporary turnarounds. Public access easements will be required for both the temporary turnaround located on the southerly R-G lands, as well as for a hammerhead turnaround over the M-G lands at the westerly terminus of the east-west lane.

Parks:

14. Construct all regional pathway routes within and along the boundaries of the plan area according to Parks' ***Development Guidelines and Standard Specifications – Landscape Construction*** (current version), including setback requirements, to the satisfaction of the Director, Parks.
15. Plant all public trees in compliance with the approved Public Landscaping Plan.
16. **Prior to the approval of the affected tentative plan**, finalized concept plans for all Municipal Reserve (MR) sites shall be submitted for Parks' review and approval.
17. **Prior to the approval of the affected tentative plan**, the developer shall confirm fencing requirements adjacent to MR parcels to the satisfaction of the Director, Calgary Parks.
18. **Prior to approval of the first tentative plan or stripping and grading permit** (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent parks and open space (MR), with all grading confined to the private property, unless otherwise approved by Parks.
19. It will be the developer's responsibility to construct the Municipal Reserve (MR) sites, subject to the terms and conditions of a Standard Development Agreement.
20. No backsloping from private lots onto adjacent MR lands will be permitted.
21. Drainage from proposed lots onto adjacent MR lands will not be permitted unless otherwise approved by the Director, Parks and Director, Water Resources.
22. Compensation for dedication of reserves in excess of 10% is deemed to be \$1.00.

OUTLINE PLAN
 SADDLE RIDGE (WARD 5)
 EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
 AVENUE NE

MAP 14NE

SUPPLEMENTARY APPENDIX II

UPDATED SUBDIVISION DATA SHEET

| | HECTARES | ACRES |
|-----------------------------|----------|-------|
| GROSS AREA OF PLAN | 1.39 | 3.43 |
| LESS: ENVIRONMENTAL RESERVE | N/A | N/A |
| LESS: LAND PURCHASE AREA | N/A | N/A |
| NET DEVELOPABLE AREA | 1.39 | 3.43 |

| LAND USE (Residential) | HECTARES | ACRES | # OF LOTS | # OF UNITS (FOR MULTI RESIDENTIAL) |
|---------------------------|----------|-------|-----------|--|
| R-G | 0.81 | 2.00 | 25 | 25 |
| M-G | 0.10 | 0.23 | 6 | 6 |
| Total Residential | 0.91 | 2.24 | 31 | 31 |
| (Non-Residential) | N/A | N/A | N/A | N/A |

| | HECTARES | ACRES | % OF NET AREA |
|--------------------|----------|-------|---------------|
| ROADS (CREDIT) | 0.24 | 0.60 | 17.2 |
| PUBLIC UTILITY LOT | N/A | N/A | N/A |

| RESERVES | HECTARES | ACRES | % OF NET AREA |
|-----------------|----------|-------|---------------|
| MR | 0.14 | 0.35 | 10.0 |
| MSR | N/A | N/A | N/A |
| MR (NON-CREDIT) | N/A | N/A | N/A |

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

SUPPLEMENTARY APPENDIX III

APPLICANT'S SUBMISSION

January 6, 2018 update:

Following up on the letter dated Dec 5th 2017 submitted by Quantum Place Development (QDP) to the Calgary Planning Commission, we have decided to remove the lane at the back of the lots on the east side. This is as per request by QPD, work mutually and assist with their planning.

We have revised our outline plan accordingly and submitted the revised plan to you on January 4th, 2018. Please proceed with our application to CPC, and if you have questions or require further input please contact us. We shall be happy to assist.

Original:

We hereby take this opportunity to submit this application to allow for the development and future subdivision in the NE of Calgary. The location of this subdivision is 6819 89th Avenue Ne Calgary, Block 6, Plan 731 001. The location is surrounded by existing subdivisions and is accessible by a developed road system and has the City amenities servicing the community.

Main amenities are the transit network and the utilities. In addition the site is south of 89th Avenue NE and 89th Ave is being developed to a dual carriageway road standard as part of the 72 lot subdivision north of 89th avenue. The 72 lot subdivision has been approved by the City and will be implemented this summer as all City approvals are completed.

This is an attractive area and great for families. There are parks, schools, recreational facilities and shops all within walking distance. Lots east and west of the proposed site, stand to benefit for future planning of subdividing these lots and set planning procedures and concepts.

The parcel size is 1.39 ha and has an existing house and driveway and potentially covered by grass and some trees. This application is to redesignate and subdivide the existing parcel to the following designations:

- R-G - Residential Single family, 19 lots. These have an option of being multifamily. Area 0.66ha.
- M-G - Multifamily, with a maximum of 7 units. Area 0.095ha.
- R-G – Row House, 6 units. There have an option of being single family. Area 0.09ha.

Area's totaling 0.145 ha have been put aside for S-SPR (MR) green space as per City of Calgary Landuse Redesignation requirements.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

SUPPLEMENTARY APPENDIX IV

UPDATED OUTLINE PLAN

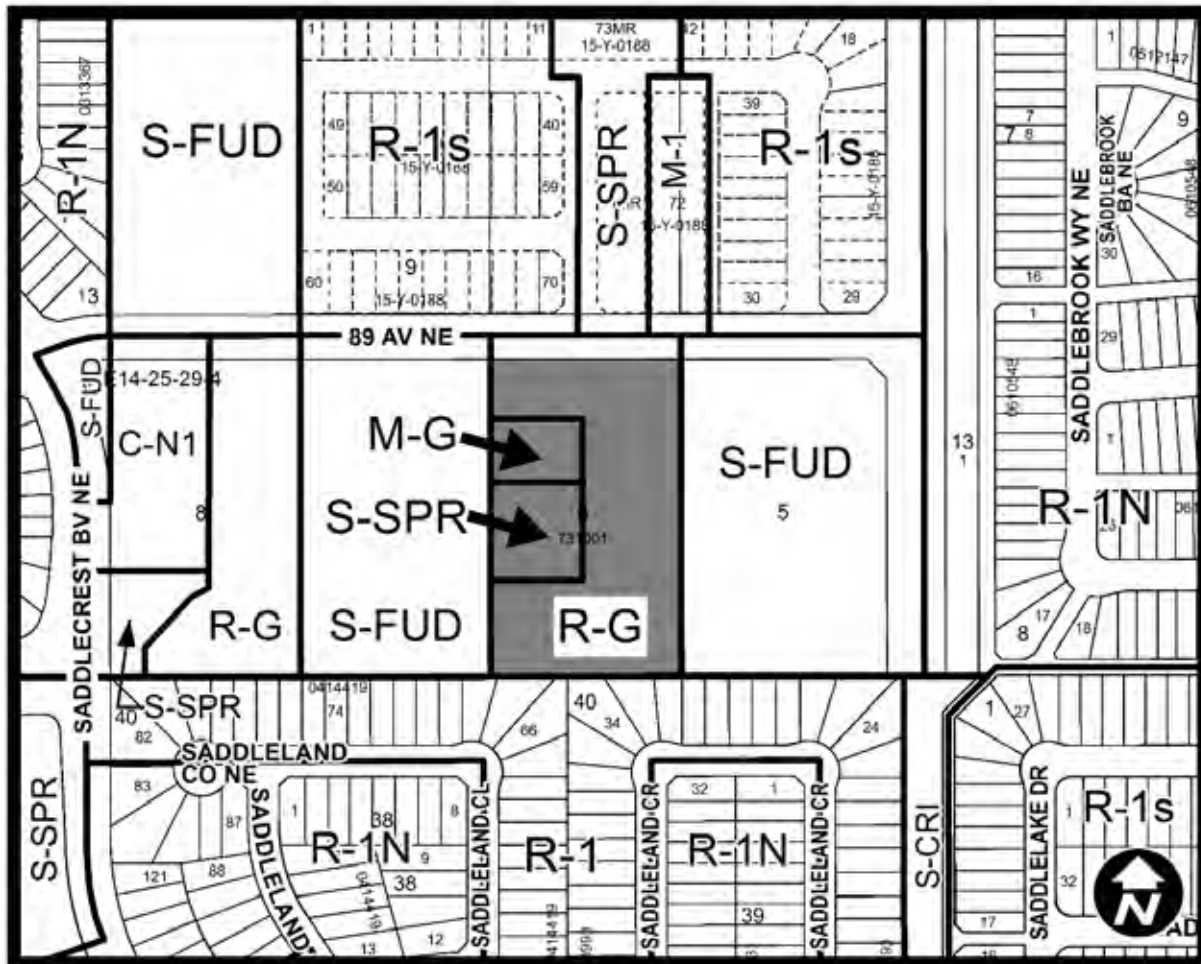


OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

SUPPLEMENTARY APPENDIX V

PROPOSED LAND USE DISTRICT MAP



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

EXECUTIVE SUMMARY

This application proposes an outline for the future subdivision and development of approximately 1.39 hectares of land in the community of Saddle Ridge. The site is situated to the east of Saddlecrest Boulevard NE and directly south of 89 Avenue NE. The subject site is one of eight acreages in this part of Saddle Ridge still awaiting urban development. Previous outline plan and land use approvals have been granted by Calgary Planning Commission and Council respectively on four of the eight acreages; three to the north of 89 Avenue NE and one to the south of 89 Avenue NE. This outline plan proposal is intended to accommodate:

- a street network that allows for comprehensive development of the subject site and adjacent, currently undeveloped properties;
- approximately 19 single-detached dwellings (R-G);
- approximately 12 rowhouse units (M-G and R-G); and
- approximately 0.14 hectares of public park space (S-SPR).

This application has been applied for with the support of a land use amendment.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2017 December 14

That Calgary Planning Commission **APPROVE** the proposed Outline Plan.

The Calgary Planning Commission **REFERRED** the proposed Outline Plan for the subdivision of 1.39 hectares ± (3.43 acres ±) in order for:

- The Applicant and adjacent landowner to have further discussion; and
- To review the north/south lane on the east portion of the plan;

to return to Calgary Planning Commission no later than 2018 January 25.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

REASON(S) FOR RECOMMENDATION:

The proposed subdivision outline plan provides a framework for development which may function independently as well as integrate with future, adjacent development in the area. The proposal is in keeping with applicable policies of the Municipal Development Plan and the Saddle Ridge Area Structure Plan (ASP). The proposal allows for a:

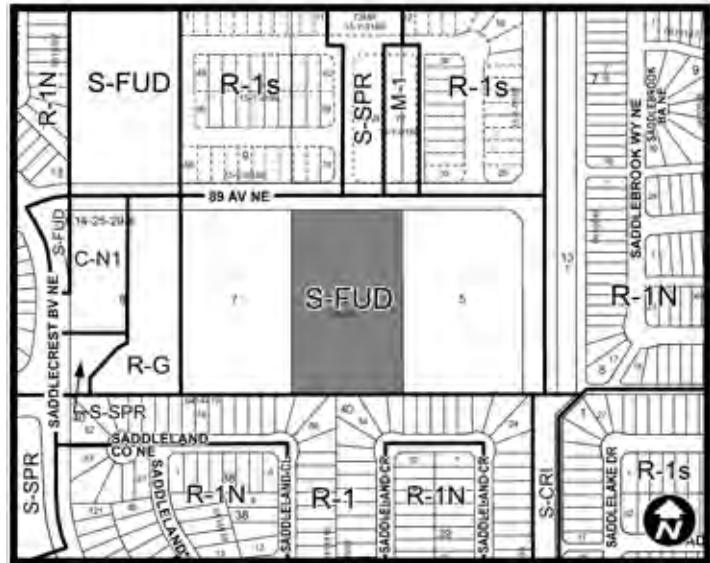
- mix of housing types that exceeds the minimum residential density requirements identified in the Saddle Ridge ASP;
- strategically located public park space; and
- street network that builds upon the existing subdivision pattern to facilitate connectivity within the area.

The conditions of approval (APPENDIX I) effectively implement the technical aspects of the plan area's future development.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

LOCATION MAPS



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Calgary Planning Commission **APPROVE** the proposed Outline Plan to subdivide 1.39 hectares \pm (3.43 acres \pm), with conditions (APPENDIX I).

Moved by: C. Friesen

NO VOTE

Absent: Ms. Juan left the room due to a pecuniary conflict of interest and did not take part in the discussion or voting and Mr. Palmiere.

2017 December 14

MOTION: The Calgary Planning Commission accepted correspondence from:

- QuantumPlace Developments Ltd dated 2017 December 12;

as distributed, and directs it to be included in the report as APPENDIX VIII.

Moved by: J. Gondek

Carried: 7 – 0

Absent: Ms. Juan left the room due to a pecuniary conflict of interest and did not take part in the discussion or voting and Mr. Palmiere.

MOTION: The Calgary Planning Commission **REFERRED** the proposed Outline Plan for the subdivision of 1.39 hectares \pm (3.43 acres \pm) in order for:

- The Applicant and adjacent landowner to have further discussion; and
- To review the north/south lane on the east portion of the plan;

to return to Calgary Planning Commission no later than 2018 January 25.

Moved by: R. Vanderputten

Carried: 7 – 0

Absent: Ms. Juan left the room due to a pecuniary conflict of interest and did not take part in the discussion or voting and Mr. Palmiere.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

Applicant:

Civil Engineering Solutions

Address

6819 – 89 Avenue NE

Landowner:

1976722 Alberta Ltd (Sada
Investments Ltd)

Legal

Plan 731001, Block 6

PLANNING EVALUATION

SITE CONTEXT

The subject site is located in the community of Saddle Ridge to the east of Saddlecrest Boulevard NE and directly south of 89 Avenue NE. Lands in the general vicinity of the subject site had developed in early 2000's as primarily low density residential. A future Community Activity Centre and the 88 Avenue LRT NE station is planned for lands located approximately 600 metres to the west.

The parcel is relatively flat and developed with a single detached dwelling. No significant vegetation exists on the site.

Surrounding development includes single detached dwellings (R-1N) to the south and west across Saddlecrest Boulevard NE. Both of the neighbouring acreages to the west and east contain a single detached dwelling. Land use and outline plans for three of the four acreages north of 89 Avenue NE were approved in 2015 and 2016. The anticipated residential development will include low density residential uses, one multi-residential site and municipal reserve space which provides a connection with the school site to the north. Land use and outline plan were also approved at the corner of Saddlecrest Boulevard NE and 89 Avenue NE on 2017 September 11 (see Figure 2 on page 7 of this report).

LAND USE DISTRICTS

A land use redesignation application has been submitted in support of this outline plan. One of the challenges in planning areas with limited parcel sizes and fragmented ownership patterns is establishing a well thought out and comprehensive neighbourhood and infrastructure plan. The proposed outline plan together with the accompanying land use redesignation and the supporting shadow plan information, adequately deal with these considerations.

The associated land use amendment application proposes to redesignate the subject site from a Special Purpose – Future Urban Development (S-FUD) District to three different land use districts.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

Special Purpose – School, Park & Community Reserve (S-SPR) District (0.14 ± ha; 0.35 ± ac)

A portion of the site will be taken as a Municipal Reserve (MR) parcel at the time subdivision. As such, approximately 0.14 hectare portion of the site is proposed to be redesignated to a Special Purpose – School, Park, and Community Reserve (S-SPR) District. Careful consideration was given to the location and use of this future MR site. The proposed location was chosen to:

- a) to facilitate expansion of the park to the east in parallel with the future subdivision and development of adjacent lands;
- b) maximize the use of, activate and provide “eyes” on the park;
- c) provide a centrally located and connected amenity for the future neighbourhood; and
- d) leverage the future multi-residential uses proposed to the north of this future park space.

Multi-residential – At Grade Housing (M-G) District (0.09 ± ha; 0.25 ± ac)

A Multi-residential – At Grade Housing (M-G) District is proposed directly to the north of the proposed public park. The proposed M-G District is primarily intended to accommodate multi-residential development within townhouse and rowhouse building forms. An assisted living and live-work units, for example, are some of the other allowable discretionary uses in the proposed M-G District. This land use district was specifically designed to restrict single-detached, semi-detached and duplex dwellings.

Residential – Low Density Mixed Housing (R-G) District (0.75 ± ha; 1.88 ± ac)

A Residential – Low Density Mixed Housing (R-G) District is proposed on the remainder of the site. The proposed R-G District is intended to accommodate a wide range of residential development such as single-detached, semi-detached as well as rowhouse and duplex dwellings. Secondary suites and backyard suites are also allowed in the R-G District.

DENSITY

The minimum residential density required by the Saddle Ridge ASP is 17.3 units per gross developable hectare. The proposed subdivision pattern and land use mix allow for a maximum of approximately 31 units per hectare (uph) or approximately 43 units. The anticipated number of units (31) is expected to achieve a residential density of approximately 22 uph. As such, the proposal will meet and exceed the minimum residential intensity threshold identified in the Saddle Ridge ASP. See Figure 1 for additional details.

**OUTLINE PLAN
 SADDLE RIDGE (WARD 5)
 EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
 AVENUE NE**

MAP 14NE

| <i>Figure 1</i> | Anticipated # of units | Maximum # of units |
|-------------------------------------|------------------------|--------------------|
| R-G | 25 | 36 |
| M-G | 6 | 7 |
| TOTAL | 31 | 43 |
| DENSITY (MIN. OF 17.3 UPH REQUIRED) | 22 uph | 31 uph |

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the South Saskatchewan Regional Plan (SSRP). The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (statutory - 2009)

The subject site is located within the Residential - Developing - Planned Greenfield with Area Structure Plan (ASP) area, according to Map 1 Urban Structure of the Municipal Development Plan (MDP). ASPs existing prior to the adoption of the MDP are to be considered appropriate policies to provide specific direction for the development of respective communities.

The proposal contributes to creating a complete community through providing flexible residential districts, a public open space and a street network designed to facilitate pedestrian and vehicular movement in the area. The MDP establishes intensity targets for new communities in Future Greenfield Areas (those areas that do not yet have a local area plan). It sets out a minimum intensity threshold of 60 people and jobs per hectare. While this parcel is in an area with a local area plan, the proposed intensity of the development will meet this general intensity minimum of the MDP.

Saddle Ridge Area Structure Plan (statutory - 1984)

The subject property is located within Cell C of the Saddle Ridge ASP and is identified as Residential land use on Map 6: Land Use Plan. Residential areas are intended for residential and associated land uses as listed in Section 4.2. The ASP states a minimum residential density of 17.3 units per gross developable hectare must be achieved within a community. The proposal has the ability to meet and exceed this residential density requirement.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

Transit Oriented Development (TOD) Policies Guidelines (non-statutory - 2004)

Although the subject site is just beyond the 600 metre radius of the future 88 Avenue NE LRT Station, the proposal was reviewed against the Transit Oriented Development Policy Guidelines.

Guidelines 4.1 and 6.1 of the document encourage quality pedestrian connections and development including transit-supportive land uses. The proposal allows for:

- a variety of housing form;
- development flexibility over time;
- density that will exceed the existing residential densities of the surrounding residential area; and
- a grid-like street network with sidewalks on both of the streets.

SUBDIVISION DESIGN

The proposal has been evaluated against and complies with the Complete Streets Policy and Guide. The proposed subdivision design and street network build upon the recently approved outline plan (LOC2014-0074) at the corner of Saddlecrest Boulevard NE and 89 Avenue NE (see Figure 2). The proposed design allows for the integration with a future development to the east and west, and thereby completing the anticipated street network for this area.

The proposed outline plan includes two residential standard streets. Saddleridge Manor NE connects this outline plan area to 89 Avenue NE, while Saddleridge Grove NE will provide future pedestrian and vehicular connections to the east and west. Two laneways are also proposed. The south and east portions of the plan are anticipated to be subdivided in a manner consistent with the majority of the surrounding neighbourhood. The western portion of the plan is designed to directly respond to the park space and allow for a future integration with the property to the west (see APPENDIX VII).

Figure 2

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE



TRANSPORTATION NETWORKS

The outline plan area has access to the nearby arterial and skeletal road networks within the broader community. Stoney Trail (to and from the south) is currently accessible via Airport Trail NE. There is accessibility to 60 Street NE via 88 Avenue NE. Metis Trail NE will soon be accessible via an extension of 88 Avenue NE which is currently being constructed.

Transit service is currently available in the area. Bus route 159 provides connection to the Saddletowne LRT station, with stops located within 400 metres of the plan area. The future 88 Avenue NE Station (extension of Blue Line LRT) will be just beyond 600 metres of the plan area. Saddlecrest Boulevard NE is an existing primary collector roadway. Additionally, 89 Avenue NE is classified as a 22.5 metre modified collector with sidewalks on both sides of the street and a median required to accommodate emergency access.

The proposed internal street networks within this plan area consist of:

- Saddlecrest Grove NE – 16 metre residential street standard with sidewalks both sides of the street.
- Saddle Manor NE – 16 metre residential street standard with sidewalks on both sides of the street.
- North-south lane – 7 metre right-of-way intended to provide vehicular access to proposed R-G parcels.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

-
- East-west lane – 7 metre right-of-way designed to provide vehicular access to the proposed M-G and R-G sites.

A Transportation Impact Assessment was not required for this application; however transportation has recently reviewed the impact of full-build for this and the adjacent 7 parcels and confirmed that the network proposed is appropriate. A TIA may be required at the subsequent development permit stage to determine potential improvements to the adjacent street network

UTILITIES & SERVICING

Sanitary, storm and water services are available to service the subject site.

ENVIRONMENTAL ISSUES

No environmental issues have been identified at this time.

PUBLIC ENGAGEMENT

Community Association Comments

The Saddle Ridge Community Association (SRCA) was circulated at the outset of the application review in 2017 January 04. In addition, the SRCA was provided with an opportunity to comment on the most recent version of the proposed land use/outline plan in 2017 November 2. The SRCA expressed concerns focused on potential impacts the anticipated development intensity may have on the function of the existing 89 Avenue NE and Saddlecrest Boulevard NE intersection.

Citizen Comments

One letter has been received objecting to the proposed north/south lane due to the following:

1. Crime
 - Saddleridge already experiences high crime rates and CPTED design principles should be made a higher priority.
 - Rear laneways do not have street lighting and experience a low level of pedestrian activity (a known deterrence to crime).
 - In nearby developments to the east, which include laned-product, crime is prevalent in the laneways. The existing land owner has witness an increase in prostitution, automobile theft, and break-ins.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

2. There are very few other lanes proposed in the Saddlecrest area
 - The only lanes proposed are associated with multi-residential sites and were used as a design solution to infill development in a fragmented (small parcel) development area.
3. Due to the width of the parcel (measuring east/west it is 175 feet or 53.34 metres) at 6827 - 89 Avenue NE, the laneway makes development of the interior parcels awkward, or potentially prohibitive.

Public Meetings

No public meetings were held by either the Applicant or Administration.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX I

CONDITIONS OF APPROVAL

Planning & Subdivision Services:

1. The relocation of any electrical and telecommunications installations shall be at the Developer's expense to the appropriate standards.
2. The standard City of Calgary Party Wall Agreement regarding the creation of separate parcels for all semi-detached dwellings, rowhouses, or townhouses where applicable in the R-G Land Use District areas, shall be executed and registered against the titles **concurrently with the registration of the final instrument.**
3. The existing buildings shall be removed prior to endorsement of the final instrument.
4. Compensation for dedication of public roadways in excess of 30% is deemed to be \$1.00.
5. All parcels abutting a lane within the outline the plan area shall have direct vehicle access from the lane (no front drive garages). A restrictive covenant shall be registered against the titles of those parcels to that effect.

Development Engineering:

7. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
8. Off-site levies, charges and fees are applicable. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
9. Prior to endorsement of the final instrument/prior to release of the Development Permit execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
10. Make satisfactory cost sharing arrangements with West Nose Creek Development Corp. for part cost of the existing storm water wet pond constructed in Lot 2PUL in Block 4 that was paid for and/or constructed by Saddlecrest Lands Inc. under Saddle Ridge, Phase 02, DA2003-030.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

-
11. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
- a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b) Construct the underground utilities and surface improvements within 89 Avenue NE along the north boundary of the plan area.
 - c) Construct the roads and lanes within the boundary of the plan area.
 - d) Construct the MR within the plan area.
 - e) Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

Transportation:

12. **Prior to the approval of the affected tentative plan**, confirm that ROW at all intersections align with the existing or approved ROW to the north of the subject property (across 89 Avenue NE).
13. **Prior to the approval of the affected tentative plan**, provide four cross-sections along 89 Avenue NE – across each of the R-G (west) parcel, Saddlecrest Manor NE, R-G (east) parcel, and the proposed lane behind the R-G uses. Ensure that the grades of the outline plan meet the approved grades of 89 Avenue NE as per the approved construction drawings for the development to the north. Since 89 Avenue NE is to be classified as a modified collector street (22.5m), the applicant will be required to dedicate any portion needed to meet the modified collector street right-of-way width (if applicable).
14. No direct vehicular access shall be permitted to or from 89 Avenue NE and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the affected Tentative Plan stage.
15. Unless otherwise approved by Transportation Planning at the Development Permit stage, the M-G site will be restricted to access via the lane only. **A restrictive covenant will be required concurrent with registration of the affected tentative plan**. Note that a Transportation Impact Assessment may be required at the Development Permit stage in support of the application.

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

-
16. Lane access to 89 Avenue NE will be restricted to right-in/right-out movements only.
 17. No direct vehicular access shall be permitted to or from 89 Avenue NE and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.
 18. Temporary turnarounds are required for all discontinuous links and must remain until such time that the links are made continuous. The temporary turnaround located on the southerly R-G lands must be a minimum radius of 15.25 metres.

Prior to approval of the first tentative plan, confirm design of temporary turnarounds (including drafting of any defined access agreements) to the satisfaction of the Director, Transportation Planning.

Concurrent with registration of the affected tentative plan register any access easements required to facilitate temporary turnarounds. Public access easements will be required for both the temporary turnaround located on the southerly R-G lands, as well as for a hammerhead turnaround over the M-G lands at the westerly terminus of the east-west lane.

Parks:

19. Construct all regional pathway routes within and along the boundaries of the plan area according to Parks' ***Development Guidelines and Standard Specifications – Landscape Construction*** (current version), including setback requirements, to the satisfaction of the Director, Parks.
20. Plant all public trees in compliance with the approved Public Landscaping Plan.
21. **Prior to the approval of the affected tentative plan**, finalized concept plans for all Municipal Reserve (MR) sites shall be submitted for Parks' review and approval.
22. **Prior to the approval of the affected tentative plan**, the developer shall confirm fencing requirements adjacent to MR parcels to the satisfaction of the Director, Calgary Parks.
23. **Prior to approval of the first tentative plan or stripping and grading permit** (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent parks and open space (MR), with all grading confined to the private property, unless otherwise approved by Parks.
24. It will be the developer's responsibility to construct the Municipal Reserve (MR) sites, subject to the terms and conditions of a Standard Development Agreement.
25. No backsloping from private lots onto adjacent MR lands will be permitted.

**OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE**

MAP 14NE

26. Drainage from proposed lots onto adjacent MR lands will not be permitted unless otherwise approved by the Director, Parks and Director, Water Resources.
27. Compensation for dedication of reserves in excess of 10% is deemed to be \$1.00.

OUTLINE PLAN
 SADDLE RIDGE (WARD 5)
 EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
 AVENUE NE

MAP 14NE

APPENDIX II
SUBDIVISION DATA SHEET

| | HECTARES | ACRES |
|-----------------------------|----------|-------|
| GROSS AREA OF PLAN | 1.39 | 3.43 |
| LESS: ENVIRONMENTAL RESERVE | N/A | N/A |
| LESS: LAND PURCHASE AREA | N/A | N/A |
| NET DEVELOPABLE AREA | 1.39 | 3.43 |

| LAND USE (Residential) | HECTARES | ACRES | # OF LOTS | # OF UNITS (FOR MULTI RESIDENTIAL) |
|---------------------------|-------------|-------------|-----------|--|
| R-G | 0.75 | 1.85 | 25 | 25 |
| M-G | 0.09 | 0.22 | 6 | 6 |
| Total Residential | 0.84 | 2.07 | 31 | 31 |
| (Non-Residential) | N/A | N/A | N/A | N/A |

| | HECTARES | ACRES | % OF NET AREA |
|--------------------|----------|-------|---------------|
| ROADS (CREDIT) | 0.4 | 1.01 | 29.3 |
| PUBLIC UTILITY LOT | N/A | N/A | N/A |

| RESERVES | HECTARES | ACRES | % OF NET AREA |
|-----------------|----------|-------|---------------|
| MR | 0.14 | 0.35 | 10.0 |
| MSR | N/A | N/A | N/A |
| MR (NON-CREDIT) | N/A | N/A | N/A |

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX III

APPLICANT'S SUBMISSION

We hereby take this opportunity to submit this application to allow for the development and future subdivision in the NE of Calgary. The location of this subdivision is 6819 89th Avenue Ne Calgary, Block 6, Plan 731 001. The location is surrounded by existing subdivisions and is accessible by a developed road system and has the City amenities servicing the community.

Main amenities are the transit network and the utilities. In addition the site is south of 89th Avenue NE and 89th Ave is being developed to a dual carriageway road standard as part of the 72 lot subdivision north of 89th avenue. The 72 lot subdivision has been approved by the City and will be implemented this summer as all City approvals are completed.

This is an attractive area and great for families. There are parks, schools, recreational facilities and shops all within walking distance. Lots east and west of the proposed site, stand to benefit for future planning of subdividing these lots and set planning procedures and concepts.

The parcel size is 1.39 ha and has an existing house and driveway and potentially covered by grass and some trees. This application is to redesignate and subdivide the existing parcel to the following designations:

- R-G - Residential Single family, 19 lots. These have an option of being multifamily. Area 0.66ha.
- M-G - Multifamily, with a maximum of 7 units. Area 0.095ha.
- R-G – Row House, 6 units. There have an option of being single family. Area 0.09ha.

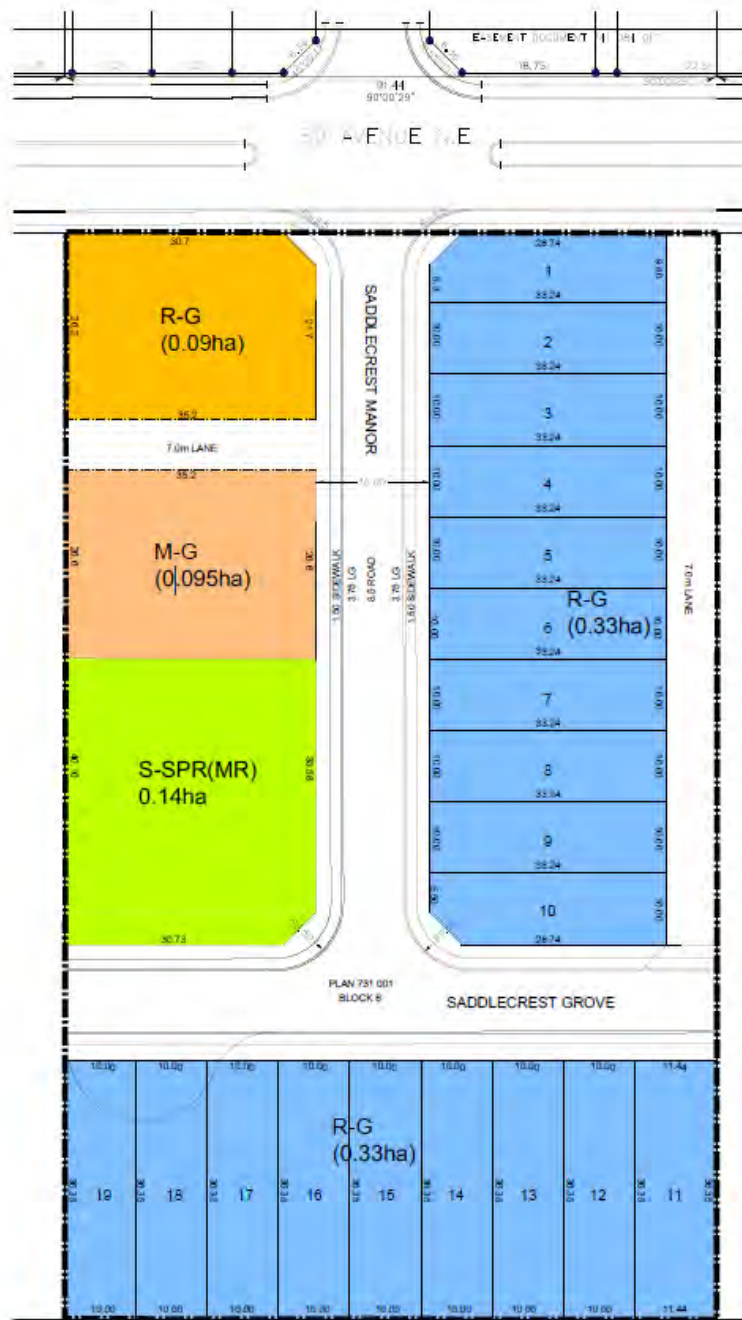
Area's totaling 0.145 ha have been put aside for S-SPR (MR) green space as per City of Calgary Landuse Redesignation requirements.

OUTLINE PLAN
 SADDLE RIDGE (WARD 5)
 EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
 AVENUE NE

MAP 14NE

APPENDIX IV

OUTLINE PLAN

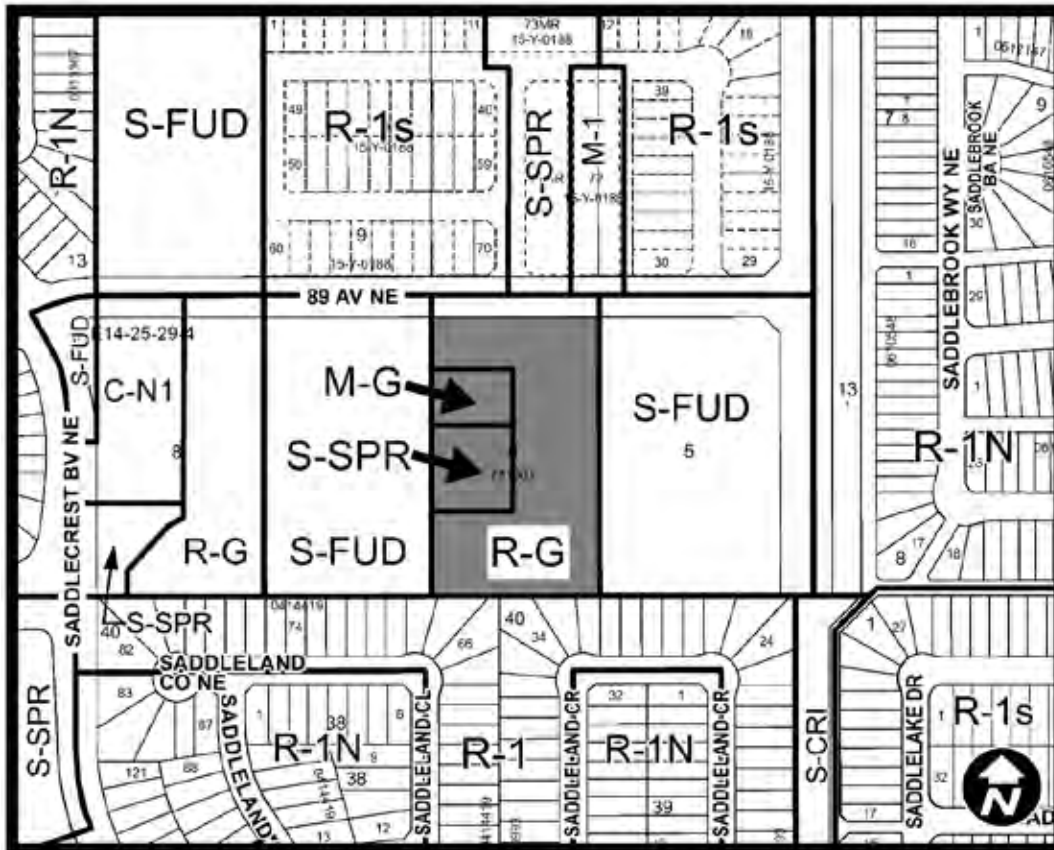


OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX V

PROPOSED LAND USE DISTRICT MAP



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX VI

LETTERS SUBMITTED

Saddle Ridge Community Association
#27 7555 Falconridge Blvd NE
T3J 0C9

November 15, 2017

Re: LOC2016-0196

The Saddle Ridge Community Association is concerned that the increased number of units proposed for this development will increase the pressure on the intersection of 89th Avenue NE and Saddlecrest Boulevard NE, which is already perceived as a problem due to its unfinished state.

We assume the plans for the park area remain as they were on the previously submitted.

Judy Brown
Land Use Chairman
Saddle Ridge Community Association

OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX VII

SHADOW PLAN



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE

APPENDIX VIII



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE



OUTLINE PLAN
SADDLE RIDGE (WARD 5)
EAST OF SADDLECREST BOULEVARD NE AND SOUTH OF 89
AVENUE NE

MAP 14NE



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument mounted on a tripod. The surveyor is looking through the instrument. The entire image is rendered in a dark grey color with white outlines.

Calgary Planning Commission 2018 January 25

Item 5.01

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE,
RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT:
LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

EXECUTIVE SUMMARY

This is a development permit for 14,857 square metres of retail development in four buildings in the new community of Medicine Hill. It is the second development permit in the community of Medicine Hill and will be the first development permit in the Trinity gateway precinct.

The retail and entertainment development proposal represents a unique architectural response to this important gateway into our city with unique highway architecture which responds to the setting of the site.

Subject to conditions of approval listed in APPENDIX II this application is in alignment with statutory and non-statutory planning policy and is recommended for approval.

PREVIOUS COUNCIL DIRECTION

None.

| | |
|---|------------------|
| ADMINISTRATION RECOMMENDATION(S) | 2018, January 25 |
|---|------------------|

That Calgary Planning Commission **APPROVE** the proposed Development Permit application.

REASON(S) FOR RECOMMENDATION:

This development permit aligns with statutory planning policy contained in the Canada Olympic Park and Adjacent Lands Area Structure Plan (ASP), the Land Use Bylaw and Direct Control District. The proposal also meets non-statutory planning policy contained in the Entranceway Guidelines and Large Format Retail Guidelines.

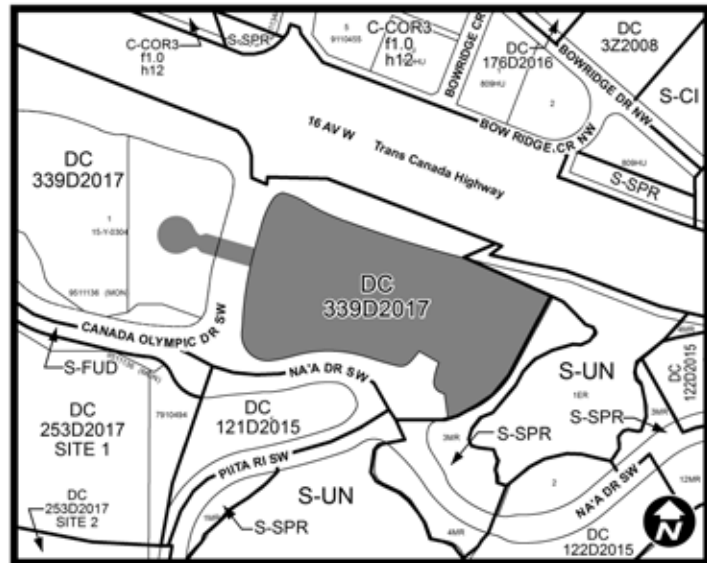
Furthermore:

- The proposal will provide a mix of retail and entertainment uses;
- The development forms an acceptable example of gateway architecture;
- The approach to the public realm continues the design concept of DP2016-4850 - Cell I by incorporating features significant to the Blackfoot culture and First Nations site history in alignment with planning policy.

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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

LOCATION MAPS



DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommends that Calgary Planning Commission **APPROVE** the application with conditions (APPENDIX II).

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

Applicant:

B&A Planning Group

Address

2200 Na'a Drive SW,
8395 Canada Olympic Drive SW

Landowner:

Plateau Village Properties Inc.

Legal

Plan 1612946, Block 1, Lot 2;
Plan 7910494, Block B

PLANNING EVALUATION

INTRODUCTION

This is a development permit for a mixed-use retail and entertainment district as part of the Medicine Hill development, in north-west Calgary.

The new community of Medicine Hill will comprise approximately 40 hectares (100 acres) of which 34 hectares will be developed land. The developer has chosen to break up the development into three unique precincts: The Town Centre, The Village and The Gateway. This application is located in the Gateway precinct.

Submitted in June 2017, this is the second development permit for the Medicine Hill community (with DP2016-4580, located in the Town Centre, approved by Calgary Planning Commission on 2017 November 30).



**DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE, RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT: LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4 BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

SITE CONTEXT

The subject site forms Cell B in Trinity’s approved Outline Plan (APPENDIX V), for a mixed-use development located at the base of the Paskapoo slopes. The site lies adjacent to the Trans-Canada Highway (to the north), with the community of Bowness further north. A future multi-residential development cell lies to the south, along with the Paskapoo slopes a future city park (approximately 66 hectares (165 acres)) further south. To the west is a future retail development cell within Canada Olympic Park. Immediately east lies steeply sloping land (designated Environmental Reserve) with future multi-residential cells further east.

Administration highlights the following relevant planning history:

| Date | Approval Details |
|------------------|--|
| 2015 July 23 | Council approved amendments to the Canada Olympic Park and Adjacent Lands Area Structure Plan and 5 Direct Control Districts as part of application LOC2014-0080. Calgary Planning Commission approves the associated Outline Plan |
| 2016 March 07 | Council approved the name of Medicine Hill for the new community at the base of the Paskapoo Slopes |
| 2016 July 28 | The Medicine Hill Concept Plan is accepted for information by Calgary Planning Commission |
| 2017 May 18 | The pre-application package for this site is presented to Calgary Planning Commission as an information item |
| 2017 June 26 | Administration met the Blackfoot Traditional Knowledge Keepers at Head Smashed In Buffalo Jump |
| 2017 August 30 | Administration and Trinity team met the Blackfoot Traditional Knowledge Keepers at Head Smashed In Buffalo Jump |
| 2017 November 06 | Council accepts changes to 3 of the 5 Direct Control Districts which allow for additional uses on the Gateway and Town Centre sites to respond to market demand. |
| 2017 November 30 | Calgary Planning Commission approved DP2016-4580 - cell I |

LAND USE DISTRICTS

The subject site is designated a Direct Control District. This DC is based on the Commercial Corridor 1 (C-COR1) District and contemplates a retail focused development with opportunity for dwelling units and offices on upper floors. The DC allows for tall buildings, with one up to 75 metres, two at 50 metres, and all other buildings being a maximum of 21 metres in height. The DC provides discretion for the Development Authority to relax land use rules subject to the relaxation clause in the Land Use Bylaw.

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

SITE CHARACTERISTICS

The subject site is visually prominent and located at the entranceway to the City along the Trans-Canada highway.

The parcel does not contain significant vegetation or plantings and has been stripped and graded in accordance with planning policy.

The Paskapoo Slopes - future City park south of the subject site comprises of complex topography and sloping lands with significant archeological history with the Paskapoo Slopes known to contain significant First Nations artefacts of Blackfoot origin. The site is a former Buffalo Kill site of Provincial significance, one of the largest in the Calgary area.

LEGISLATION & POLICY

Administration considered the proposal against relevant planning policies listed below and found the proposal to be acceptable.

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Canada Olympic Park and Adjacent Lands Area Structure Plan (ASP) (statutory)

The subject site (along with Cell A which does not form part of this application) falls within the Gateway District of the ASP – an area intended to provide a commercial street, focused on retail and entertainment as well as cafes, restaurants and bars with the possibility of office and hotel uses.

The proposal aligns with the purpose, intent and relevant planning policies of the ASP. The development permit provides a mix of retail uses in each building, and along the commercial main street anchor tenants are supported by smaller retail bays, with further variety provided by entertainment uses (such as the fitness centre), cafes and restaurants.

Through negotiation with the Applicant, First Nations images have been incorporated into the public realm (such as on walkways) and ancillary structures (such as retaining walls). This design response continued the urban design approach of Cell I (in DP2016-4580), to respect the First Nations history and Blackfoot culture (this is further explored below – Site Layout and Building Design).

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

Improving Calgary's Entranceways: A Guide for Development Adjacent to Entranceways (non-statutory)

Due to its topography and visibility the proposed development will form a prominent feature from the Trans-Canada Highway - an identified gateway in the above planning policy. This development permit fulfills expectations of the above planning policy and will create an acceptable example of gateway architecture on this important site. This is achieved through a combination of the following:

- The massing of buildings: Provide variety in stepping of height (with parts of buildings stepped down where others are stepped up) and setbacks (with a combination of recesses and projections);
- The materials along the Trans-Canada (elevation): Pay reference to and seek to harmonise with the landscape – in the banding of metal panels which pay reference to the striations in rock formations laid down over time;
- Signage: Is appropriately scaled, and sensitively incorporated. The number of signs on the Trans Canada façade is limited, and serves to provide visual breaks in the highway elevation with most signage occurring at the building corners.

Large Retail/ Commercial Urban Design Guidelines (non-statutory)

This development permit complies with non-statutory planning policy contained in the Large Retail Commercial Urban Design Guidelines, in particular by:

- achieving a sensitive interface to the Trans-Canada Highway and adjacent future development sites (south and west);
- orientating all buildings to front the internal commercial main street;
- integrating anchor stores (such as building B4) with smaller retail units;
- activating edges at the entrance to the development (for example patios to restaurants east of building B2 (as shown on the Level 1 Overall Plan (DP-A200));
- locating loading, waste and recycling away from primary retail streets.

Land Use Bylaw (and Direct Control District) (339D2017)

Administration highlights the following relaxations to the Land Use Bylaw. Administration has reviewed each relaxation individually and considers each relaxation to be acceptable for the reasons outlined in the table (below).

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 BUILDINGS)
 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W

| Land Use Bylaw Relaxations | | |
|---|--|---|
| Regulation | Standard | Provided |
| 782 Building Location and Orientation | (2) The max. building setback from a property line shared with a commercial street is 3.0 m. | <p>Plans indicate all of the proposed buildings on site are setback more than 3.0m from the commercial streets to the South and West of the site.</p> <p>All buildings front the internal street (Na'a Common) rather than Na'a Dr or Canada Olympic Drive. The intent of the ASP Policy was to create a main street internal to the development which this design achieves.</p> <p>Administration is satisfied that the placement of buildings positively contributes to the commercial street concept and does not detract from the surrounding landscape</p> |
| 693 Additional Landscaping Requirements | (7) Every sidewalk provided must: (b) be a minimum width of 2.0 metres; | <p>Plans indicate 4 proposed sidewalks have a width less than 2.0m.</p> <p>Administration is satisfied that the sidewalks provide ample pedestrian walking width</p> |
| 790 Landscaping In Setback Areas | (4) Where a setback area shares a property line with a parcel designated as a commercial, industrial or special purpose district, the setback area: (a) be a soft surfaced landscaped area; Indicate each Setback area | <p>Plans indicate the drive aisles extend within the East setback area.</p> <p>Administration is satisfied this encroachment will not detract from the use, enjoyment or value of surrounding dwellings nor visual amenity and will be satisfactorily screened through retaining walls incorporating public art and landscaping</p> |
| 247 Outdoor Café | (f) has a maximum area of 25.0 square metres in the C-N1, C-N2, C-C1, C-C2, C-COR1, C-COR2, C-O, C-R1, I-B, CC-MHX, CC-X, CC-COR, CC-ER, CC-ERR, CC-EMU, CC-EIR, CC-EPR, CC-ET, MU-1, MU-2, M-H1, M-H2, M-H3, M-X1 and M-X2 districts. | <p>Plans indicate the size of all the outdoor cafes on site exceeds 25.0m².</p> <p>Administration is supportive of an increase in the size of outdoor patio areas which contributes to good urban design, a vibrant street and outdoor space and good place making</p> |

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

SITE LAYOUT & BUILDING DESIGN

This development permit proposes approximately 14,857 square metres of retail development in 4 buildings fronting onto a commercial main street (Na'a Common SW). The Applicants note the inspiration for this development comes from a geode. To the north of the site buildings B4 and B1 exhibit gateway (or highway) architecture (with a unique building form/design/materials contributing to a memorable structure to the passer by and visitor of the site). The northern elevations of the site seek to harmonise with the Paskapoo Slopes in the arrangement and colours of metal panels taking inspiration from sedimentary rock striations, with rock layers formed over time. These coloured metal panels are intended to represent the geode shell.

The commercial main street which all buildings front, is like the jewel in the geode with variety in materials and variety in the design of building facades. This variety allows the front elevation of buildings to appear like a series of small shop 'fronts' with angled at grade parking to the front of retail units contributing to an individual feel - similar to a small town retail street in the larger overall Medicine Hill retail destination. Removing the loading entrance for waste and recycling from the centre of the retail main street (as originally envisaged as part of the pre-app for this site) was a key design outcome and allows the commercial main street to comprise retail frontage.

The development permit includes a private bridge which will provide a pedestrian and vehicle link between development Cells B and A located above Canada Olympic Drive SW. The bridge is approximately 42 metres in length, and 18 metres in width. Sidewalks will be located on both sides (approximately 4 metres in width) with planters for vegetation located in the sidewalks on top of the bridge. The proposed bridge will be visible from the Trans-Canada Highway and from Bowfort Road interchange. The Applicants note the design of the bridge is to include First Nations images (such as pictographs of teepees and mountains) and there is a Prior to Release condition which requires the Applicant work with the Blackfoot Traditional Knowledge Keepers to ensure the final design of the bridge pays appropriate tribute to the First Nations site history.

Public Realm and Screening Enhancements – First Nations Images

In line with statutory planning policy in the Canada Olympic Park and Adjacent Lands ASP First Nations images are incorporated into the public realm in the hard standing at corners and building entrances (such as the Wolf Plaza south west of building B1 and the Foothills Plaza west of building B1). First Nations images are also incorporated in retaining walls (such as to the east of building B1, north of buildings B4 and B1 and west of building B4). Buffalo are a predominant image in the public realm and on retaining walls in an effort to pay tribute to the previous historic Blackfoot use of the site. First Nations images on this cell were developed in collaboration with and in response to feedback and support received from the Blackfoot Traditional Knowledge Keepers.

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

Public Art

As noted on drawing sheet L4 the Applicant identified a location for the installation of a sculptural art piece to the west of building B1. The sculpture will be inspired by the First Nations site history and there is a prior to release condition to ensure that the art piece is reviewed and supported by the Blackfoot Traditional Knowledge Keepers. The public art location will be highly visible when entering this development cell, is considered acceptable to Administration and exceeds the planning policy requirements in the ASP for this development cell.

Citywide Urban Design

The design was reviewed by the City Wide Urban Design team throughout the CPAG review process. A number of revisions were requested with regard to enhancing the street/edge interface of retail buildings with Trans-Canada highway, edge and street interface with the surrounding landscape and with the internal retail street, architecture, materiality, colour, place making, and an integrated approach of symbols and acknowledgement of the sites indigenous history. The Applicant's rationale and the resulting additional revisions were deemed appropriate and sufficient.

Urban Design Review Panel

This application was presented to the Urban Design Review Panel (UDRP) on 2017 June 28, who supported the application with comments. Key comments from UDRP included taking maximum advantage of the gateway interface to the Trans-Canada highway and the opportunity this creates for place making, in particular the entrance into the development from Bowfort Road interchange, as well as opportunities to naturalise the retaining walls to integrate the architecture with the landscape. UDRP comments are contained in their entirety in APPENDIX IV together with the Applicant's response. The Applicant's rationale and the resulting additional revisions were deemed appropriate and sufficient.

CPTED - CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN

This development permit was designed in line with CPTED principles (of natural access control, natural surveillance, territorial reinforcement and maintenance). The applicant submitted a CPTED report with this file which was reviewed by the Calgary Police Service with no concerns raised by them nor Administration.

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

LANDSCAPING

The Applicant developed a cohesive response to landscaping as part of this application. Trees and shrubs are carefully positioned along the perimeter of the site to add screening from the Trans-Canada Highway (such as to the north of building B4 and east of building B1). Planting also provides additional screening south of buildings B3 and B2 from Na'a Drive SW. At the request of Administration and based on liaison with the Blackfoot Traditional Knowledge Keepers, and specialist advice from Calgary Neighbourhoods, trees were grouped at the south west corner of building B3 to emulate lush vegetation found at the Buffalo kill site of Head-Smashed-In Buffalo Jump. Grouping this vegetation will provide an enhancement to this corridor, to be enjoyed when entering and exiting the development along Na'a Drive in a vehicle and walking on the footpath.

A total of 35 trees and 116 shrubs are provided incorporating a variety of native species in alignment with feedback provided by the Traditional Knowledge Keepers.

SITE ACCESS & TRAFFIC

Vehicular Access

The Medicine Hill community is accessed via Bowfort Road interchange off the Trans-Canada Highway and also from Sarcee Trail. Due to the proximity of this cell to Bowfort Road interchange Administration is of the opinion this will be the primary vehicular access utilizing Canada Olympic Drive, Na'a Drive SW and into the development cell.

Pedestrian Access

The proposal will provide sufficient pedestrian access within Cell B and between this cell and the adjacent Trinity lands. South of the subject site four crossings will be created to provide pedestrian access across Na'a Drive SW and Canada Olympic Drive SW to future multi-family development in Cell C and future retail development in Cell A. Within Cell B four elevators and stairwells provide pedestrian access from the parkade to grade (with one elevator located in each of the four buildings). A stairwell is provided south west of building B4 to provide access to the pedestrian sidewalk adjacent to Canada Olympic Drive SW.

PARKING

Fifty-five angled parking stalls are provided at grade along the commercial main street with 425 parking stalls provided below grade in underground parking stalls. The underground parkade is accessed at the north eastern extent of the site (between the side elevation of building B4 and the front elevation of building B1), a secondary exit to the parkade is provided to the rear of building B2 and a third exit is provided in the form of a slip road to the rear of building B4 which provides direct vehicular access to the Trans-Canada Highway.

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NA'A DRIVE SW**

MAP 27W

The provision of the majority of parking below grade exceeds the requirement of statutory planning policy in the ASP which requires 60 percent structured or underground parking on cells B and A in the gateway.

The proposal provides 66 class 1 bicycle parking stalls and 9 class 2 bicycle parking stalls in line with requirement of the Land Use Bylaw.

A Transportation Impact Assessment and Parking Study was submitted in support of the development permit and was reviewed by CPAG Transportation and considered to be acceptable.

UTILITIES & SERVICING

The developer installed water, sanitary and storm sewer connections at Na'a Drive SW in 2017 to serve this development cell.

The Storm Water Management Report has been submitted to Water Resources and is under review and is a prior to release condition of this development permit.

ENVIRONMENTAL ISSUES

There are no significant environmental issues which this development permit.

ENVIRONMENTAL SUSTAINABILITY

The Applicants note a variety of sustainable building techniques will be used in this file including:

- Building Materials: such as the use of low volatile organic compound building materials and local materials (for example rundlestone)
- Energy Efficiency: High energy efficiency heating and cool roof technology minimize the heat island affect; and building envelopes designed for optimum energy performance to meet NECB requirements.
- Wildlife/Environmental Sensitivity: Bird friendly design by orienting glazing internally; locating garbage enclosures within building to minimize conflict with wildlife;

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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

PUBLIC ENGAGEMENT

Community Association Comments

This development permit was circulated to:

- Bowness Community Association, Coach Hill Patterson Heights Community Association and West Springs/ Cougar Ridge Community Association in July 2017. No comments were received at the time of writing this CPC report.
- Rocky View County who returned correspondence with no objection.

Citizen Comments

No Citizen comments were received by the CPC report submission date.

Public Meetings

There were no public meetings required for this development permit.

First Nations Liaison

As noted above the Paskapoo Slopes is known to contain a number of archaeological sites of Blackfoot origin. Due to this, with the assistance of Calgary Neighbourhoods, Planning and Urban Design staff were fortunate to have the opportunity to engage three Blackfoot Traditional Knowledge Keepers at Head-Smashed-In Buffalo Jump on June 26, 2017 and August 30, 2017. The first meeting was an opportunity for Administration to learn and understand the importance of the subject site to the First Nations and Blackfoot Culture and begin discussions to determine how to integrate this knowledge in this development permit. The second meeting allowed the Applicant team to present the Applicants approach to incorporate First Nations images in the public realm with Administration in attendance. Following meetings with the Blackfoot Traditional Knowledge Keepers in subsequent negotiations with the Applicant team Administration is satisfied that the plans presented as part of this file align with Planning Policy expectations for incorporation of First Nations images on this site.

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NA'A DRIVE SW

MAP 27W

APPENDIX I

DEVELOPMENT PERMIT PLANS



**DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE, RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT: LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4 BUILDINGS)
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 NA'A DRIVE SW**

MAP 27W

1. LOOKING SOUTH FROM NA'A DRIVE SW - SUMMER DAY VIEW

2. LOOKING NORTH FROM NA'A DRIVE SW - WINTER NIGHT VIEW

3. VIEW FROM NA'A COMMON SW LOOKING SOUTH EAST TOWARDS PROPOSED BUILDING #1 - SUMMER DAY VIEW

4. VIEW FROM NA'A COMMON SW LOOKING SOUTH EAST TOWARDS PROPOSED BUILDING #1 - WINTER NIGHT VIEW

TRINITY HILLS
 Site B, Trinity Hills
 Mixed-Use Development
 1515000
 11:500
 DP-A001
 2018-11-02 Pn

B+H
 B+H Architecture
 1515000
 11:500

TRINITY HILLS
 1515000
 11:500

TRINITY HILLS
 1515000
 11:500

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NA'A DRIVE SW**

MAP 27W



DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE,
RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT:
LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



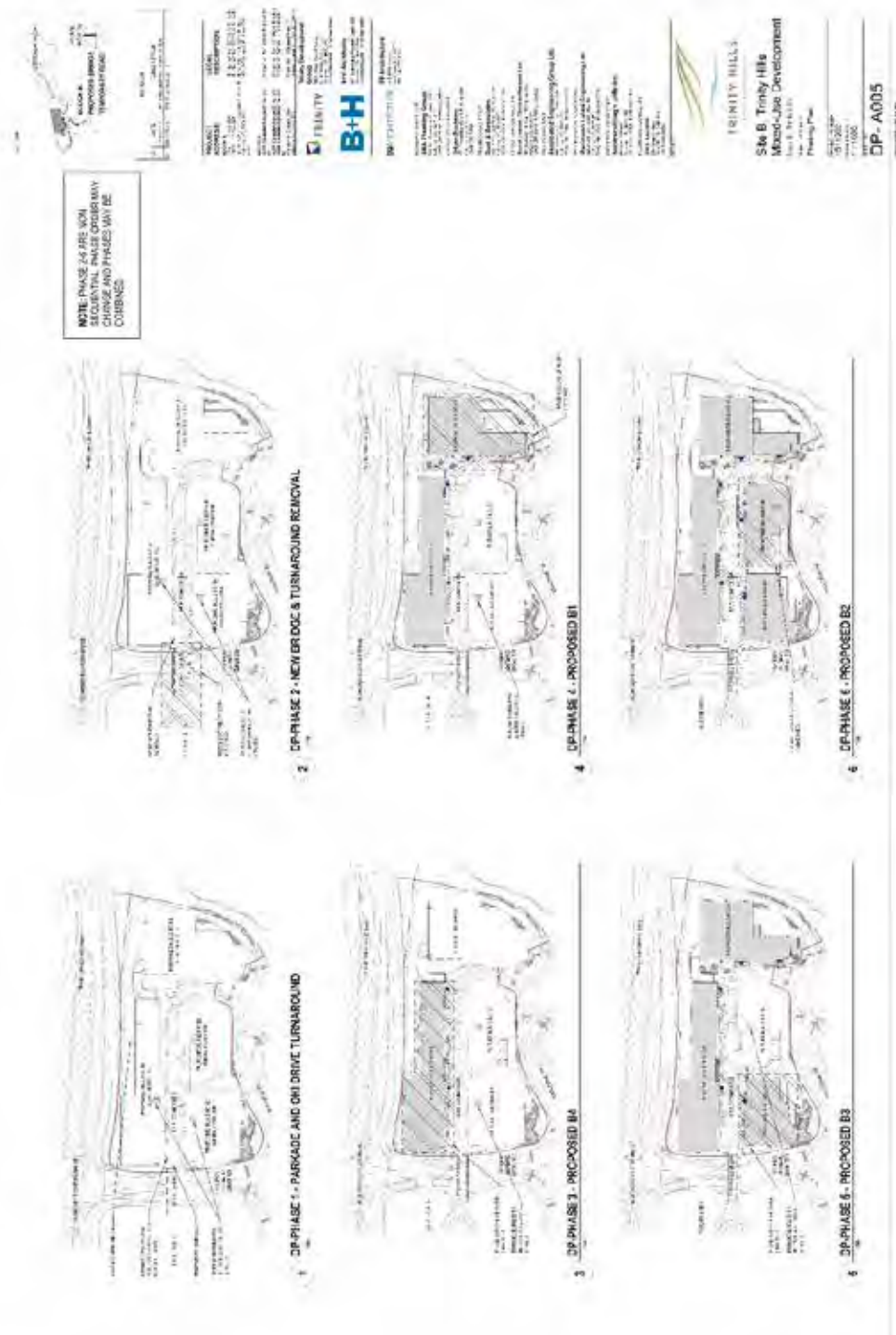
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MAP 27W



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NA'A DRIVE SW

MAP 27W



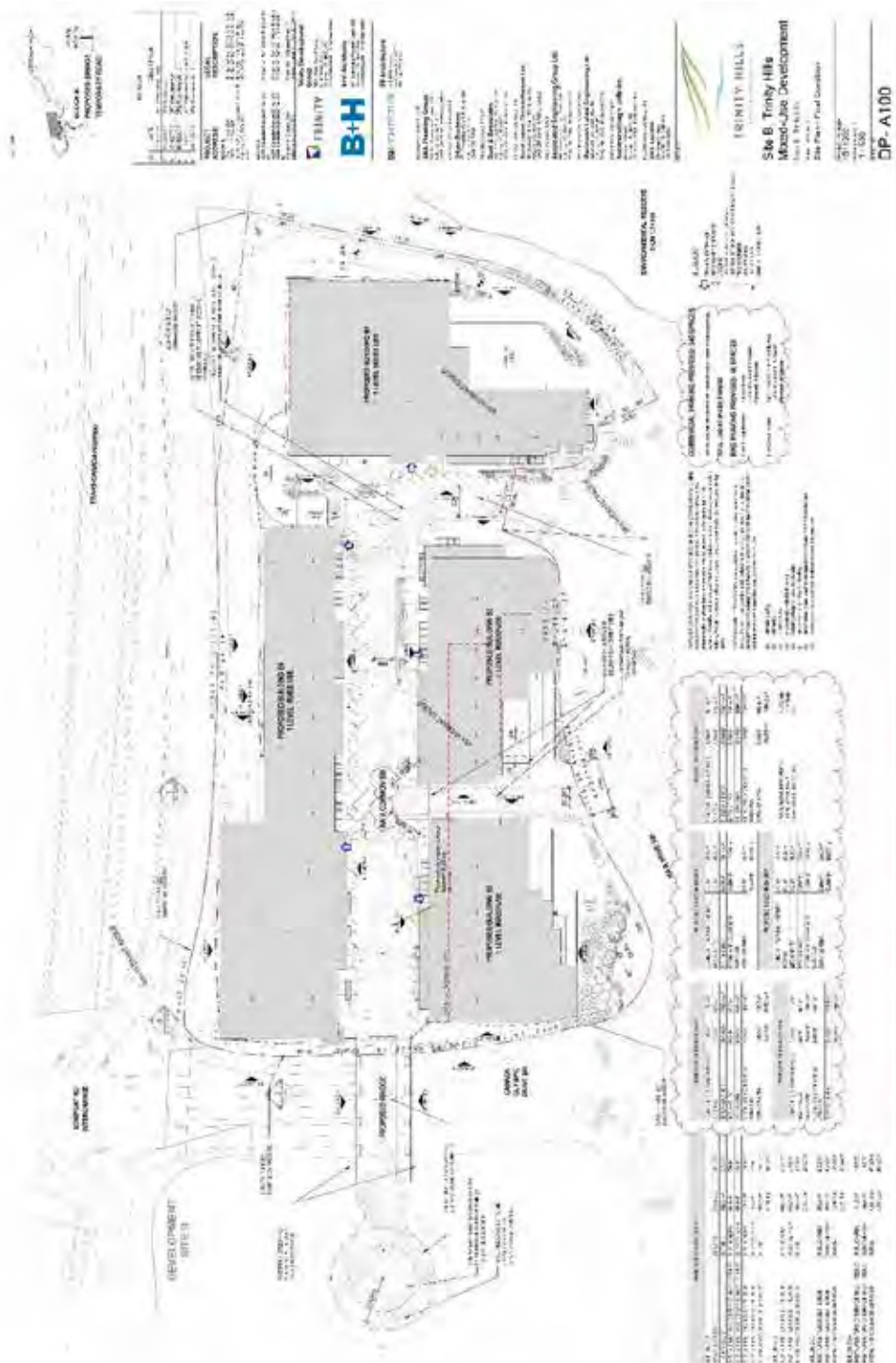
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NA'A DRIVE SW

MAP 27W



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 BUILDINGS)
 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W



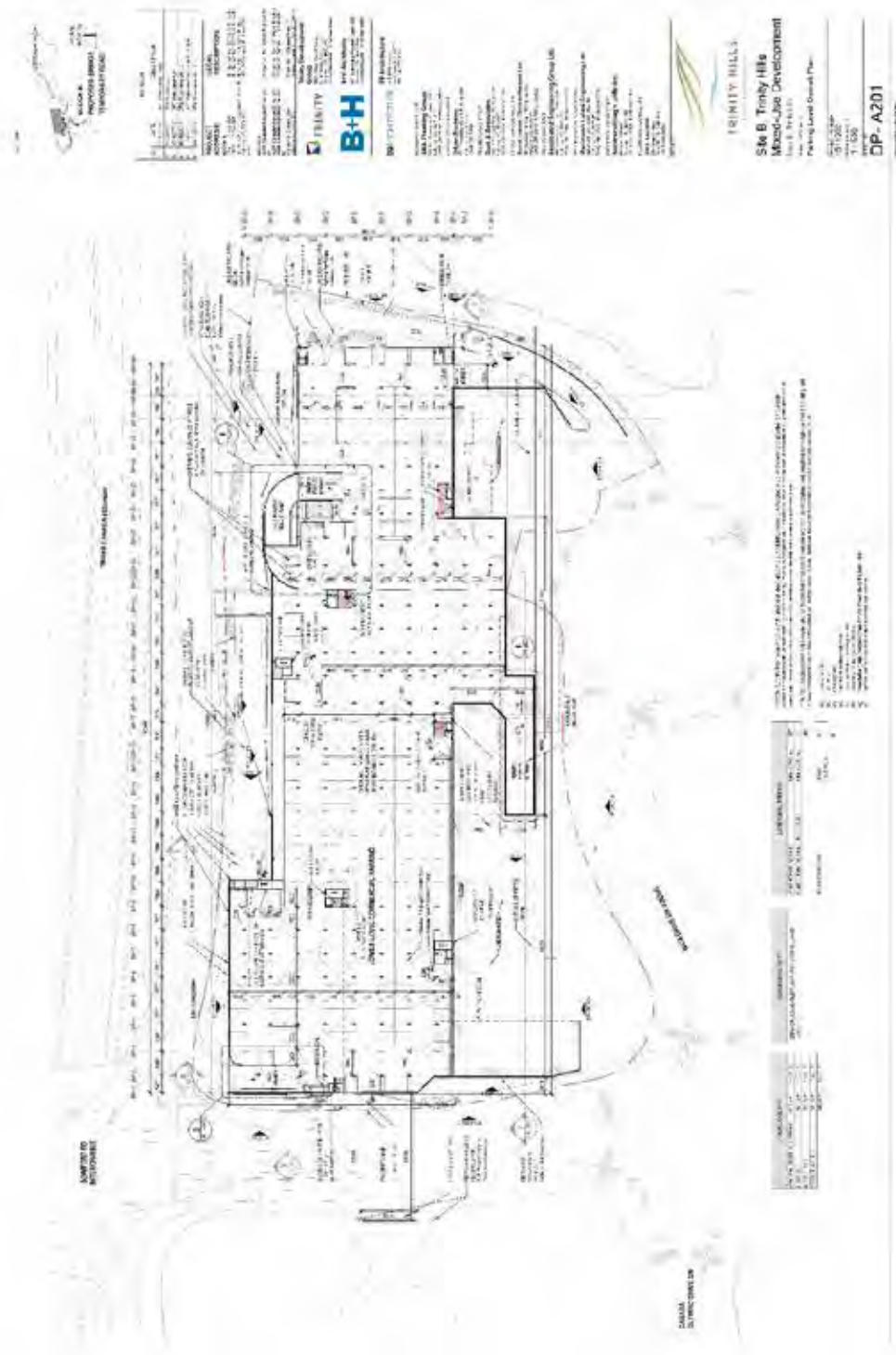
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LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



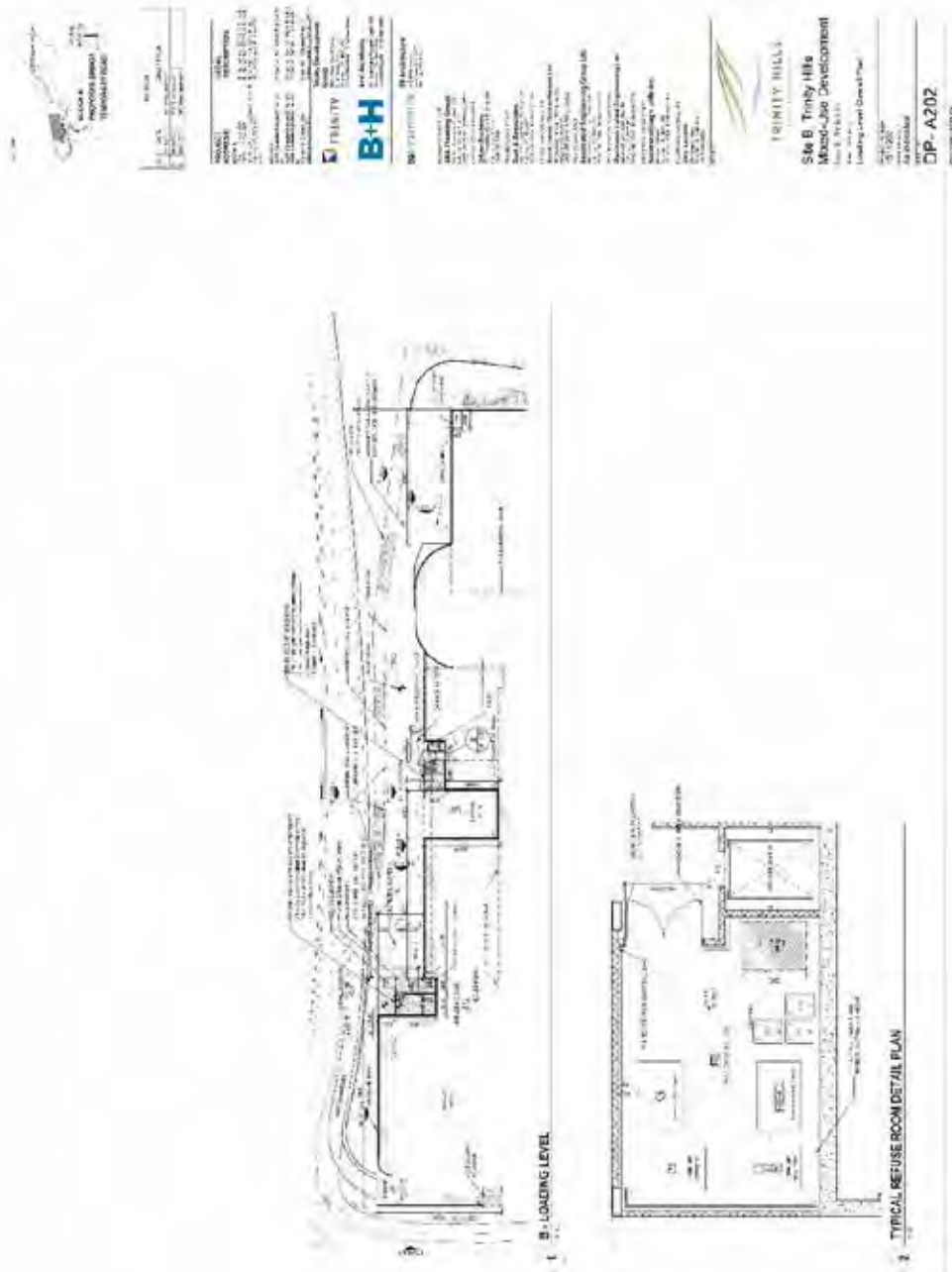
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LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



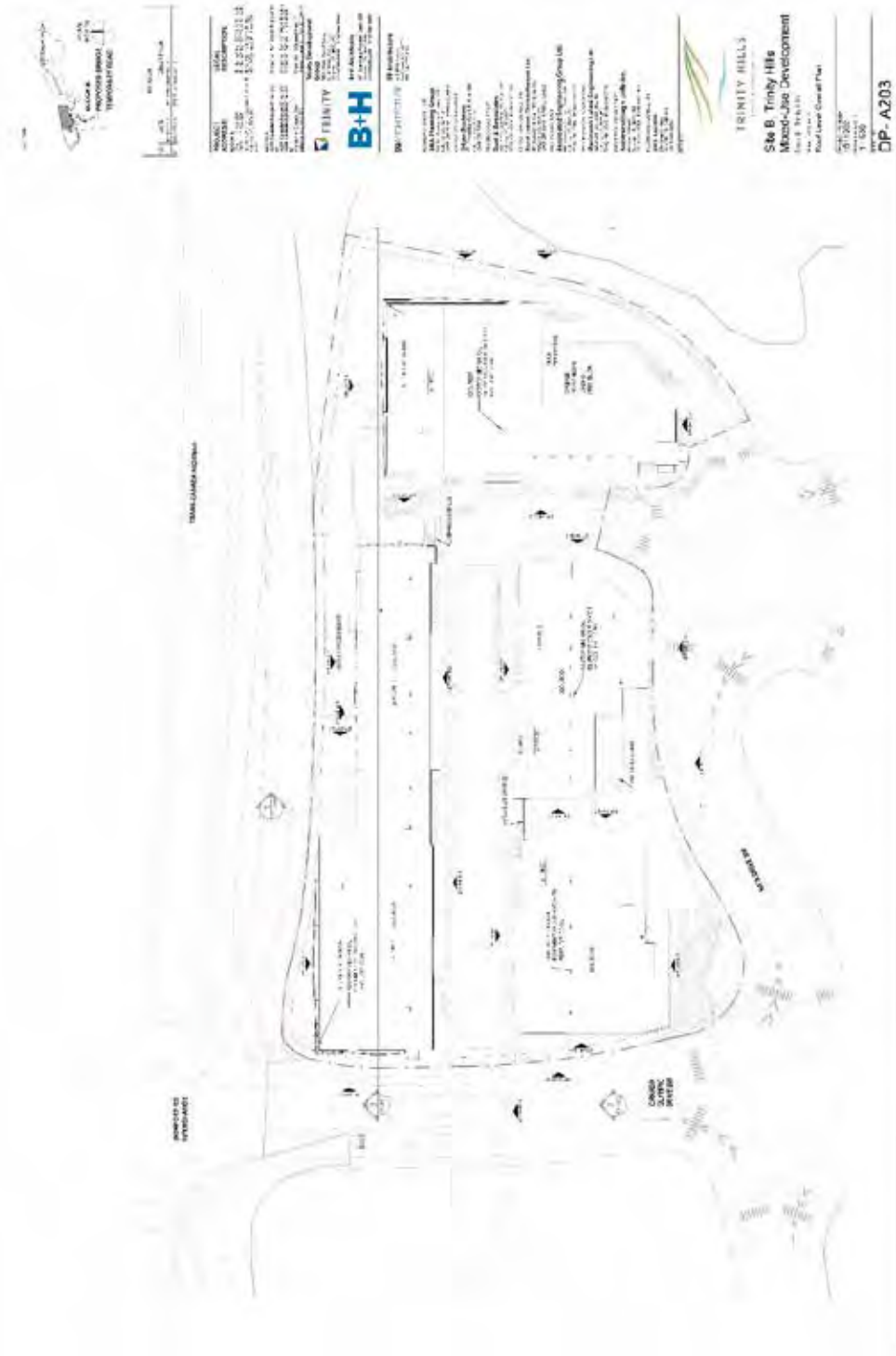
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



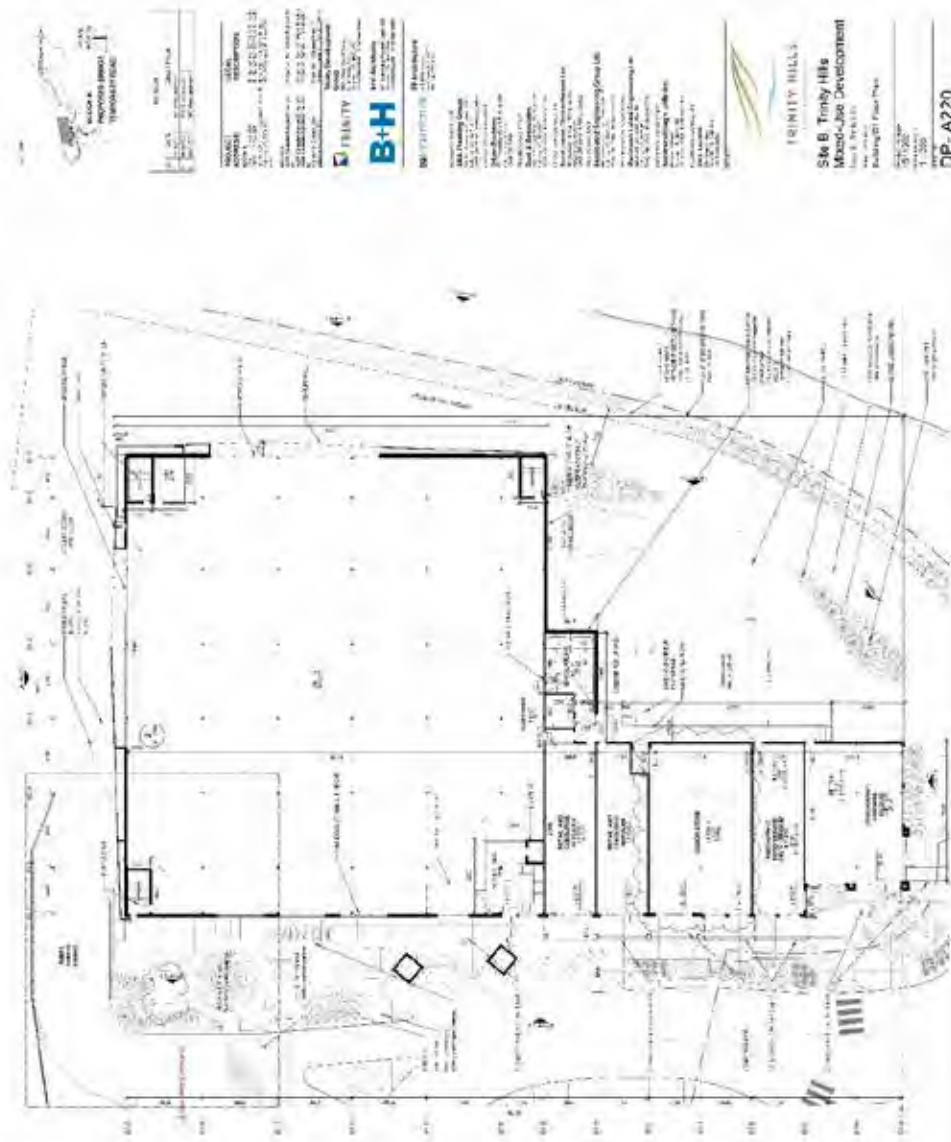
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



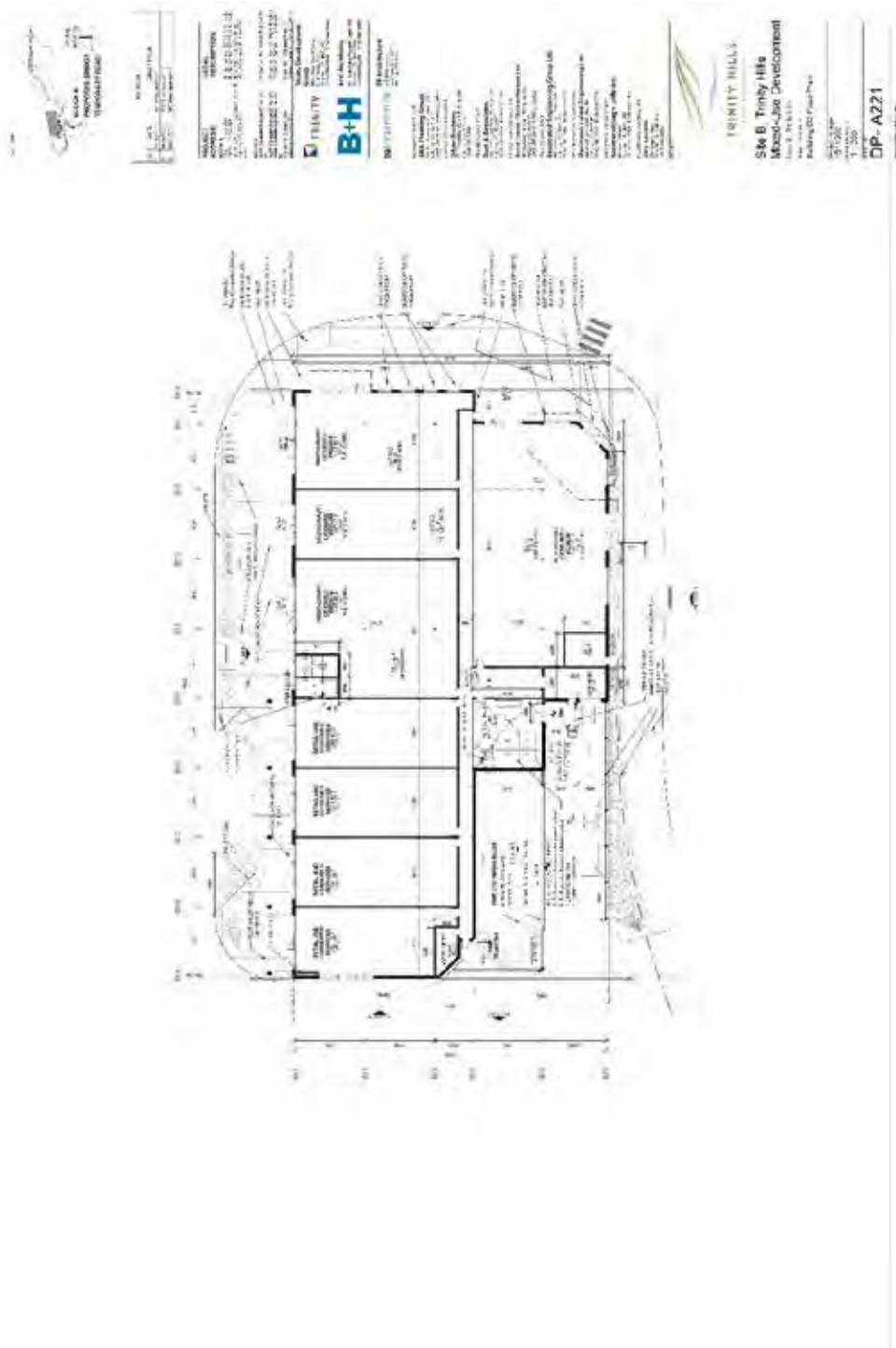
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



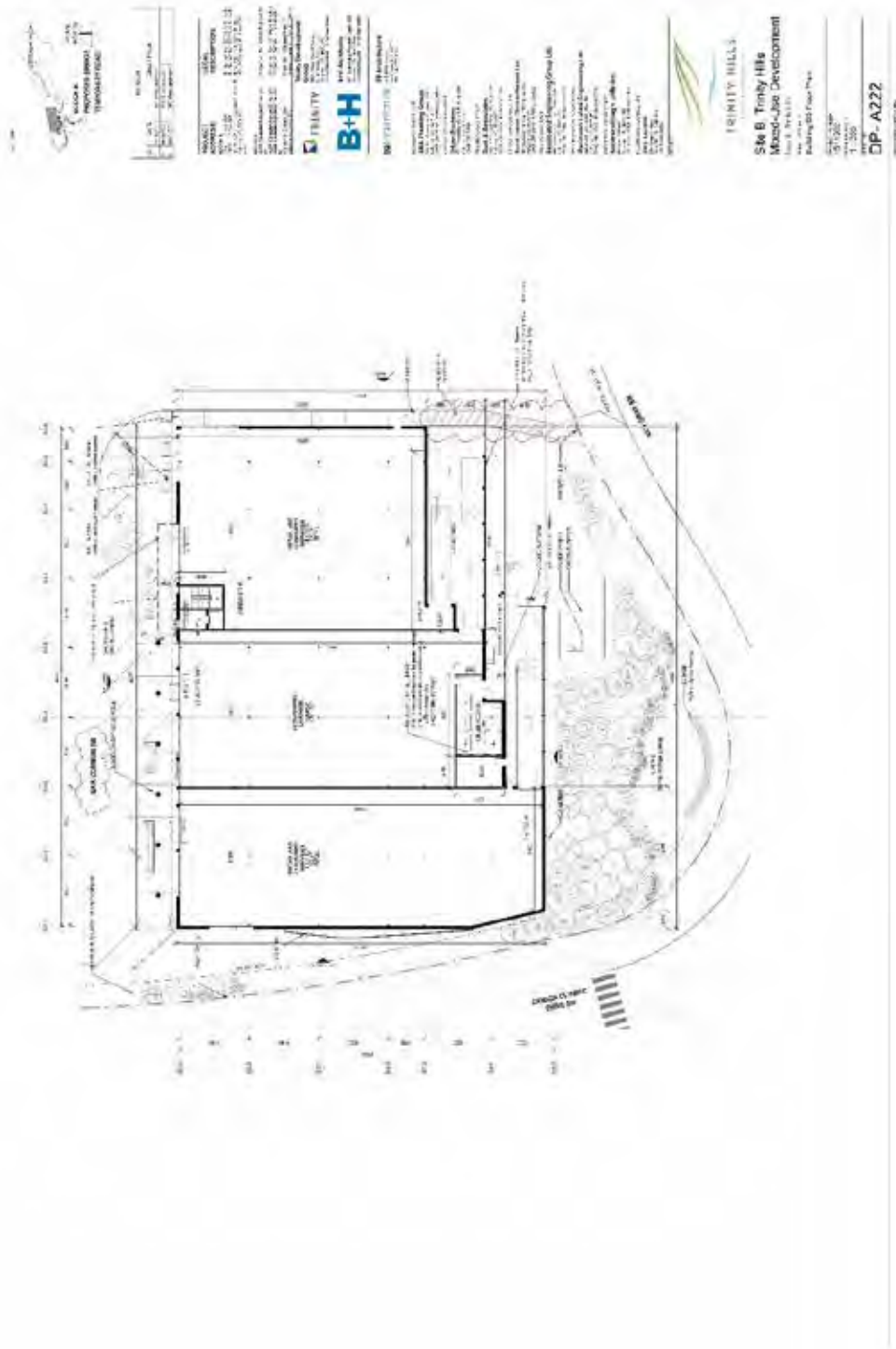
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NA'A DRIVE SW

MAP 27W



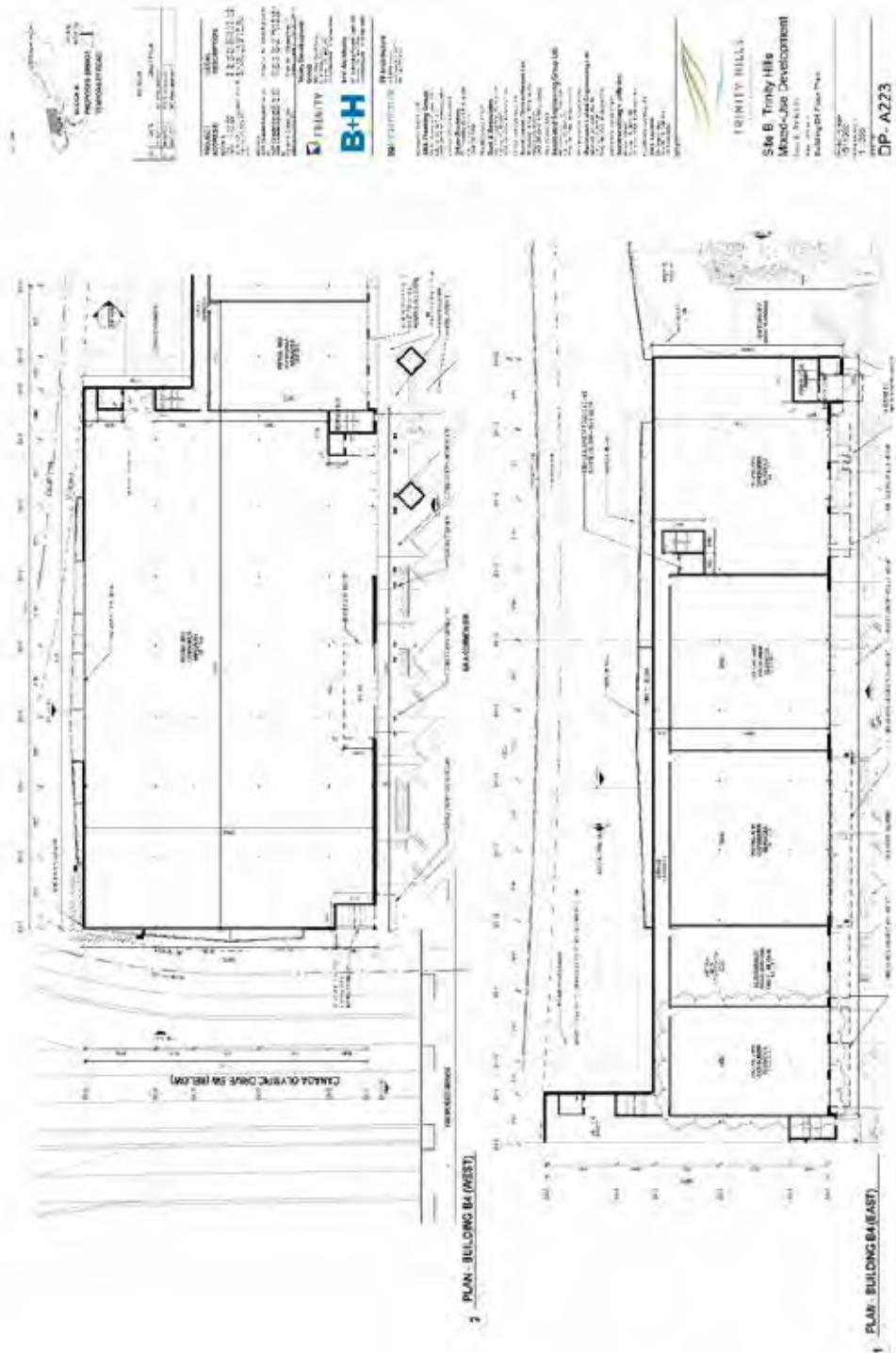
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NA'A DRIVE SW

MAP 27W



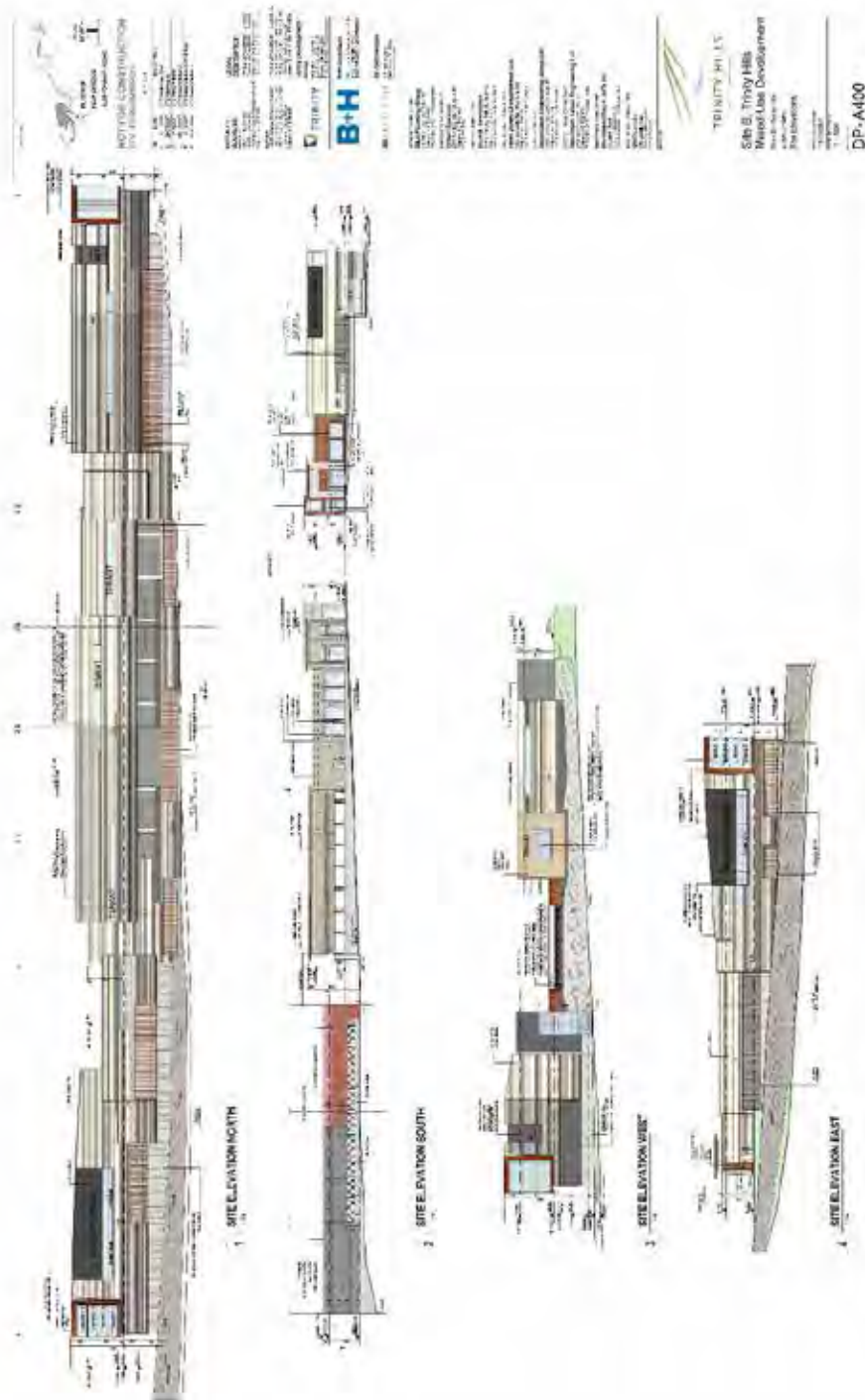
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



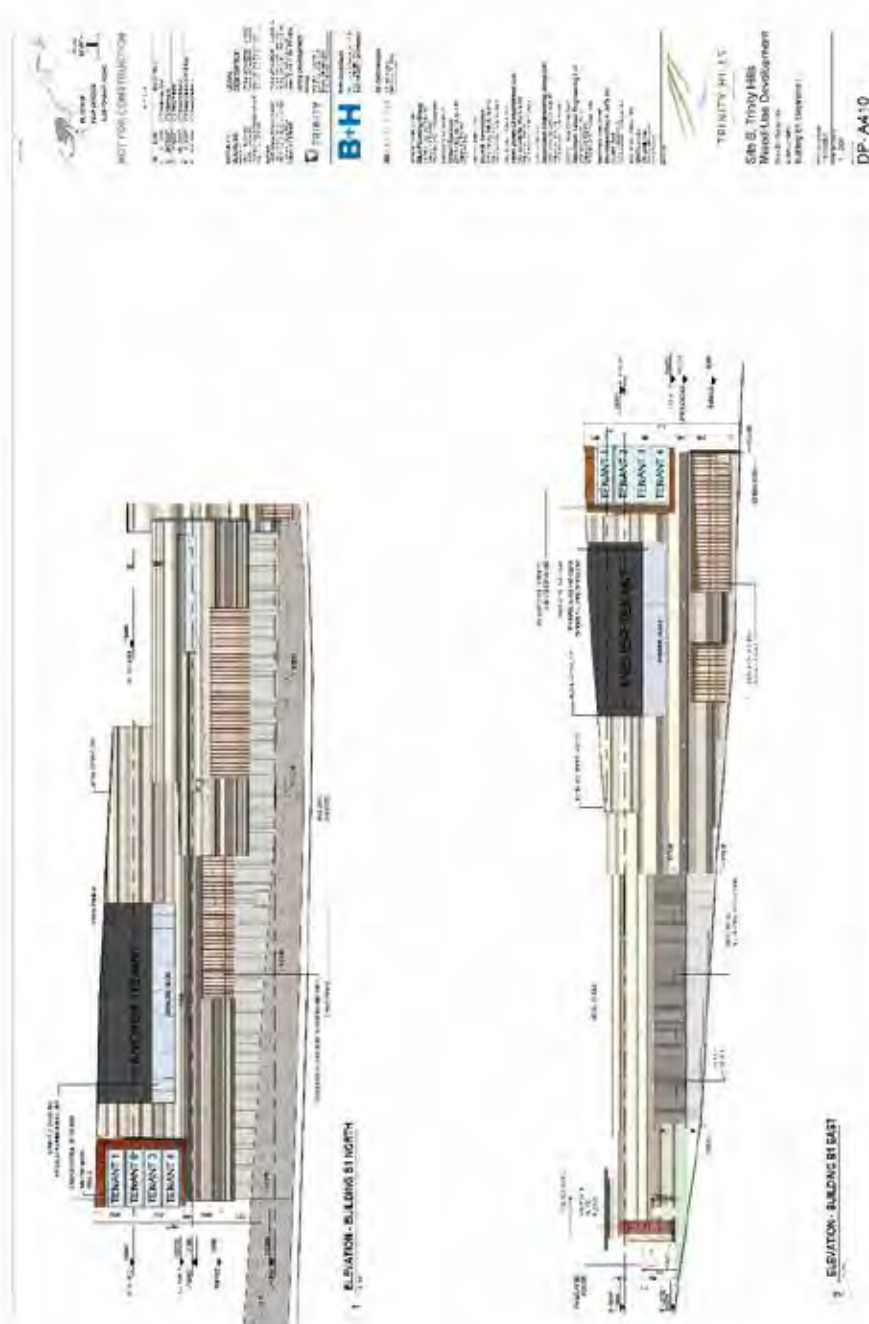
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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



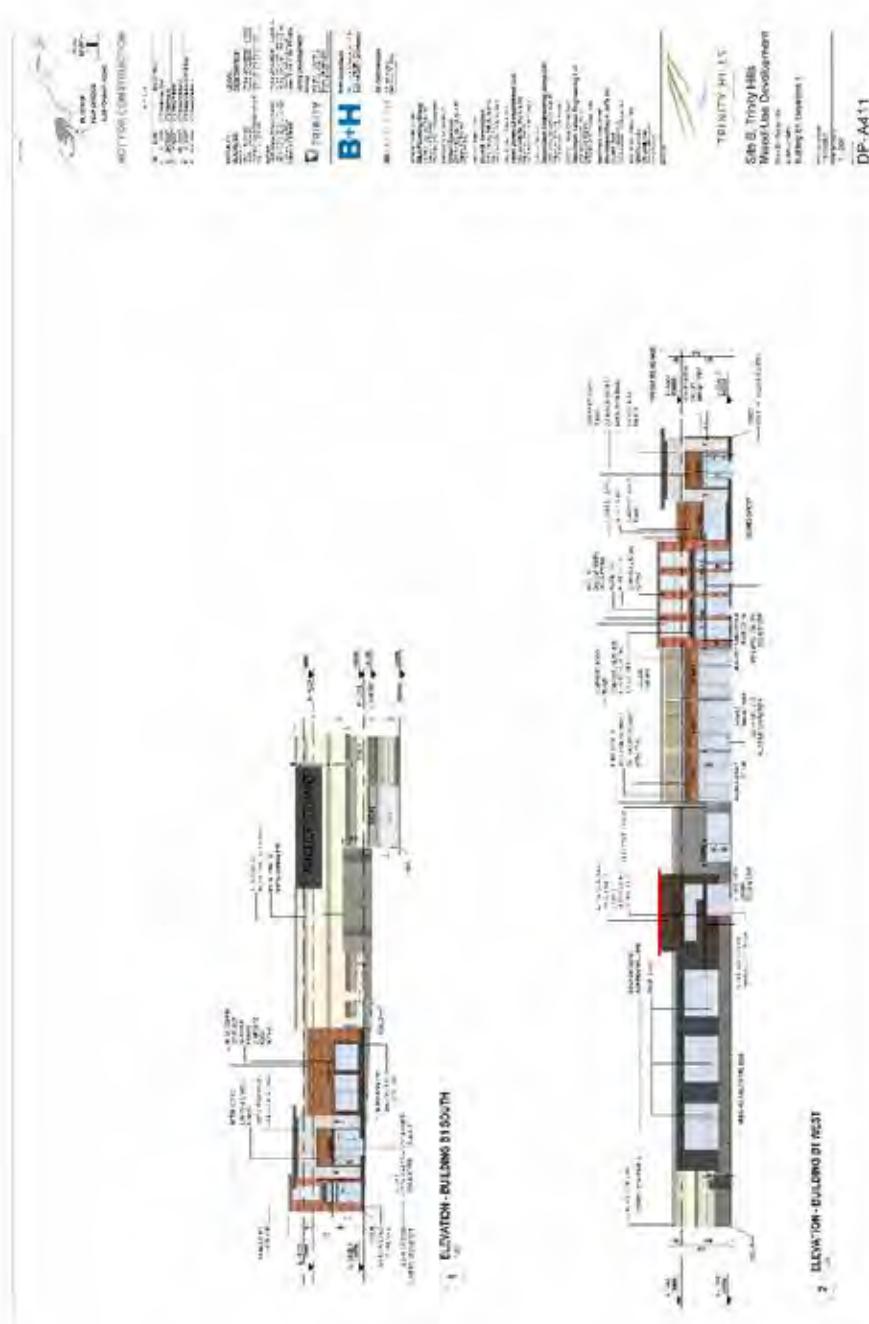
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NA'A DRIVE SW

MAP 27W



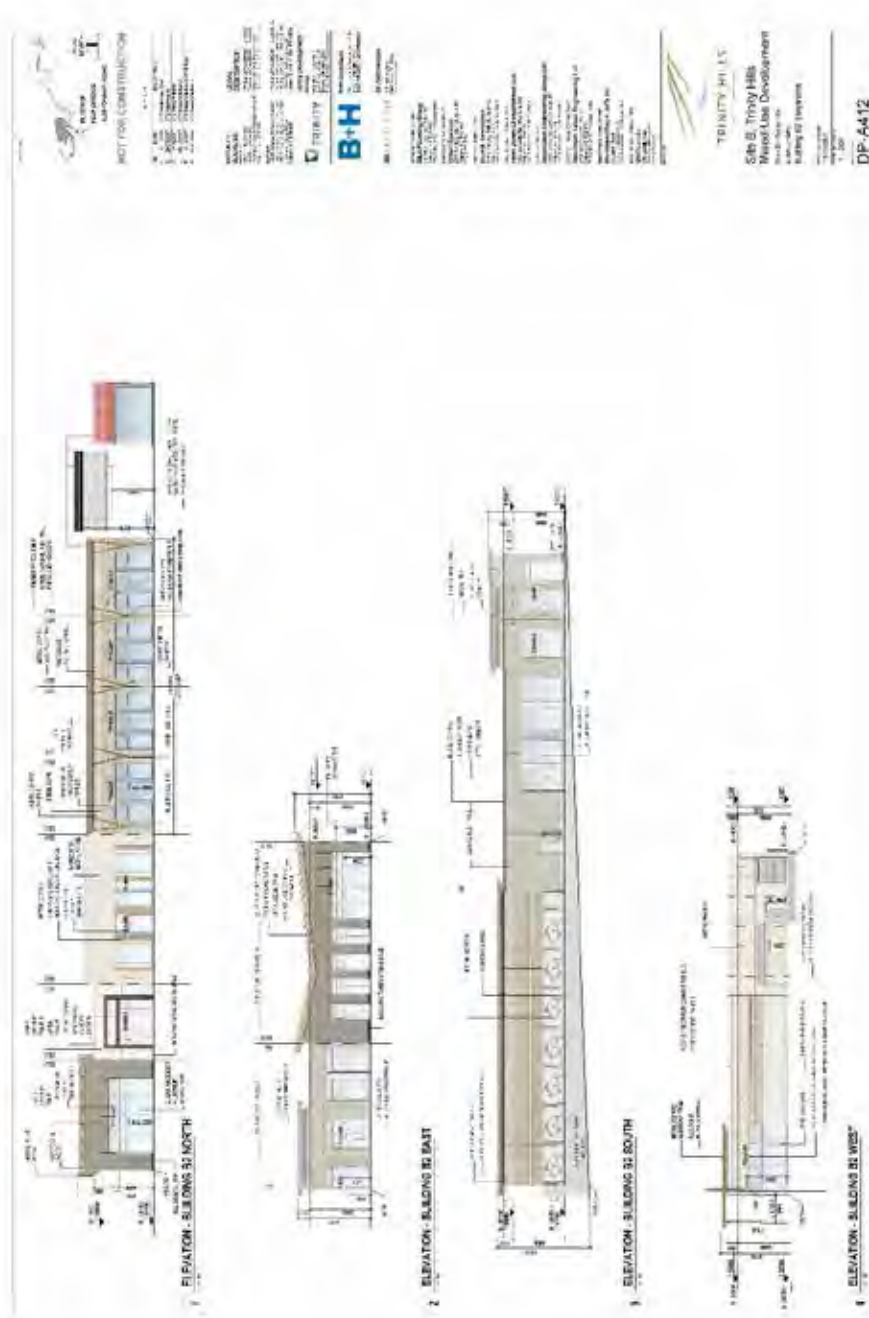
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NA'A DRIVE SW

MAP 27W



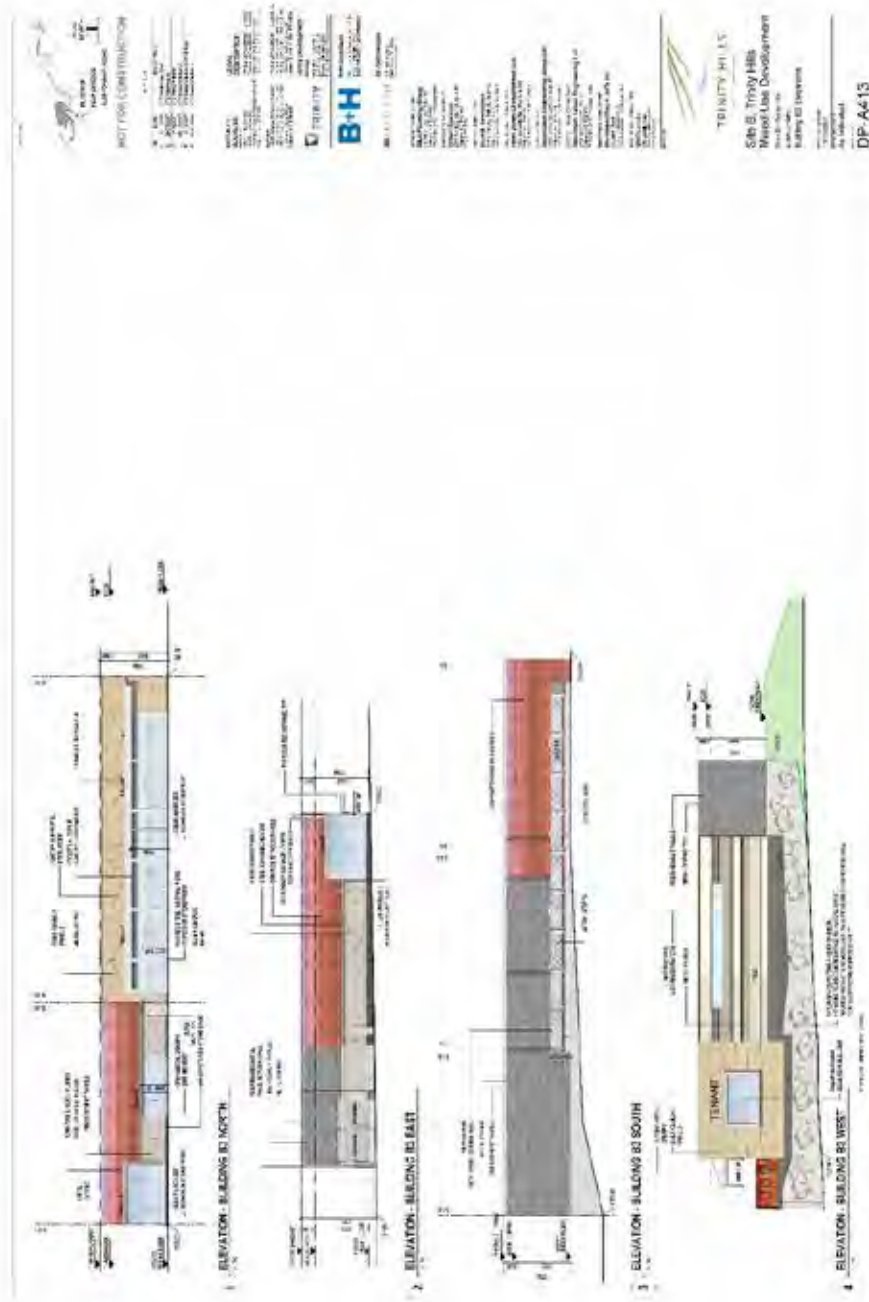
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NA'A DRIVE SW

MAP 27W



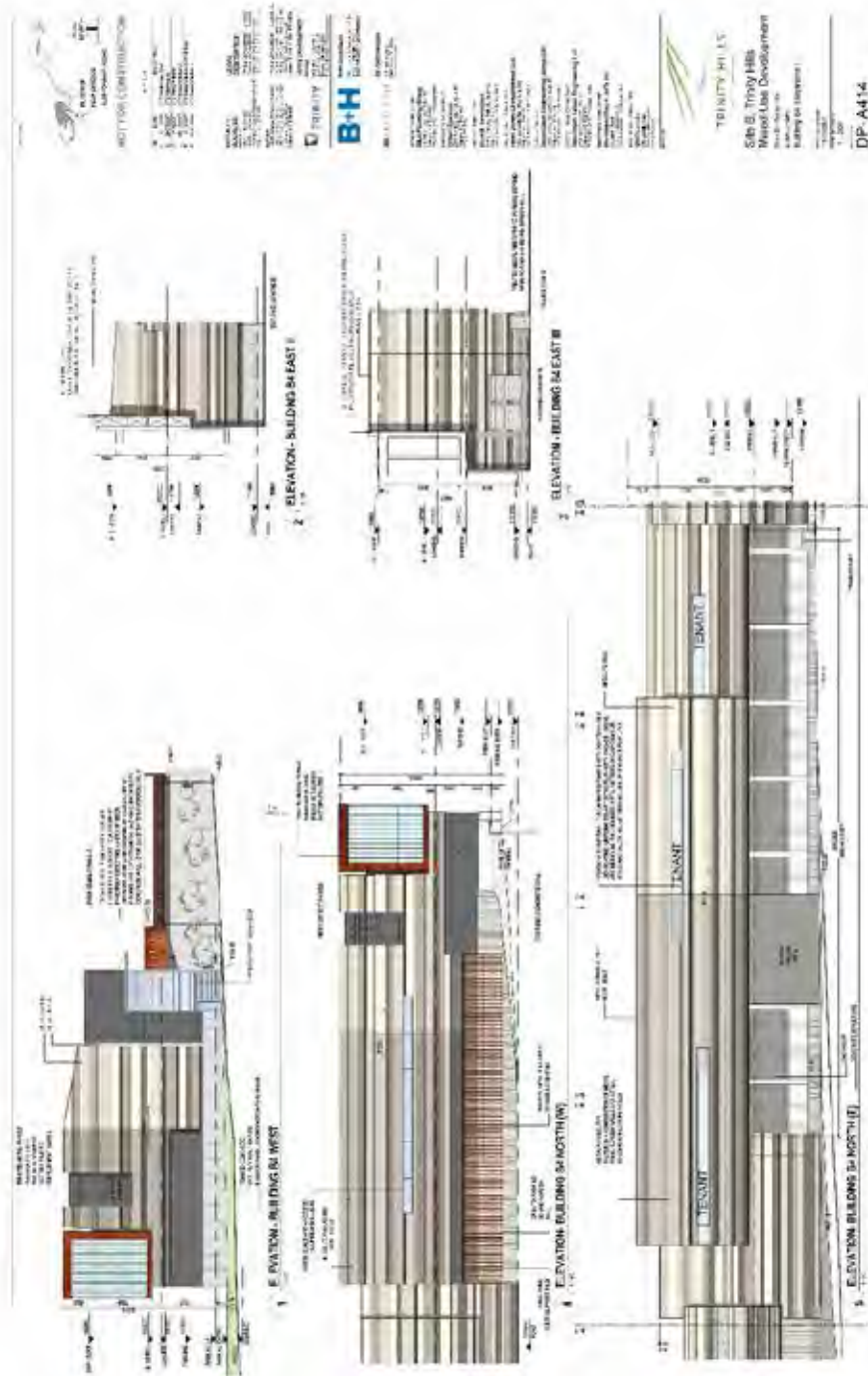
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MAP 27W



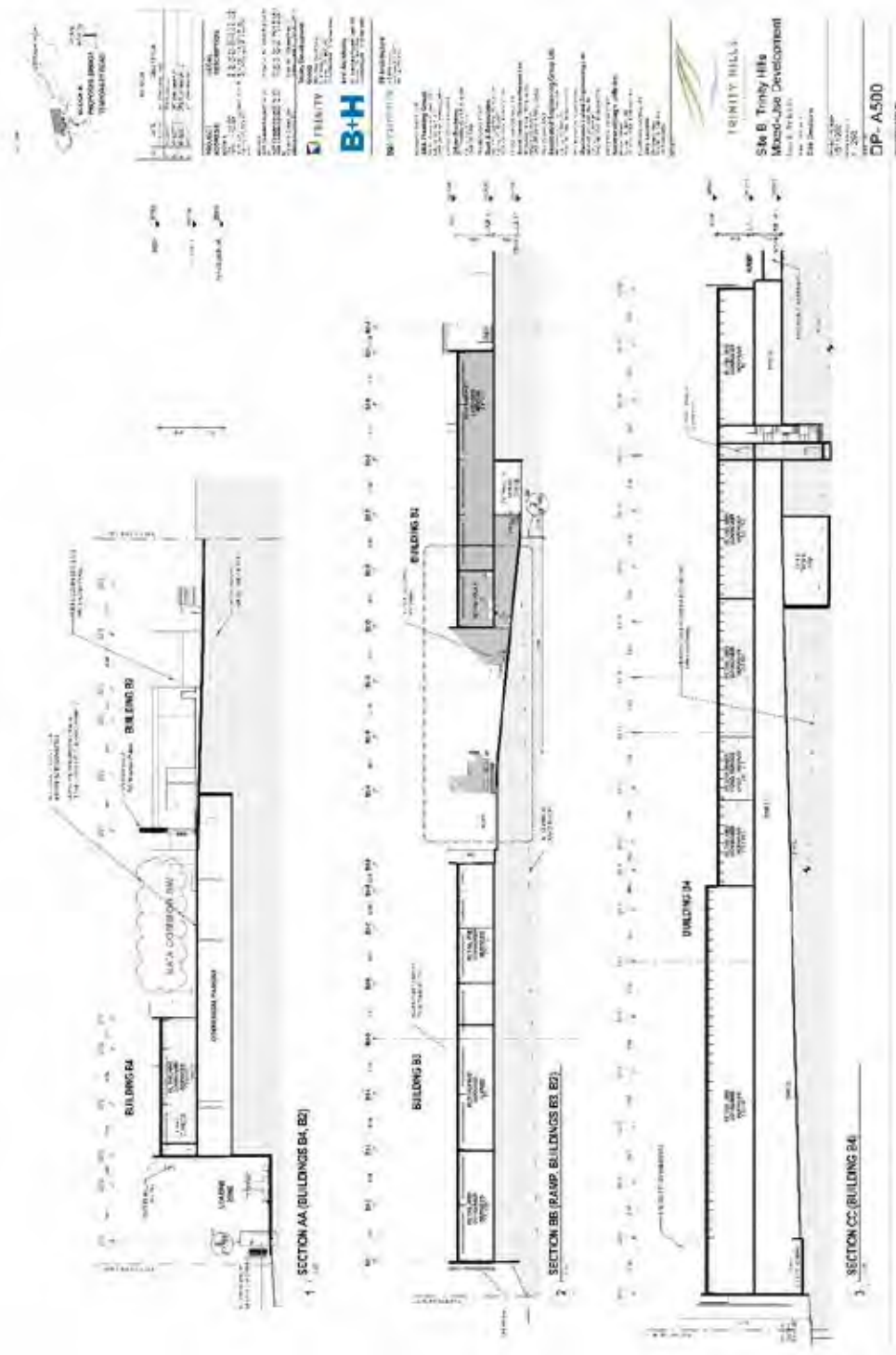
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MAP 27W



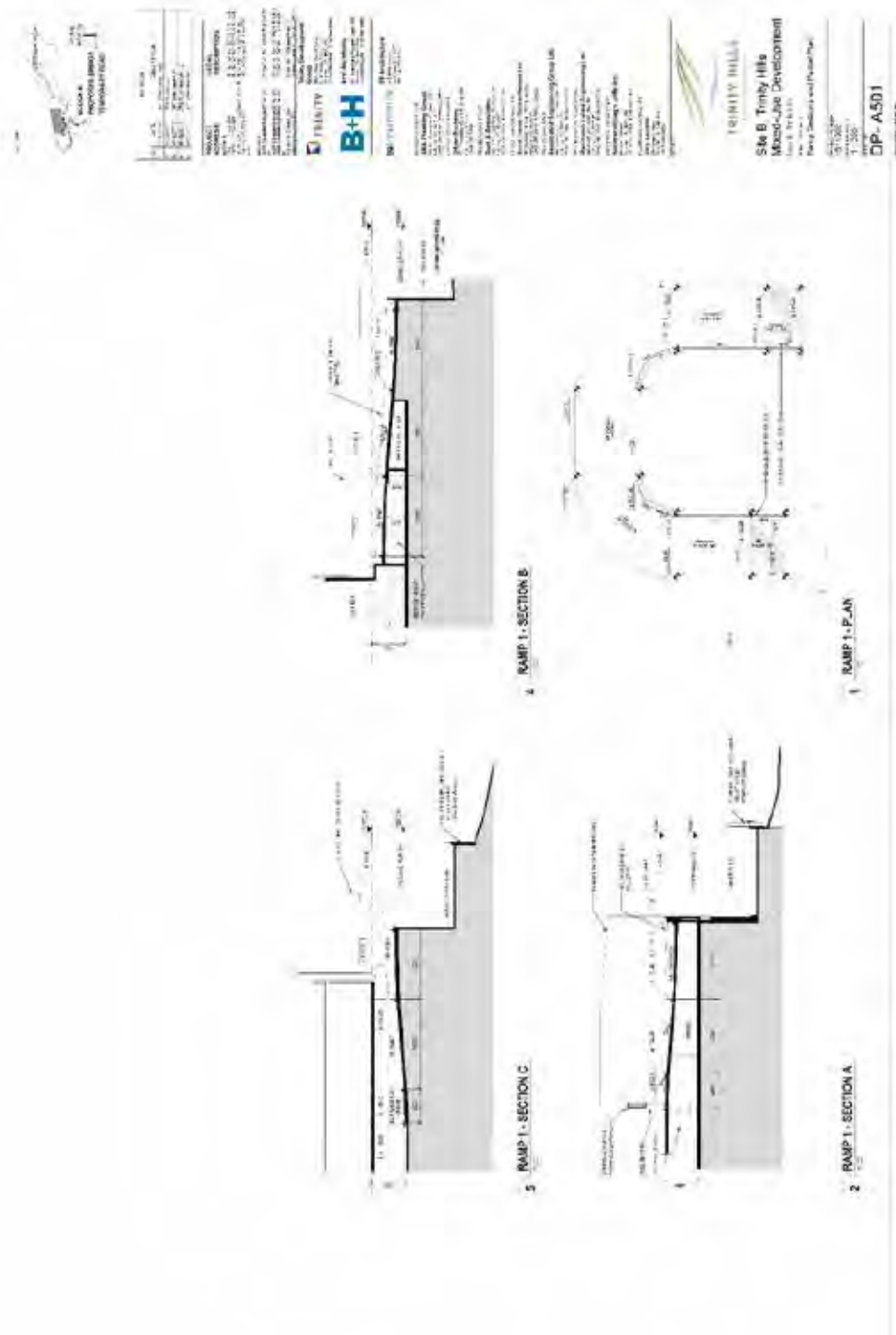
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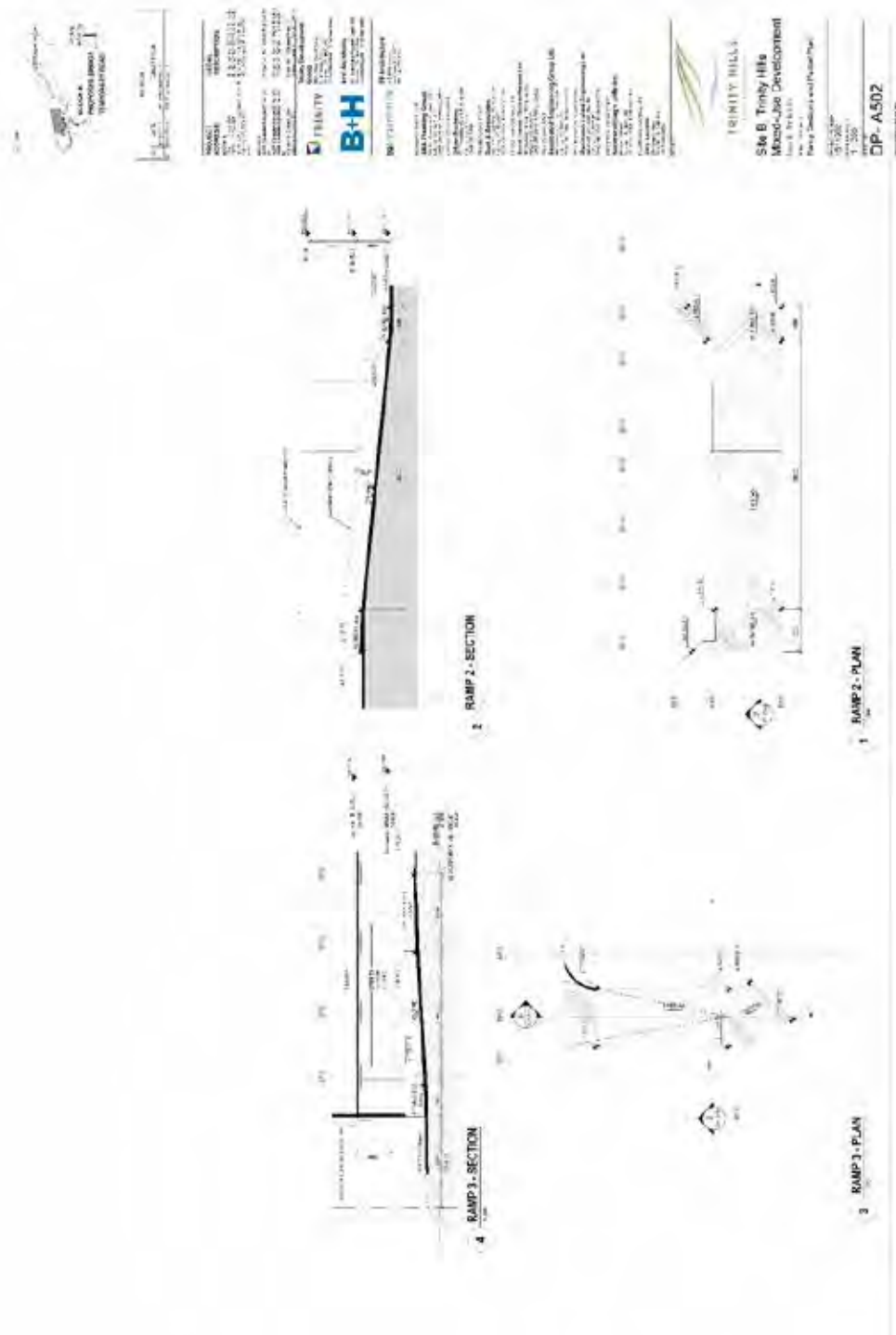
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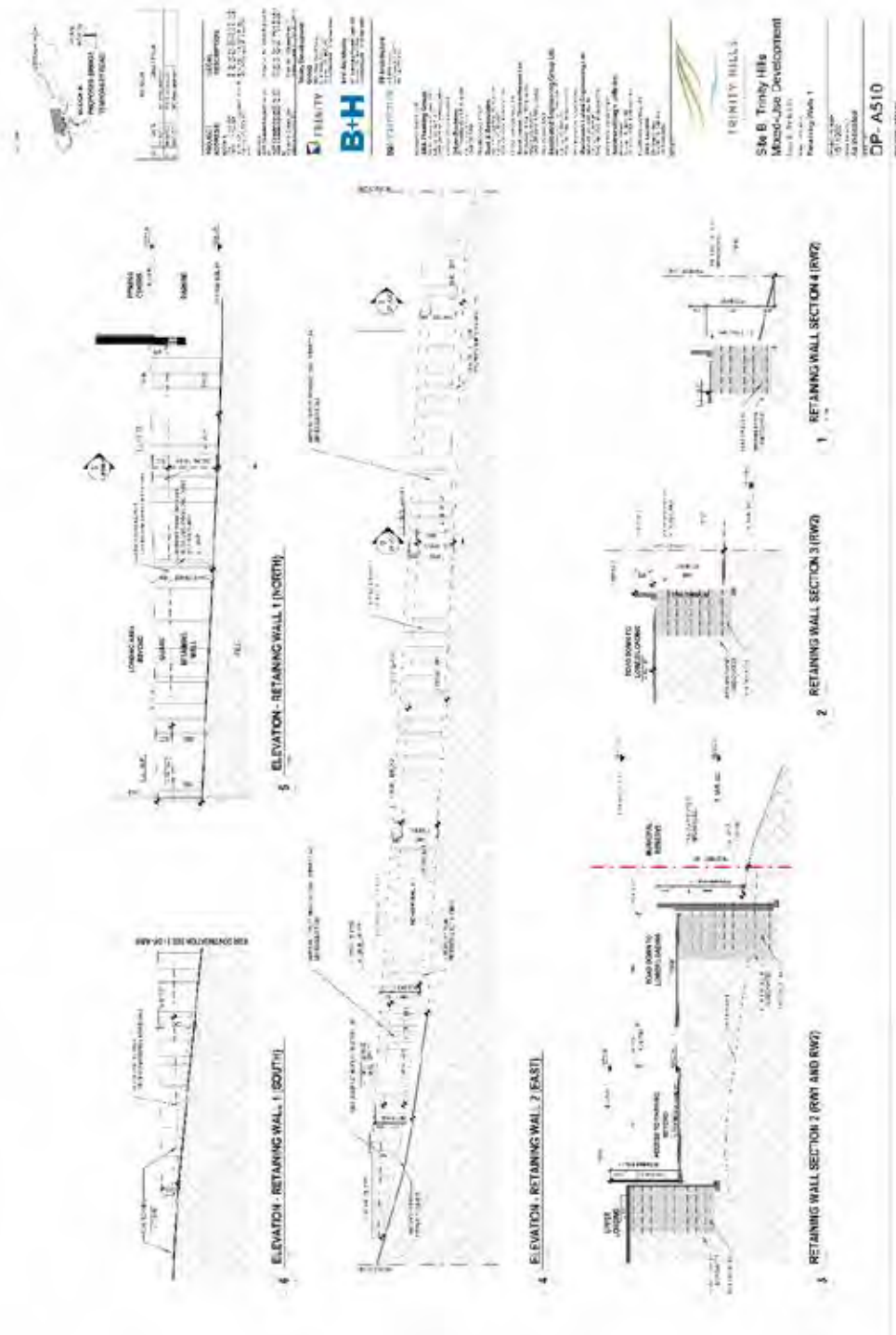
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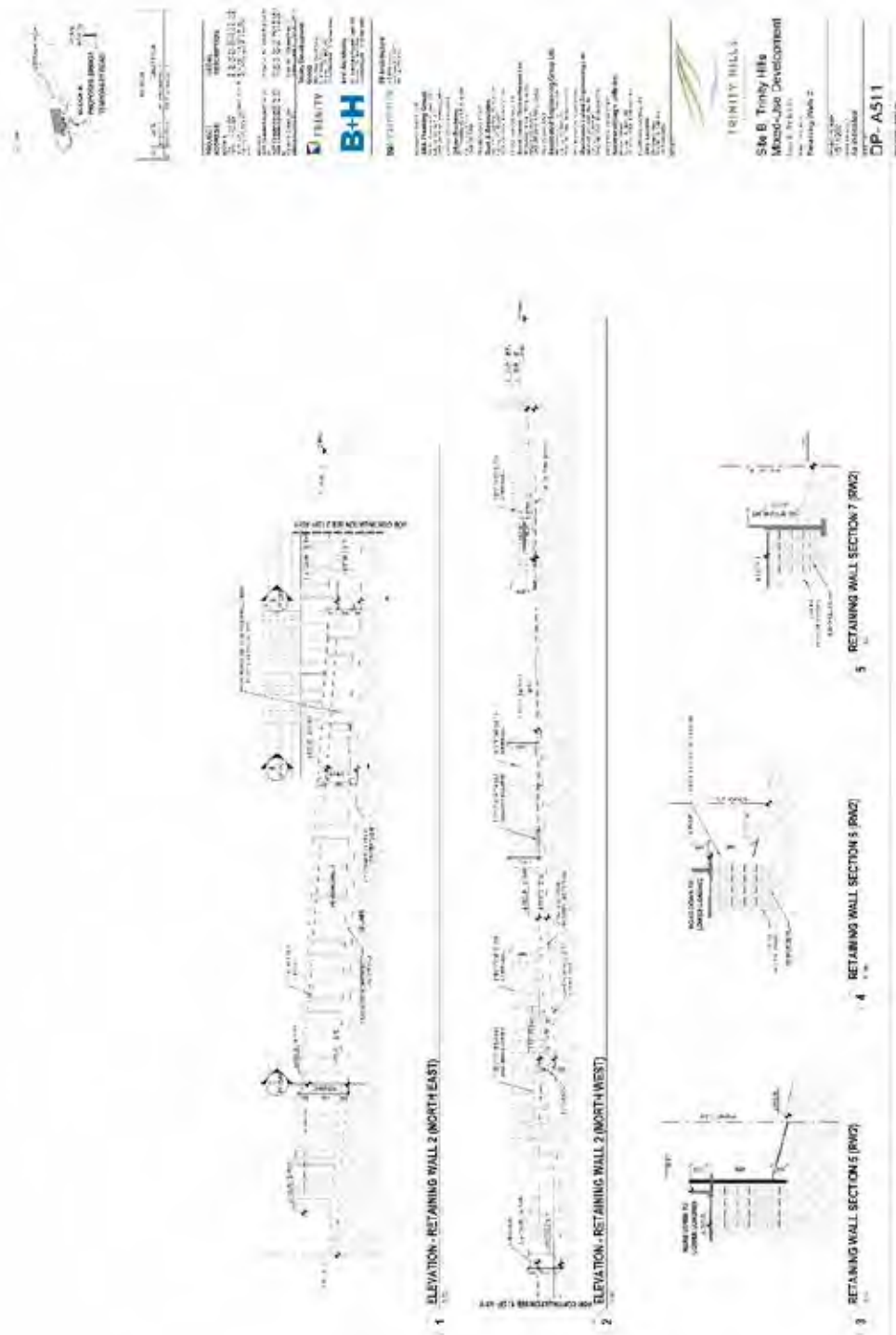
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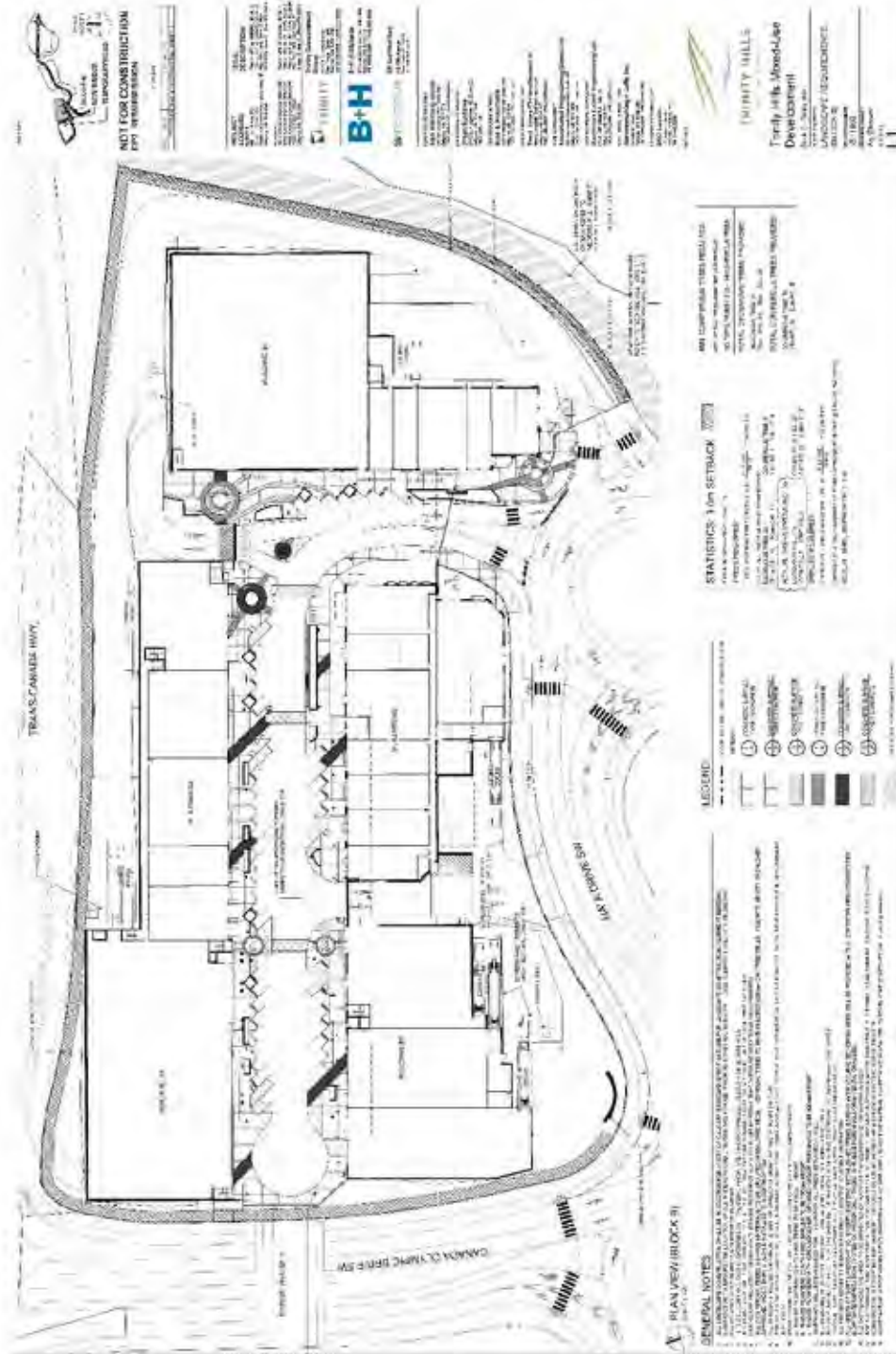
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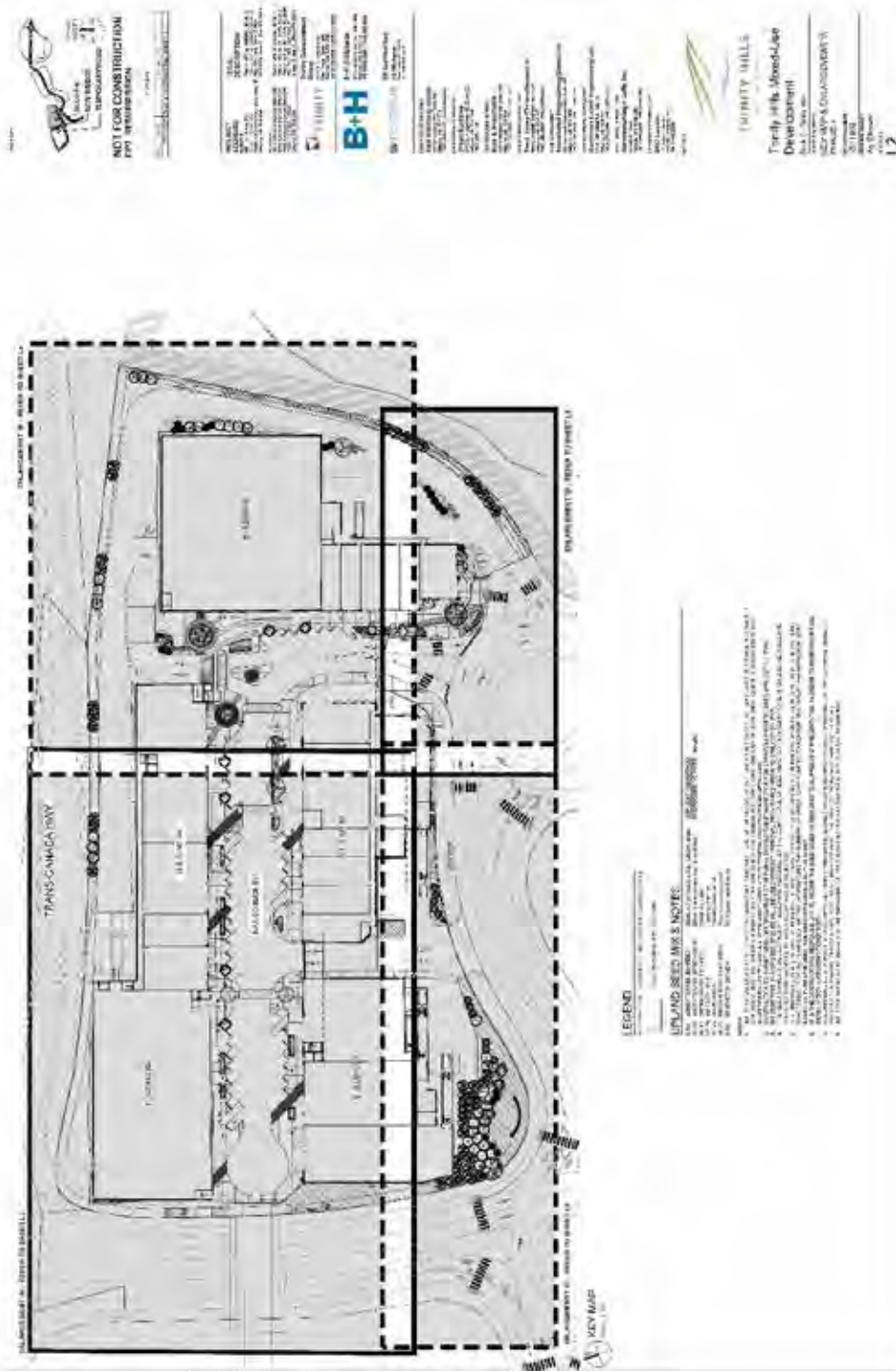
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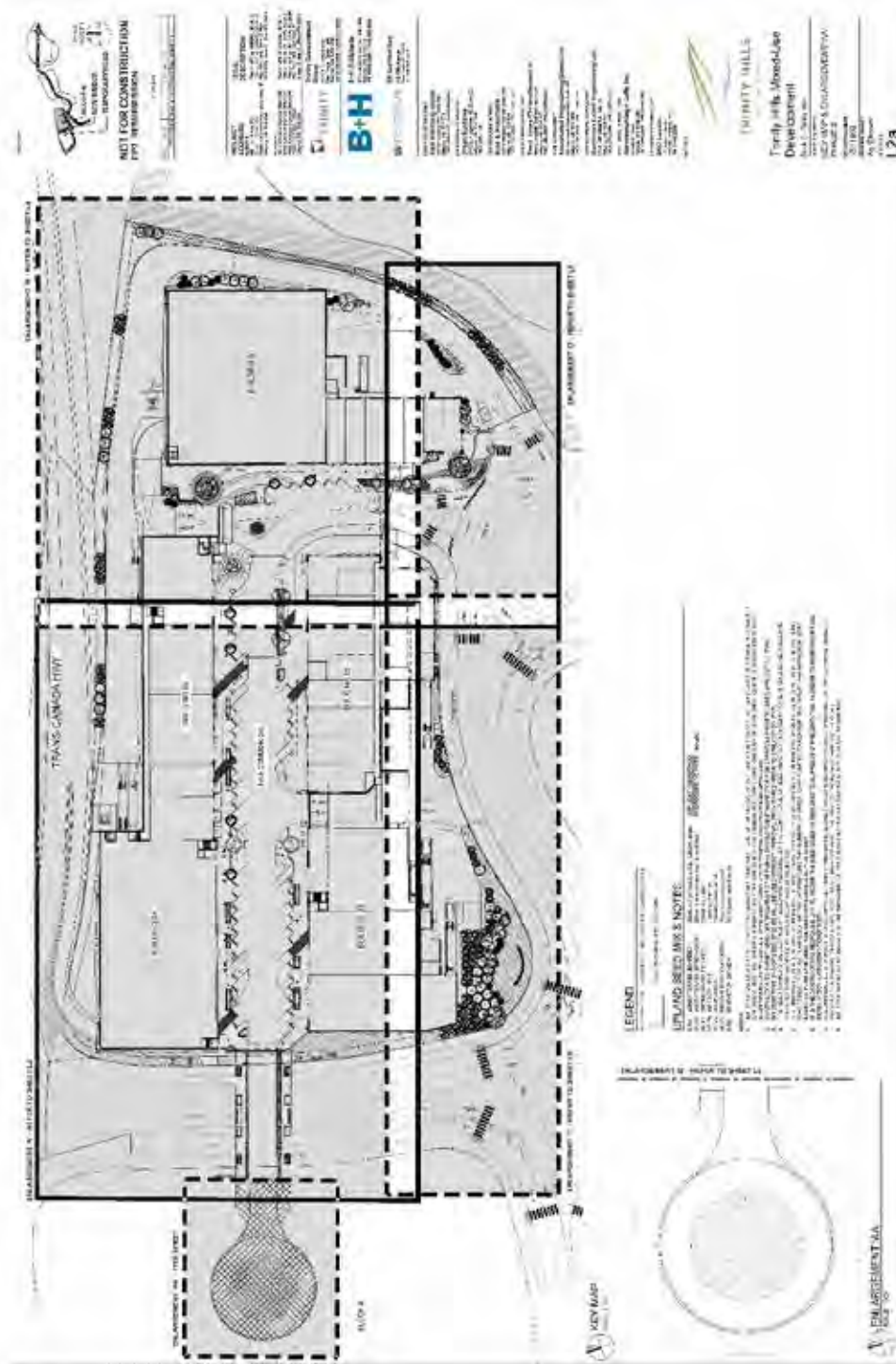
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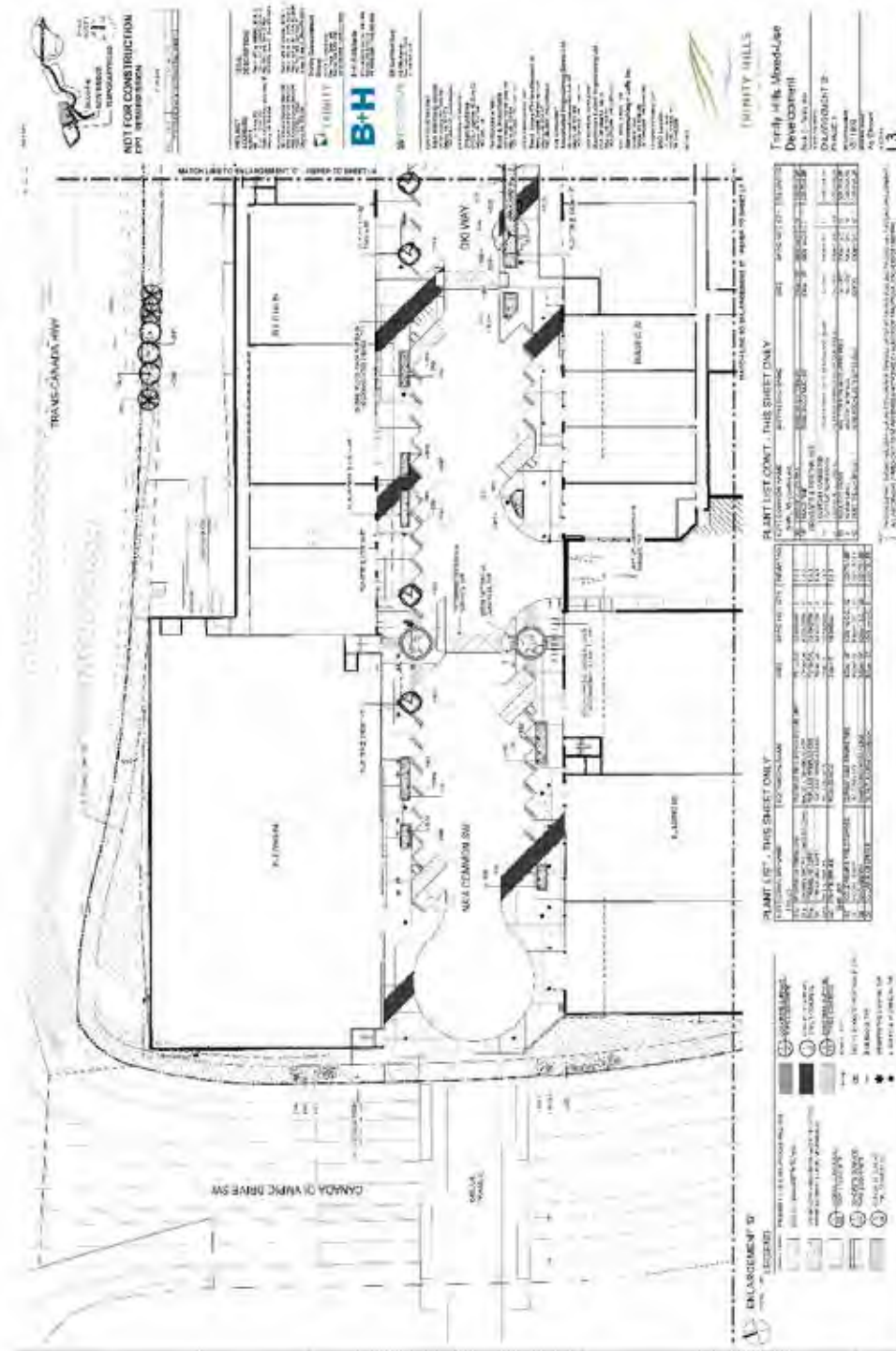
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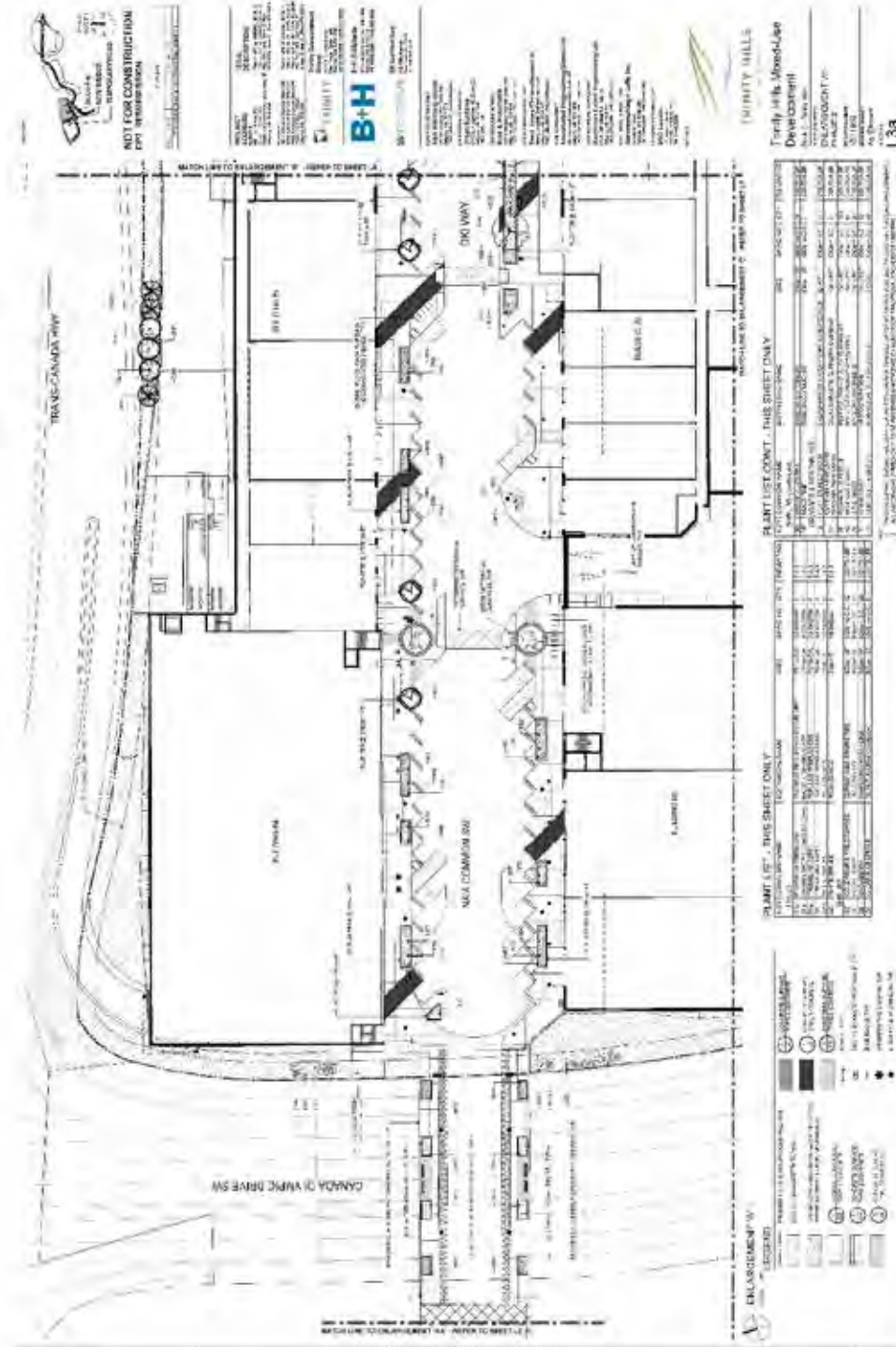
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 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW**

MAP 27W

UNITY HILLS
 Unity Hills Redevelopment
 04-7700001 C

| NO. | SYMBOL | PLANT NAME | SIZE | QUANTITY | REMARKS |
|-----|----------|------------|------|----------|---------|
| 1 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 2 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
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| 4 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
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| 7 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 8 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 9 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 10 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 11 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 12 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
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| 49 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |
| 50 | (Symbol) | PLANT NAME | SIZE | QUANTITY | REMARKS |

LEGEND

- PLANT SYMBOLS
- PLANT SIZES
- PLANT SPACING
- PLANT GROUPINGS
- PLANT NOTES
- PLANT ELEVATION

ENLARGEMENT C
 ARCHITECTURAL MALLERY

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

1 CONCRETE RISERFACES
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2 EXPANSION JOINT
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DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE,
RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT:
LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W



DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

APPENDIX II

CONDITIONS OF APPROVAL

Prior to Release

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of nine (9) complete sets of Amended Plans to the Planning Generalist that comprehensively address the Prior to Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. All of the plan set(s) shall highlight all of the amendments.
- b. Five (5) detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Terms of Reference/Request for Proposal (ToR/RFP) for the art piece that the developer intends to install shall be submitted to the Development Authority. These ToR/RFP will be reviewed by the Development Authority in consultation with the Blackfoot Traditional Knowledge Keepers to ensure that the art piece is reflective of Blackfoot culture and significance to the area.
3. Terms of Reference/Request for Proposal (ToR/RFP) for the design details on the bridge between Cells B and A that the developer intends to install shall be submitted to the Development Authority. These ToR/RFP will be reviewed by the Development Authority in consultation with the Blackfoot Traditional Knowledge Keepers to ensure that the design of the bridge is reflective of Blackfoot culture and significance to the area.
4. Amend the drafting area on drawing sheet DP A005 (and all other affected plans) to replace the reference 'DP-Phase 1 – parkade and Oki Drive turnaround' with 'DP-Phase 1 – parkade and Na'a Common SW turnaround'

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

Development Engineering:

Note: The Post-Development Global Stability Review (McIntosh Lalani Engineering Ltd, Dec. 13, 2017) was submitted on Dec. 13, 2017 to the Development Engineering Generalist, and is under review by the Geotechnical Engineer, Roads.

5. Submit an electronic version of a Geotechnical Report / Slope Stability Report to the Development Engineering Generalist, for review and acceptance, prepared by a qualified Geotechnical Engineer under seal and permit to practice stamp to the satisfaction of the Geotechnical Engineer, Roads.

Slope stability reports are required when:

- a. Any slope across the property line is fifteen (15%) percent or greater; and/or
- b. The development is to be located within a zone where an imaginary line, drawn from the toe to the top of an embankment, exceeds a slope of one to three, and/or
- c. Required by the Manager of Infrastructure Planning

Geotechnical Report requirements are outlined in Section V: Geotechnical and Hydro-Geological Requirements in the *Design Guidelines for Subdivision Servicing*. This publication can be found on the City of Calgary website at www.calgary.ca/ud under Publications. Contact the Geotechnical Engineer, Roads at 403-268-4568 for further details on the scope of the report.

A caveat or agreement may be required prior to release of the Development Permit, and a certification of foundation work by the Soils Consultant may be required prior to the completion of the Building Permit.

OR

If the proposed development does not have existing or proposed grades in excess of 15%, submit a letter to that effect signed and sealed by a professional Geotechnical Engineer.

If required, a Development and Geotechnical Covenant may be registered against the affected lot(s) concurrent with the registration of the final instrument/prior to release of the development permit, prohibiting the development of the lot(s), except in strict accordance with the development restriction recommendations in the Slope Stability Report.

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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

6. Amend the plans to:

Fire – Alarm Panel Location

- a. Indicate the location of the fire alarm panel such that:
- i. An exterior strobe light is shown at the exterior door to the fire alarm panel location as principal entrance may not be clearly defined. Strobe lights are indicated on electrical drawing set ES. Please add notes to site plan.
 - ii. Confirm details for Building B4 (SE corner). Exit details seem to be incorrect.

7. Amend the plans to:

Waste & Recycling Services - General

- a. Provide protection to ensure all parts of the storage area will be protected from contact by any part of a container. Refer to the "Development Reviews: Design Standards for the Storage and Collection of Recyclable Materials and Waste" Found at: <http://www.calgary.ca/UEP/WRS/Pages/Commercial-Services/Development-Permits-Waste-Recycling.aspx>

8. Submit a Stormwater Management Report (2 copies), for review and acceptance by the Development Approvals Team Leader, Water Resources, prior to submitting a Development Site Servicing Plan. This is a requirement for sites over 2 hectares as per the current *Stormwater Management Design Manual*.

Note: The SWMR was submitted on Dec. 15, 2017 and is under review by Water Resources.

9. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

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BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW**

MAP 27W

Note: Any changes to the development permit based on a currently not approved Stormwater Management Report or any additional changes may require a resubmission of the DSSP to coincide with the development permit once development permit is approved. Water Resources is allowing the Applicant to submit the DSSP ahead of SWMR approval.

10. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng.), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

11. Provide a completed copy of the “Retaining Wall Design Disclosure Statement”

AND

Submit an electronic version of Structural Design Drawings, for review and acceptance, for the retaining wall(s), prepared by a qualified Professional Engineer under seal and permit to practice stamp to the satisfaction of the Chief Structures Engineer, Roads. The intent of the drawings is to show the feasibility of the proposed retaining wall(s) at the location(s) indicated.

Note: The *Trinity Hills Block B Commitment to Design and Field Review of Retaining Walls* document with a copy of the “Retaining Wall Design Disclosure Statement is currently under review by Roads Structures.

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT:
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Transportation:

12. The proposed driveway located between the two traffic circles is to be amended to provide at minimum a standard 10.0m commercial driveway crossing complete with 3.0m flares on Na'a Drive S.W. as per Roads Specification. Curb returns are not permitted.

NOTE: The driveway flares are to tie to the front of walk as shown in the detail above, not to the property line as proposed. Also, a number of drawings in the plan set have not been amended to show a driveway crossing, and still show curb returns. Amend all plans accordingly. This change may also require a revision to the driveway cross section to show the pathway at 2% slope, and then the portion from the front of the pathway to the curb at a steeper angle. Amend accordingly.

13. Transportation confirms that a detailed design is not required on this DP with respect to the bridge details. The Applicant is to amend the Plans to include a note as follows:

Proposed traffic bridge will be designed and constructed in accordance with the latest version of the City of Calgary's Design Guidelines for Bridges & Structures and the Canadian Highway Bridge Design Code, CSA S6-14.

14. The Applicant will be required to submit a Road Closure application and subdivision application to subdivide the volumetric air space for the proposed traffic bridge along with an application to lease the volumetric air space to the satisfaction of the Real Estate & Development Services Dept. (formerly Corporate Properties).

As part of the road closure and subdivision applications the Applicant will be required to submit a plan showing the design, clearances for the appropriate TAC design vehicle as determined by Transportation Planning (the minimum City Standard for clearance is 5.5 metres). The Applicant will be required to submit agreements, to the satisfaction of Transportation Planning, Roads, Real Estate & Development Services and Facility Management. Contact Jarret Moisan of Real Estate & Development Services at jarret.moisan@calgary.ca for more information

15. Prior to Release of the Development Permit, the Applicant will have to confirm, through sight line analysis, the location of road guide signage for the Bowfort Interchange with Roads Traffic. The signage under consideration is for vehicles exiting the Medicine Hills development that are heading Northbound to the Bowfort Interchange. Signage work will either involve:

Leaving existing road guide signage as is.

OR

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Remove and relocate existing road guide signage.

It should be noted that new road guide signage may be required and may need to be hung from the proposed private bridge following decision on the proposed Road Closure and Strata and the sight line analysis.

Amend the Plan accordingly after confirmation with Roads Traffic. All work regarding this road guide signage shall be at the expense of the Developer and to the Satisfaction of the Director of Roads.

16. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of new driveway crossings on Na'a Drive S.W.,
- b. Construction of new merge ramp to City standards,
- c. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
- d. Road Guide Signage

Parks:

17. No comments

Permanent Conditions

The following permanent conditions shall apply:

Planning:

18. In accordance with Condition no 2, the art piece shall be installed in the location specified on the submitted plans prior to occupancy of the development. The art piece selected shall conform to the ToR/RPF previously reviewed by the Development Authority, and such an art piece shall be retained throughout the lifetime of the development.

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NA'A DRIVE SW**

MAP 27W

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19. In accordance with Condition no 3 the design of the bridge between Cells B and A shall conform to the ToR/RPF previously reviewed by the Development Authority, and such design details on the bridge shall be retained throughout the lifetime of the development.
 20. The retaining walls and screening in the subject site shall contain First Nations images as shown on the submitted plans, received by the Development Authority on January 4, 2018 or similar First Nations images. The affected retaining walls and screens shall retain such First Nations images throughout the lifetime of the development unless otherwise agreed in writing by the Development Authority.
 21. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
 22. No changes to the approved plans shall take place unless authorized by the Development Authority.
 23. This approval recognizes 6 phases on the approved plans. A Development Completion Permit will be issued for each phase. All the road works, landscaping and provisions for garbage collection shown within each phase shall be completed prior to the issuance of a Development Completion Permit for that phase. Call Development Inspection Services at 403-268-5311 to request site inspections for the Development Completion Permits.
 24. Prior to proceeding with Phase 2 enter into agreements satisfactory to the Director of Transportation Planning and the Director of Real Estate and Development Services for the construction and maintenance in perpetuity of the private bridge and subdivide, obtain land use, and close the portion of the road that consists of the airspace for the bridge, unless otherwise agreed in writing by the Development Authority.
 25. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
 26. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
 27. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
 28. Each parking stall, where located next to a sidewalk, shall have a properly anchored concrete wheel stop or equivalent material to the satisfaction of the Development Authority (100mm in height and 600mm from the front of the parking stall).

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29. This Development Permit provides approval only to the location of signage. Future comprehensive application(s) outlining sign classification, dimensions and copy area will require additional development permit approval(s).
 30. All trees, shrubs and grasses located within the subject property and shown on the approved plans, which cannot be retained during development, must be replaced by a species and size which is acceptable to the Development Authority within twelve months of the issuance of the development completion permit.
 31. All roof top mechanical equipment shall be screened as shown on the approved plans.
 32. The grades indicated on the approved Development Permit plans must match the grades on the Development Site Servicing Plan for the subject site as per the Lot Grading Bylaw.
 33. Loading and delivery shall take place in the designated loading stall as shown on the approved plans and shall, at no time, impede the safety of pedestrian movements and use of the parking lot.

Development Engineering:

34. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - The *Trinity Hills Block B Commitment to Design and Field Review of Retaining Walls* document (Oct. 18, 2017) and all further updates in correspondence with Roads
 - Post-Development Global Stability Review (McIntosh Lalani Engineering Ltd, Dec. 13, 2017)
35. Single retaining walls 1.2m in height or greater or terraced retaining walls 1.2m in height or greater with a horizontal separation between walls of less than 3.6m (3x height) require the approval of a Building Permit prior to construction.

For retaining wall(s) that meet these criteria, the developer may either:

- a. Include the retaining walls with the Building Permit for the building, or
- b. Apply for a separate Building Permit for the retaining walls.

It should be noted that the Building Permit for the building on site will not be released until the separate Building Permit for site retaining walls is approved.

36. Prior to the issuance of each Development Completion Permit, the developer/builder is required to provide the form, *Assurance of Engineering Field Review and Compliance*, (final page of the Retaining Wall Design Disclosure Statement) under seal and permit to practice stamp by the Engineer of Record to the Development Authority for the field review of the retaining wall(s).

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37. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
- a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
38. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

39. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.

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40. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
 41. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
 42. No trees, shrubs, buildings, permanent structures or unauthorized grade changes are permitted within the utility rights-of-way.

Transportation:

43. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.
44. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the Applicant's expense, upon completion of the foundation. **Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.**

Parks:

45. Throughout the development process, adhere to the mitigation measures outlined in the *Trinity Hills Development Lands Biophysical Impact Assessment* by ECOTONE Environmental, which was approved by the City of Calgary on June 30, 2015.
46. Any damage to public parks, boulevards or trees resulting from development activity, construction staging or materials storage, or construction access will require restoration at the developer's expense. The disturbed area shall be maintained until planting is established and approved by the Parks Development Inspector. Contact the Development Inspector at 403-268-5325 or 403-476-8323 for an inspection.

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Any surface or subterranean damage to public parks resulting from the installation of building construction tie-backs or other construction practices requires remediation at the developer's expense, to the satisfaction of the Director, Parks. All materials associated with the encroachments must be removed and any subterranean and surface disturbances to the parcel must be remediated. All site remediations must be approved by the Parks Development Inspector. Contact the Development Inspector at 403-268-5325 or 403-476-8323 for an inspection.

47. Any landscape rehabilitation on public parks shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition).
48. Stormwater or other drainage from the development site onto the adjacent Municipal Reserve or Environmental Reserve parcel is not permitted. Any drainage from private lots onto the adjacent Municipal Reserve or Environmental Reserve parcels upon development completion of the subject site must be resolved to the satisfaction of the Director, Parks and any damage resulting from unauthorized drainage will require restoration at the developer's expense. Resolution of drainage issues must be approved by the Parks Development Inspector. Contact the Development Inspector at 403-268-5325 or 403-476-8323 for an inspection.
49. There shall be no construction access, stockpiling or dumping of construction materials on the adjacent Environmental Reserve lands.

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APPENDIX III

DC BYLAW

BYLAW NUMBER 339D2017

**BEING A BYLAW OF THE CITY OF CALGARY
TO AMEND THE LAND USE BYLAW 1P2007
(LAND USE AMENDMENT LOC2017-0118)

WHEREAS it is desirable to amend the Land Use Bylaw Number 1P2007 to change the land use designation of certain lands within the City of Calgary;

AND WHEREAS Council has held a public hearing as required by Section 692 of the *Municipal Government Act*, R.S.A. 2000, c.M-26 as amended;

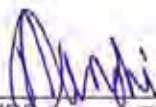
NOW, THEREFORE, THE COUNCIL OF THE CITY OF CALGARY ENACTS AS FOLLOWS:

1. The Land Use Bylaw, being Bylaw 1P2007 of the City of Calgary, is hereby amended by deleting that portion of the Land Use District Map shown as shaded on Schedule "A" to this Bylaw and substituting therefor that portion of the Land Use District Map shown as shaded on Schedule "B" to this Bylaw, including any land use designation, or specific land uses and development guidelines contained in the said Schedule "B".
2. This Bylaw comes into force on the date it is passed.

READ A FIRST TIME THIS 6TH DAY OF NOVEMBER, 2017.

READ A SECOND TIME THIS 6TH DAY OF NOVEMBER, 2017.

READ A THIRD TIME THIS 6TH DAY OF NOVEMBER, 2017.



MAYOR
SIGNED THIS 6TH DAY OF NOVEMBER, 2017.



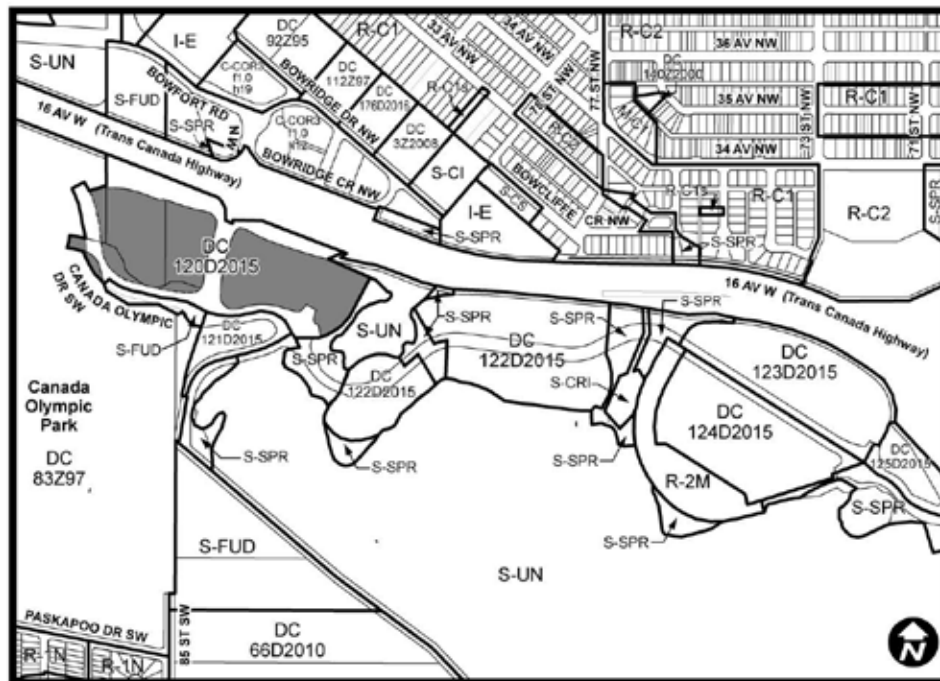
ACTING CITY CLERK
SIGNED THIS 6TH DAY OF NOVEMBER, 2017.

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SCHEDULE A

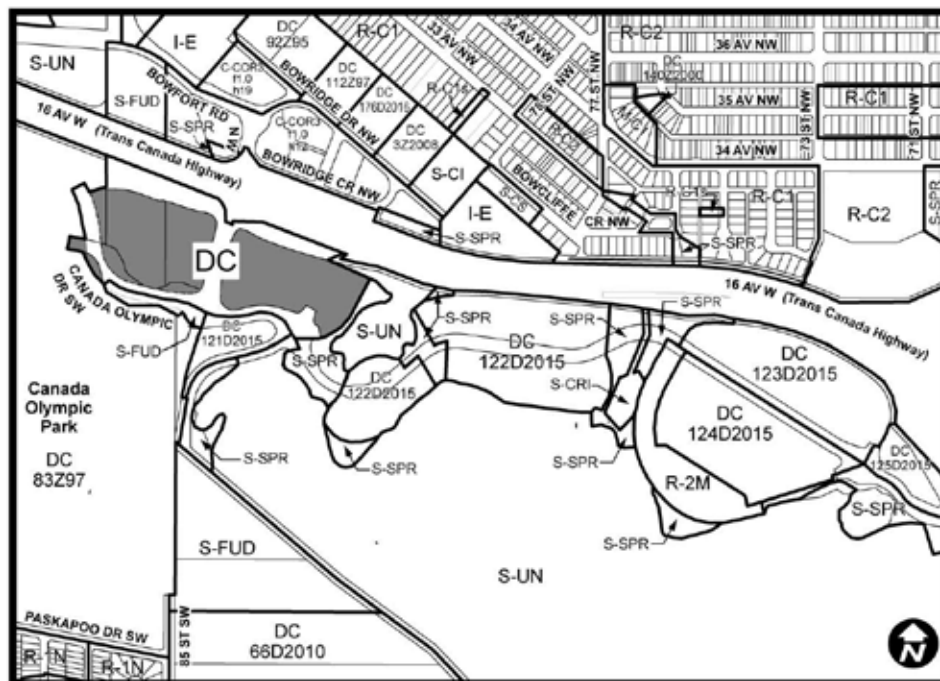


DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
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SCHEDULE B



DC DIRECT CONTROL DISTRICT

Purpose

1 This Direct Control District is intended to provide:

- (a) commercial *uses* along a continuous block face on a commercial main *street*;
- (b) *buildings* that are close to each other, the *street* and the public sidewalk on a commercial main *street*;

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- (c) opportunities for commercial *uses* on the ground floor of *buildings* and *Dwelling Units* and *Offices* on upper floors;
- (d) for varying *building densities* and height within a block; and
- (e) *Offices* and *Multi-Residential Development* that are not located along a commercial main *street*.

Compliance with Bylaw 1P2007

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3, 4 and 6 of Bylaw 1P2007 apply to this Direct Control District.

Reference to Bylaw 1P2007

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

Permitted Uses

- 4 The *permitted uses* of the Commercial - Corridor 1 (C-COR1) District of Bylaw 1P2007 are the *permitted uses* in this Direct Control District.

Discretionary Uses

- 5 The *discretionary uses* of the Commercial - Corridor 1 (C-COR1) District of Bylaw 1P2007 are the *discretionary uses* in this Direct Control District with the addition of:

- (a) **Multi-Residential Development;**
- (b) **Restaurant: Food Service Only – Large; and**
- (c) **Restaurant: Licensed – Large.**

Bylaw 1P2007 District Rules

- 6 Unless otherwise specified, the rules of the Commercial Corridor 1 (C-COR1) District of Bylaw 1P2007 apply in this Direct Control District.

Use Area

- 7 Except as referenced in 8(2) there is no minimum *use area* or maximum *use area* in this Direct Control District.

Location of Uses Within Buildings

- 8 (1) The following *uses* must not locate on the ground floor of *buildings*:

- (a) **Assisted Living;**
- (b) **Catering Service – Minor;**
- (c) **Child Care Service;**
- (d) **Counselling Service;**
- (e) **Health Services Laboratory – With Clients;**
- (f) **Instructional Facility;**
- (g) **Live Work Unit;**
- (h) **Medical Clinic;**

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- (i) Place of Worship – Small;
 - (j) Post-Secondary Learning Institution;
 - (k) Residential Care;
 - (l) Social Organization; and
 - (m) Veterinary Clinic.
- (2) With the exception of Multi-Residential Development, "Commercial Uses" must occupy a minimum of 80 per cent of the area of the floor closest to *grade*.
- (3) A "Commercial Use" that is located on the floor closest to *grade* must have:
- (a) an individual, separate, direct access to *grade*; and
 - (b) an entrance that is visible from the *street* that the *use* faces.
- (4) "Commercial Uses" and Live-Work Units:
- (a) may be located on the same floor as Addiction Treatment, Custodial Care and Residential Care; and
 - (b) must not share an internal hallway with Addiction Treatment, Custodial Care and Residential Care.
- (5) Where this section refers to "Commercial Uses", it refers to the *uses* listed in Section 4 and 5 of this Direct Control District other than Addiction Treatment, Custodial Care, Dwelling Units, Multi-Residential Development and Residential Care.

Building Height

- 9 (1) A maximum of one *building* within this Direct Control District may have a maximum *building height* of 75.0 metres.
- (2) A maximum of two *buildings* within this Direct Control District may have a maximum *building height* of 50.0 metres.
- (3) In all other cases, the maximum *building height* is 21.0 metres.

Relaxations

- 10 The *Development Authority* may relax any of the rules contained in this Direct Control District in accordance with Sections 31 and Section 36 of Bylaw 1P2007.

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APPENDIX IV

UDRP COMMENTS

Summary: This project relates to an application previously reviewed by UDRP (DP2016-4580). It has a long history within a significant urban gateway interface and its regional context. UDRP is generally supportive of the project. This proposal is the gateway to the overall development and suggested refinements will make for a stronger application. Consistency between the adjacent application contributing distinctive architecture to the area should be emphasized in realizing the full place making potential. The main entrance at Bowfort Road should reinforce the 'gateway' concept in the architecture on the flanking elevations. Where retaining walls are required to address site conditions/grade separations, options for a more natural interface should be explored to create a more authentic solution to the original and man-made ravines. Better integrating the architecture and landscape will offer a context-sensitive outcome.

| Urban Vitality | | | |
|--|--------------------------------|--|----------------------|
| | Topic | Best Practice | Ranking |
| 1 | Retail street diversity | Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided. | Support with comment |
| <p>UDRP Commentary Although the project displays strong considerations for the Winsport Way and Pomenade from a retail experience, it has not given the 16 Avenue 'outside-in' the same level of attention. Suggest that further review and wrapping building mass at the Gateway Circle would strengthen the edge condition and allow the retail to 'turn the corners' along the Bowfort Road connection. The architecture on the retail side would be improved by incorporating some of the architectural elements used on the exterior sides for continuity and for a more distinctive appearance. It was felt by the panel that the retail was not distinctive to this site and the suggestion above was one way in which this might be achieved.</p> | | | |
| <p>Applicant Response: There is no need to give 16th Avenue (Trans-Canada Highway on-ramp) the same pedestrian consideration as the internal street as it is a loading area for the centre and a highway on-ramp is not suitable for a pedestrian environment. Furthermore, additional retail facing the Trans –Canada Highway would create a loading zone dilemma, would reduce the amount of underground parking and would create unrentable tenant space.</p> <p>Additional architectural elements have been wrapped around the edge of building B3 to strengthen the edge treatment. The grades, traffic and separation distance from the other shops isolate the southwest corner so that it is not economically viable to create a building that turns the corner. Additional landscaping has been added to the corner at the request of Planning to create an attractive feature at the entrance to the development.</p> <p>The architect’s inspiration comes from a geode with distinctive interior and exterior appearances. The interior appearance conforms to the architect’s vision for the site.</p> | | | |

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| | | | |
|---|---|---|----------------------|
| 2 | Retail street transparency, porosity | Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows. | Support |
| UDRP Commentary Design package contained images that reflect extensive glazing. | | | |
| | | | |
| 3 | Pedestrian-first design | Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience. | Support with comment |
| UDRP Commentary The North-South connection with Bowfort Road should be given further consideration in how it relates to the pedestrian environment. This walkable interface should reinforce the gateway aspect inherent with the development. Potentially an exterior stair connection to the Promenade would assist, as currently no external path between these areas exist. Mountable low-rise rolled curbs with expandable sidewalks in street parking areas would significantly change the public realm experience. The panel suggests that the roadway material on the Winsport Way Promenade (Main Street) be more distinctive, perhaps the same material as the sidewalks (rolled curbs) to make the space more pedestrian focused. | | | |
| Applicant response: A glass enclosed staircase has been provided linking Canada Olympic Drive to the Main Street. Using the same material for sidewalk and road will make the surface look homogenous and not distinctive. Preference is to differentiate the driving area from the pedestrian sidewalk. Because the road is private, it can be easily closed for special events and used as a promenade. Hard curbs are preferred adjacent to the angled parking stalls to reduce chances of vehicles mounting the sidewalk to improve pedestrian safety. | | | |
| 4 | Entry definition / legibility | Entry points are clear and legible | Support with comment |
| UDRP Commentary The plan feels dominant from the vehicular perspective. Further to the above comment, more focus at Bowfort Road from the pedestrian first design is required. As well, two high rise residential buildings (future phase) are indicated, with very little consideration to pedestrians crossing the Gateway Circle (or otherwise) to access the commercial development. Greening of the bridge as gateway feature was discussed and the Panel expressed the 'walkable/environmental connective tissue' benefits of exploring a Green Bridge option. | | | |
| Applicant Response: The site is surrounded on three sides with roads including two local arterials and a highway. The site design encourages an enjoyable walking experience along the Main Street with enhanced sidewalks, street furniture, patios, and very little traffic and parking. Nearly 90% of parking is underground. This allows the focus to be on the street experience instead of a surface parking lot typical of suburban strip malls. Road design includes boulevard tree planting and a 3.0 m wide multi-use pathway. A glass enclosed stairway has been provided for pedestrians linking Canada Olympic Drive to the Main Street above. More articulation has been added to the walls of building B3 in addition to the inclusion of buffalo artwork on the bridge abutment and parkade walls. Lighting and additional landscaping is provided within the setback area to provide a more pleasurable | | | |

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE, RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT: LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4 BUILDINGS)
 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W

| | | | |
|---|---|--|----------------------|
| | pedestrian experience. Landscaping has been added to the bridge to make it green. As well, artistic references to the historical significance of the area has been incorporated into the architectural design of the bridge. | | |
| 5 | Residential multi-level units at grade | Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use. | N/a |
| | UDRP Commentary N/a | | |
| | Applicant Response N/a | | |
| 6 | At grade parking | At grade parking is concealed behind building frontages along public streets. | Support with comment |
| | UDRP Commentary: The Panel recognizes that the majority of parking has been provided below grade. While considerations for parallel parking instead of angled parking would reduce the overall dimension of the roadway, another consideration is to jog the curb face with the parking stall to allow for additional planting areas and create a more interesting edge condition with the sidewalk. | | |
| | Applicant Response: Angle parking is desired by tenants. The curb face has been jogged to allow for additional planting areas and for a shorter cross-walk dimension. | | |
| 7 | Parking entrances | Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first. | Support with comment |
| | UDRP Commentary Parking entrances are located in generally discrete locations. Acknowledging that a large retaining wall will be required adjacent to the east entrance, the Panel reiterates that the retaining wall design can be executed in a more natural manner, including green technologies and steep wall interface. | | |
| | Applicant Response: A retaining wall with artwork referencing the historical importance of buffalo to the area has been included along with landscaping to soften the interface with the buildings. | | |
| 8 | Other | Applicant to consider tying together the unique landscape design that is contextually sensitive to the architectural elements in the project. The landscape connective continuity should seamlessly weave together the fabric between the development sites and the planned development areas. | |
| | Applicant Response: We agree. The unique landscape streetscape design internal to the site has been extended within the roundabout right-of-way (ROW) to create a unique and special entrance into the development. The Roads Department typical does not support any enhanced landscaping or sidewalk treatment within a public ROW. We appreciate your challenge of typical city standards as it relates to sidewalk urban design. | | |

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 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W

| Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i> | | | |
|--|-------------------------------------|---|----------------------|
| Topic | Best Practice | | Ranking |
| 9 | LRT station connections | Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas. | N/a |
| UDRP Commentary N/a | | | |
| Applicant Response N/a | | | |
| 10 | Regional pathway connections | Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas. | Support with comment |
| UDRP Commentary Although various Trail Heads are located in the immediately surrounding areas, they are not as visible and relevant as they deserve to be, given the project context. Connecting to the trails and the slopes need to be more intentional and meaningful between the Paskapoo Slopes within the Bow River watershed. Pedestrian connections to future villages and regional pathways need to be more than a sidewalk adjacent to the roadway. | | | |
| Applicant Response: There are no trails in the immediate vicinity. Those located in the adjacent ravine have been closed off by Parks. Further up the slope along Piita Rise there is a Municipal Reserve parcel with parking used as a staging area to access the adjacent open space dirt trails. These dirt trails and parking are easily accessible via the multi-use pathway which extends along Canada Olympic Drive to Na'a Drive, up Piita Rise to Cougar Ridge. The sidewalks internal to the development are connected to these multi-use pathways which extend the full width of the community and from Bowness to Cougar Ridge. The multi-use pathways also connect to the dirt trails found on the land dedicated for a future regional park. These are off-site pathway conditions that were addressed at the outline plan stage. | | | |
| 11 | Cycle path connections | Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade. | N/a |
| UDRP Commentary No bicycle lanes were shown in the design. If bicycle lanes are to be provided, applicant should demonstrate mobility design integration. | | | |
| Applicant Response: Multi-use pathways are located on Na'a Drive and Canada Olympic Drive. | | | |

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 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W

| | | | |
|--|---|---|----------------------|
| 12 | Walkability - connection to adjacent neighbourhoods / districts / key urban features | Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use. | Support with comment |
| UDRP Commentary: Better design-response integration of the Paskapoo Slopes and its hillside ravines relative to the development (natural areas vs. land development) is necessary. Needed to assist with being more authentic in the design. The interface of green space and hardscapes need to dovetail better in a manner the blurs the hard-line edge of the development. In the same thematic manner. Surface treatments along the promenade and side street connectivity defined with mountable, low-profile rolled curbs can blur the lines for a more walkable shared space with cars. | | | |
| Applicant Response: This site is not adjacent to the slopes. A future residential development will exist on the south side of Na 'a Drive between the slope and this site. Due to the spacing of the roundabout legs accessing this site we are left with the difficulty of including retaining walls adjacent to the ravine. All attempts have been made to reduce the height of the retaining walls and including artwork and landscaping to blur the hard-line edge of the development. The access road to the loading area at the north end of the site was designed in response to pre-application comments received by the City to remove loading areas from the main street. The amount of landscaping next to the retaining wall is limited by a utility ROW. Additional landscaping has been added next to the fitness centre's east wall to soften the interface between the green space and the development. | | | |
| 13 | Pathways through site | Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries. | Support with comment |
| UDRP Commentary The pedestrian environment under the promenade along Bowfort Road needs further design development. The underpass experience is fundamentally importance to the overall gateway theme of the development. Also, connection across Na'a Drive SW requires design development to achieve accessible and safe walkability. | | | |
| Applicant Response: These comments relate to an off-site condition that was addressed at the outline plan stage when the roundabout design was designed to City standards and approved. These city standards included appropriate cross-walks. A bridge between block A and B will provide safe pedestrian links between sites. | | | |
| 14 | Open space networks and park systems | Connects and extends existing systems and patterns. | Support with comment |
| UDRP Commentary See comment 10 and 12. | | | |
| Applicant Response: The site connects to the multi-use pathway system which connects to the open space parks. Park connectivity is an off-site condition that was addressed at the outline plan stage. | | | |

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

| | | | |
|--|--|--|----------------------|
| 15 | Views and vistas | Designed to enhance views to natural areas and urban landmarks. | Support with comment |
| | UDRP Commentary The applicant did not discuss view corridors. Given site location, further design development and definition of natural views is suggested. | | |
| | Applicant Response: A view corridor towards the iconic Olympic ski jump is provided when exiting the site | | |
| | | | |
| 16 | Vehicular interface | | No comment |
| | UDRP Commentary | | |
| | Applicant Response | | |
| | | | |
| 17 | Other | | No comment |
| | Applicant Response | | |
| Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i> | | | |
| | Topic | Best Practice | Ranking |
| 18 | Massing relationship to context | Relationship to adjacent properties is sympathetic | Support with comment |
| | UDRP Commentary: Generally acceptable massing. | | |
| | Applicant Response | | |
| | | | |
| 19 | Massing impacts on sun shade | Sun shade impacts minimized on public realm and adjacent sites | TBD |
| | UDRP Commentary No Sun Shade study provided; necessary for comment. | | |
| | Applicant Response: Shading does not impact any residential property | | |
| | | | |
| 20 | Massing orientation to street edges | Building form relates / is oriented to the streets on which it fronts. | Support with comment |
| | UDRP Commentary The project appears to turn its back to 16 Avenue with the presence of loading docks. Although the image in the package of the view from above 16 Ave does not look undesirable, there are questions that this is an accurate depiction. Additional screening of the loading areas may be necessary. | | |
| | Applicant Response: In order to create a quiet and pedestrian environment, the decision was made early in the concept design to have an internal commercial street and not face the Trans-Canada Highway. As the side is surrounded by roads on three sides, loading docks have to face the Trans-Canada Highway. Additional screening has been included to screen the loading areas from view. | | |
| | | | |

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 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW

MAP 27W

| | | | |
|---|--|---|----------------------|
| 21 | Massing distribution on site | | Support with comment |
| UDRP Commentary Potential to wrap retail building mass along north edge of Gateway Circle would strengthen gateway edge condition. | | | |
| Applicant Response: The site grades, lack of on-street parking and separation distance from the other shops isolate the southwest corner so that it is not economically viable to wrap the building mass along the north edge of Gateway Circle. Additional landscaping has been added to the corner at the request of Planning to create an attractive landscaping feature at the entrance of the development | | | |
| 22 | Massing contribution to public realm at grade | Building form contributes to a comfortable pedestrian realm at grade | Support with comment |
| UDRP Commentary The Panel would like to see further considered response to the uniqueness of the site and how it might beneficially influence layout of the massing. | | | |
| Applicant Response: Massing has been limited to one storey along the pedestrian orientated commercial street to minimize massing. Architectural elements such as projections, wall folds, and a variety of building materials and colours, symbolic of the sedimentary soil in the area, helps provide interest to the east and north facing walls where the building adapts to slope grades. | | | |
| 23 | Other | | No comment |
| Applicant Response | | | |
| Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i> | | | |
| | Topic | Best Practice | Ranking |
| 24 | Safety and security | CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances. | Support with comment |
| UDRP Commentary Internal to the site, appears to conform. Outside edge facing considerations may not comply due to reduced glazing. | | | |
| Applicant Response: CCTV cameras and lighting are used in loading area not frequented by the general public. | | | |

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 MEDICINE HILL (WARD 6)
 NA'A DRIVE SW**

MAP 27W

| | | | |
|----|---|--|---------|
| 25 | Pedestrian level comfort - wind | Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions. | TBD |
| | UDRP Commentary No pedestrian level wind study was provided. The height of the development may not warrant it. The applicant is to review this further with Urban Development. | | |
| | Applicant Response: A wind study is not necessary due to the limited height of the development. | | |
| 26 | Pedestrian level comfort - snow | Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions. | TBD |
| | UDRP Commentary No snow drifting analysis was provided. The height of the development may not warrant it. The applicant is to review this further with Urban Development. | | |
| | Applicant Response: A snow drift study is not necessary due to the limited height of the development | | |
| 27 | Weather protection | Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages. | Support |
| | UDRP Commentary Various images show canopy protection at entrances. | | |
| | | | |
| 28 | Night time design | | TBD |
| | UDRP Commentary: Exterior lighting design information was not given or discussed at this meeting. A full lighting design should be provided for the public realm. | | |
| | Applicant Response: Lighting plan was submitted with Development Permit plans | | |
| 29 | Barrier free design | Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps. | TBD |
| | UDRP Commentary The applicant is to confirm that all exterior ramps are 5% or less. | | |
| | Applicant Response: Wheel chair ramps will conform to the Building Code | | |
| 30 | Winter city | Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity. | TBD |
| | UDRP Commentary This was not addressed by the applicant. Positioning public open space areas exposed to sunlight are important urban design considerations. | | |

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MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

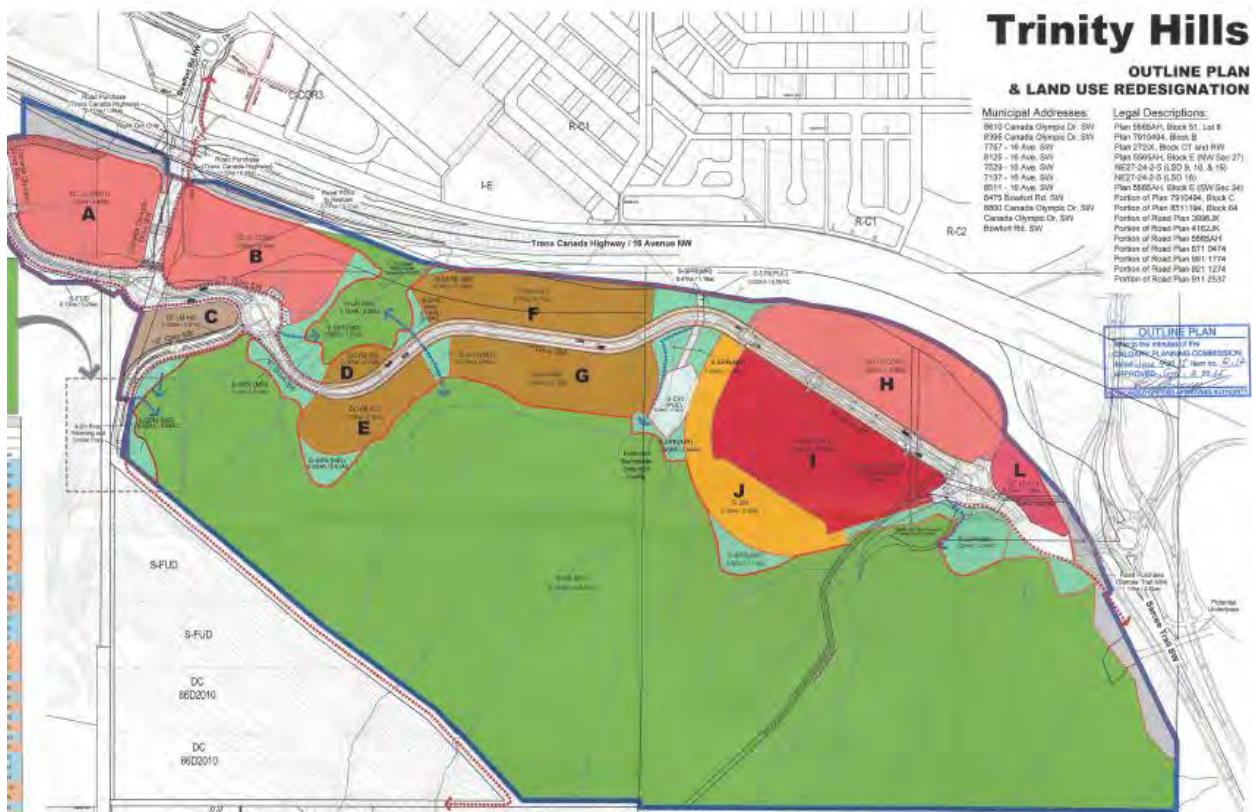
| | | | |
|---|--|---|----------------------|
| | Applicant Response: Plaza location is in an area that will take advantage of the afternoon sun. | | |
| 31 | Other | | No comment |
| | Applicant Response | | |
| Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i> | | | |
| | Topic | Commentary | Ranking |
| 32 | Waste / recycling | | TBD |
| 33 | Enmax (Power) / Atco (Gas) | | TBD |
| 34 | Transformer / switchgear | | TBD |
| 35 | Exhaust / intake | | TBD |
| 36 | Electrical vaults | | TBD |
| 37 | Loading | UDRP Commentary A large degree of loading is situated on the south side of the development, that interfaces with 16 Avenue. While grade-change will mitigate direct sight lines, additional screening (through landscape and architecture) would be a benefit to the project and surrounding area. Applicant response: Screening has been added. | Support with comment |
| 38 | Fire truck access | | TBD |
| 39 | Other | No comment | |

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, OUTDOOR
CAFE, FITNESS CENTRE, RETAIL AND CONSUMER SERVICE,
RESTAURANT: FOOD SERVICE ONLY – MEDIUM, RESTAURANT:
LICENSED – MEDIUM, RESTAURANT: LICENSED - LARGE (4
BUILDINGS)
MEDICINE HILL (WARD 6)
NA'A DRIVE SW

MAP 27W

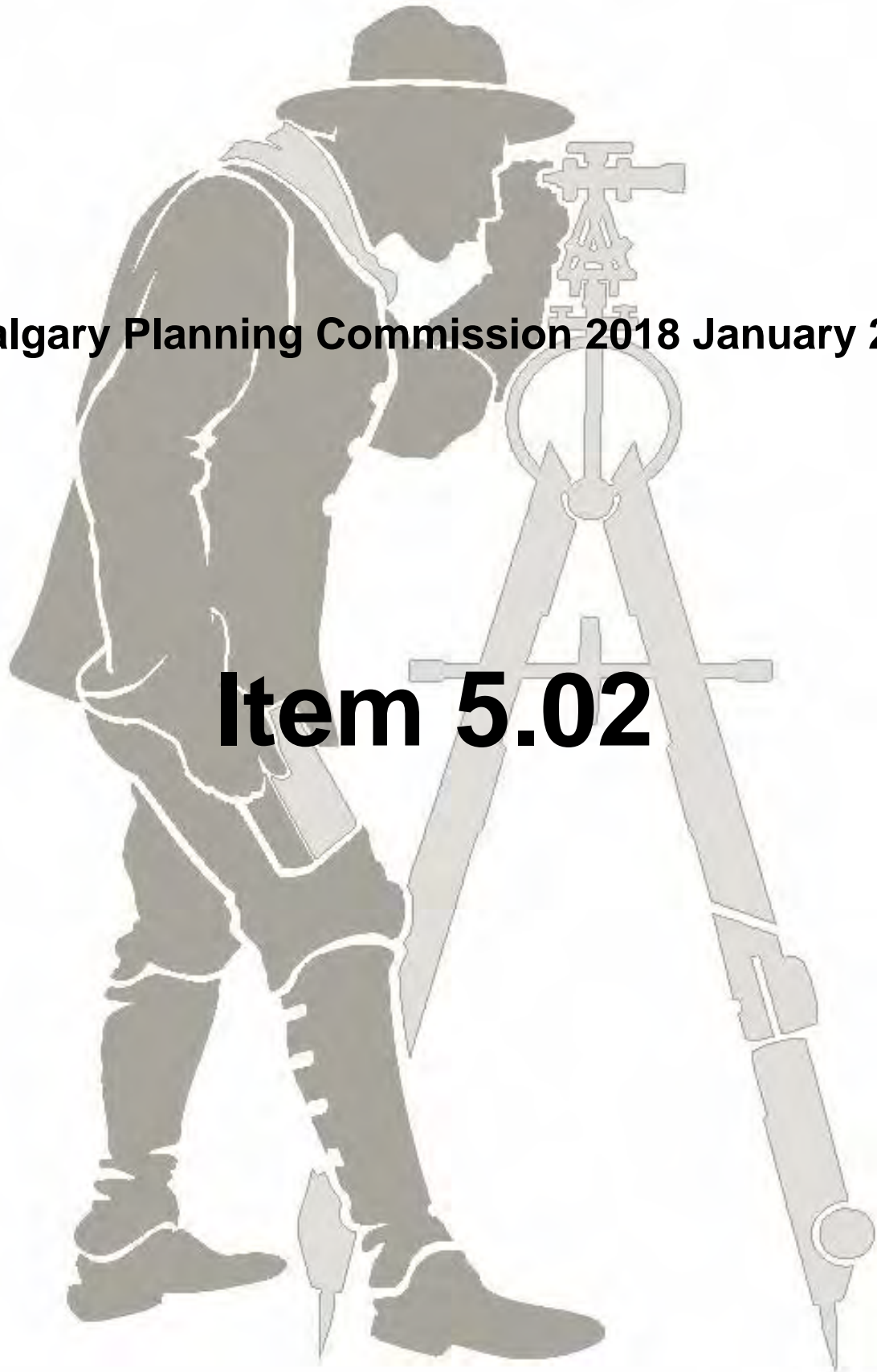
APPENDIX V

APPROVED OUTLINE PLAN LOC2014-0080



Calgary Planning Commission 2018 January 25

Item 5.02



DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
OUTDOOR CAFÉ, RESTAURANT: FOOD SERVICE ONLY – SMALL,
FITNESS CENTRE, CONVENIENCE FOOD STORE, SUPERMARKET,
RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

EXECUTIVE SUMMARY

This development permit application proposes an eight building commercial development located in the Saddle Ridge Community Activity Centre (CAC), in proximity to the future 88 Avenue NE LRT Station. The ±2.90 hectares (±7.17 acres) site is located at 9320 - 52 Street NE in the community of Saddle Ridge. The land use designation is Commercial Corridor 2 f1.0h26 (C-COR2 f1.0h26) District. The proposal was considered under the rules of the Land Use District and is consistent with City of Calgary policies and bylaws.

The proposed development is themed with a ‘bazaar’ concept including architectural and design motifs, and future selection of tenants to be based on local demographics.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Development Permit application.

REASON(S) FOR RECOMMENDATION:

The proposed development meets the intent of City policies. Specifically, the proposal is consistent with the overarching policies of the Municipal Development Plan (MDP) and conforms to the intent and direction of the Saddle Ridge Area Structure Plan (ASP). The proposed development includes a high quality of design, will provide opportunities for pedestrian accessible shopping for nearby residents, and creates strong pedestrian and street linkages to a future LRT station and Transit Hub. The proposed development meets the intent of the Land Use Bylaw, with supportable relaxations.

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
OUTDOOR CAFÉ, RESTAURANT: FOOD SERVICE ONLY – SMALL,
FITNESS CENTRE, CONVENIENCE FOOD STORE, SUPERMARKET,
RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

LOCATION MAPS



DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommends that Calgary Planning Commission **APPROVE** the development permit application with conditions (APPENDIX II).

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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FITNESS CENTRE, CONVENIENCE FOOD STORE, SUPERMARKET,
RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

Applicant:

Abugov Kaspar

Landowner:

Genstar Titleco Limited

Address

9320 - 52 Street NE

Legal Description

NW1/4 14-25-29-4

PLANNING EVALUATION

INTRODUCTION

The proposed development is an eight building commercial development located at 9320 - 52 Street NE in the community of Saddle Ridge, located within the Community Activity Centre and within 400 metres of a future LRT station and Transit Hub. The development proposes a 'main street' style private street along the northern property line, as established at the time of Outline Plan, to serve as connection to the future LRT site. Buildings are located around the perimeter of the site with a large central parking area. This format is in alignment with the Large Retail/Commercial Urban Design Guidelines and facilitates future intensification as per the Intensification Plan (APPENDIX I, Sheet DP0.3D).

SITE CONTEXT

The subject site is currently undeveloped. Located to the south across 88 Avenue NE are single detached dwellings. Located to the west across Savanna Street NE are townhouses. Located to the north are undeveloped parcels designated as Multi-Residential – Low Profile Support Commercial (M-X1) District and Multi-Residential – Medium Profile Support Commercial (M-X2) District. Located to the east across Savanna Drive NE is an undeveloped C-COR2 parcel and an undeveloped Special Purpose – City and Regional Infrastructure (S-CRI) District parcel. The S-CRI parcel will accommodate a future Transit Hub; however, this infrastructure is unfunded and there is no timeline for its development. A Transit Hub is defined as a place of connectivity where different modes of transportation (walking, cycling, bus, and rail transit) come together, and where there is an attractive, intense, and diverse concentration of uses and amenities around a major transit station.

Outline Plan and Land Use Redesignation for the area were approved as per Savanna Stage 2 in 2015. The entire subject site sits within the 400 metre Transit Oriented Development radius of the future LRT station. The design and location of the private street along the northern property line of the subject site were established as part of the Outline Plan. The private street is envisioned as a primary pedestrian linkage from residential districts to the west to the future LRT station and Transit Hub.

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RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
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MAP 14NE

LAND USE DISTRICTS

The Commercial – Corridor 2 (C-COR2) District accommodates a wide range of commercial uses and allows for residential development above main floor commercial uses. Modifiers on the land use district limit the subject site to a maximum floor area ratio of 1.0, and a maximum height of 26 metres. The proposed development meets the rules of the land use district, with minor supportable relaxations described below.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP). The proposal will contribute to a healthy environment, healthy economy, and high quality of life, by providing a safe and efficient pedestrian-accessible shopping area in close proximity to a future LRT station and Transit Hub.

Municipal Development Plan (MDP)

The subject site is located in the Developing (Planned Greenfield with Area Structure Plan) area of the city according to Map 1, Urban Structure. Community Activity Centres are located central to a number of residential communities or business areas, may be located at transit stations, and are intended to accommodate a broad mix of uses. The proposed development includes commercial and office uses, and will compliment other uses within the CAC including low- and medium-density residential uses, parks, and a Transit Hub. The proposed development will contribute to a vibrant, transit-supportive, mixed-use CAC, in alignment with MDP policies.

New Communities Planning Guidebook (MDP Volume 2 Part 1)

The New Communities Planning Guidebook indicates that development within a Community Activity Centre should consist of small and medium format retail uses, be integrated with other uses on the same or different sites, include a site for a community-scale food store, and accommodate employment uses. The proposed development is aligned with these policies.

Saddle Ridge Area Structure Plan (ASP)

The Saddle Ridge ASP identifies the location of the subject site as a Community Activity Centre. Commercial development in the CAC should consist of small and medium format retail uses, shall be integrated horizontally within the CAC and/or vertically within buildings, and should include a site for a supermarket. The proposed development is aligned with these policies. Office and Child Care Service uses are encouraged within the CAC and are proposed in this application. A minimum Floor Area Ratio (FAR) of 0.3 is recommended, with a FAR of 0.29

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RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

provided. The slight discrepancy is considered appropriate as the proposed site plan reserves a meaningful area for future intensification if and when market conditions permit, and meets the overall intent of the policy for the area.

The subject site is located within the Transit Station Planning Area as defined in the ASP, in which development should be in accordance with The City's Transit Oriented Development Policy Guidelines.

Transit Oriented Development Policy Guidelines

The Transit Oriented Development Policy Guidelines apply to development of areas typically within 600 metres of a transit station, including where an LRT station will eventually develop. The guidelines encourage higher-density, walkable, mixed-use environments within station areas in order to optimize use of infrastructure, create mobility options, and benefit local communities and city-wide transit riders. The proposed development includes transit supportive land uses, provides appropriate density in proximity to the proposed station, creates a pedestrian-friendly design, and creates a sense of place in context with the local community through the 'bazaar' branding concept. The proposal is considered to be consistent with the Transit Oriented Development Policy Guidelines.

Large Retail/Commercial Urban Design Guidelines

The proposed development is aligned with the principles of the Large Retail/Commercial Urban Design Guidelines. The proposal builds the outside edges of the site, creates a primary retail street, and allows for future intensification, as indicated in the Intensification Plan. The site provides a safe, convenient, and comfortable pedestrian realm and will be appropriately linked to the future LRT station.

Land Use Bylaw 1P2007

The Land Use District for the subject site is Commercial Corridor 2 f1.0h26 (C-COR2) District. The proposed development is consistent with the purpose of this district with supportable relaxations as noted in the following table and discussed below:

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 RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
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 AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
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 SADDLE RIDGE (WARD 5)
 88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

| Bylaw Relaxations | | |
|---------------------------------------|--|--|
| Regulation | Standard | Provided |
| 808 Landscaping In Setback Areas | (1) Where a setback area shares a property line with an LRT corridor or street, the setback area must: (b) 1.0 trees and 2.0 shrubs: (i) per 35.0 m ² of Req. L.S. | Plans indicate 0(-18) trees in the South setback area and 11(-1) trees in the East setback area. |
| | (2) Where a setback area shares a property line with a parcel designated as a residential district the setback area must: (a) be a soft surfaced landscaped area; (b) 1.0 trees: (i) per 30.0 m ² of Req. L.S. (c) provide trees planted in a linear arrangement along the length of the setback | Plans indicate 3 (-34) trees in the North setback area. There are parking stalls where there should be soft landscaped area. |
| Landscaping Large Parking Areas | (3) Islands provided in the parking area must: (c) be a minimum area of 12.0 square metres with at least one side of the island being a minimum length of 2.0 metres; | Plans indicate two islands which do not meet the minimum required size. |
| | (4) Strips provided in the parking area must: (a) be provided every four (4) rows of motor vehicle parking stalls with no more than four (4) rows between strips; | Plans indicate two sections of the parking area with six rows of parking stalls before a strip is implemented. |
| 691 Planting Requirements | (2) 25.0 % of all trees required must be coniferous. | Plans indicate 14 (-13) coniferous trees provided within the designated areas. |
| | (3) Deciduous trees min. calliper of 50 mm, 50.0% of the provided trees must have min. calliper of 75 mm. | Plans indicate a total of 12 (-58) trees with a 75mm caliper have been provided. |
| | (4) Coniferous trees min. height of 2.0 m, at least 50.0% of the provided trees must have a min. height of 3.0m | Plans indicate 7 (-5) large coniferous trees. |

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 ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
 AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
 RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
 SADDLE RIDGE (WARD 5)
 88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST**

MAP 14NE

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| 699 Screening | When a parcel shares a property line with: (a) a parcel designated as a residential district or special purpose district, a fence with a max. height of 2.0 m must be provided for screening along the property line; | Plans do not indicate a fence along the north property line shared with a multi residential district. |
| 247 Outdoor Café | (f) has a maximum area of 25.0 square metres in C-COR2 districts. | Plans indicate the outdoor café for Building H is 82.05m ² (+57.05m ²) and the outdoor café for Building C is 67.93m ² (+42.93m ²) |
| 279 Restaurant: Food Service Only - Medium | (c) must not have any openings, except emergency exits, loading bay doors or non-opening windows, on a façade that faces a residential district or abuts a lane separating the parcel from a residential district; | Plans indicate exit doors facing the residential district on B1 and C2 where a medium restaurant is indicated. |
| | (d.1) must not be within 45.0m of a residential district when the use is located within the C-COR2 Districts, which must be measure from the building containing the use to the nearest property line of a parcel designated as a residential district. | Plans indicate both Buildings B1 and C2 are 23.08m (-21.92m) and 23.43m (-21.58m) from the nearest residential district. |
| 282 Restaurant: Licensed – Medium | (d.1) must not be within 45.0m of a residential district when the use is located within the C-COR2, Districts, which must be measure from the building containing the use to the nearest property line of a parcel designated as a residential district. | Plans indicate Units H1 and C1 are 23.14m (-21.86m) from the nearest residential parcel to the North. |
| Motor Vehicle Parking Stalls | Total requirement for the parcel: 408 stalls | Plans indicate 399 (-15) stalls. |

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MAP 14NE

Landscaping Relaxations

Relaxations for landscaping in setback areas are supported as the south and east setback areas contain utility rights-of-way which prohibit plantings, and the north setback is to be developed as a private street. The required trees and shrubs are located elsewhere on site and overall proposed plantings are in excess of bylaw minimums. Boulevard trees will also be planted along 88 Avenue NE by the area developer.

Relaxations for landscaping in the large parking area are supported as the overall plan provides adequate landscaping and includes safe, convenient, and attractive pedestrian connections throughout.

Restaurant and Café Rule Relaxations

Screening, prohibition of doors, and setback requirements along the north property line facing a residential district are not appropriate, as the north of the property is to be developed as a 'main street' style private street fronted on both sides with active uses. Future development of the parcels to the north of the subject site are expected to develop with at-grade commercial uses.

Relaxations for maximum patio sizes are intended to support an active and vibrant pedestrian realm.

Parking Relaxations

A parking study was reviewed and supports the proposed 15 stall relaxation.

SITE LAYOUT AND BUILDING DESIGN

Buildings are located around the perimeter of the parcel in order to appropriately frame the site. An east-west private street along the northern property line is designed with a 'main street' format of fine-grained storefronts, angled parking, and a comfortable pedestrian realm. Significant efforts were made towards providing continuous frontage and numerous storefronts along the private street, which provides a direct connection to the future LRT station.

Administration requested an Intensification Plan for this first-generation commercial development located within a future Transit Oriented Development area. As per the Intensification Plan provided, the large central parking area allows for future intensification of the site and the inclusion of residential uses if and when transit infrastructure development and market conditions support such redevelopment. This approach creates an appropriate balance between the current auto-oriented reality of the area and future opportunities for Transit Oriented Development.

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MAP 14NE

DESIGN REVIEW

The design was reviewed by City Wide Urban Design throughout the CPAG review process. A number of revisions were requested with reference to the City Wide Large Commercial Urban Design Guidelines, particularly regarding enhancing street interfaces through framing the streets with built form and enhancing the pedestrian connections through and around the site to the adjacent neighbourhoods.

As well, this application has been reviewed by the Urban Design Review Panel (UDRP). Upon review of the original submission UDRP provided a ranking of ‘Support with Conditions’. Significant changes were suggested by UDRP which reflected and reinforced staff direction. These changes were incorporated by the applicant, including relocation of buildings to appropriately frame the site and activate the private street. The revised site design was presented to UDRP and is fully supported. Full UDRP comments and the applicant’s responses are in APPENDIX IV.

LANDSCAPING

The site is abundantly planted with trees and shrubs in excess of the total numbers required by the Land Use Bylaw. Bylaw relaxations are granted for setback areas where plantings are not possible due to utility rights-of-way and the provision of a private street along the northern property line.

A ‘central plaza gathering area’ is provided within the parking area, which includes hard surface landscaping, benches, and tables.

TRANSPORTATION NETWORK

The proposed development is bounded by 88 Avenue NE to the south, Savanna Street to the west, Savanna Drive to the east. The future 88 Avenue LRT station, Park & Ride, and Transit Hub are located to the east of the site, across Savanna Drive. A private street runs east-west along the northern property line of the site and will serve as a pedestrian connection from the broader community to the future LRT station and Transit Hub.

The 88 Avenue LRT station is currently unfunded; however, it is first station beyond the existing (north) blue line LRT. The existing Saddletowne station is located within approximately 1 kilometre of the site directly south along 60 Street NE.

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MAP 14NE

Near-term transit service (pre-Blue line extension) will consist primarily of service to and from Saddletowne station, potentially via a slight modification to routes 59 and/or 159. Future transit plans include: a local route originating at the 88 Avenue station which runs along 88 Avenue NE, providing service to businesses and residents along the corridor; a local route which travels between the 88 Avenue and Saddletown stations to provide service to residents of Saddleridge Cell 'D'; and a regional service which travels along the 60 Street/52 Street NE corridors over a longer distance. The development is required to provide a transit shelter for the stop located on 88 Avenue, directly adjacent the site.

SITE ACCESS & TRAFFIC

Vehicular access to the site is provided via a right-in/right-out at 88 Avenue NE, as well as two driveways located along the private street connection to the north. A Transportation Impact Assessment (TIA) was submitted in support of the proposed development permit application. The TIA identified requirements for signalization at the intersection of Savanna Street with the private east-west street.

PARKING

A parking study was reviewed as part of this application and supports the proposed variance to the bylaw parking requirement.

All parking for the subject site is provided at grade. Angled parking is provided along the private street, and the remainder is provided in a large central parking area. Future intensification plans for the site anticipate redevelopment of the central at-grade parking area and the requirement for underground parking.

UTILITIES & SERVICING

Water, sanitary, and storm sewer mains are available and can accommodate the proposed development of the subject site. A Development Site Servicing Plan (DSSP) will be required prior to release of this development permit.

ENVIRONMENTAL ISSUES

Environmental review was conducted at the time of Outline Plan and no environmental issues were identified. No further review was required for this development permit application.

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MAP 14NE

ENVIRONMENTAL SUSTAINABILITY

The proposed development will allow a significant number of people to access shopping and services by walking, cycling, and ultimately by LRT. The site design allows for intensified mixed-use development if and when transportation infrastructure development and market conditions permit.

PUBLIC ENGAGEMENT

Community Association Comments

The Saddle Ridge Community Association reviewed the application and expressed support, particularly in regards to the site design, landscaping, and variety of proposed businesses, which they feel will be a valuable addition to the neighbourhood.

Citizen Comments

No comments were received by the CPC report submission date.

Public Meetings

No public meetings were held by the applicant or administration.

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MAP 14NE

APPENDIX I

DEVELOPMENT PERMIT PLANS

| ISSUED FOR: | DATE |
|--------------------|------------------|
| DEVELOPMENT PERMIT | March 17, 2017 |
| DTR RESPONSE | August 7, 2017 |
| DTR2 RESPONSE | December 4, 2017 |



Savanna Bazaar

Calgary

Alberta



ABUCOV - KASPAR

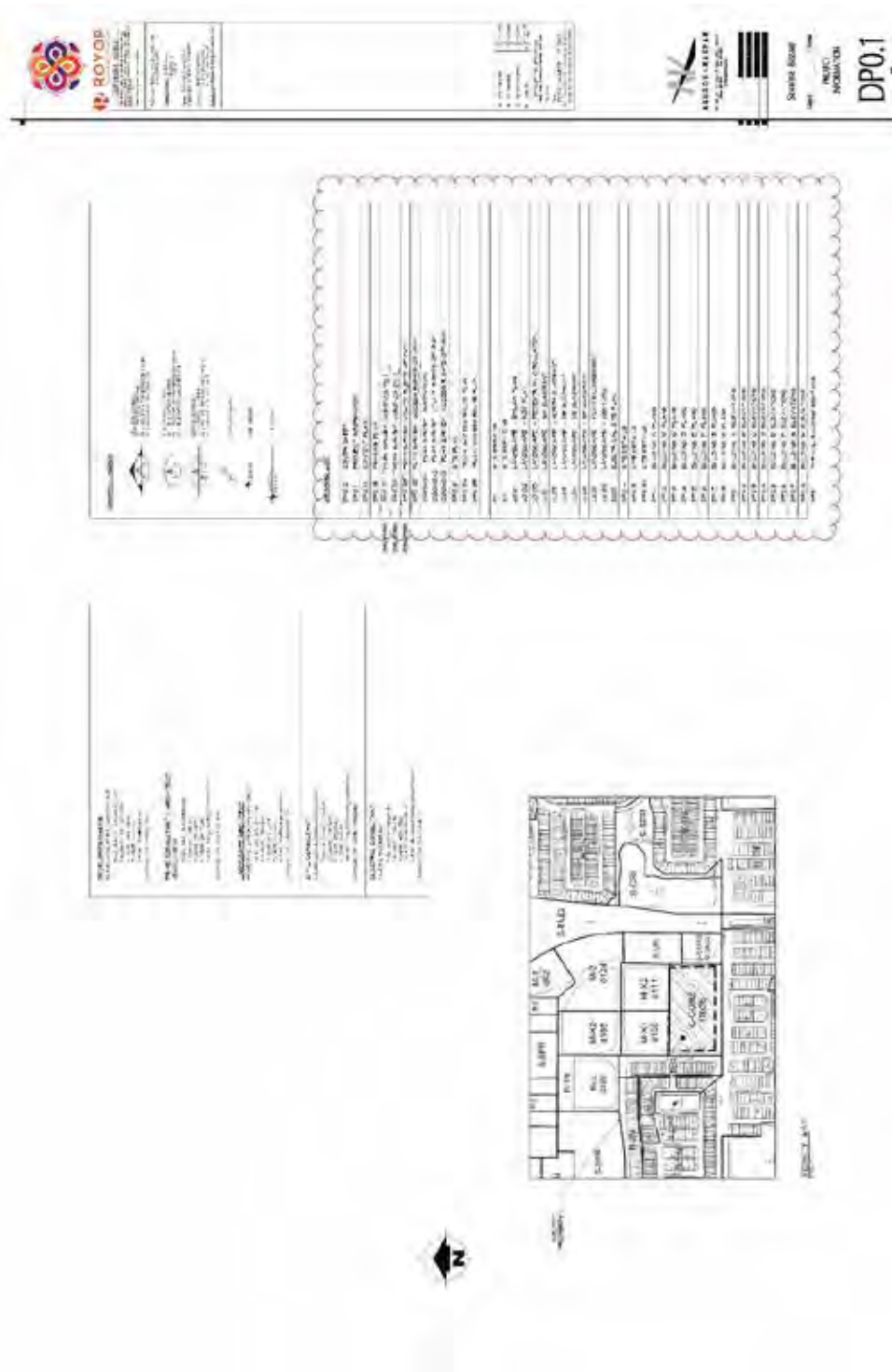


BUSINESS
EXCEPTIONAL
EXPERIENCES

ROYOP
DEVELOPMENT - EXPERIENCES

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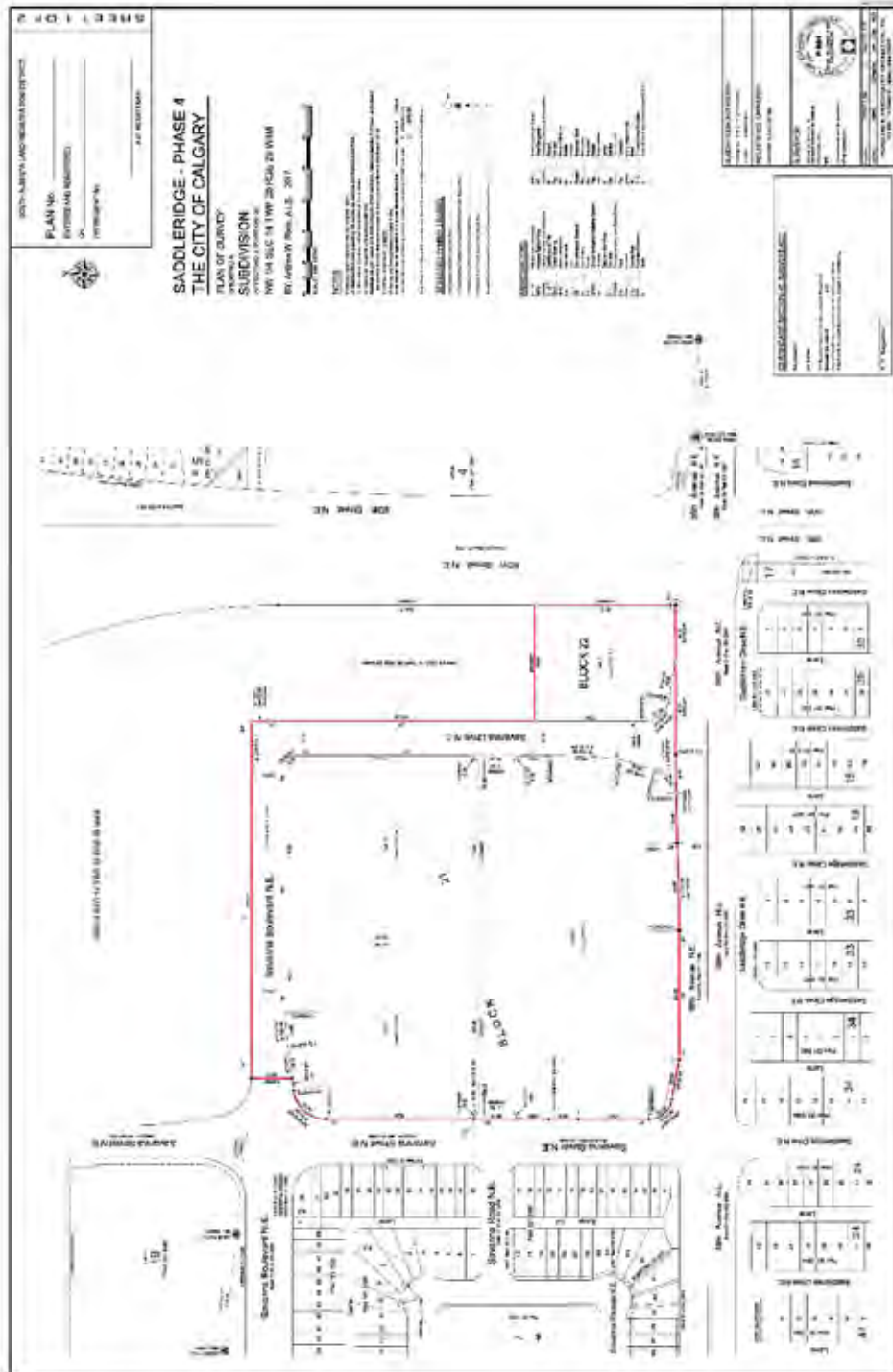
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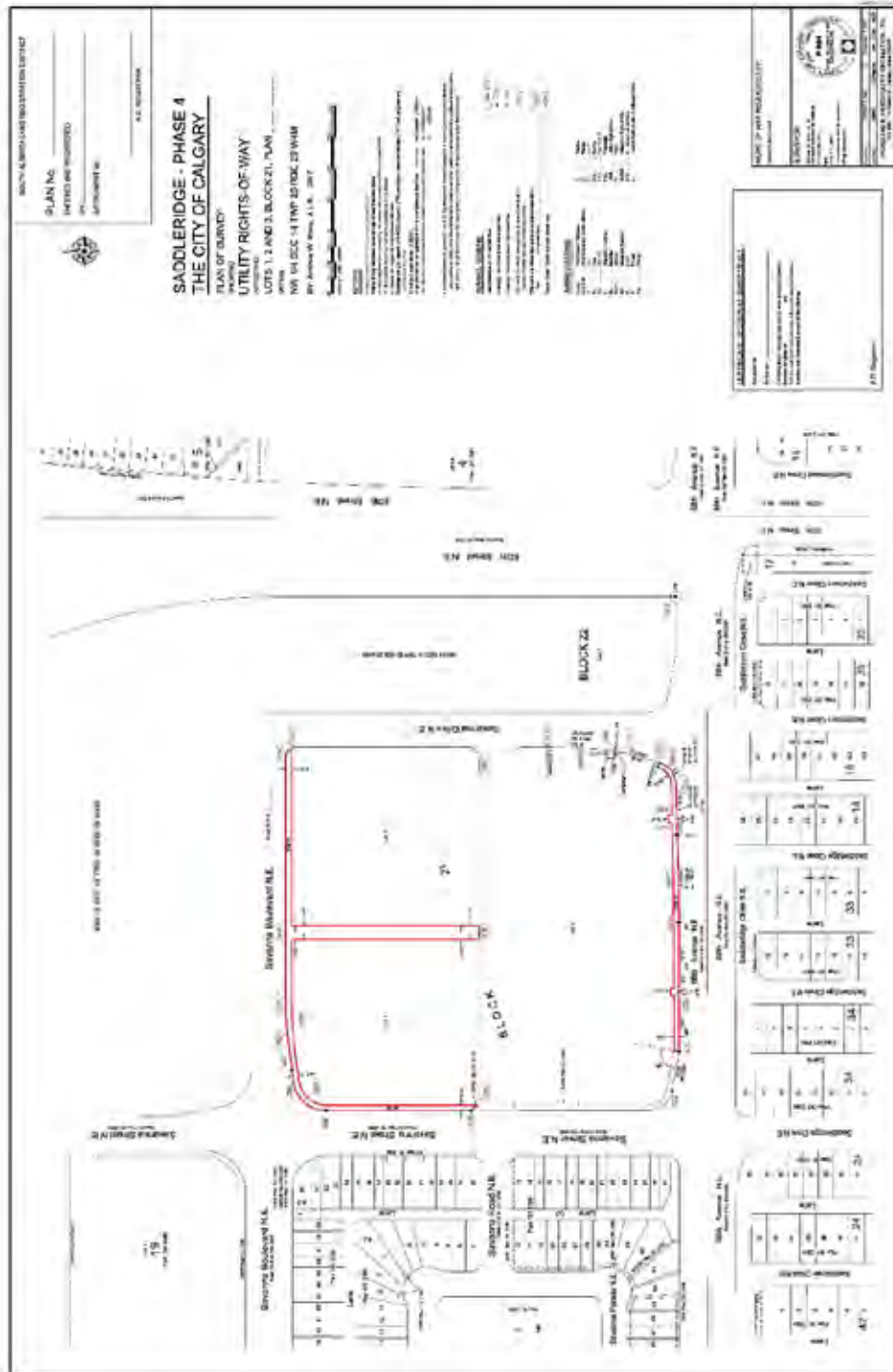
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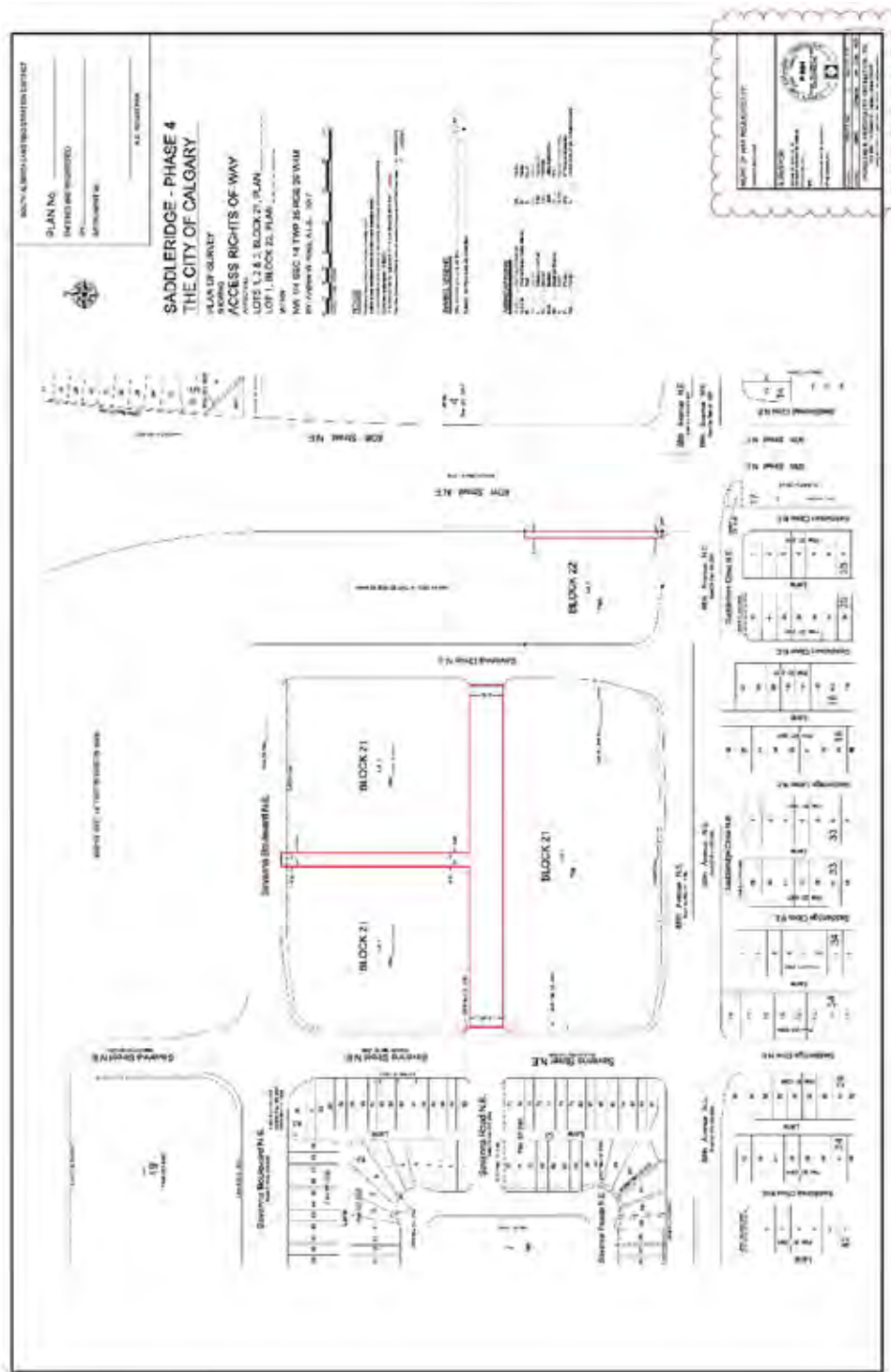
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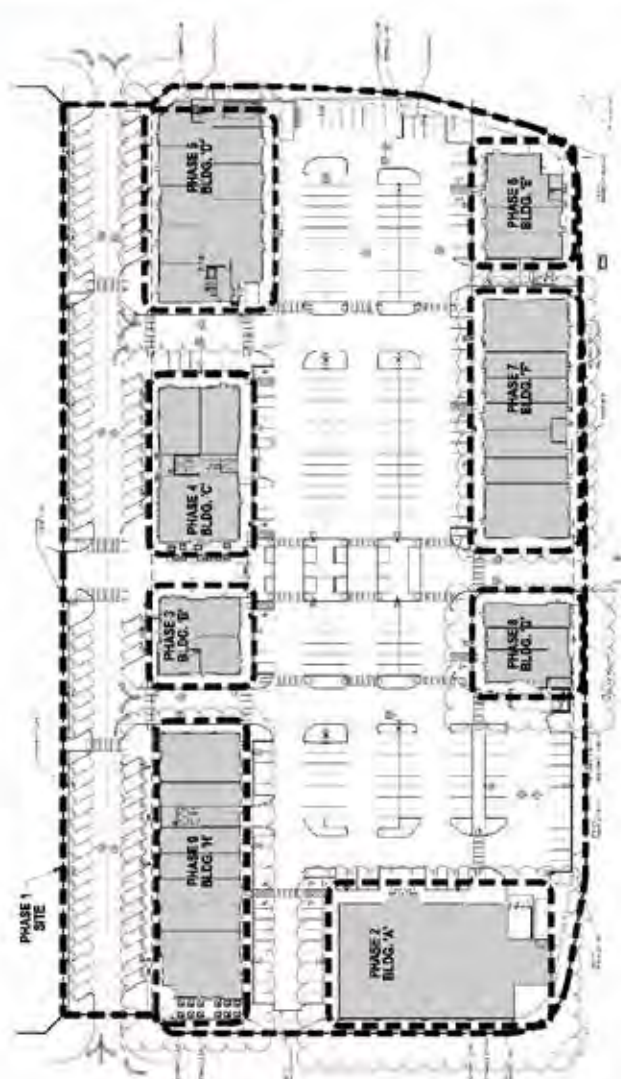
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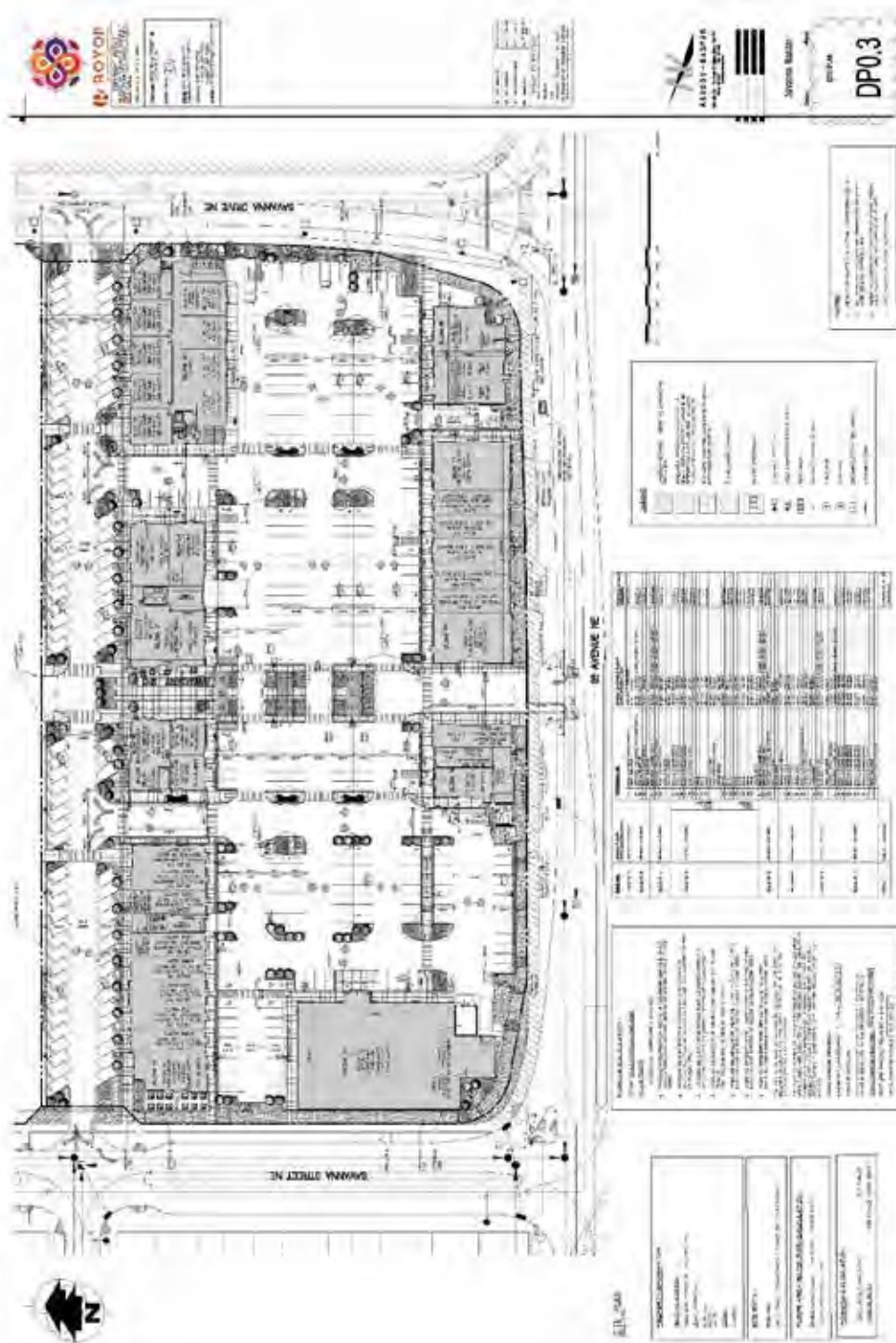
MAP 14NE



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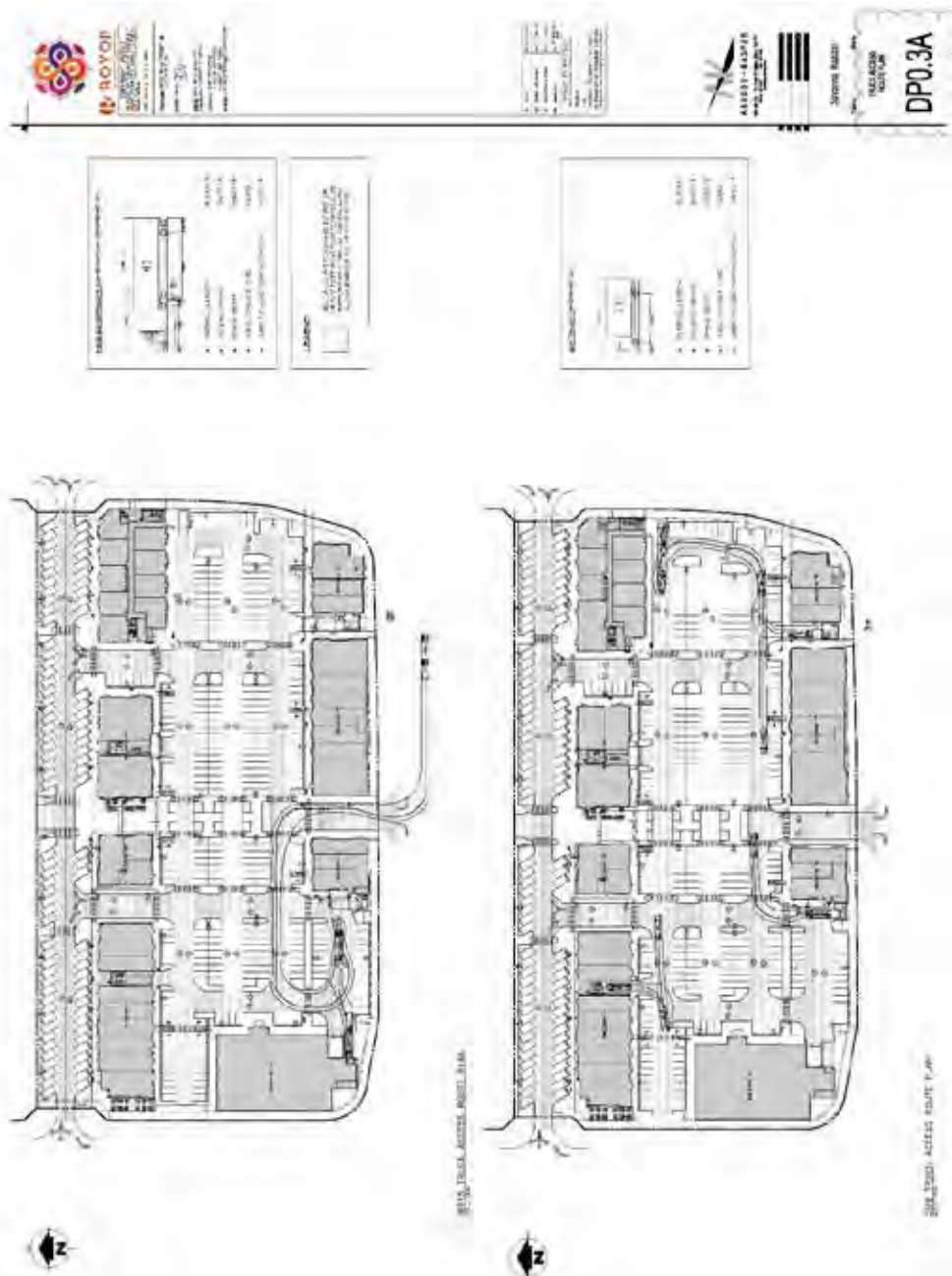
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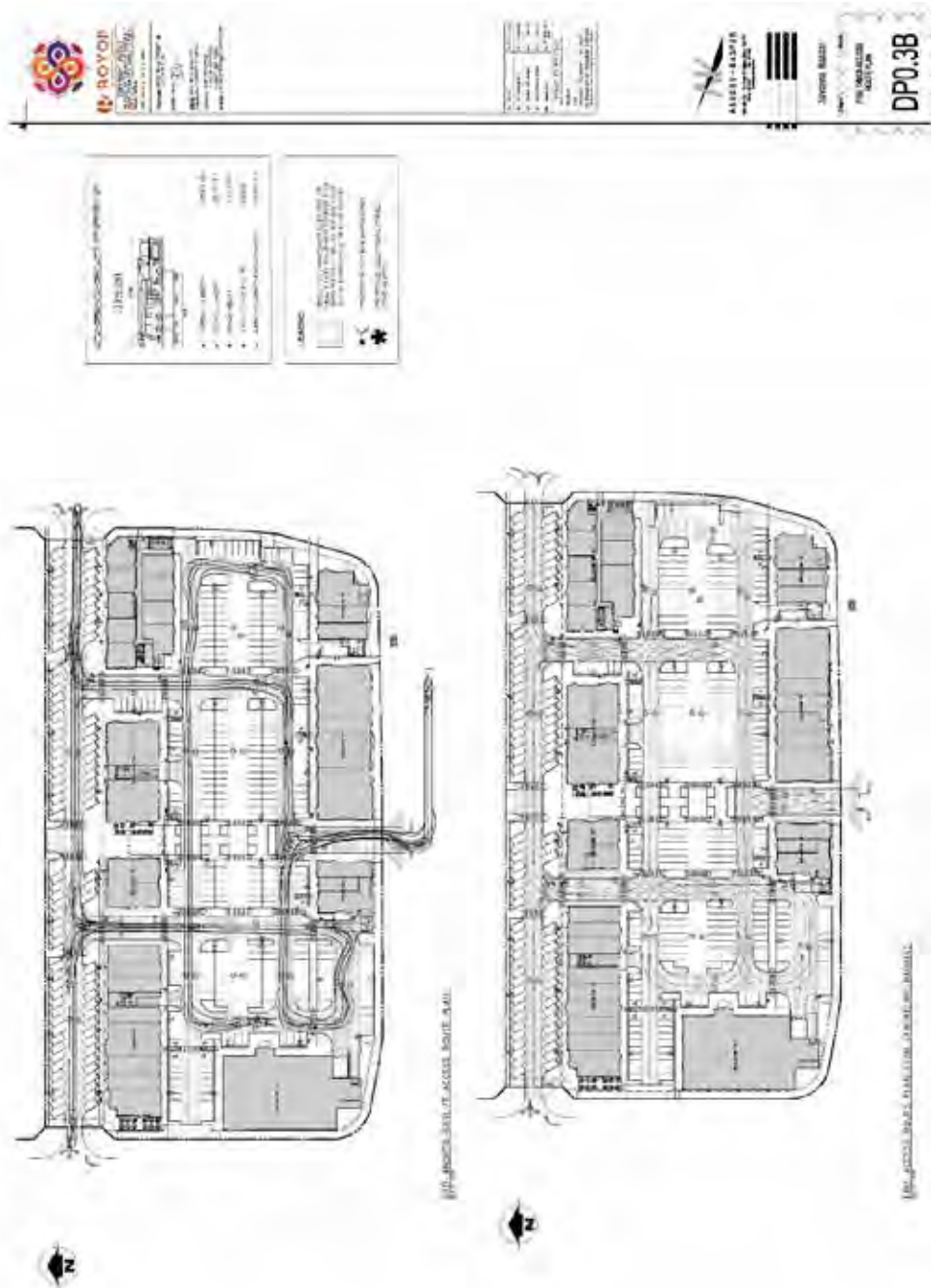
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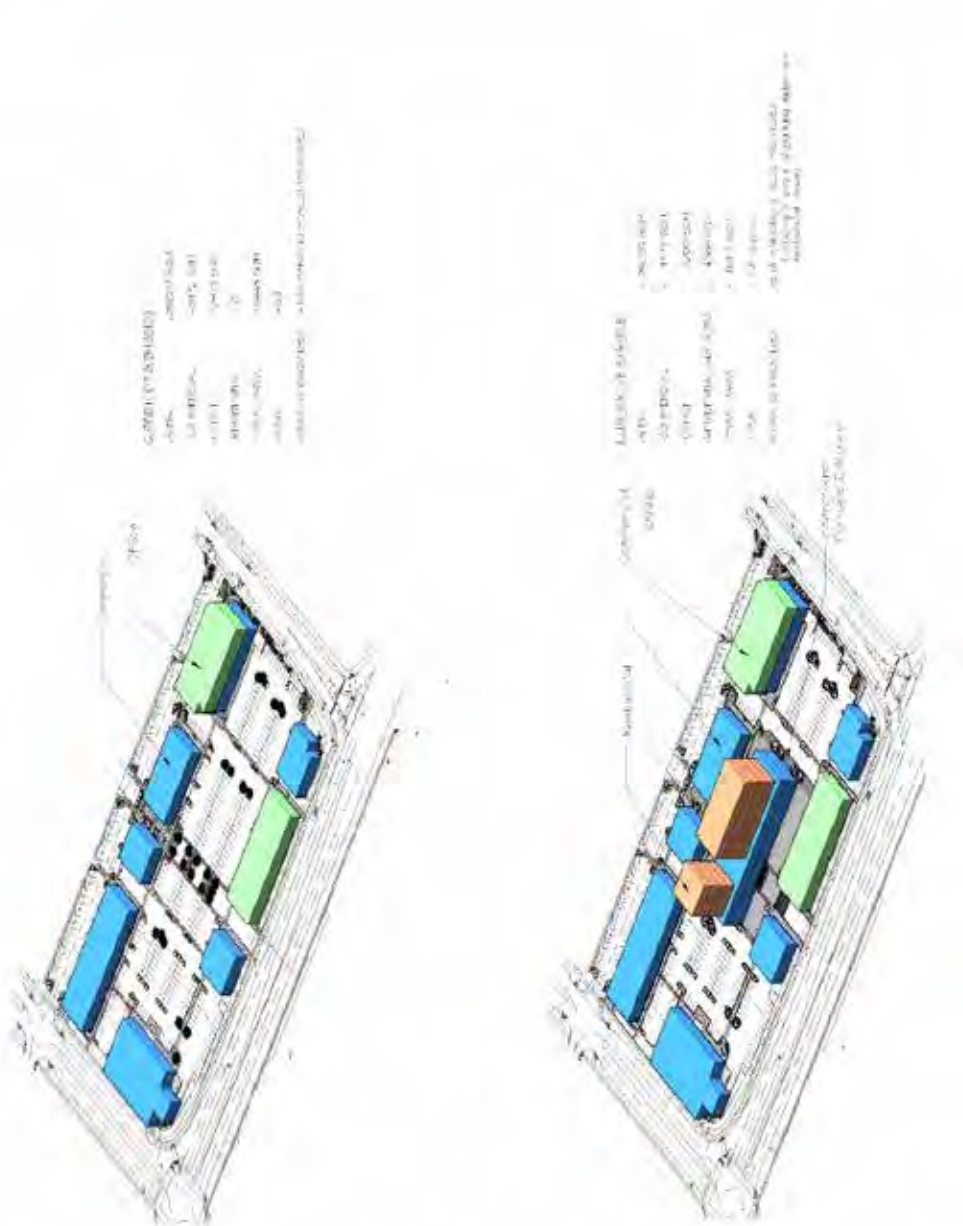
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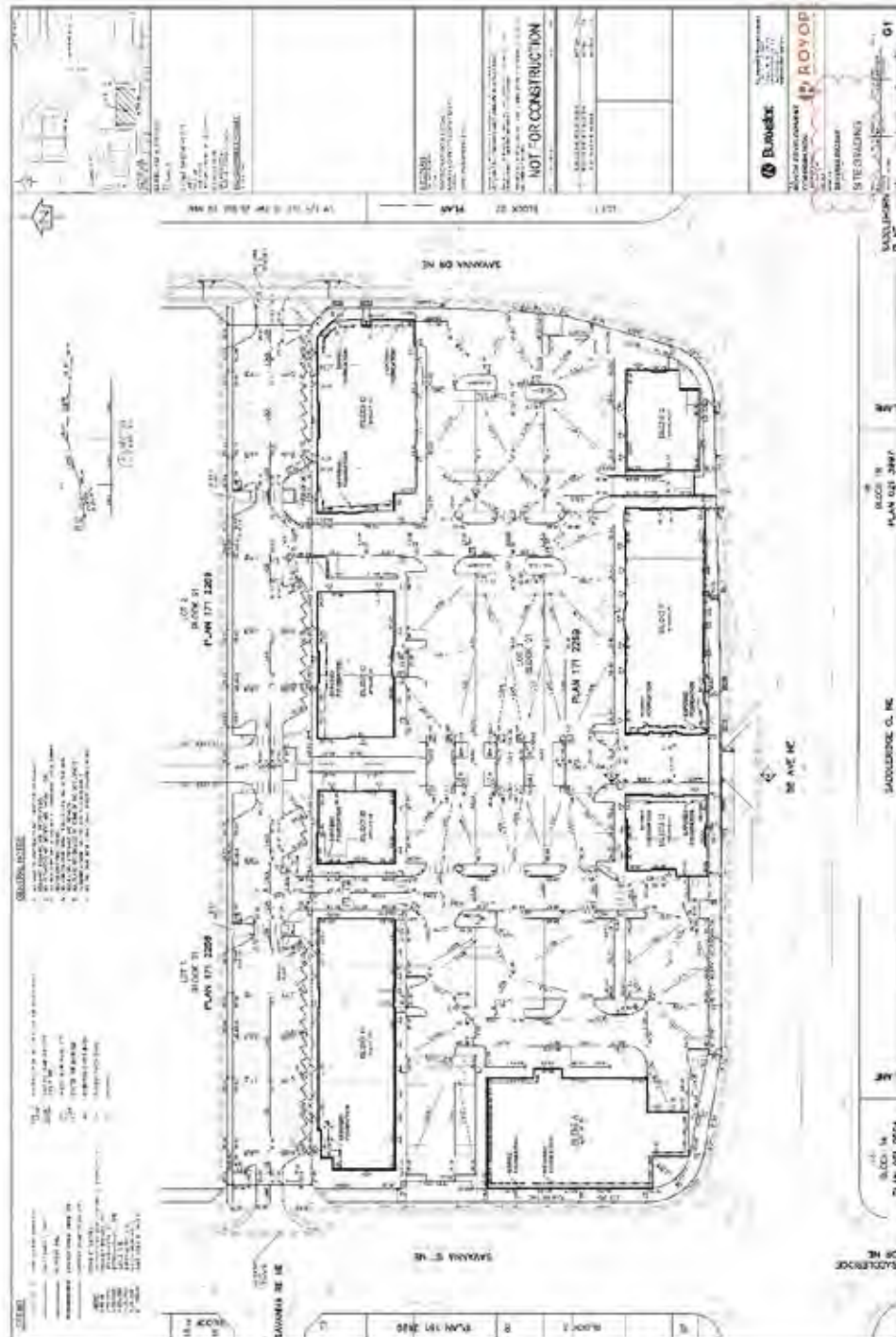
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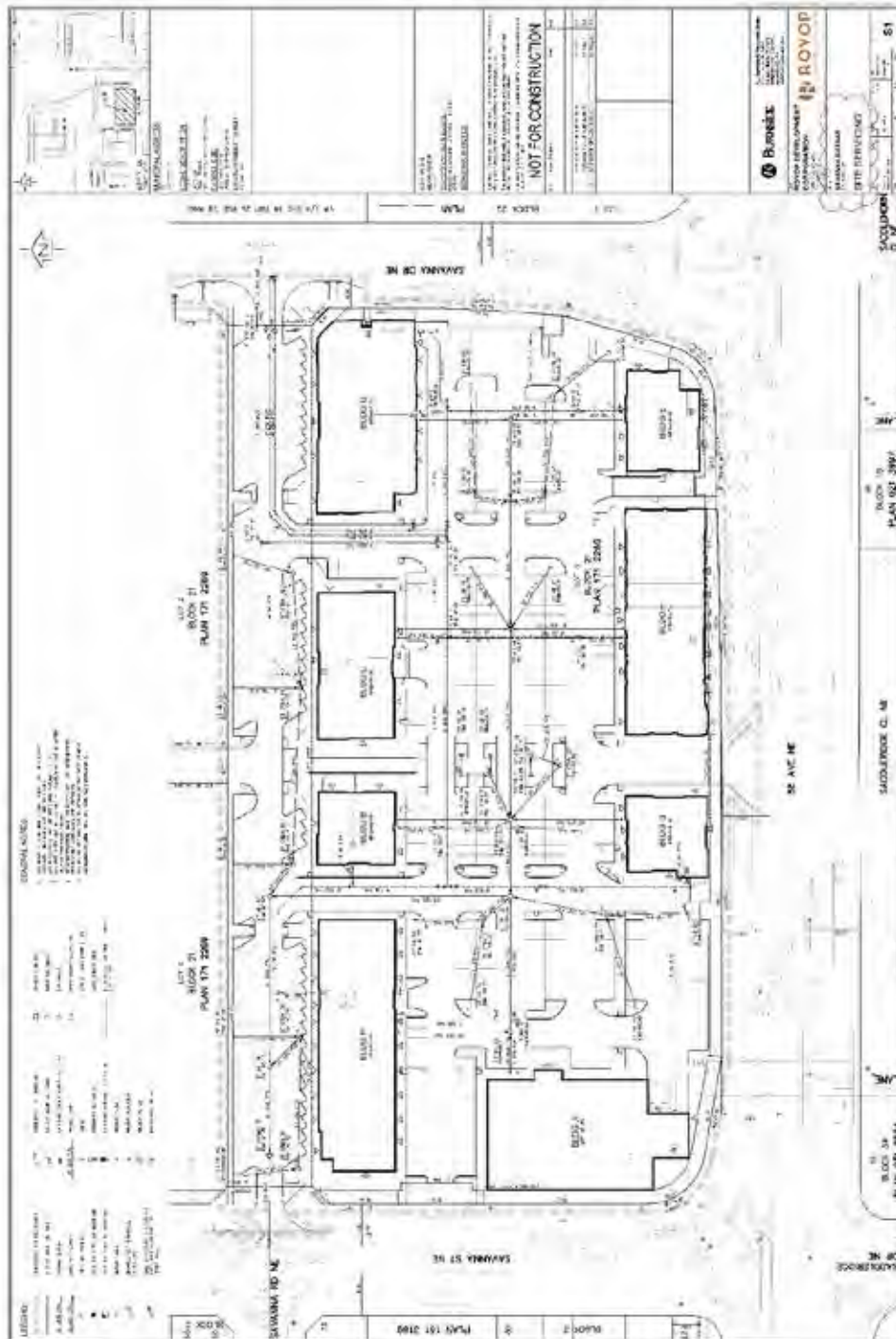
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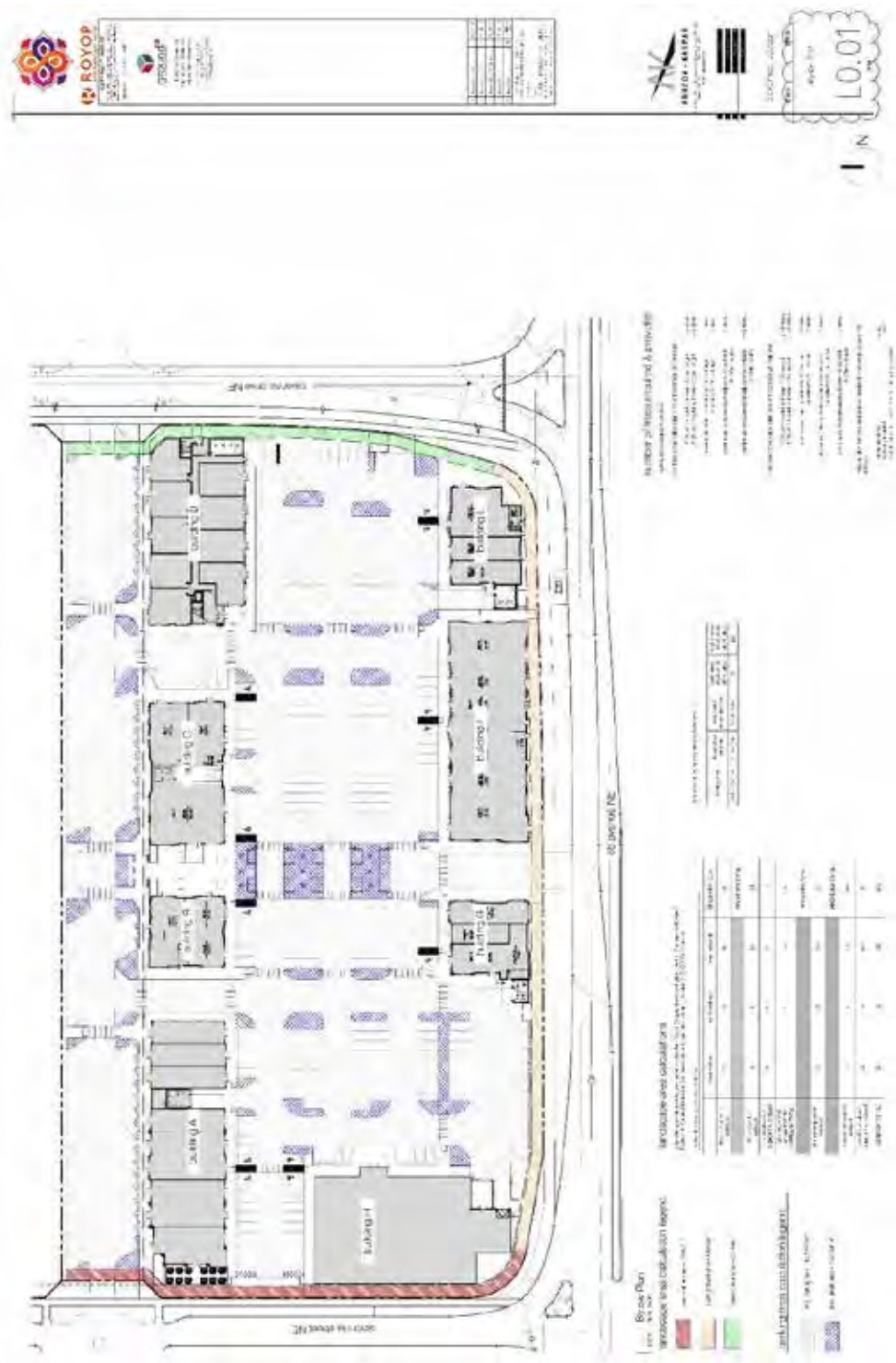
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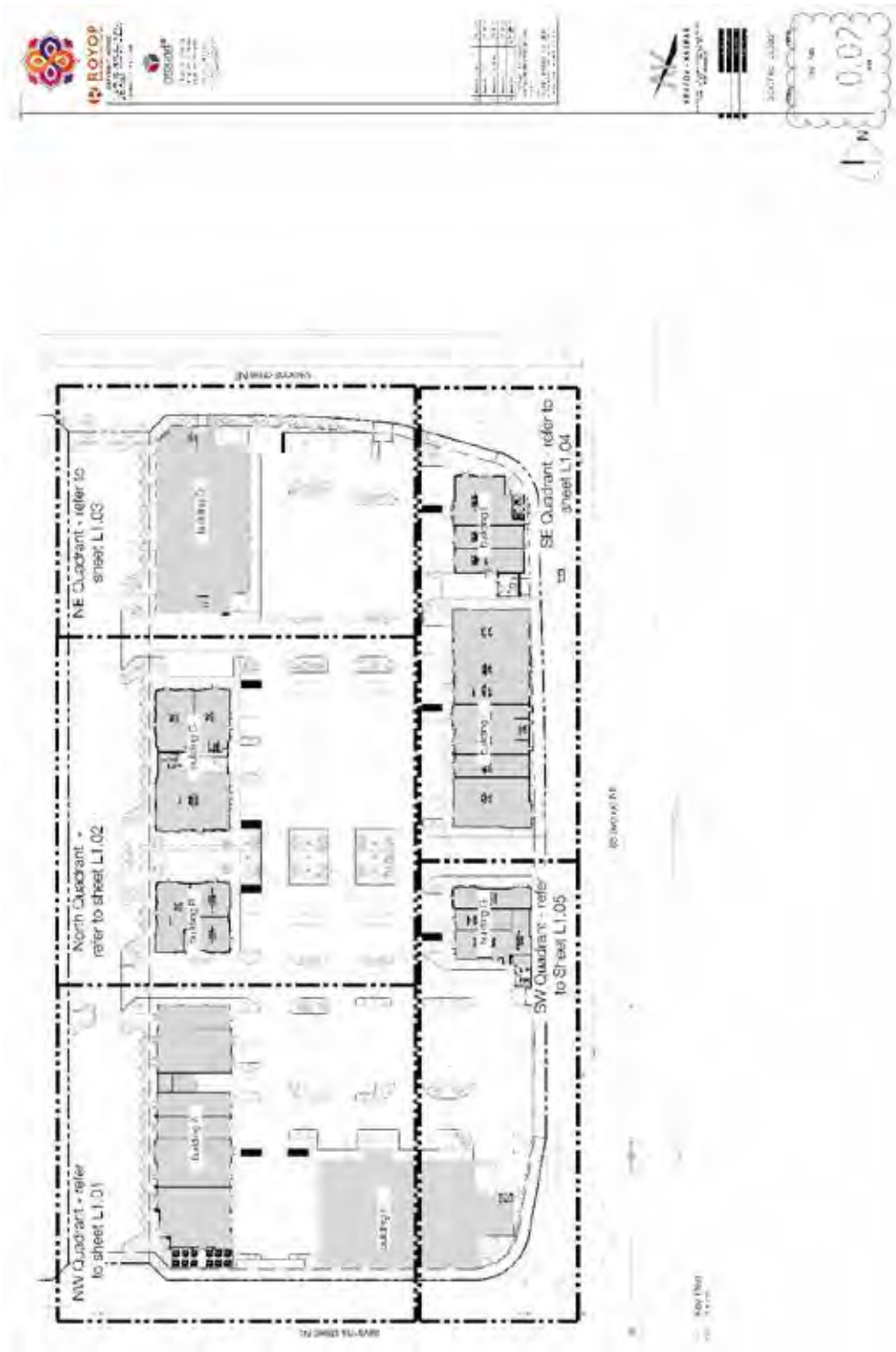
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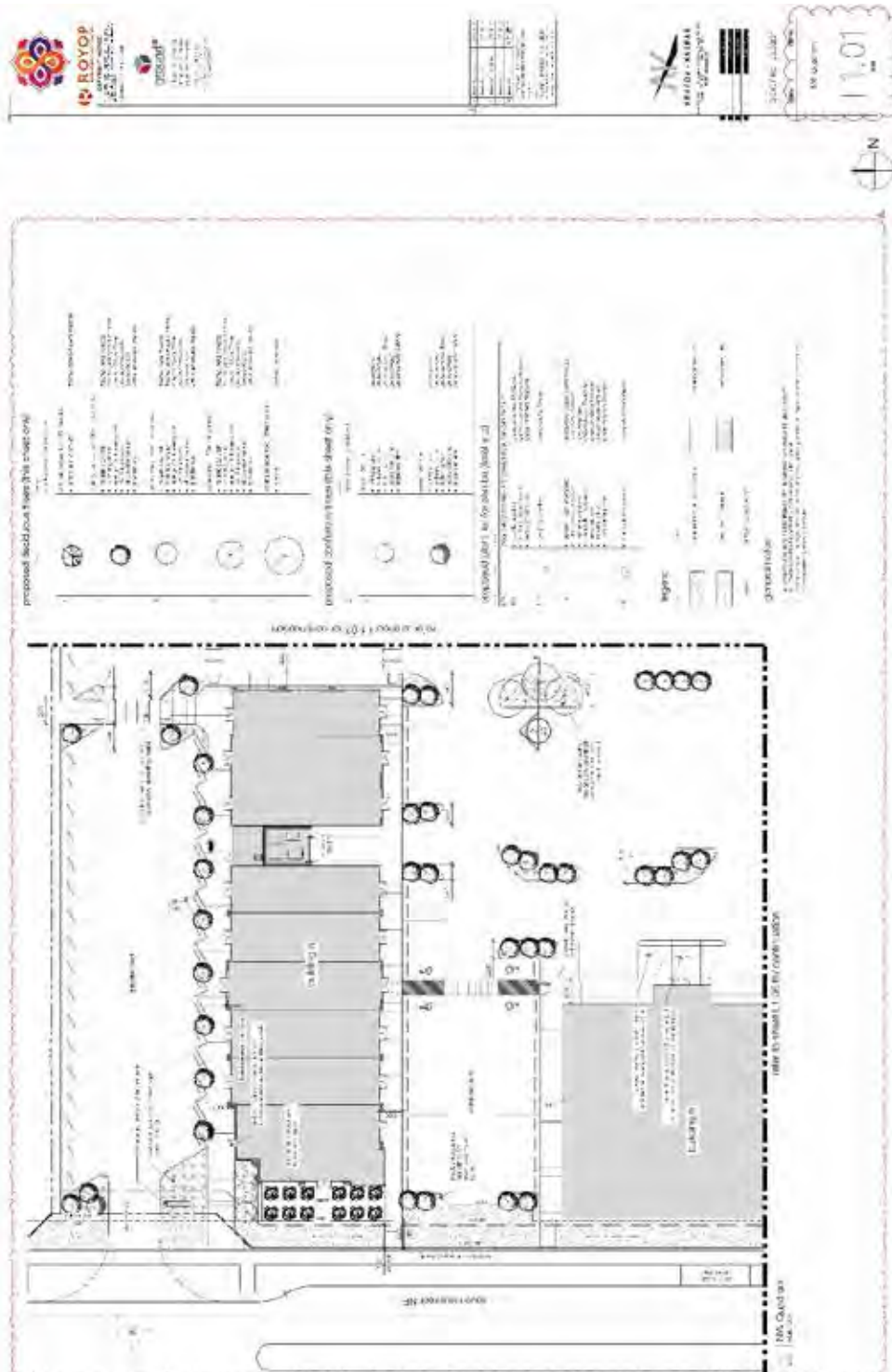
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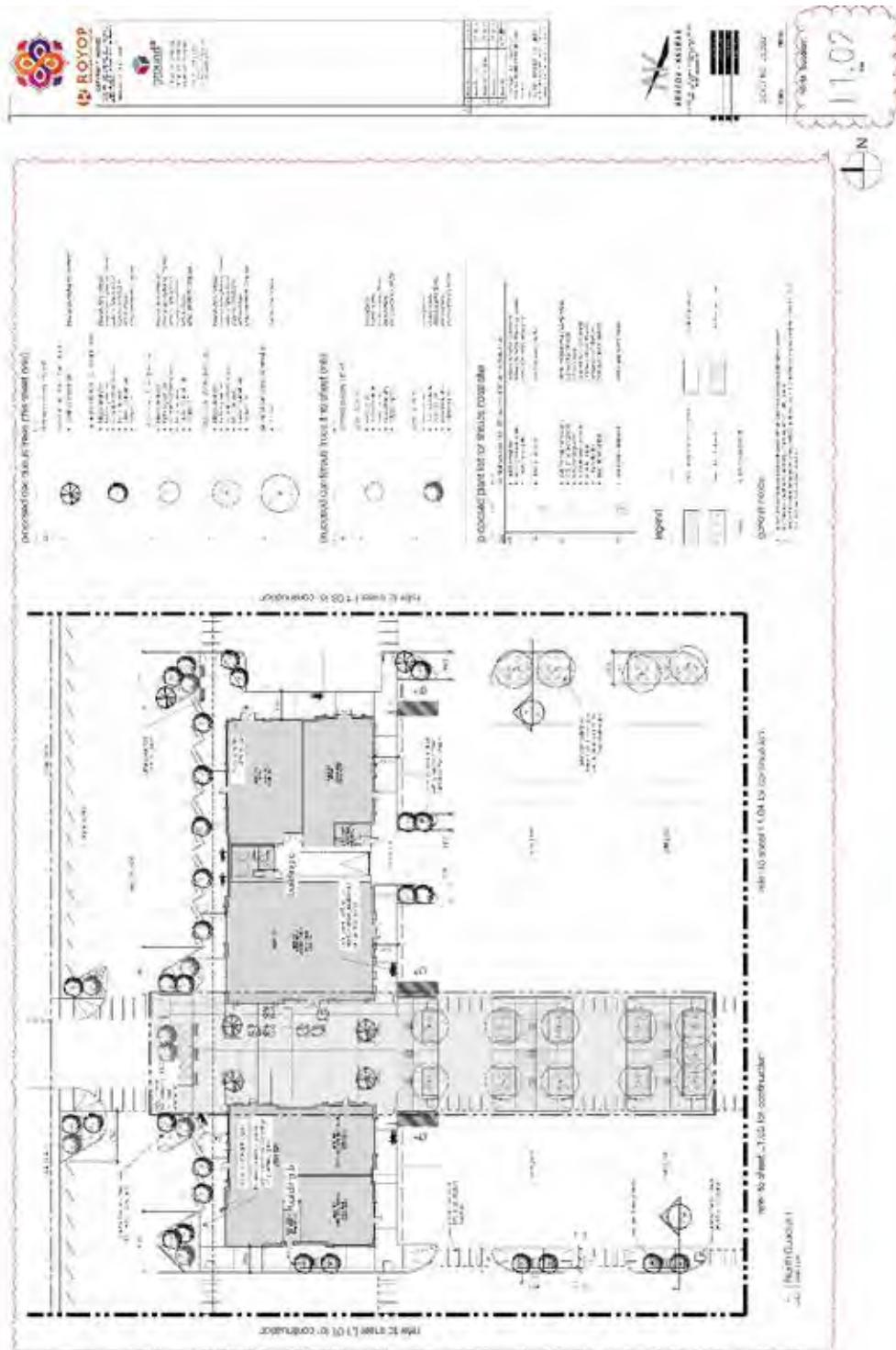
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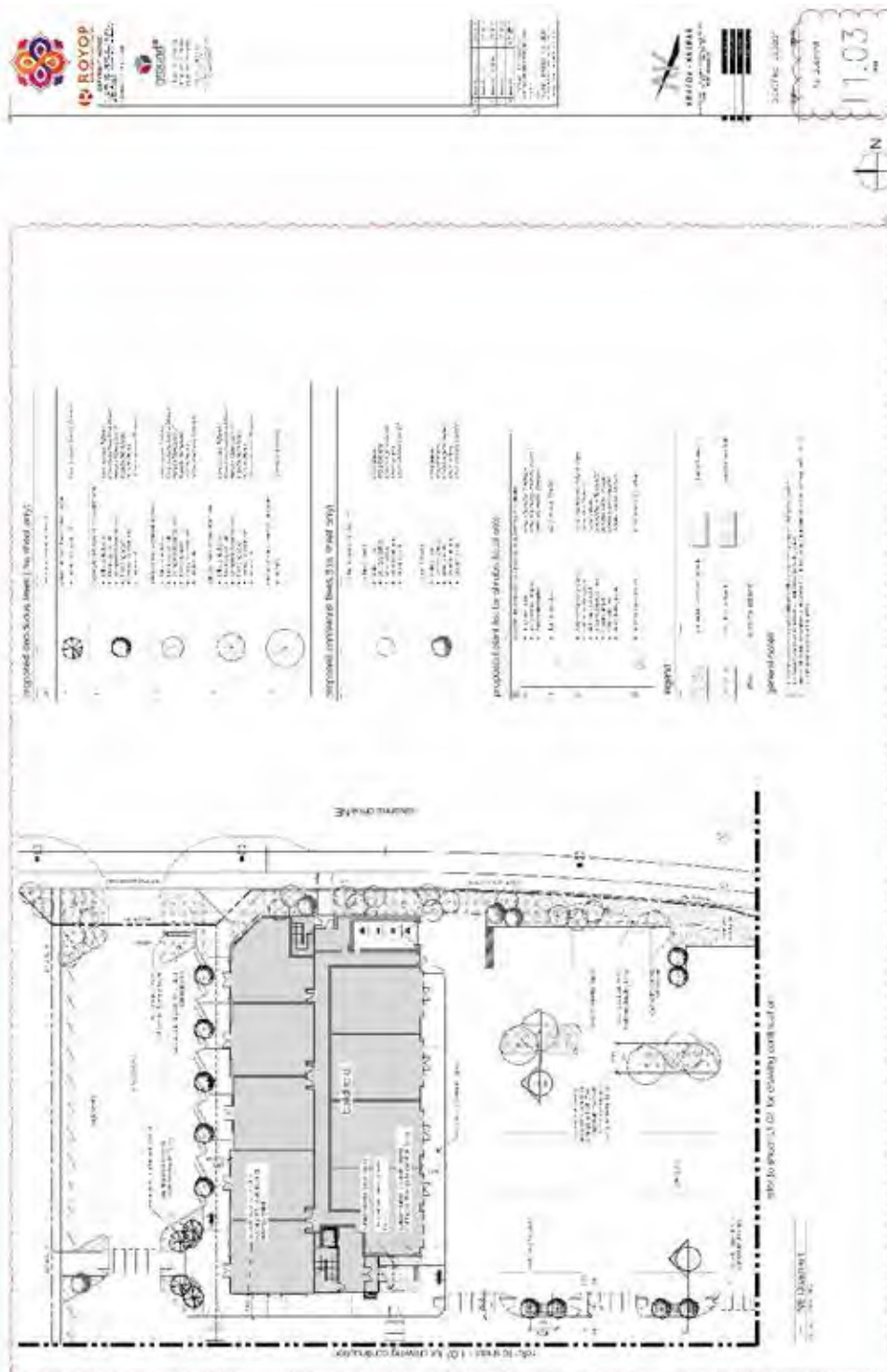
MAP 14NE



S. Gagnon

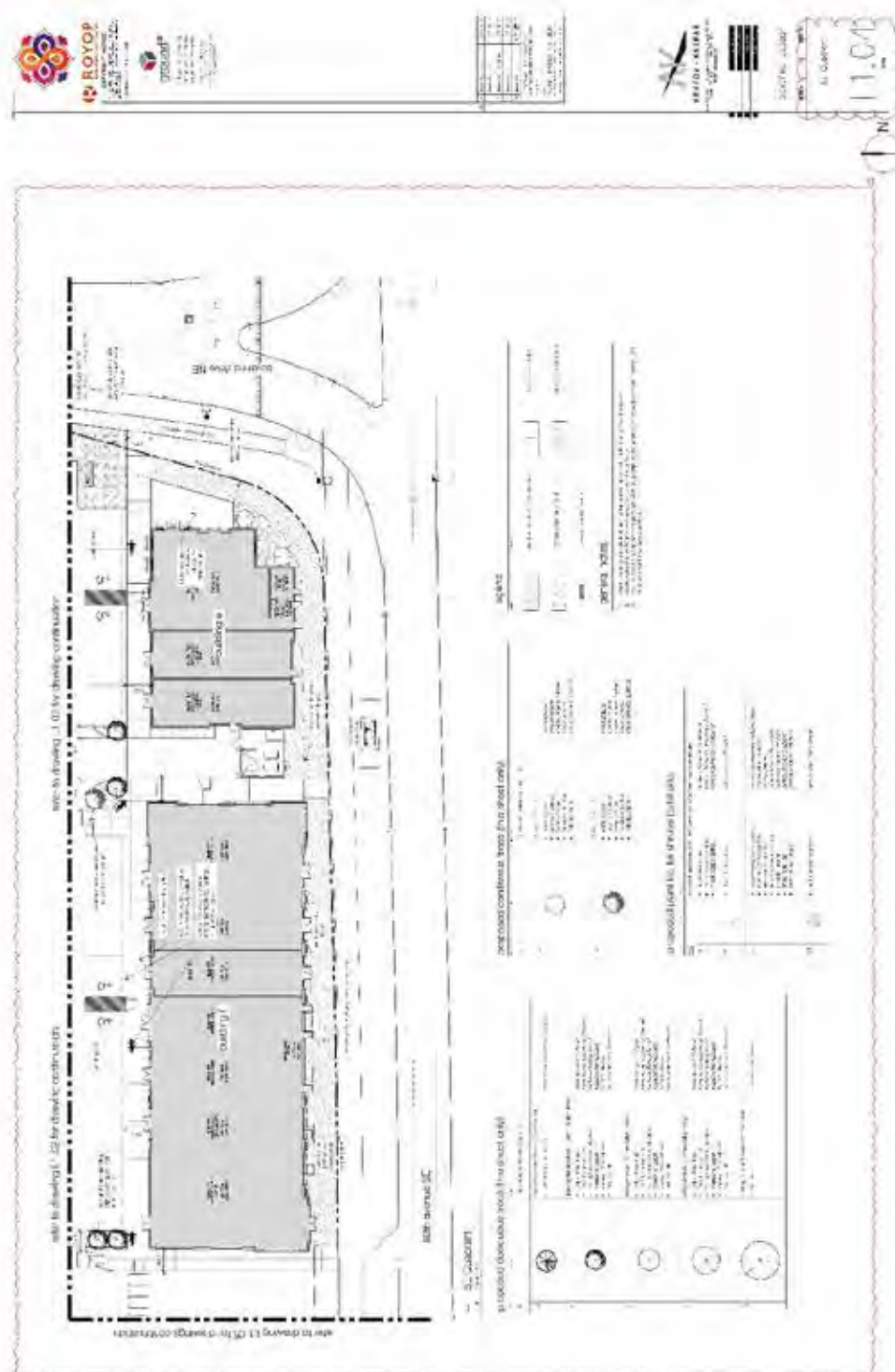
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88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE



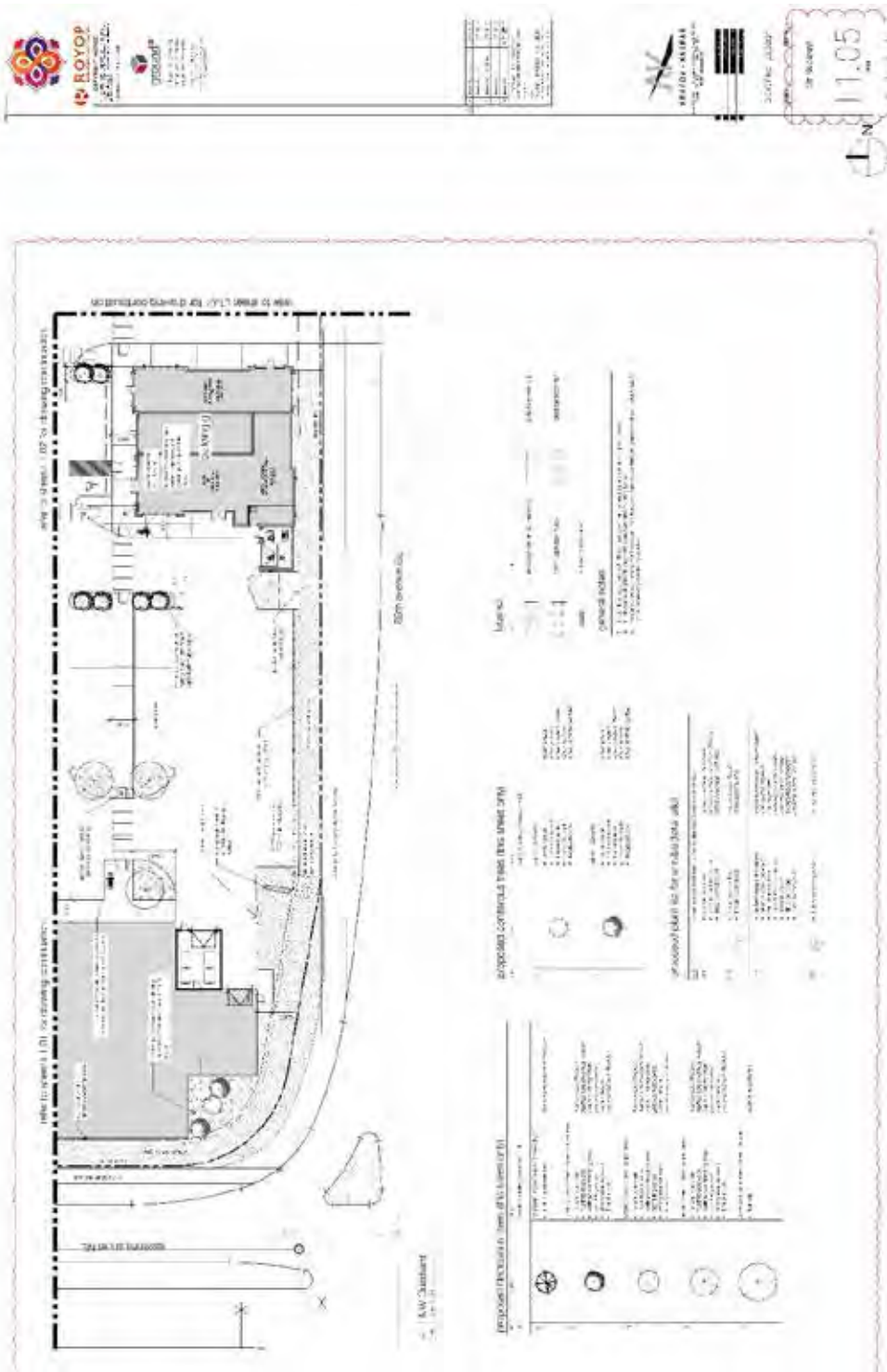
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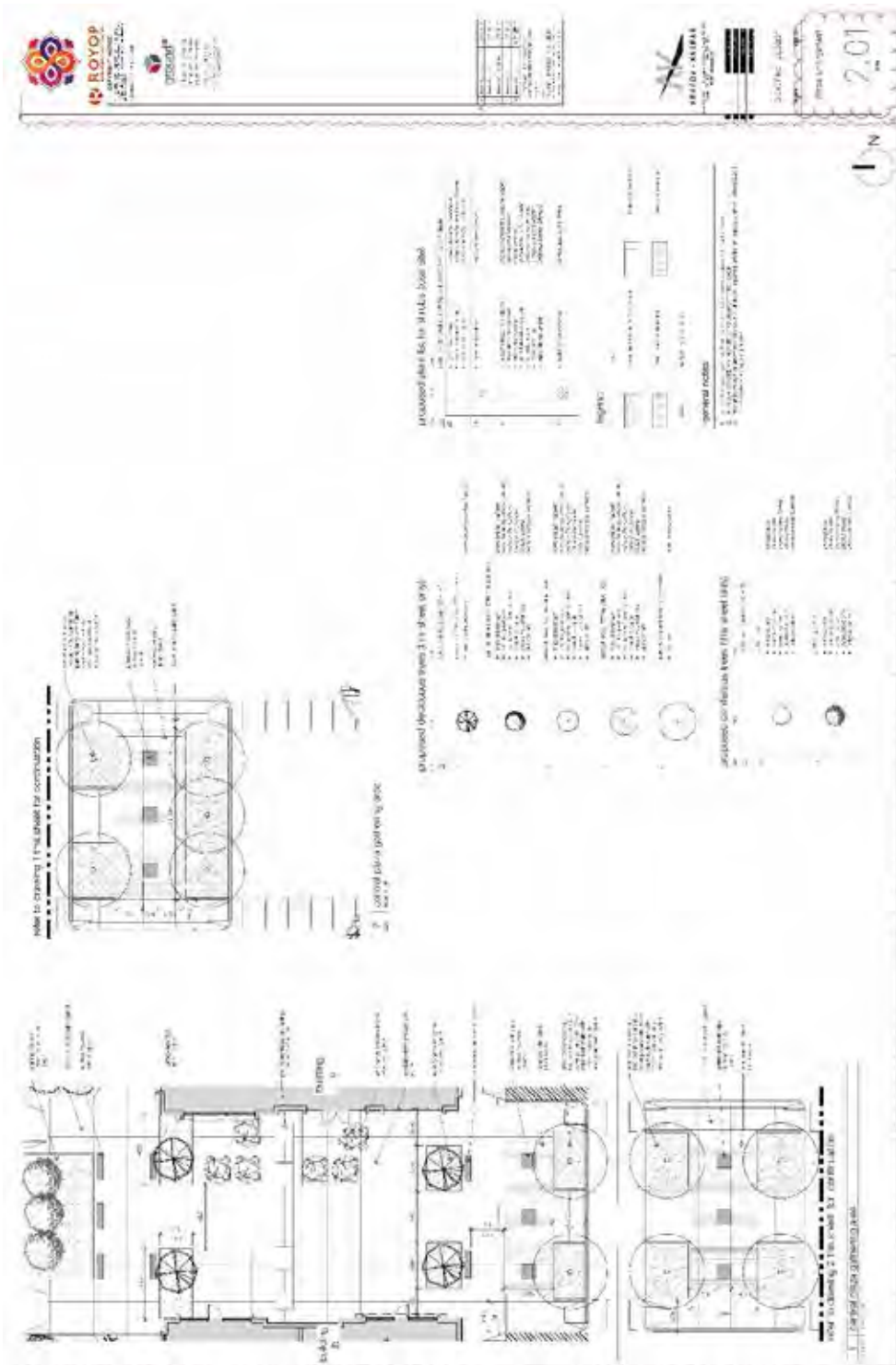
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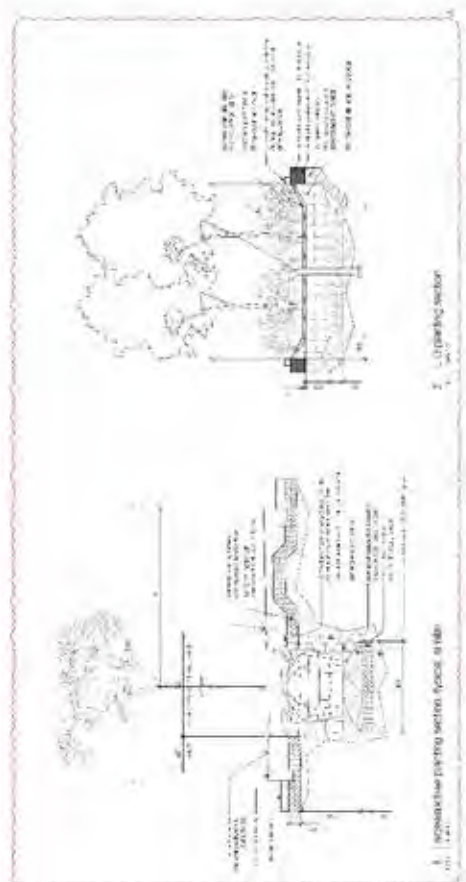
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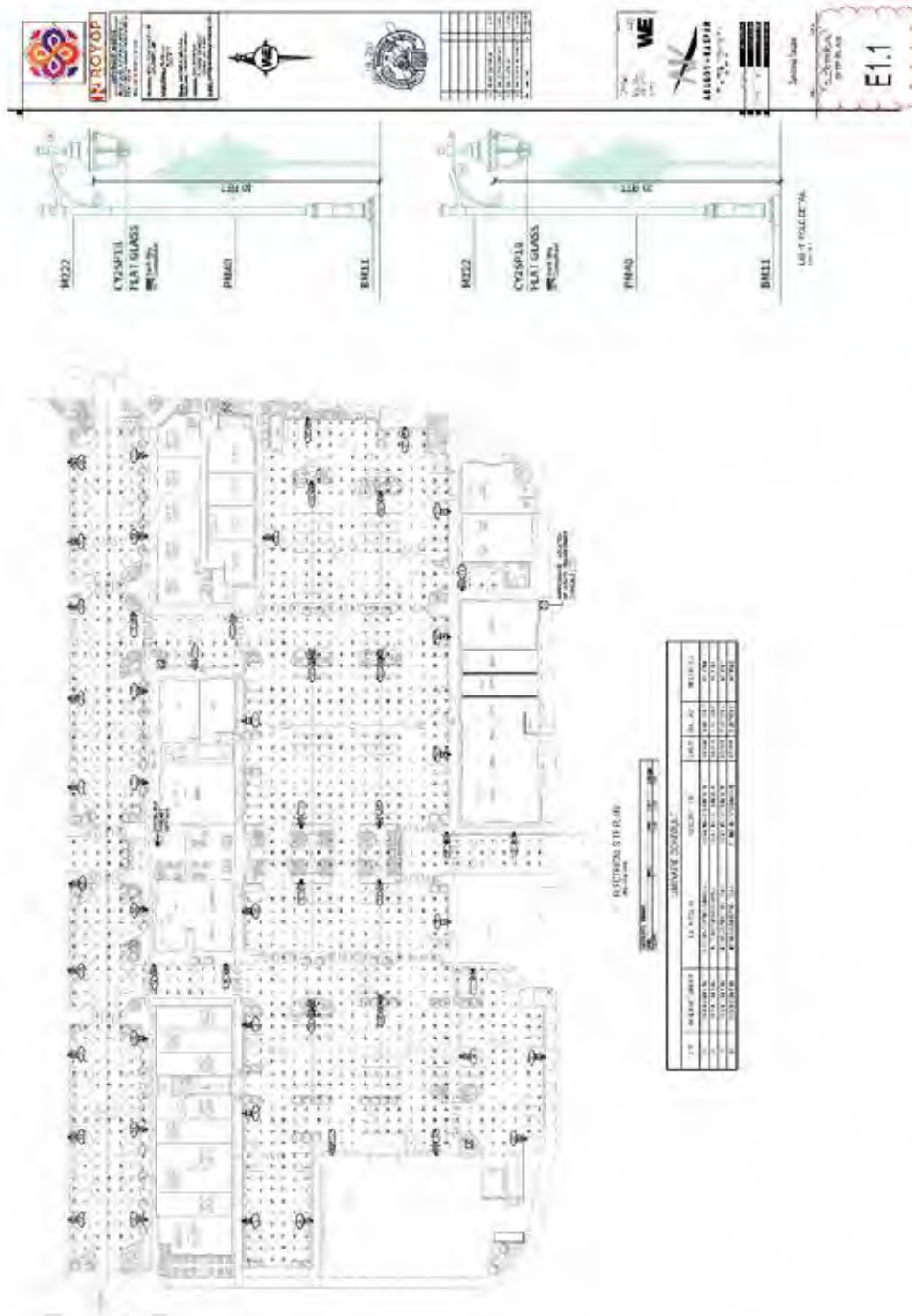
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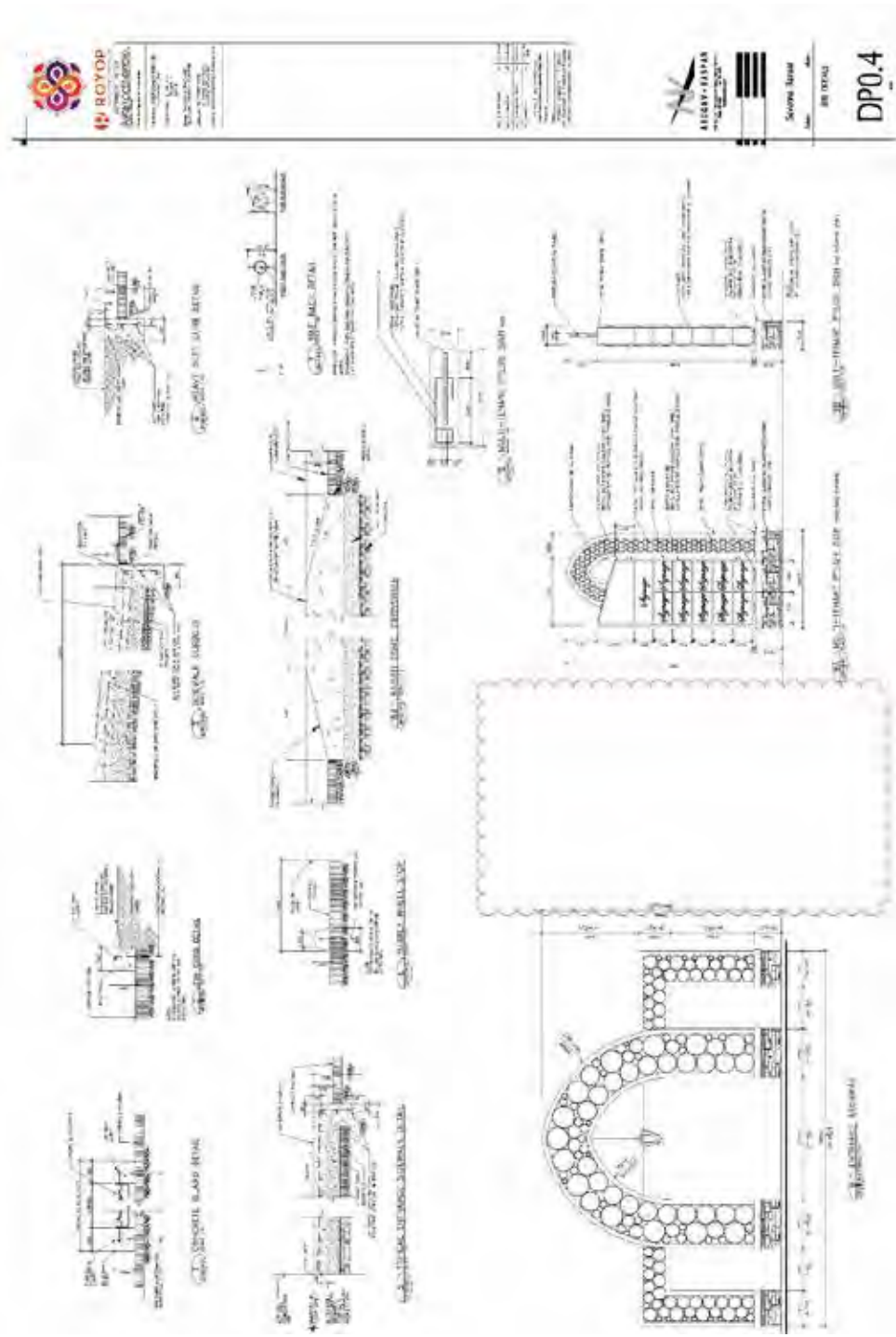
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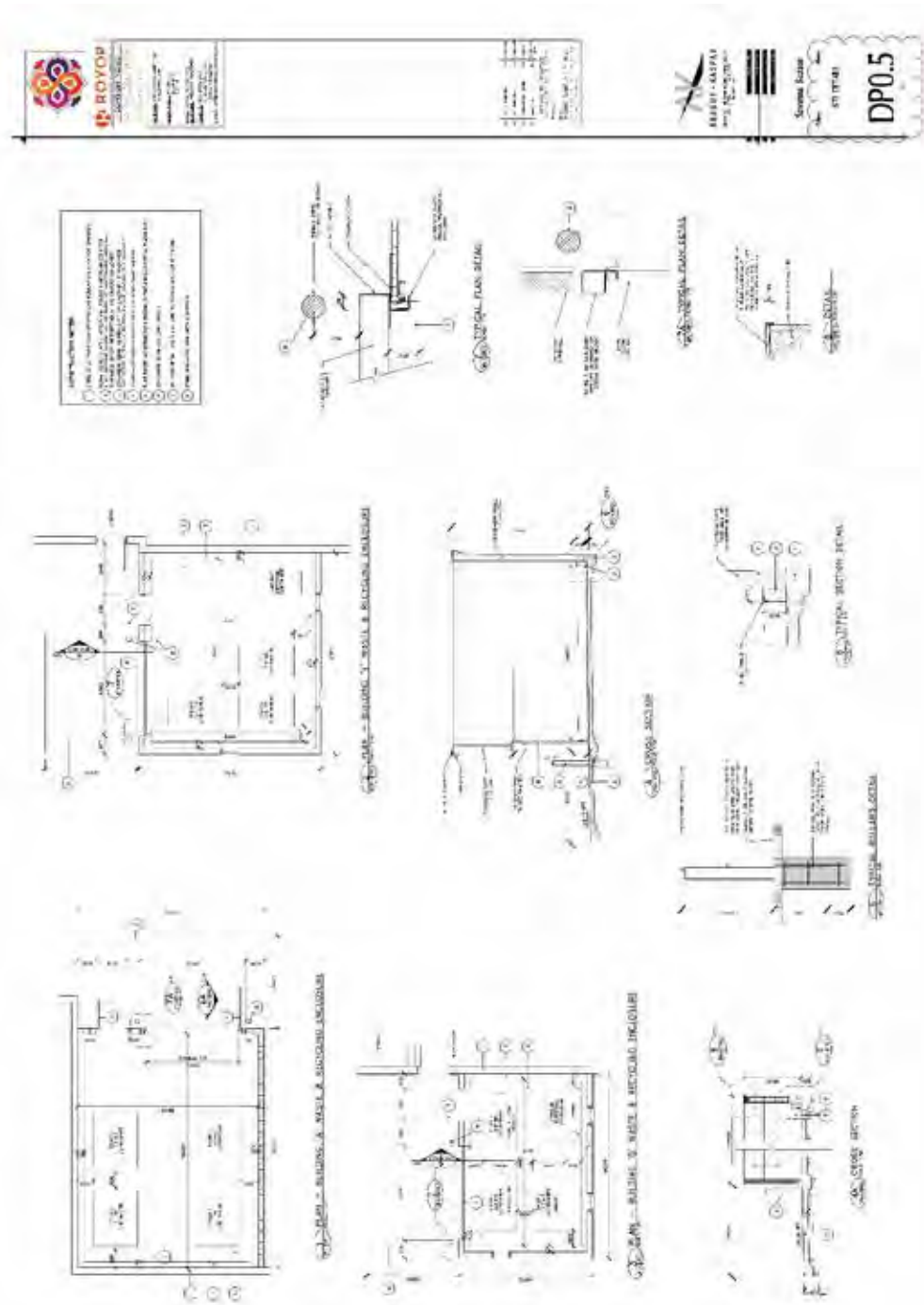
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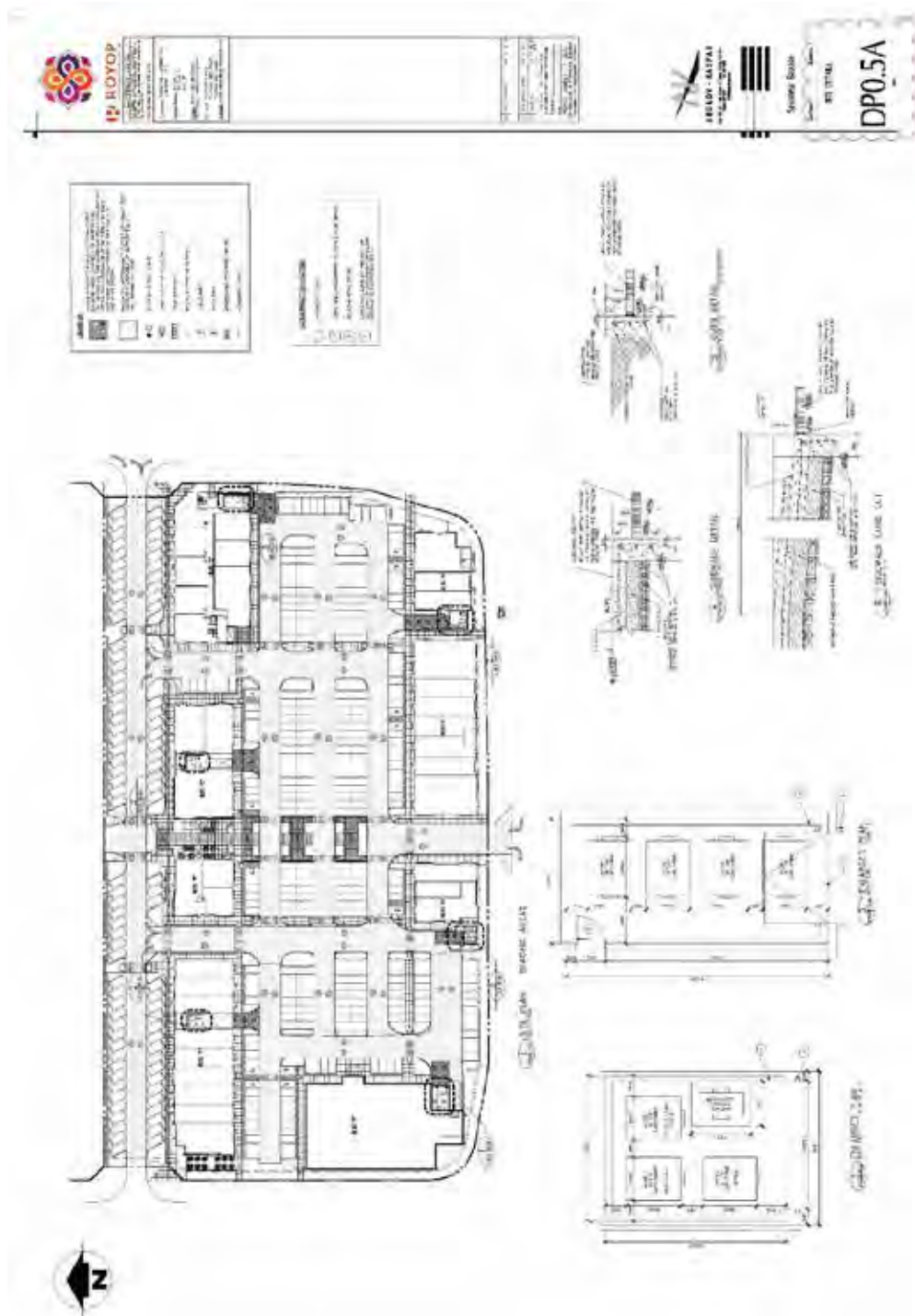
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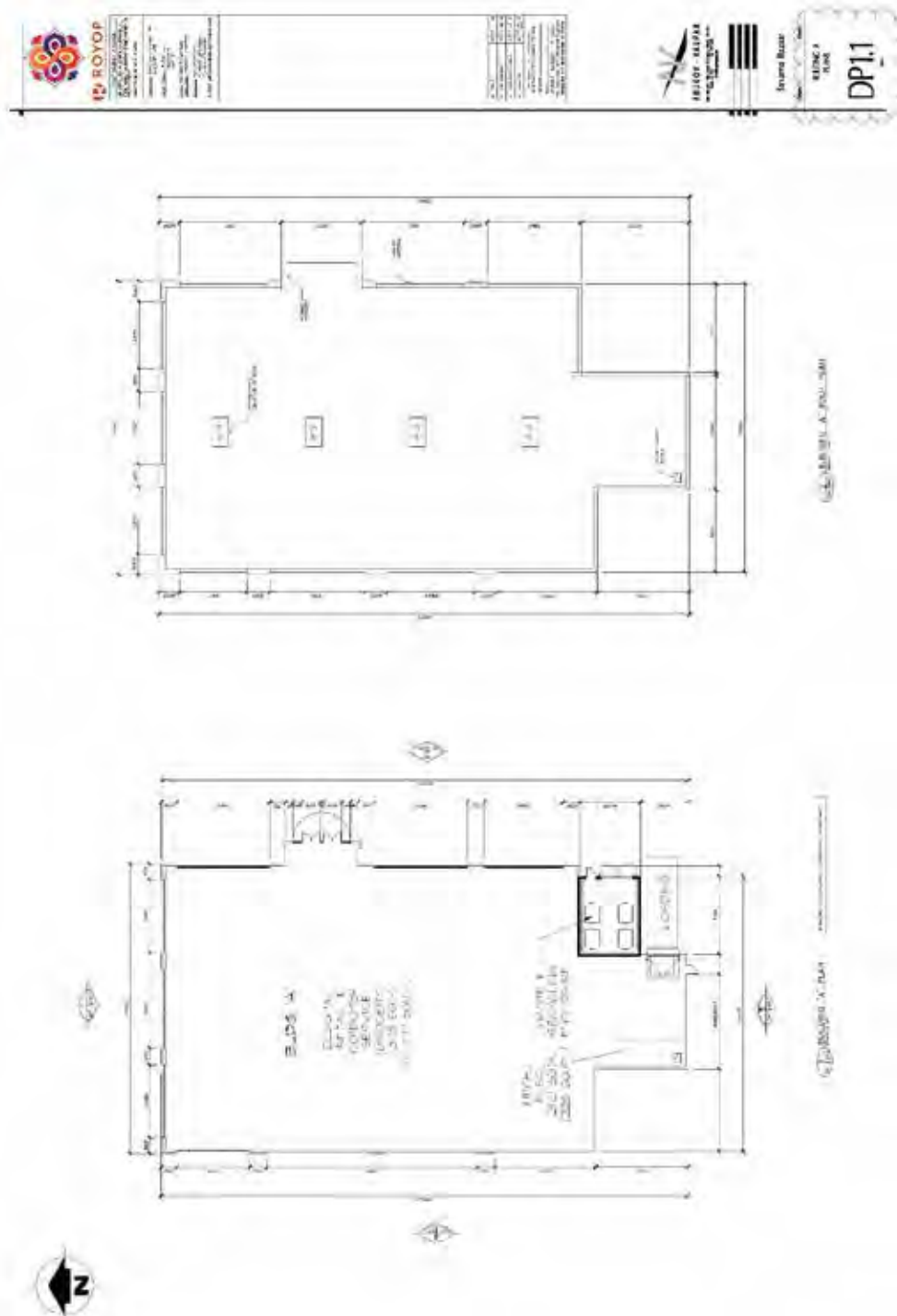
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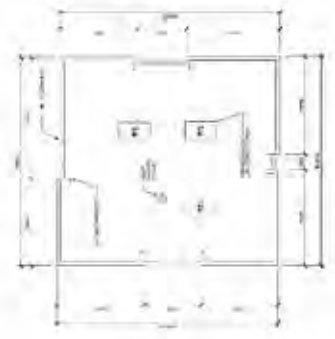
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MAP 14NE



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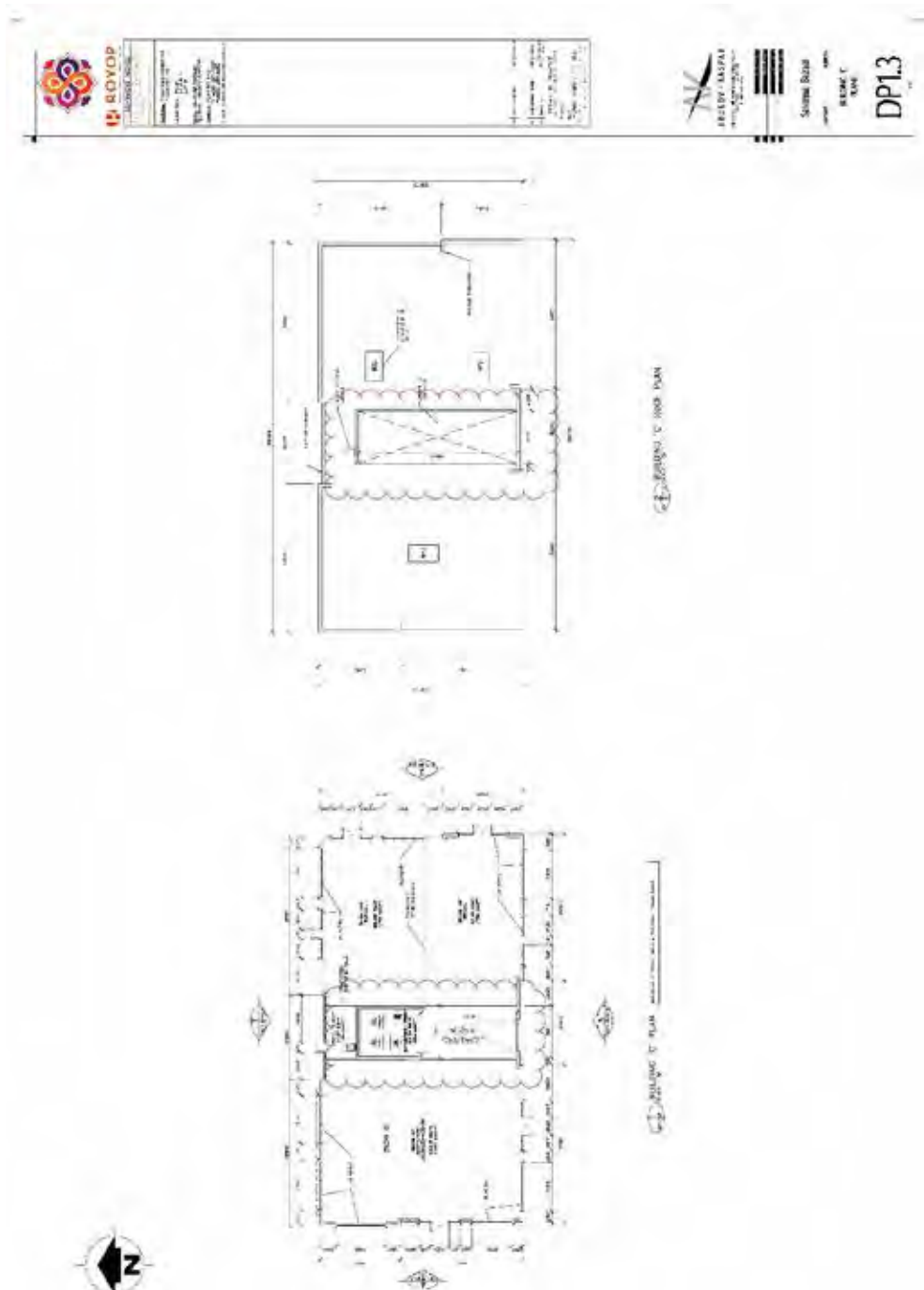


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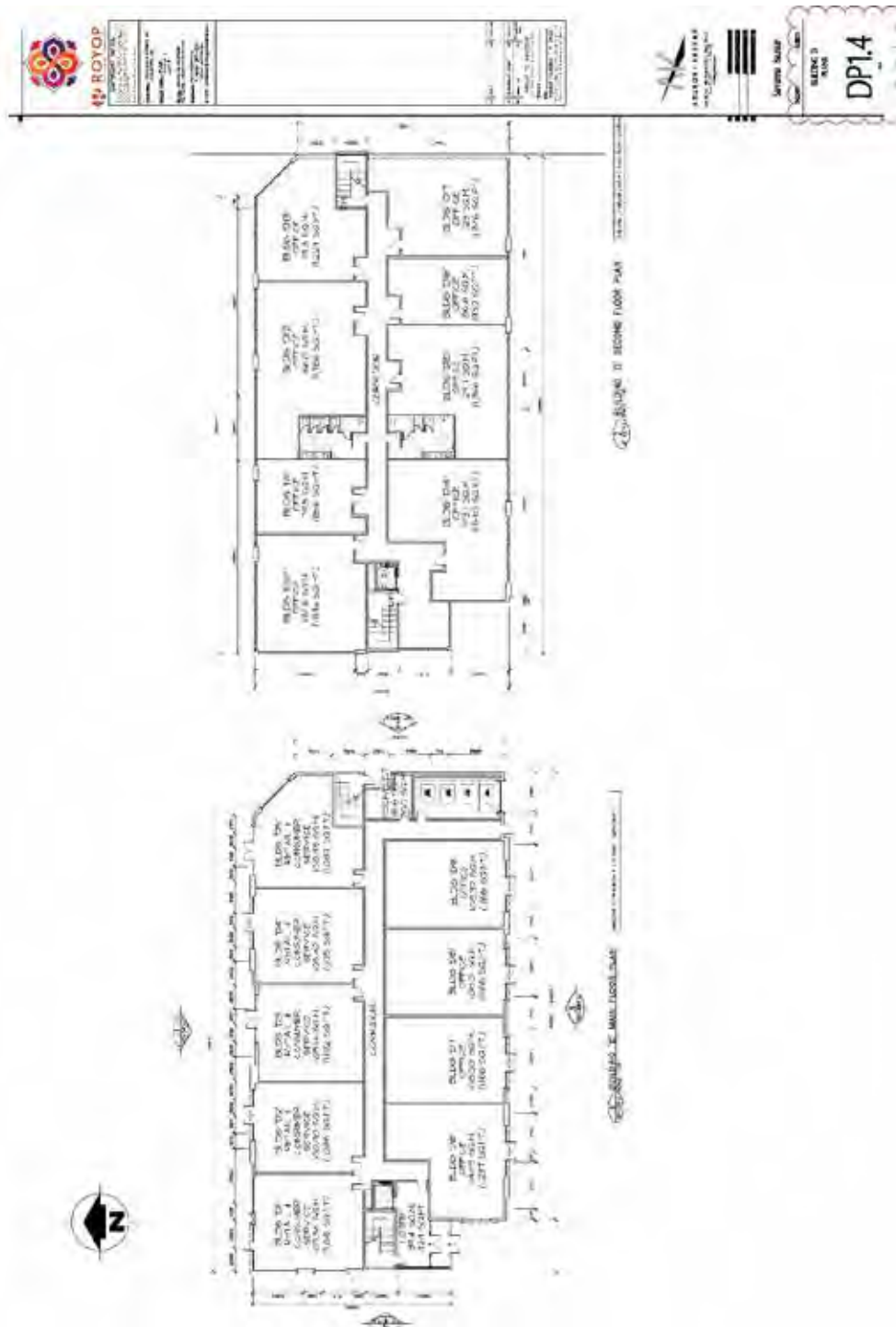
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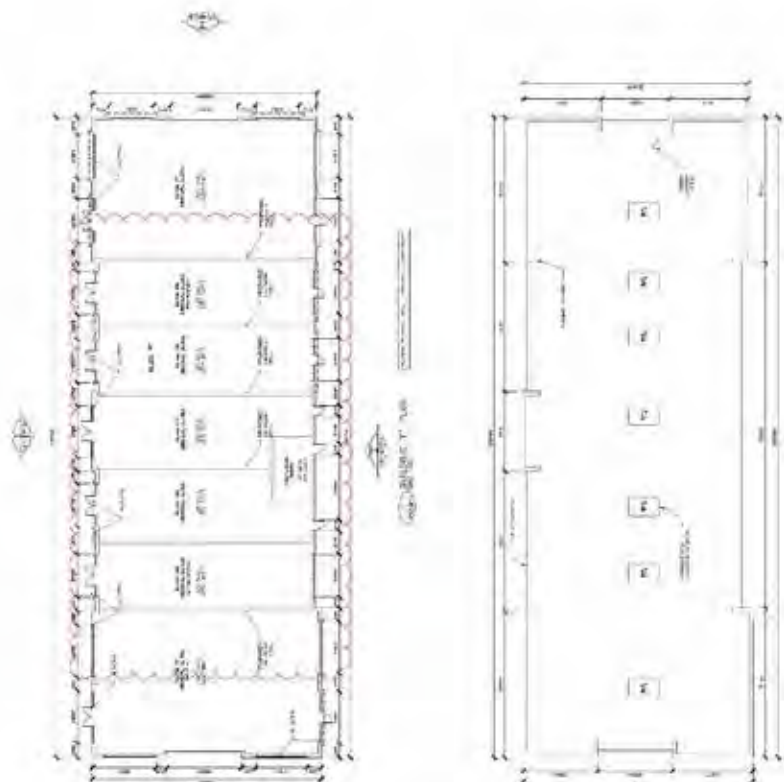


Professional seal and title block for the architect, including the name 'Suzanne Lalonde' and the project title 'DP1.5'.



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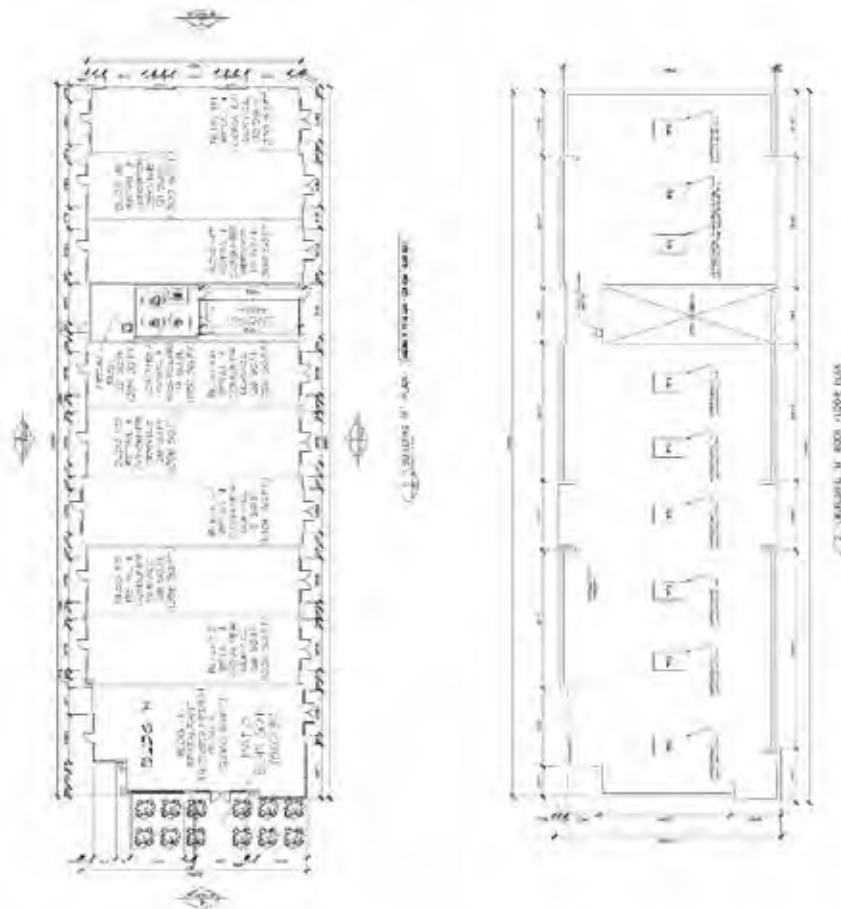
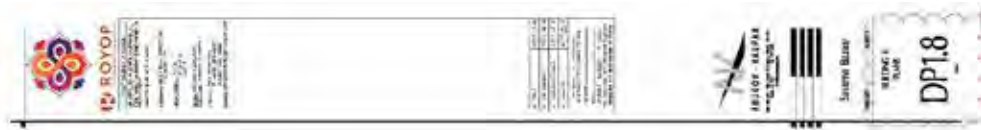
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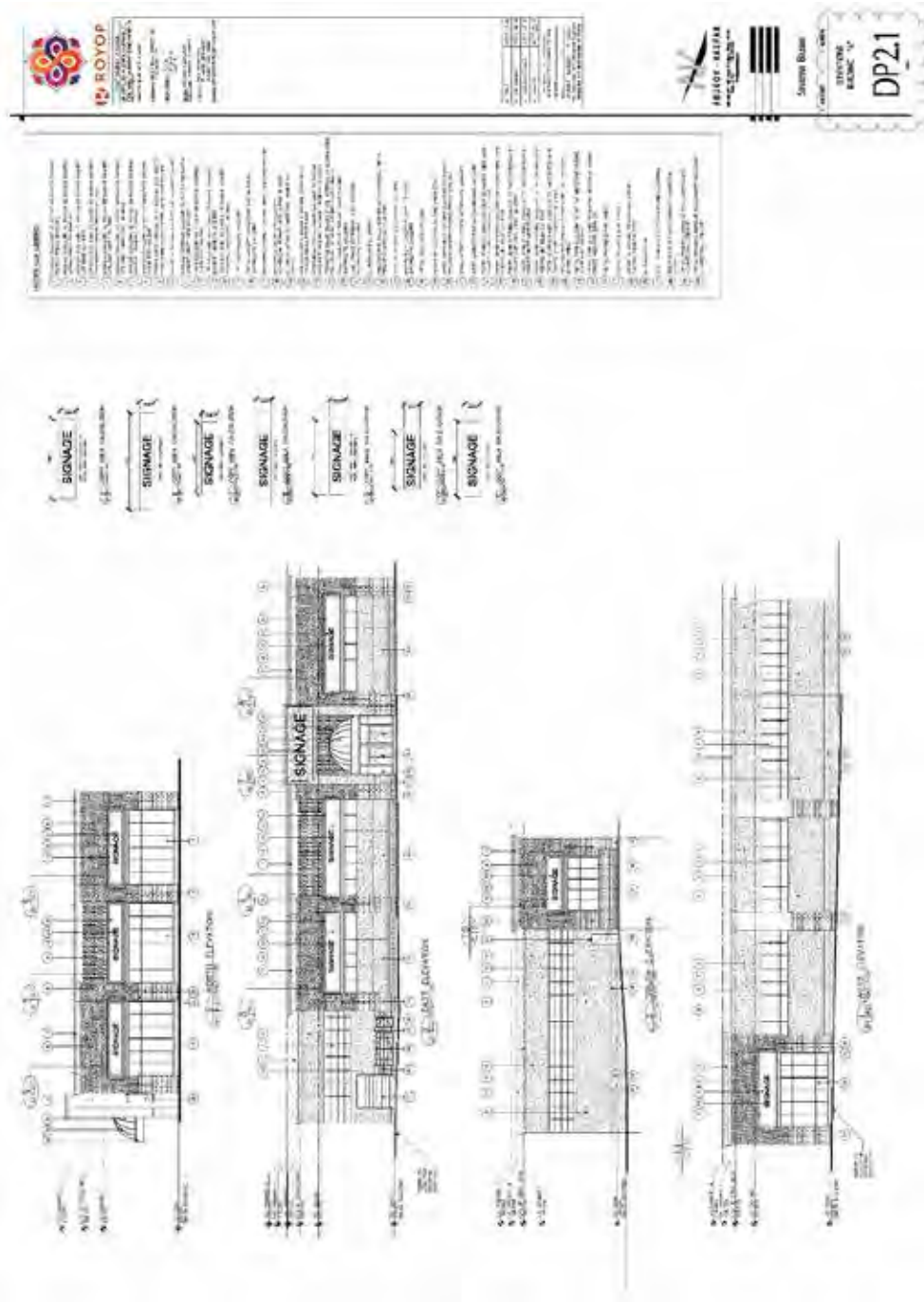
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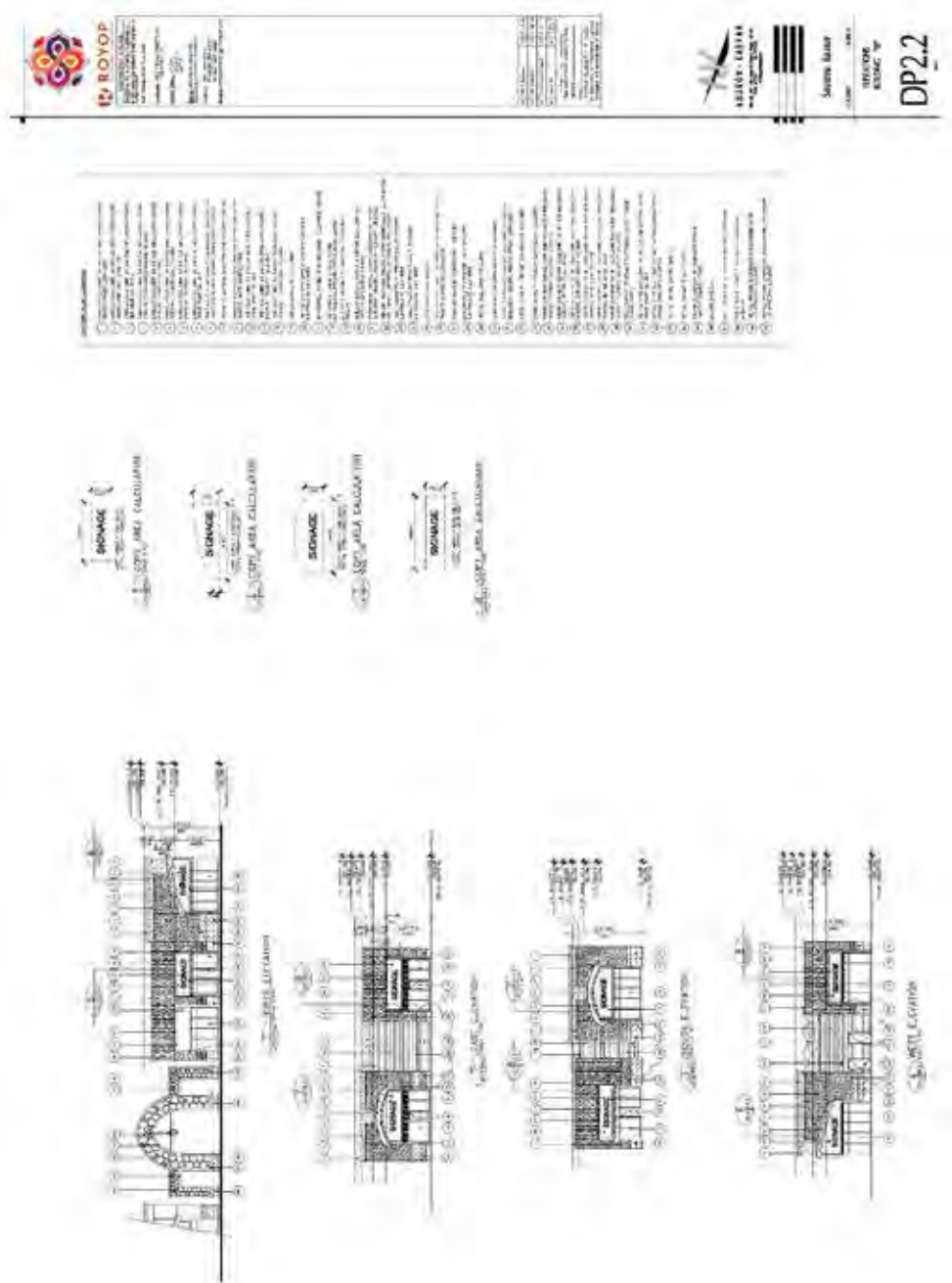
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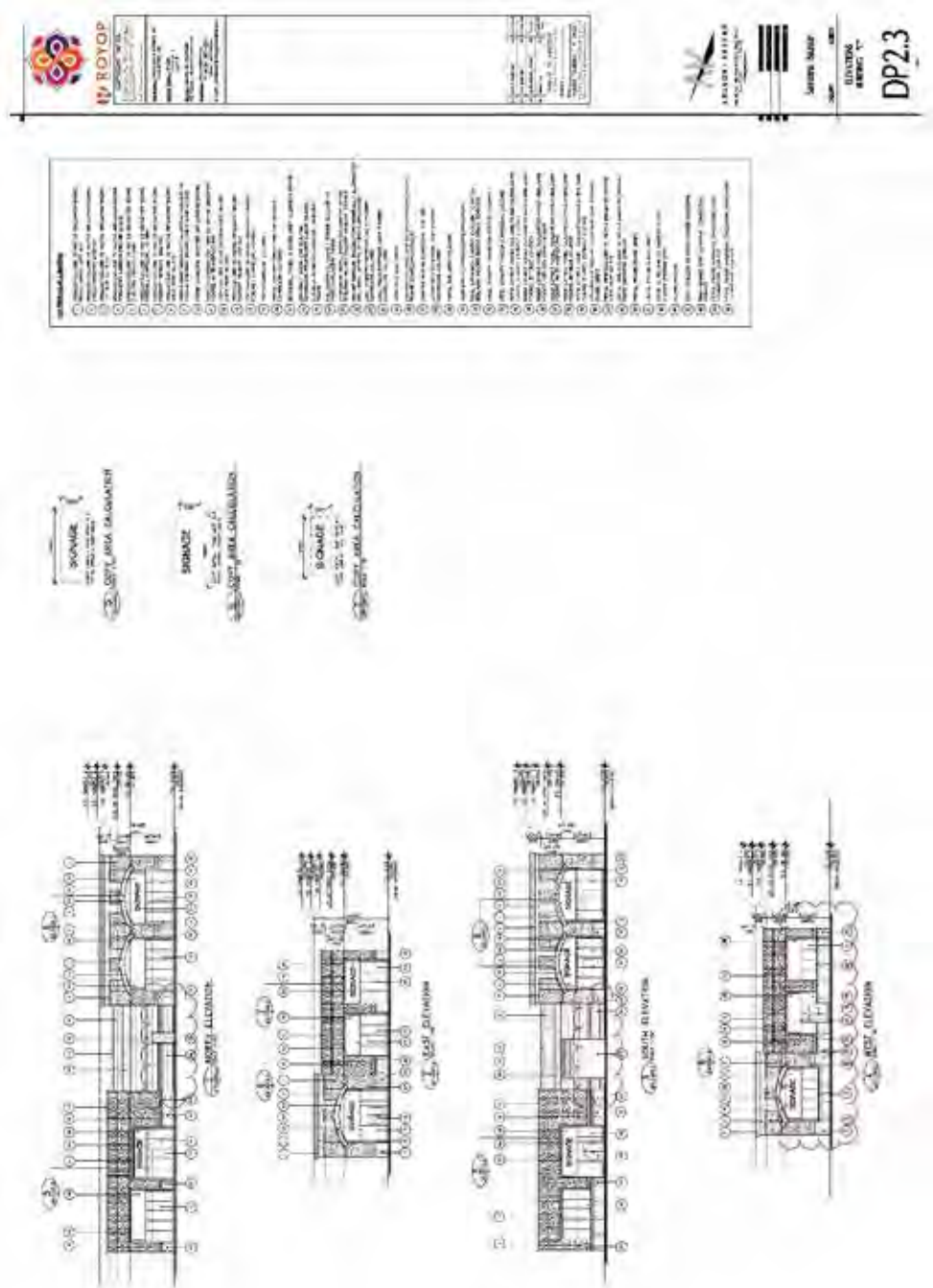
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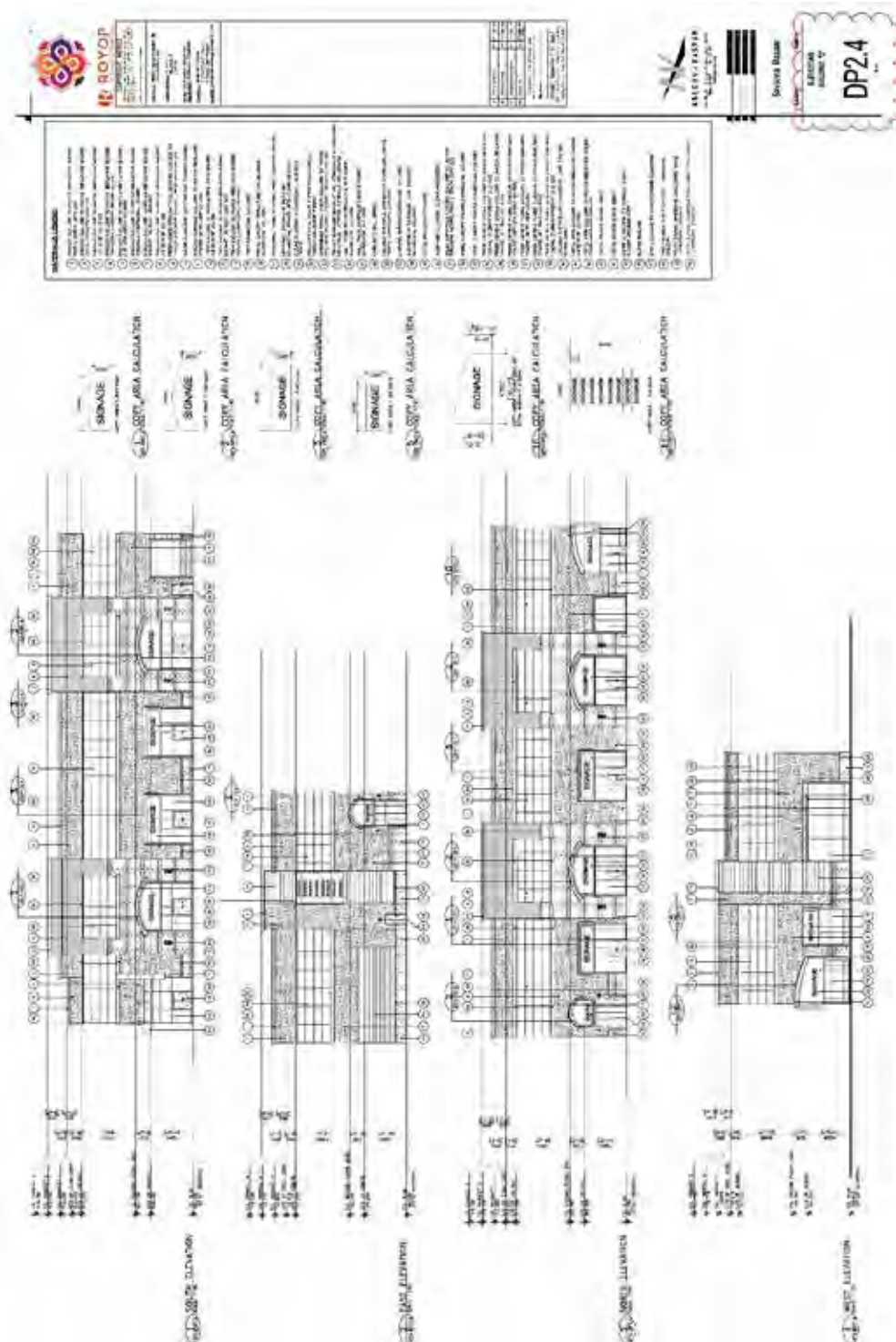
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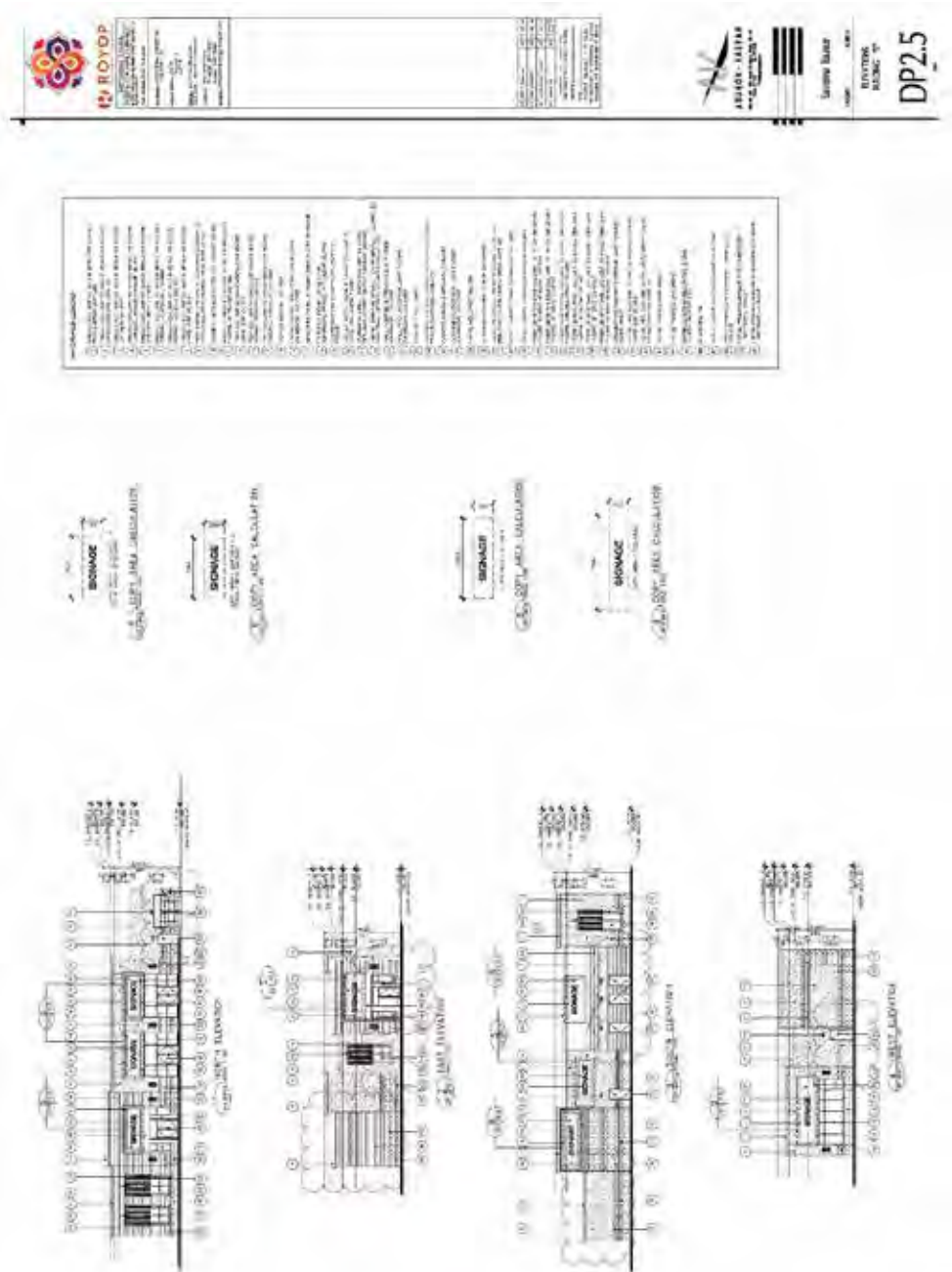
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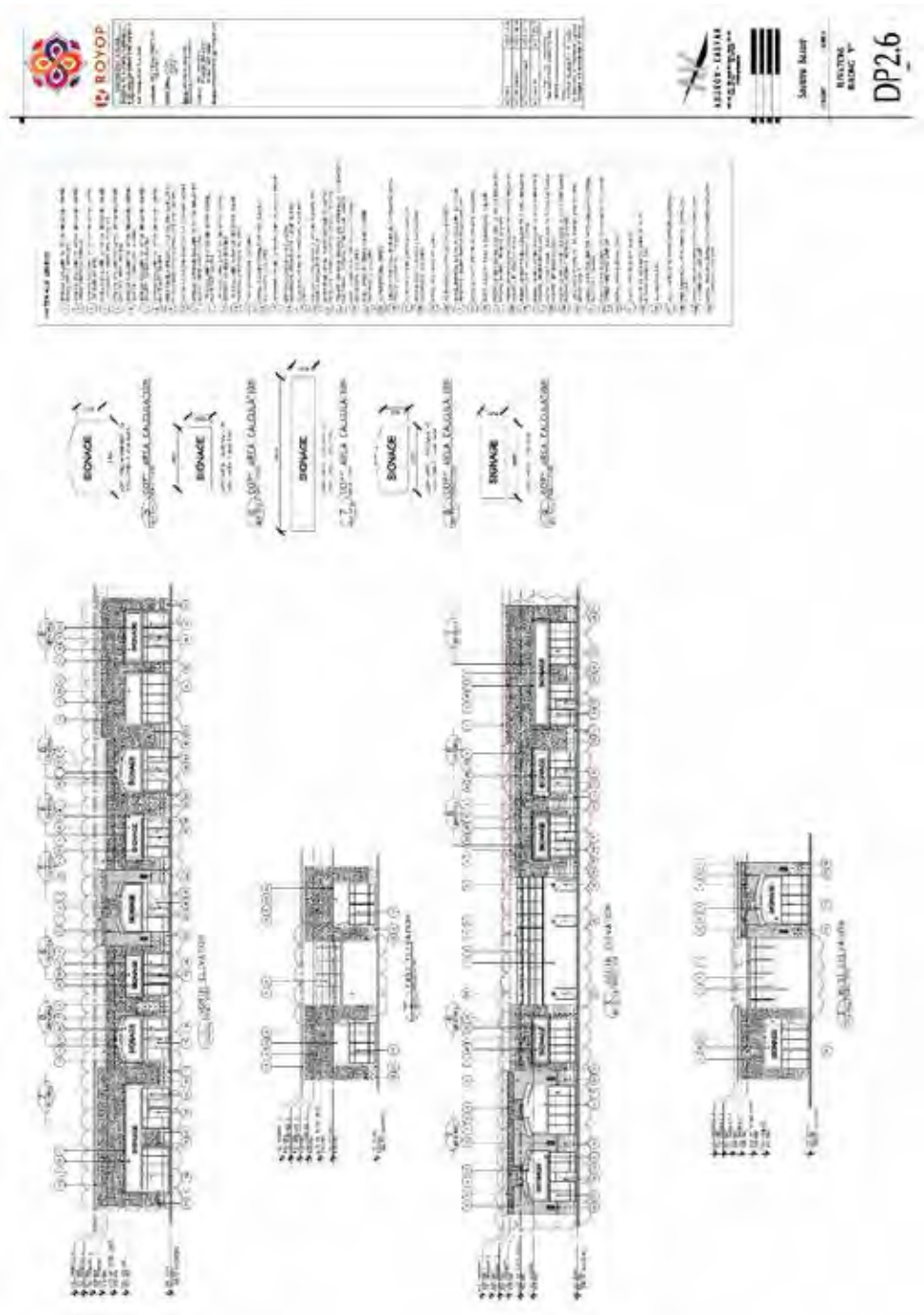
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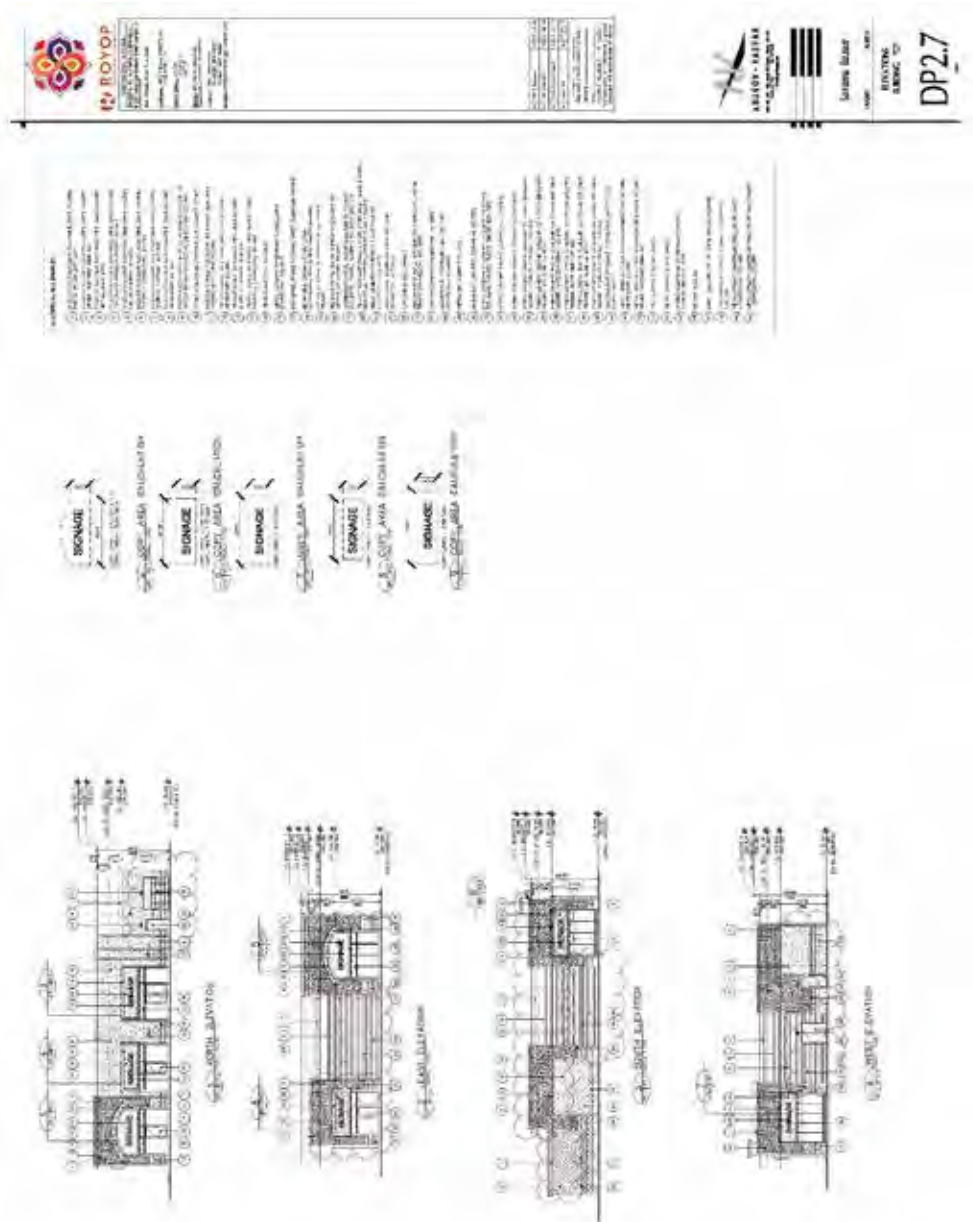
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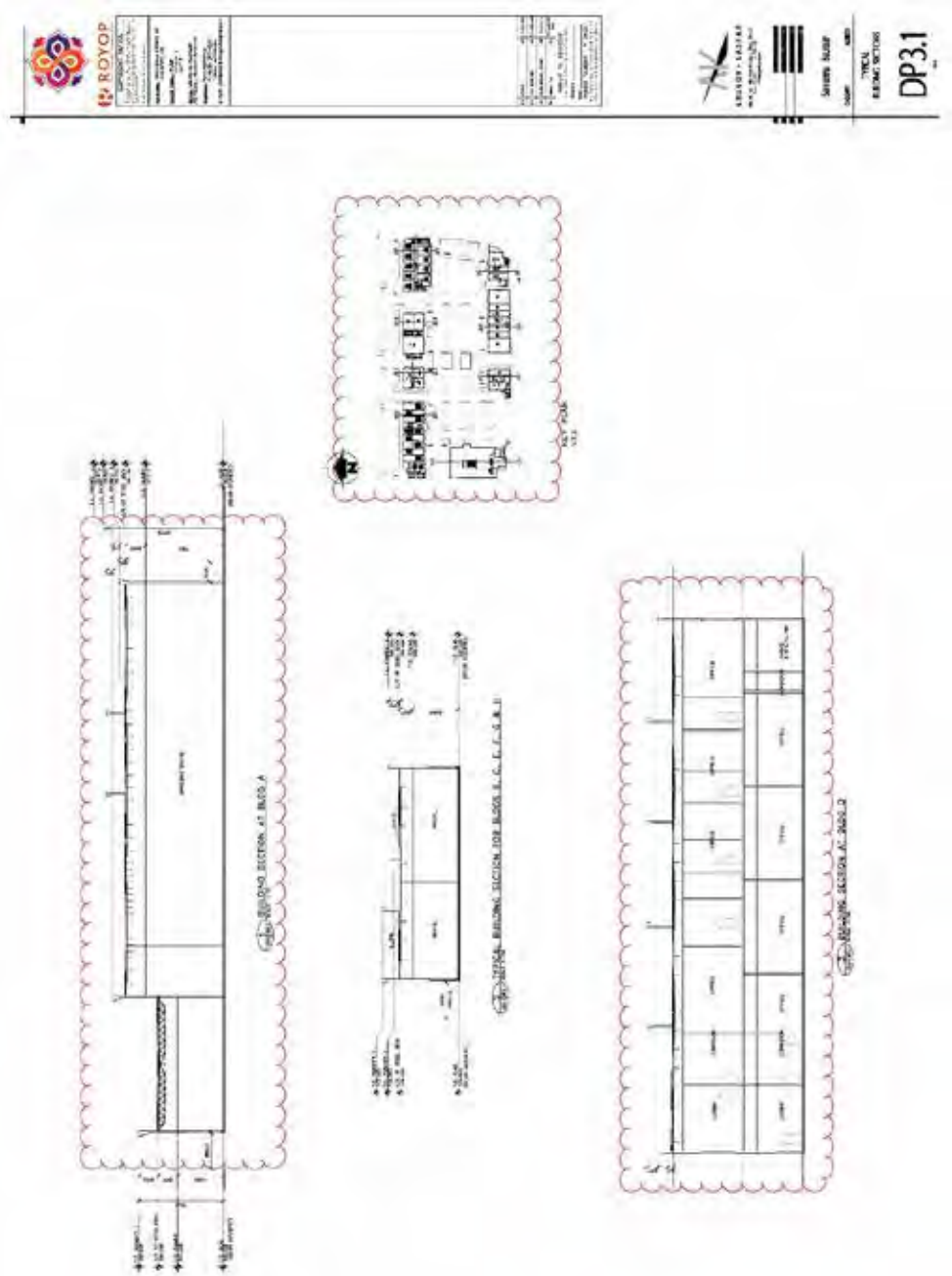
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MAP 14NE

The image displays a set of architectural drawings for a development permit. At the top, there is a header with the City of Calgary logo and the text 'IV ROTOP'. Below this is a table with technical specifications, including columns for 'ITEM', 'DESCRIPTION', and 'REMARKS'. To the right of the table is a logo for 'Savanna Builders' and a permit number 'DP2.8'. The main body of the drawings consists of several floor plans and elevations. The floor plans show the layout of the buildings, with rooms labeled 'Kitchen', 'Dining', 'Bar', and 'Lounge'. The elevations show the exterior of the buildings, with details of the roof, windows, and doors. The drawings are oriented vertically on the page.

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MAP 14NE

APPENDIX II

CONDITIONS OF APPROVAL

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of **6** complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. **3** of the plan set(s) shall highlight all of the amendments.
- b. **3** detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Amend the plans to address the following bylaw discrepancies:
 - Provide 2 additional Class 1 bicycle parking stalls.
 - Provide appropriate screening for the outdoor play area of the Child Care Service as per **bylaw rule 165(c)**.
 - Designate 6 pick-up/drop-off stalls for the Child Care Service as per **bylaw rule 165(e)**.
3. Submit 8 additional DCP fees for Phases 2-9 (\$245/DCP = \$1960.00)

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MAP 14NE

Development Engineering:

4. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

Transportation:

5. Amend the plans to provide at a minimum, 2 class 1 bicycle stalls as required by the land use bylaw.
6. Amend the plans to provide standard “U-rack” bicycle parking for all class 2 bicycle parking.
7. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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FITNESS CENTRE, CONVENIENCE FOOD STORE, SUPERMARKET,
RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
SADDLE RIDGE (WARD 5)
88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

Roads

- a. Construction of one (1) new driveway to 88 Avenue NE;
 - b. Construction of one (1) new driveway to Savanna Street NE;
 - c. Construction of one (1) new driveway to Savanna Drive NE;
 - d. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
8. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to the site
9. Remit payment (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

Roads

- a. Signalization of Savanna Street site access
10. Amend the plans to include a standard bus shelter for the transit stop located Westbound on 88 Avenue far side of Savanna Drive NE. The shelter shall be provided at the expense of the developer. Please contact the Transportation Generalist for further details including cost estimate.
11. Remit payment (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

Transit

- a. Standard bus shelter

Parks:

No comment.

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MAP 14NE

Permanent Conditions

The following permanent conditions shall apply:

Planning:

12. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
13. No changes to the approved plans shall take place unless authorized by the Development Authority.
14. A Development Completion Permit shall be issued for the development; **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
15. Each parking stall, where located next to a sidewalk, shall have a properly anchored concrete wheel stop or equivalent material to the satisfaction of the Development Authority (100mm in height and 600mm from the front of the parking stall).
16. Handicapped parking stalls shall be located as shown on the approved plans released with this permit. Handicap parking stall(s) shall be clearly designated, signed and located close to the entrance of the building with barrier-free accessibility.
17. All areas of soft landscaping shall be provided with an underground sprinkler irrigation system as identified on the approved plans.
18. All plantings shown on the approved plans shall be retained for the life of the development. Any plantings that die shall be replaced by a specimen of comparable size/species during the next growing season.
19. This approval recognizes 9 phases on the approved plans. A Development Completion Permit may be issued for each phase. All the road works, landscaping and provisions for garbage collection shown within each phase shall be completed prior to the issuance of a Development Completion Permit for that phase. Call Development Inspection Services at 403-268-5311 to request site inspections for the Development Completion Permits.

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MAP 14NE

Development Engineering:

20. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
21. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

22. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.

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23. Stormwater runoff must be contained and managed in accordance with the “Stormwater Management & Design Manual’ all to the satisfaction of the Director of Water Resources.
24. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
25. The proposed development is within the Nose Creek catchment boundary and is subject stormwater volume control measures. Based on the Watershed Management Plan the average annual run-off volume shall be limited to 90mm.
26. No trees, shrubs, buildings, permanent structures or unauthorized grade changes are permitted within the utility rights-of-way.

Transportation:

27. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.
28. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

Parks:

29. Any damage to regional pathway, boulevards or trees resulting from development activity, construction staging or materials storage, or construction access will require restoration at the developer's expense. The disturbed area shall be maintained until planting is established and approved by the Parks Development Inspector. Contact the Development Inspector at 403-268-1358 for an inspection.

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MAP 14NE

APPENDIX III

COMMUNITY ASSOCIATION LETTER

April 16, 2017
Saddle Ridge Community Association
#27, 7555 Falconridge Blvd NE
Calgary Ab
T3J 0C9

RE DP2017-1042

The SRCA Land Use and Development Committee has studied this proposal and we are pleased with what we have seen. From the design, to the landscaping, to the variety of proposed businesses, this development will be a valuable addition to our neighbourhood.

We do have a couple of lingering concerns. The first is that the variety of businesses be preserved during the leasing process, so that we don't end up with an overload of one type of businesses, such as all the banks in Saddletowne Circle. Also of concern, but perhaps out of the developer's control is pedestrian access from the east side of 60th Street. Without a pedestrian overpass we are concerned that jay walking will be a problem.

We look forward to the addition of this development to our neighbourhood.

Respectfully submitted,

Judy Brown

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APPENDIX III

URBAN DESIGN REVIEW PANEL

Urban Design Review Panel Comments

Date: Dec xx , 2017
May 3, 2017

Time:

Attendance:

Panel Members:

| Present: | Absent: |
|----------------------|--------------------|
| Janice Liebe (Chair) | Robert LeBlond |
| Terry Klassen | Philip Vandermeij |
| Brian Horton | Chad Russill |
| | Yogeshwar Navagrah |
| | Bruce Nelligan |

Advisor: David Down, Chief Urban Designer

Application number: DP2017-1042

Municipal address: 9320 52 St NE

Community: Saddle Ridge

Project description: New: Liquor Store, Medical Clinic, Outdoor Cafe, Restaurant: Food Service Only - Small, Fitness Centre, Convenience Food Store, Supermarket, Restaurant: Licensed - Medium, Restaurant: Food Service Only - Medium, Drinking Establishment - Medium, Conference and Event Facility, Retail and Consumer Service (6 buildings)

Review: second

File Manager: Shane Gagnon

City Wide Urban Design: Afrah Rayes

Applicant: ROYOP Corporation

Architect: Abugov Kaspar

Owner: Genstar Titleco

Ranking: **Review 2:**
Review 1: Support with conditions

Summary

Review 2:

Review 1: Project Vision – UDRP commends the applicant for aspiring to curate its vision for a kinetic, dynamic, ethnically diverse bazaar – much about the ‘cultural experience’, with a transecting walkway (presented as an open-market concessionaire promenade) east-west through the development parcel connecting future residential uses (west) and mixed uses (north) to a future off-site transit hub (east).

Main Street Character – UDRP has provided comment and suggestion that re-positioning of Building A with frontage onto the promenade, same as Buildings B, C and D would improve continuity to the main street character and interaction with the promenade.

Shift Implication – The resulting expanded parking area to the south on the back-side of Building A is an opportunity for integrated landscape treatments which will be highly visible from the intersection of 88 Avenue NE and Savanna Street NE.

Shared Space – UDRP recognizes the efforts for a connective walkable structure in the plan but feels the central space is inadequate and strongly encourages more investment in expandable, accessible sidewalks across the south, angled-parking areas along the north and central promenades – noting that the angled-parking roadway is not public (not a city-dedicated road) and therefore allows more flexibility.

Building Interface – UDRP further interprets the applicant-described public realm vision as an opportunity for buildings that open-windows-wide for more innovative inside/outside connectivity. UDRP recommends

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consideration of folding, sliding, overhead-opening window/door designs that optimize climate adaptation and inside-outside connectivity.

Connection to the Sky – A taller archway feature between Buildings B and C should be considered. A landscape plan is recommended for the 2nd floor roof top amenity area of Building D. Greening of partial roof areas, visible from the ground-level would benefit the up-down, inside-outside visual relationships of the public realm.

88 Avenue Streetscape – UDRP recommends better street interface solutions to Buildings E, F and G as they turn their back on 88 Avenue NE. There is no allowance for tree and shrub landscaping due to what appears to be Utility Right of Way and Setback restrictions. UDRP recommends windows here to achieve a more porous and transparent streetscape presence.

Thematic Relevance and Sufficiency of Landscape – UDRP suggests consideration of ornamental grasses integrated with the mow-able perimeter of the surrounding public roadways where there appears to be a deficiency of trees and shrubs restricted by URW and setback.

Further Review Recommended – For these reasons, UDRP requests that the project returns to the panel following revisions and resubmission.

Follow-up – On return, the panel requests that the applicant provide a comprehensive shared-use public realm strategy that demonstrates enriching the stated vision and how UDRP's comments have been addressed.

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MAP 14NE

| Urban Vitality | |
|----------------|--|
| 1 | <p>Retail street diversity Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided.</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> <p>UDRP Commentary 1</p> <p>Support with Conditions, further review required</p> <p>UDRP heard from the applicant that the ethnic diversity of the area is an opportunity to meet the market with a kinetic, dynamic, porous inside-outside environment where the relationship of sights, sounds and aromas of food enrich the connection with community. Diversity of food, fashion, and other essential services, along with banquet facilities was presented as the project mix and market opportunity. UDRP feels the project is positioned to be a local destination and therefore the public realm design needs to be strengthened.</p> <p>Applicant Response</p> <p>-in response to UDRP and CPAG comments we have reviewed the design of the public realm and building locations. As part of an ongoing discussion with the CPAG team Building 'A' has been split into two buildings. Building 'H' has been added to the south side of the private road comprised of smaller tenants to address this comment. We revised the public realm design at the direction of the CPAG team and updated the site pedestrian routes, landscaping and the central linked pedestrian plazas where the public are encouraged to loiter. Precast tables have been added to the central plazas to encourage the use for board games during pleasant weather.</p> |
| 2 | <p>Retail street transparency, porosity Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> <p>UDRP Commentary 1</p> <p>Support with Conditions, further review required</p> <p>UDRP reviewed the proposed building elevations and find the prescriptive facades could be more open and responsive to sunlight. Natural light on two or more sides of retail units is encouraged. Buildings E, F and G turn their back to the 88 Avenue NE streetscape. UDRP feels the 'back of house' is an opportunity to place windows to reveal a level of authentic 'behind the scenes' activity that is part of making the bazaar market what it aspires to be – a local area market-place destination. In the same way, the stairwells to the second-floor banquet facilities in Building D should be public open stairs – glass, transparent and visually connected to the ground-level public realm.</p> <p>Applicant Response</p> <p>-Where possible on the site this design has integrated a 'three sided' architecture approach. Buildings 'E', 'F', and 'G' face the site interior due to the utility right of way that is against the south property line. This limits any hard paving for access or planting to screen the rear elevation. The current design allows for glazing in multiple of locations however we feel that additional glazing would be obstructed by true back of house operations. Buildings 'B' and 'C' allows for visible back of house within the loading areas. Glazing areas have been carefully inserted while keeping the character of the building exterior design. The entrance lobby to Building 'D' has large glazed areas to provide transparency to the ground level public realm while providing limited privacy to the guests with the use of solid areas all creating interest into</p> |

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MAP 14NE

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| | <i>The façade design.</i> | |
| 3 | Pedestrian-first design <i>Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP was engaged in imagining a diversity of people coming to the bazaar – walking, cycling, by transit and car – spilling over the promenade-like sidewalks, the plaza, and the parking areas with a level of organization that is convenient for those arriving by car and comfortable for those walking. However, because the car parking dominates the site plan, UDRP feels strongly that expandable, universally accessible sidewalks into adjacent rows of parking (i.e. not taking away any parking) would better balance the imagined diversity and activity in the shared-use public realm. | |
| | Applicant response | |
| 4 | Entry definition / legibility <i>Entry points are clear and legible</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support |
| | Walkable connections are part of the pedestrian framework with perimeter boulevard sidewalks along all public streets. The promenade along the east-west private angled-parking roadway connects directly to three off-site destinations: a neighbourhood park to the west, a transit hub to the east, and an urban amenity to the north. Internally the pedestrian promenade meets in the middle at a plaza between Buildings C and D. | |
| | Applicant Response | |
| 5 | Residential multi-level units at grade <i>Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | NA |
| | Applicant Response | |
| 6 | At grade parking <i>At grade parking is concealed behind building frontages along public streets.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP notes that at-grade parking dominates the site and recommends expandable sidewalks for shared-use of parking areas to better balance the public realm aspirations of the open-market concessionaire intentions. | |
| | Applicant Response | |
| | <i>Refer to revised Entry 0F.1.3 Site Plan. The bonus north-south public spine will be...</i> | |

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MAP 14NE

| | |
|----|---|
| | <p>revised to offer expanded pedestrian areas as well as area for community seating. Permanent concrete tables have been added to these areas to encourage board game play in pleasant weather such as Carrom. We feel that by providing an expanded public area buffered by landscaping and permanent street furniture the space will allow for greater community interaction within the site.</p> |
| 7 | <p>Parking entrances Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> <p>UDRP Commentary 1</p> <p>Support</p> <p>a) 88 Avenue North West entry, with two-way right-in and right-out only and sidewalks on both sides of the driving lane, offers a tight tree-lined passage between Buildings F and G; b) Savanna Drive NE and Savanna Street NE entries connect a two-way private roadway (not public and not a city-dedicated roadway) with angle-parking on both sides with two access points south into the site and with future provision of two off-site access points north.</p> <p>Applicant Response</p> |
| 8 | <p>Other</p> |
| | <p>Urban Connectivity Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</p> |
| 9 | <p>LRT station connections Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> <p>UDRP Commentary 1</p> <p>Support</p> <p>UDRP feels the project is ideally located to optimize future LRT and Transit Hub connections including proposed bus stops on 88 Avenue NE and Savanna Street NE.</p> <p>Applicant Response</p> |
| 10 | <p>Regional pathway connections Supports walkability via intentional urban design connections to pathway systems</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> <p>UDRP Commentary 1</p> <p>Support with Conditions, further review required</p> <p>There was no apparent reference to regional pathway connections in the applicant's information package to UDRP. UDRP's request for clarification should be provided in follow-up.</p> <p>Applicant Response</p> <p>Please refer to Pedestrian Circulation drawing LD.03 for three connection locations to the 3m regional pathway along 88 Avenue NE. They are located east of Building 'G', both sides of the 88th Avenue NE vehicle entrance as well as to the east and west of Building 'F'.</p> |
| 11 | <p>Cycle path connections Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.</p> <p>UDRP Commentary 2</p> <p>Applicant Response</p> |

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MAP 14NE

| | | |
|----|--|--|
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | Pathways have not been shown/described as cycle-friendly or as designated cycle routes through the site. | |
| | Applicant Response | |
| | -Please refer to Pedestrian Circulation drawing LD.03 for designated cycle routes information. Within the site raised crosswalks provide traffic calming measures to create a more pedestrian and cycle friendly site. Bicycle storage is noted on the revised site plan. | |
| 12 | Walkability - connection to adjacent neighbourhoods / districts / key urban features <i>Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support |
| | Walkable connections are part of the pedestrian framework with perimeter boulevard sidewalks along all public streets. The promenade along the east-west private angled-parking roadway connects directly to three off-site destinations: a neighbourhood park to the west, a transit hub to the east, and an urban amenity to the north. Internally the pedestrian promenade meets in the middle at a plaza between Buildings C and D. | |
| | Applicant Response | |
| 13 | Pathways through site <i>Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | Although UDRP feels the walkable network of pathways through the site provide good connectivity there is a need to improve and balance the scale and expandability of these pathways as shared space with the adjacent parking areas. | |
| | Applicant Response | |
| | -Refer to revised site plan for extent of expandable pathways/ plazas. We feel that the expanded public plazas in the central spine create a public amenity that is a shared space which can be used by many different users in a multitude of ways. Permanent concrete tables have been proposed to give public amenity space to offer space for board games as well as gathering space protected from the parking areas with a landscape buffer. | |
| 14 | Open space networks and park systems <i>Connects and extends existing systems and patterns.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support |
| | UDRP recognizes the positive benefit of how connections to off-site neighbourhood parks and open-space networks in the community context plan are achieved. | |
| | Applicant Response | |
| 15 | Views and vistas <i>Designed to enhance views to natural areas and urban landmarks.</i> | |
| | UDRP Commentary 2 | |

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
 OUTDOOR CAFÉ, RESTAURANT: FOOD SERVICE ONLY – SMALL,
 FITNESS CENTRE, CONVENIENCE FOOD STORE, SUPERMARKET,
 RESTAURANT: LICENSED – MEDIUM, RESTAURANT: FOOD SERVICE
 ONLY – MEDIUM, DRINKING ESTABLISHMENT – MEDIUM, RETAIL
 AND CONSUMER SERVICE, CHILD CARE SERVICE, OFFICE,
 RESTAURANT: LICENSED – SMALL (8 BUILDINGS)
 SADDLE RIDGE (WARD 5)
 88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

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| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |
| UDRP feels the scale and art-like qualities of the featured archway/overhead structure between Buildings B and C should be scale-optimized (i.e. made taller) so that it is more visible when entering the site. The featured archway/overhead structure is near an important intersection of two axial promenades that connect to the mixed-use community context. | |
| Applicant Response | |
| -The intent of this feature gateway was not to tower over the buildings but be at the pedestrian scale and add to the north-south pedestrian connection acting as a gateway to the internal "bazaar" space. We feel that this feature structure is appropriately scaled for its location. | |
| 16 | Vehicular Interface |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |
| UDRP recommends: a) optimizing the walkability of the public realm with expanded sidewalks in adjacent parking areas; b) provision of bicycle parking and cycle path line-markings; and c) enhancing of landscape buffers for any altered parking areas SW corner of the site (resulting from UDRP's recommendation to shift Building A north). | |
| Applicant Response | |
| A) Expandable plaza areas have been added with raised parking. B) Refer to drawing 'DP0.8 Site Plan' for bicycle parking locations. C) Although Building 'A' has not been shifted north attempts have been made to screen the loading and add landscaping where possible outside of the utility right of way. | |
| 17 | Other |
| Contextual Response Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities. | |
| 18 | Massing relationship to context Relationship to adjacent properties is sympathetic |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | Support |
| UDRP recognizes off-site residential building types in the immediate Community Context Plan that will effectively achieve a mixed-use public realm environment that achieves positive liveable neighbourhood relationships. | |
| Applicant Response | |
| 19 | Massing impacts on sun shade Sun shade impacts minimized on public realm and adjacent sites |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |
| UDRP recommends the sunny-side parking area immediately south of Buildings B and C be raised to sidewalk elevation as a multi-use parking/plaza area. | |

DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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 88 AVENUE NORTHEAST AND SAVANNA STREET NORTHEAST

MAP 14NE

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| | Applicant Response | |
| | <i>The design intent was to provide a mix of multi-use parking/ plaza spaces. Expandable plaza areas have been provided to the north of buildings 'B' and 'C' as well as the north south central pedestrian spine. We feel that splitting up the plaza areas will create nodes of activity throughout the site which allows for maximum expansion of public shared space.</i> | |
| 20 | Massing orientation to street edges <i>Building form relates / is oriented to the streets on which it fronts.</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP recommends shifting Building A north to match the frontage orientation of Buildings B, C and D along the private angled-parking roadway. This will improve the main street appeal of the shared-use/open-market/east-west/concessionaire promenade. | |
| | Applicant Response | |
| | <i>Refer to revised drawing (DP0.3 Site Plan) for revised building layout. Previous Building 'A' has been revised into two buildings; one as new building 'M' that fronts the private road and the other is a grocery store located in the south west corner of the site.</i> | |
| 21 | Massing distribution on site | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP has provided comment and suggestion that re-positioning of Building A with frontage onto the promenade, same as Buildings B, C and D would improve continuity to the main street character and interaction with the promenade. | |
| | Applicant Response | |
| | <i>Refer to item # 20.</i> | |
| 22 | Massing contribution to public realm at grade <i>Building form contributes to a comfortable pedestrian realm at grade</i> | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP recommends the sunny-side parking area immediately south of Buildings B and C be raised to sidewalk elevation as an multi-use parking/plaza area. | |
| | Applicant Response | |
| | <i>Refer to item #19.</i> | |
| 23 | Other | |
| | Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i> | |
| 24 | Safety and security <i>CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.</i> | |
| | UDRP Commentary 2 | |

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MAP 14NE

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| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |
| UDRP has encouraged the applicant to consider a more in-depth exploration of public realm lighting design, transparency of movement objectives, and presence of people moving between buildings and parking. | |
| Applicant Response | |
| -Refer to updated landscaping design drawings for public realm feature lighting and lighting drawing for overall site lighting strategy. Decorative bollards along with low level lighting highlight pedestrian areas without creating light pollution. The site design allows for transparency and open areas for views for multiple of tenancy spaces. | |
| 25 | Pedestrian level comfort – wind Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions. |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | NA |
| Applicant Response | |
| 26 | Pedestrian level comfort – snow Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions. |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |
| Snow removal planning not part of the applicant-described presentation. | |
| Applicant Response | |
| -The strategy to prevent snow drifting will be the same as other sites of this size and use. Landscaping will be trimmed back prior to winter to minimize snow collection onsite. Independent contractors will be hired to manage snow removal and snow drifting management. | |
| 27 | Weather protection Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages. |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | NA |
| 28 | Night time design |
| UDRP Commentary 2 | |
| Applicant Response | |
| UDRP Commentary 1 | Support with Conditions, further review required |

**DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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MAP 14NE

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| | UDRP recommends the applicant further consider integrated lighting design solutions for a more intentional vitalization of night-time placemaking activities and events, including the provision for public art installations. | |
| | Applicant Response | |
| | -Refer to comment #28 for lighting strategy. | |
| 29 | Barrier free design Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps. | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | Raised and at-grade crossings are shown. Further design development of expanded sidewalk areas need to integrate barrier-free design considerations. UDRP encourages barrier-free building access, raised sidewalk crossings and expandable shared-use sidewalks/plazas. | |
| | Applicant Response | |
| | -The site design limits areas where ramps are required. Barrier-free building access has been provided as part of our design. | |
| 30 | Winter city Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity. | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP suggests the placemaking design effort at the Savanna Bazaar could be comfortably activated in the shoulder seasons and throughout the winter months with radiant heating of outdoor patio areas. | |
| | Applicant Response | |
| | -Although it is the design intent to extend the use of the bazaar into shoulder seasons, the addition of portable outdoor radiant heating may be problematic and could pose safety hazard. Radiant heating devices would be limited to patio areas dependent on tenant need. | |
| 31 | Other | unused |
| | Service / Utility Design Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture. | |
| 32 | Waste / recycling | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support |
| | Waste and recycling enclosures are integrated into the design of Buildings A, B, C and D. Outdoor waste and recycling enclosures are included in the design of Buildings E, F and G. | |
| | Applicant Response | |
| 33 | Enmax (Power) / Atco (Gas) Utility boxes should not be visible from major pedestrian routes. | |
| | UDRP Commentary 2 | |

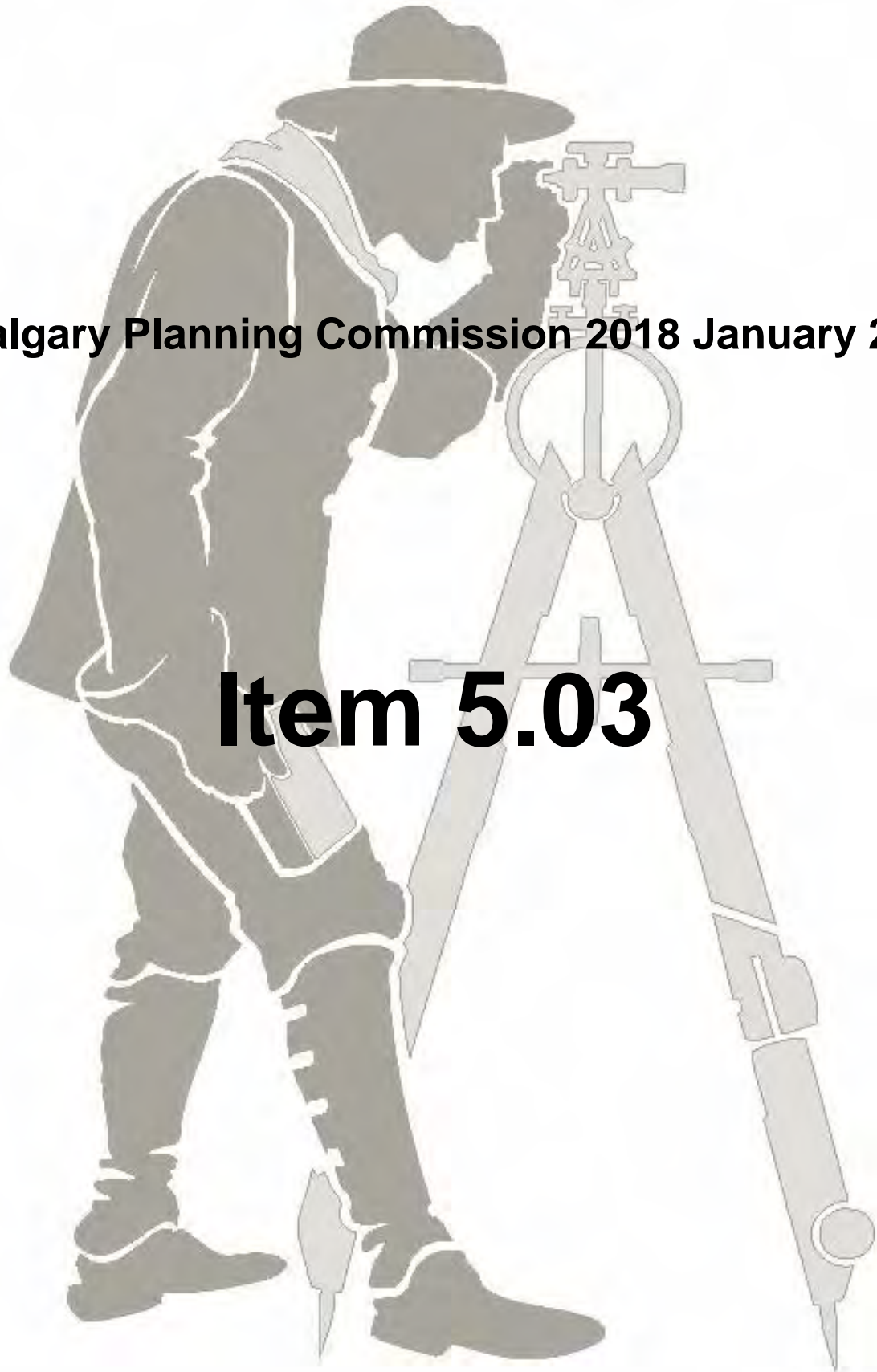
DEVELOPMENT PERMIT – NEW: LIQUOR STORE, MEDICAL CLINIC,
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MAP 14NE

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| | Applicant Response | |
| | UDRP Commentary 1 | Not reviewed. |
| | Applicant Response | |
| 34 | Transformer / switchgear | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Not reviewed. |
| | Applicant Response | |
| 35 | Exhaust / intake | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Not reviewed. |
| | Applicant Response | |
| 36 | Electrical vaults | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Not reviewed. |
| | Applicant Response | |
| 37 | Loading | |
| | UDRP Commentary 2 | |
| | Applicant Response | |
| | UDRP Commentary 1 | Support with Conditions, further review required |
| | UDRP feels that if Building A were shifted north to meet the main street character along the angled-parking roadway then the expanded parking area to the southwest (most visible from the 88 Avenue NE and Savanna Street NE intersection) should be landscaped to buffer the view of the Building A loading area. | |
| | Applicant Response | |
| | Screening of the loading area for Building 'A' has been provided and extra landscaping where possible outside of the Utility Right-of-Way. The amount of landscaping is limited in the south edge of the site due to the utility right of way in the setback area usually used for planning and landscaping screening. A landscape fence with vegetative screening has been proposed to the south parking area to the south west corner of the site to address this concern. | |
| 38 | Fire truck access | |
| | UDRP Commentary 2 | |

Calgary Planning Commission 2018 January 25

Item 5.03



LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

EXECUTIVE SUMMARY

This land use amendment proposes to redesignate a parcel from DC Direct Control District to Multi-Residential – Contextual Grade-Oriented (M-CG) District to allow for multi-residential development.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommend **APPROVAL** of the proposed Land Use Amendment.

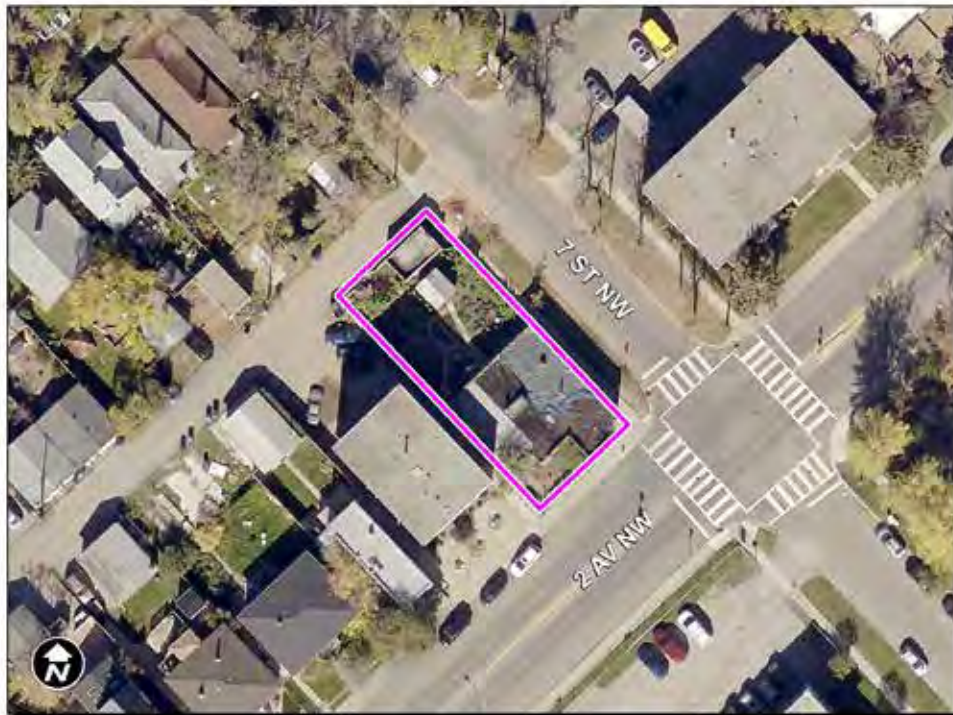
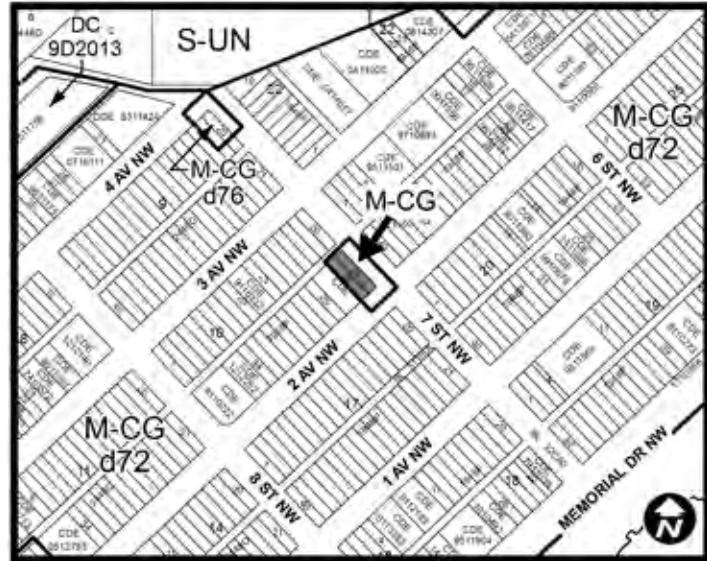
REASON(S) FOR RECOMMENDATION:

The proposal is in keeping with the applicable policies of the Municipal Development Plan and the Hillhurst/Sunnyside Area Redevelopment Plan. The proposed M-CG land use district is intended for parcels in proximity to or adjacent to low density residential development. The proposal for this inner city parcel of land allows for a development that can be compatible with the character of the existing neighbourhood. In addition, the subject parcel is a corner site, is located within walking distance of several transit stops, and has direct lane access.

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

LOCATION MAPS



LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares ± (0.14 acres ±) located at 802 – 2 Avenue NW (Plan 1948P, Block 10, Lots 21 and 22) from DC Direct Control District **to** Multi-Residential – Contextual Grade-Oriented (M-CG) District.

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

Applicant:

O2 Planning and Design

Landowner:

Rob Froese

PLANNING EVALUATION

SITE CONTEXT

The subject site is located in the community of Sunnyside, at the northwest corner of 7 Street NW and 2 Avenue NW. The site is currently occupied by a single storey commercial building with a detached garage at the rear lane. The subject property, historically known as the 'Sunnyside Grocery', and built in 1911, is listed on Calgary's Inventory of Evaluated Historic Resources (the 'Inventory') and formally acknowledges the site to have 'Community Significance'. The property's listing on the Inventory does not protect or regulate the property however. Surrounding land uses include a mix of low to medium density residential development including apartments as well as single detached and semi-detached residential dwellings. To the south of the site, across 2 Avenue NW, is the Sunnyside Elementary School. The site is also within walking distance of the Sunnyside LRT Station, the Bow River Pathway and the Peace Bridge.

LAND USE DISTRICTS

The proposed land use district is Multi-Residential – Contextual Grade-Oriented (M-CG) District. The M-CG District is intended to accommodate multi-residential, grade-oriented development in a variety of forms. The rules of the M-CG District provide for development that is low in height and sensitive to adjacent low-density residential development, such as single detached and semi-detached dwellings. The proposed M-CG District would allow for up to 6 units on the subject site.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP). While the SSRP makes no specific reference to the site, the recommendation ensures an appropriate mix of land uses are developed in an orderly, efficient, safe and economical manner (subsection 8.11). The proposal also provides for a variety of residential environments which feature innovative designs and densities and which make use of existing facilities, infrastructure and public transportation (subsection 8.14).

**LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW**

MAP 21C

Municipal Development Plan (Statutory / Approved by Council – 2009)

The subject site is located within the Developed Residential –Inner City Area as identified on Map 1 of the Municipal Development Plan (MDP). Both City-Wide policies and Inner City Area policies apply. In general, these policies encourage redevelopment in inner city communities that is similar in scale and built-form to existing development, including a mix of housing. In addition, MDP policies encourage higher residential densities in areas that are more extensively served by existing infrastructure, public facilities, and transit.

Hillhurst/Sunnyside Area Redevelopment Plan (Statutory / Approved by Council – 1988)

The subject site is located in the *Low Density Multi-Unit* area as identified in Part I of the Hillhurst/Sunnyside Area Redevelopment Plan (ARP). The *Low Density Multi-Unit* area policies envision redevelopment in the form of low profile family-oriented redevelopment. Appropriate redevelopment would consist of single and two unit dwellings, and small multi-unit developments.

TRANSPORTATION NETWORKS

A Traffic Impact Assessment was not required as part of this application. The subject site is located within walking distance from the Sunnyside LRT Station as well as nearby transit stops along 2 Avenue NW. The site is also within close distance of the Bow River Pathway and Peace Bridge providing for excellent pedestrian and bicycle connectivity.

UTILITIES & SERVICING

Water, sanitary, and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements at this time.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment (ESA) was not required as part of this application.

ENVIRONMENTAL SUSTAINABILITY

An analysis of sustainability measures to be incorporated into the development will occur at the Development Permit stage.

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

GROWTH MANAGEMENT

The proposed Land Use Amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns.

PUBLIC ENGAGEMENT

Community Association Comments

The Hillhurst Sunnyside Planning Committee (HSPC) was circulated as part of this application. A letter was submitted by the HSPC, which indicated general support for the land use amendment (APPENDIX II). However, they did raise some concern regarding parking for a proposed development and indicated they would like to see some form of commemoration of the heritage of the existing building with new development. Administration, and particularly Heritage Planning, has worked with applicants to provide for commemoration of former historic resources within the community of Hillhurst/Sunnyside. The applicant has been made aware of this request and is willing to work with Administration on a creative approach to commemorating the former historic building through the redevelopment of the site.

Citizen Comments

There were three letters received from the surrounding residents all in support of the proposed land use.

Public Meetings

The applicant held a public open house for the project in 2017 December. Approximately 50 people attended the open house and verbal feedback was primarily positive. Opportunity for written feedback was provided. Approximately 26 comments were received and were generally positive.

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

APPENDIX I

APPLICANT'S SUBMISSION

Sunnyside Sustainable Living is proposing to develop a 6-unit family focused townhouse project on the parcel located at 802 2nd Avenue NW. In support of the development, O2 Planning + Design has submitted a land use amendment application to redesignate the parcel from a Direct Control District (DC785) to Multi-Residential Contextual Grade-Oriented (M-CG). Hindle Architects will be submitting a development permit to support this land use application. The proposed development has been designed to respond to the specific conditions of the site and the development permit provides details on how the proposed M-CG land use will be implemented. The key attributes of the proposed development include:

- **Transit Oriented Development:** Situated within 400 metres of the Sunnyside LRT station, the development contributes to the evolution of Hillhurst/Sunnyside as a complete transit oriented community.
- **Moderate Density:** Fulfills the City of Calgary goal of encouraging redevelopment to increase density and create a pedestrian-friendly environment. Sunnyside Village will provide the necessary affordable, moderate density housing that other large developers are not interested in pursuing.
- **Family Friendly:** Located across the street from the Sunnyside School, the site provides an ideal opportunity to provide “family-oriented and affordable housing, thus enhancing the viability of local schools by creating opportunities for more families with children to move into the community” – as outlined in the Hillhurst-Sunnyside ARP.
- **Community & Social Objectives:** The Sunnyside Village Project has played an active role in the community, partnering with local artists, urban gardeners, and school children over the past three years. The aim of this redevelopment is to continue to create a positive contribution to the neighbourhood.
- **Variety of unit sizes.** The proposed mix of unit sizes ranging from 765 square feet to 1400 square feet will be attractive to a variety of future residents of different family makeups and demographics.
- **Walkability:** Located within walking distance of the shops and services of the 10th Street and Kensington Road main streets, the development will offer the opportunity for its residents to live a less auto-reliant lifestyle.
- **Street vitality.** The site’s location on a corner offers the opportunity for frontages on two streets. The introduction of two new doors on 2nd Avenue and three new doors on 7th Street will contribute to the street vitality of the neighbourhood through increased pedestrian activity.
- **Community Interaction.** Patios large enough to accommodate dining tables and chairs will increase the interactivity between the residents and their neighbours.
- **Context.** Building materials will be chosen to respect the heritage of the neighbourhood.
- **Landscape.** Landscaping will be used to introduce colour and texture and provide privacy and shade for residents

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

APPENDIX II

LETTERS SUBMITTED



January 3, 2018

Steve Jones, M.Pl., MCIP, RPP
Senior Planner
Community Planning (North Team)

Emailed to: steve.jones2@calgary.ca

RE: LOC2016-0079 | 802 2 Avenue NW / Former Sunnyside Grocery Site | Land Use Amendment
from DC 785 to M-CGd111 (Recirculated)

Dear Mr. Steve Jones,

The Hillhurst Sunnyside Planning Committee (HSPC) is pleased to provide comments on the above re-circulated application. HSPC and neighbours have been watching the evolution of this project with interest. We have not yet received the Development Permit plans, but have seen the renderings as presented at the recent open house and on the applicant's website. We understand that the applicant seeks a Land Use Amendment from the current DC 785 grocery store use to M-CGd111 to accommodate a multi-residential use.

Hillhurst Sunnyside Area Redevelopment Plan (ARP)

Generally, we are pleased to see an adherence to the vision in the Hillhurst Sunnyside Area Redevelopment Plan, particularly with the developer's interest in providing gentle density in the form of family-oriented housing. We are generally supportive of the inclusion of affordable housing, but are uncertain about long-term affordability as that is to be determined, should the developer partner with another agency, such as Norfolk Housing and is beyond the scope of this review.

Ground-oriented housing is a welcome feature in this neighbourhood and aligns with the proposed Multi-Residential – Contextual Grade Oriented designation. The M-CG Land Use Designation is consistent with the surrounding community.

While the existing Sunnyside Grocery building is on the City of Calgary Inventory of Historic Resources, it is not formally designated and protected. We would like to see the heritage of the building and community celebrated with the new development.

Density

The Land Use Redesignation from its current restrictive DC use to M-CGd111 is generally favourable as it matches the surrounding community, which is zoned as M-CGd72. We understand that a density of 111/HA would allow 6 units for this site. Given the location of the site, a corner lot adjacent to older,

LAND USE AMENDMENT
SUNNYSIDE (WARD 7)
EAST OF 7 STREET NW AND NORTH OF 2 AVENUE NW

MAP 21C

non-conforming multi-residential buildings that pre-date the ARP as well as its proximity to Sunnyside Elementary School, we see merit in the proposed zoning.

Parking

We understand that the application seeks a parking relaxation of 3 stalls from the required 8. Parking is always an issue in inner-city neighborhoods, and the proposed relaxation seems extreme. Generally, the argument is that it is not appropriate to download a parking issue from the development to the neighborhood, streets and immediate neighbors.

To that end, we would strongly suggest a DC zoning that limits the issuance of parking passes to 3, one for each unit that does not have a designated onsite parking spot. Additionally, per the ARP, multi-residential building are not to receive on-street permits and we would like to work with the City and the Calgary Parking Authority to ensure enforcement for no new parking permits.

Public Engagement

The developer's public engagement the new recirculated application has been excellent so far:

- The applicant's team presented to the HSCA planning committee on November 14, 2017.
- The applicant's team hosted an open house at Vendome Café on December 6, 2017. We were provided material and a placeholder website to circulate to our membership and online.
- The applicant has provided a dedicated project website (sunnysideliving.ca).
- The applicant continues to provide project updates over social media (facebook.com/sunnysidegrocery).
- The applicant informed us that there will be an additional open house in the future once the plans are more finalized.

We advertised the City of Calgary's *Request for Comment on Application* online and on social media to invite residents to submit feedback to the City File Manager directly and to copy the HSCA and councillors office. Feedback has been positive, however we temper our initial comments as we are not certain if the adjacent and most affected neighbours have been involved with the new application.

We will defer comments on the building design to the later Development Permit stage.

Sincerely,

Hillhurst Sunnyside Planning Committee
Hillhurst Sunnyside Community Association

Cc: Robert McKercher, Chair, Hillhurst Sunnyside Planning Committee
Peter Bolton, Decker Butzner, Lorna Cordeiro, Patti Dawkins, Eliot Tretter, Members, Hillhurst Sunnyside Planning Committee
Lisa Chong, Community Planning Coordinator, Hillhurst Sunnyside Community Association
Brian Horton, O2 Planning + Design, Applicant's Planning Team
Dale Calkins, Senior Policy & Planning Advisor, Ward 7 Councillor's Office
City of Calgary Circulation Control (cpag.circ@calgary.ca)

A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is mounted on a tripod and has a circular level at the top. The surveyor is holding a pencil in his right hand, which is positioned near the instrument's base. The entire scene is rendered in a dark grey silhouette against a light background.

Calgary Planning Commission 2018 January 25

Item 5.04

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

EXECUTIVE SUMMARY

This land use amendment application in the community of Sage Hill seeks to redesignate a 0.36 hectares \pm (0.89 acres \pm) L-shaped portion of subject site from Multi-Residential – Low Profile (M-1d60) District to Commercial - Neighbourhood 2 (C-N2) District. This proposed amendment to a portion of the 3.88 hectares \pm (9.58 acres \pm) site is intended to increase the size of the existing Commercial – Neighbourhood 2 (C-N2) District area in the northwest corner of the site to a total size of 0.72 hectares \pm (1.78 acres \pm) in order to accommodate a proposed neighbourhood-scale commercial development with a range of uses that complements and serves the adjacent residential developments.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION'S RECOMMENDATION

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

REASON(S) FOR RECOMMENDATION:

The proposed land use redesignation is compatible with applicable policies identified in the Municipal Development Plan (MDP) and the Symons Valley Community Plan. The proposal involves minor changes to the previously approved land use amendment application by expanding the area of the existing commercial land use district in order to accommodate a neighbourhood-scale commercial development that complements and serves the needs of the adjacent residential developments.

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.36 hectares ± (0.89 acres ±) located at 45 Sage Meadows Circle NW (a portion of Plan 1711731, Block 7, Lot 10) from Multi-Residential – Low Profile (M-1d60) District **to** Commercial – Neighbourhood 2 (C-N2) District.

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

Applicant:

B&A Planning Group

Landowner:

Genesis Land Development Corporation

PLANNING EVALUATION

SITE CONTEXT

The subject site is located south of 136 Avenue NW and east of Symons Valley Road NW within the community of Sage Hill. The site's total area is approximately 3.88 hectares \pm (9.58 acres \pm) in size, and is predominately flat and currently vacant and undeveloped. This land use amendment application affects a 0.36 hectares \pm (0.89 acres \pm) L-shaped portion of site.

A municipal reserve (MR) parcel intended for a future public elementary/junior high site exists to the north of the parcel, directly across 136 Avenue NW. A mix of newly constructed and under construction single detached dwellings exist to the east, while a range of constructed multi-residential and single detached dwellings exist to the south. One residential acreage and a future multi-residential development site exist to the west.

According to data from The City of Calgary 2017 Census highlighted below, the population of the still growing new community of Sage Hill peaked in 2017. Accordingly, the community continues to experience ongoing growth and development.

| Sage Hill | |
|------------------------------------|-------|
| Peak Population Year | 2017 |
| Peak Population | 6,083 |
| 2017 Current Population | 6,083 |
| Difference in Population (Number) | 0 |
| Difference in Population (Percent) | 0% |

On 2016 March 07, Council approved Bylaw 37D2016 and redesignated the site from DC Direct Control District (DC36Z2007) to Multi-Residential – Low Profile (M-1d60) District and Commercial – Neighbourhood 2 (C-N2) District.

In 2017 August, the review of a subdivision application (SB2017-0278) by Administration to create two parcels - a small parcel in the northwest corner of the site to accommodate a future commercial development and a larger parcel for a future multi-residential development led to the identification that the proposed tentative plan boundaries did not conform with the approved land use district boundaries. This subdivision application is on hold, pending the outcome of this land use amendment application to adjust the land use boundaries.

**LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW**

MAP 31N

In addition to these two identified applications, Administration is also reviewing two development permits received separately for the site for a proposed small-scale commercial development and a proposed multi-residential development. These applications are currently under review.

LAND USE DISTRICTS

This proposal seeks to redesignate an L-shaped portion of the site currently designated as Multi-Residential Low Profile (M-1d60) District to the Commercial-Neighbourhood 2 (C-N2) District. The C-N2 District is intended to allow small-scale commercial developments. The proposed adjustments to the land use boundaries would constitute a minor bylaw amendment and would ensure the parcel continues to support a mix of uses that maintain the purpose of the Neighbourhood Node Area as outlined in the Symons Valley Community Plan.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP). While the SSRP makes no specific reference to this site, the recommendation ensures an appropriate mix of land uses are developed in an orderly, efficient, safe and economical manner (subsection 8.11). The proposal also contributes to a healthy environment, a healthy economy, and a high quality of life by expanding an existing commercial land use district area to accommodate a proposed development that meets local community needs and has direct and walkable access to adjacent residential developments (subsection 8.12).

Municipal Development Plan (2009)

The subject site is located within the Residential Developing - Planned Greenfield with Area Structure Plan (ASP) area, according to Urban Structure Map (Map 1) of the Municipal Development Plan (MDP). While ASPs existing prior to the adoption of the MDP area are considered appropriate policies to provide specific direction for development of respective communities, there are several MDP policies, which support the proposed land use redesignation. These include supporting the development of complete communities, ensuring a mix of land uses surrounding transit stops, creating neighbourhood retail centres to serve community needs, and locating local commercial developments to serve and be directly accessible to adjacent higher density residential areas of a community.

The Symons Valley Community Plan (2001)

The Symons Valley Community Plan, the area structure plan (ASP) for the local area, identifies the subject site as a Neighbourhood Node Area. The purpose of a neighbourhood node is to provide a social and transit focus for the surrounding residential neighbourhood within the

**LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW**

MAP 31N

community. A neighbourhood node is intended to accommodate a concentration of housing located near a central park as well as one or more transit stops. Other uses, including public or separate schools and neighbourhood-scale commercial, institutional and recreational uses, may also be located within a neighbourhood node. In terms of surrounding areas, the ASP also indicates a joint use site to the north, a residential area and a community park to the east, a residential area to the south, and a transit oriented planning area to the west of the subject site.

When Bylaw 37D2016 was approved in 2016, the land use designation was intended to allow for largely residential uses of low height and medium density with an opportunity for neighbourhood-scale commercial uses in order to align with the purpose of the Neighbourhood Node Area and respond to changes in the market. Expanding the existing Commercial Neighbourhood 2 (C-N2) District would continue to meet this purpose and would align with the direction of the previously approved land use designations. The proposed land use designations conform to the ASP as the proposed expansion of the commercial area will assist in accommodating and realizing a slightly larger neighbourhood-scale commercial development with a range of uses that complements and serves the adjacent residential developments.

TRANSPORTATION NETWORKS

The subject site is bounded by 136 Avenue NW to the north, Symons Valley Road NW to the west, Sage Meadows Circle NW to the south, and existing residential development to the east.

The proposed commercial site is located within an approximately 1 kilometre (15 minute) walk of the future Transit Hub along Sage Hill Drive NW. Transit Service will be provided immediately adjacent the subject lands on 136 Avenue NW and on Symons Valley Road NW. Transit stops will be located immediately adjacent the commercial site.

Vehicular access to the site will be determined at the subdivision and development permit stage.

UTILITIES & SERVICING

Water, sanitary and storm sewer mains are available to service the site and can accommodate the proposed land use without the need for off-site improvements at this time.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required for this land use amendment application.

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

ENVIRONMENTAL SUSTAINABILITY

The proposed land use redesignation will enable commercial development that can accommodate a greater range of uses that are within a short walking distance of and have direct pedestrian connections to transit and adjacent residential developments. This will contribute to creating a healthy and complete community and may enhance the long-term economic viability of various uses within the proposed development through ensuring local residents will have direct and convenient access to daily commercial and retail services. Sustainability measures are currently under review as part of the evaluation of the development permit application received separately for the commercial site.

GROWTH MANAGEMENT

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

PUBLIC ENGAGEMENT

Community Association Comments

The Sage Hill Community Association identified by email correspondence on 2017 November 25 that they had no objection to this proposed application and development. The email correspondence is attached in APPENDIX II of this report.

Citizen Comments

No comments were received by the CPC report submission date.

Public Meetings

No public meetings were held by the Applicant or Administration in association with this application.

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

APPENDIX I

APPLICANT'S SUBMISSION



Applicant's Submission - 45 Sage Meadows CI NW

Introduction

The subject property is a portion of Lot 10, Block 7, Plan 1711731 which is located in the northwest quadrant of the City of Calgary. This property is currently zoned Multi-Residential Low Profile (M-1d60) district and Commercial – Neighbourhood 2 (C-N2) district, is vacant of uses and has been stripped and graded. The land owner, Genesis Land Development Corporation, is proposing to redesignate a ±0.36 hectares (0.89 acres) L-shaped portion of the site from M-1d60 to C-N2 in order to expand the existing C-N2 land use area to accommodate a future commercial development. Please note that subdivision and development permit applications have been submitted to support this development.

Proposed Development

The subject site is intended to be developed to accommodate local commercial uses. It is proposed that the commercial development share vehicular and pedestrian access from 136 Avenue NW with the adjacent future multi-residential development to the east to ensure strong community linkages. A right-in from Symons Valley Road NW is also identified to serve the development and support the proper internal circulation of vehicles.

Current & Proposed Land Use Designation

The subject ±0.36 hectares (0.89 acres) are designated as the M-1d60 district under the City of Calgary Land Use Bylaw. The M-1 district allows for multi-residential uses of low height and medium density. The density modifier limits the density of the site to 60 units per hectare. To allow for the envisioned development, it is proposed that the land be redesignated to the C-N2 district. The purpose of the C-N2 district is to allow for small scale commercial developments.

Policy Considerations

The subject site falls under the Symons Valley Community Plan (ASP) and is identified to be within a Neighbourhood Node. The purpose of a Neighbourhood Node is to provide a social and transit focus for the surrounding residential neighbourhood within the community. Neighbourhood-scale commercial is one of the uses anticipated for a Neighbourhood Node.

Summary

The proposed land use redesignation from M-1d60 to C-N2 would expand the area already designated as C-N2 on the subject property. The additional area will allow for a slightly larger commercial development to serve the daily needs of the surrounding residential area. Providing local commercial uses on this site is in conformity with the Neighbourhood Node requirements of the Symons Valley Community Plan. In consideration, we respectfully request the support of the City of Calgary Administration and Council for this Land Use Bylaw amendment.

LAND USE AMENDMENT
SAGE HILL (WARD 2)
SOUTH OF 136 AVENUE NW AND EAST OF SYMONS VALLEY
ROAD NW

MAP 31N

APPENDIX II

COMMUNITY ASSOCIATION LETTER

From: [Pete Steenaerts](#)
To: [CPAG Circ](#)
Cc: [McLeod, Fraser A.](#)
Subject: [EXT] RE: LOC2017-0296- Circulation Package
Date: Saturday, November 25, 2017 10:29:13 AM
Attachments: [image003.png](#)
[image004.png](#)

Hello Fraser,

Thanks for the email and call on this, my apologies, I have been away on vacation and am just getting caught up on emails.

By way of this email, the Sage Hill Community Association has no objection to this proposed development. The plans and request for the change have been socialized with us by the Ward 2 councillor and his staff and the board of directors has not expressed any concerns with this project.

Thank you for your diligence in ensuring our feedback was considered, and again I apologize for the delay in replying.

Cheers,
Pete

Pete Steenaerts | President
Sage Hill Community Association
Secretary | **Federation of Calgary Communities**

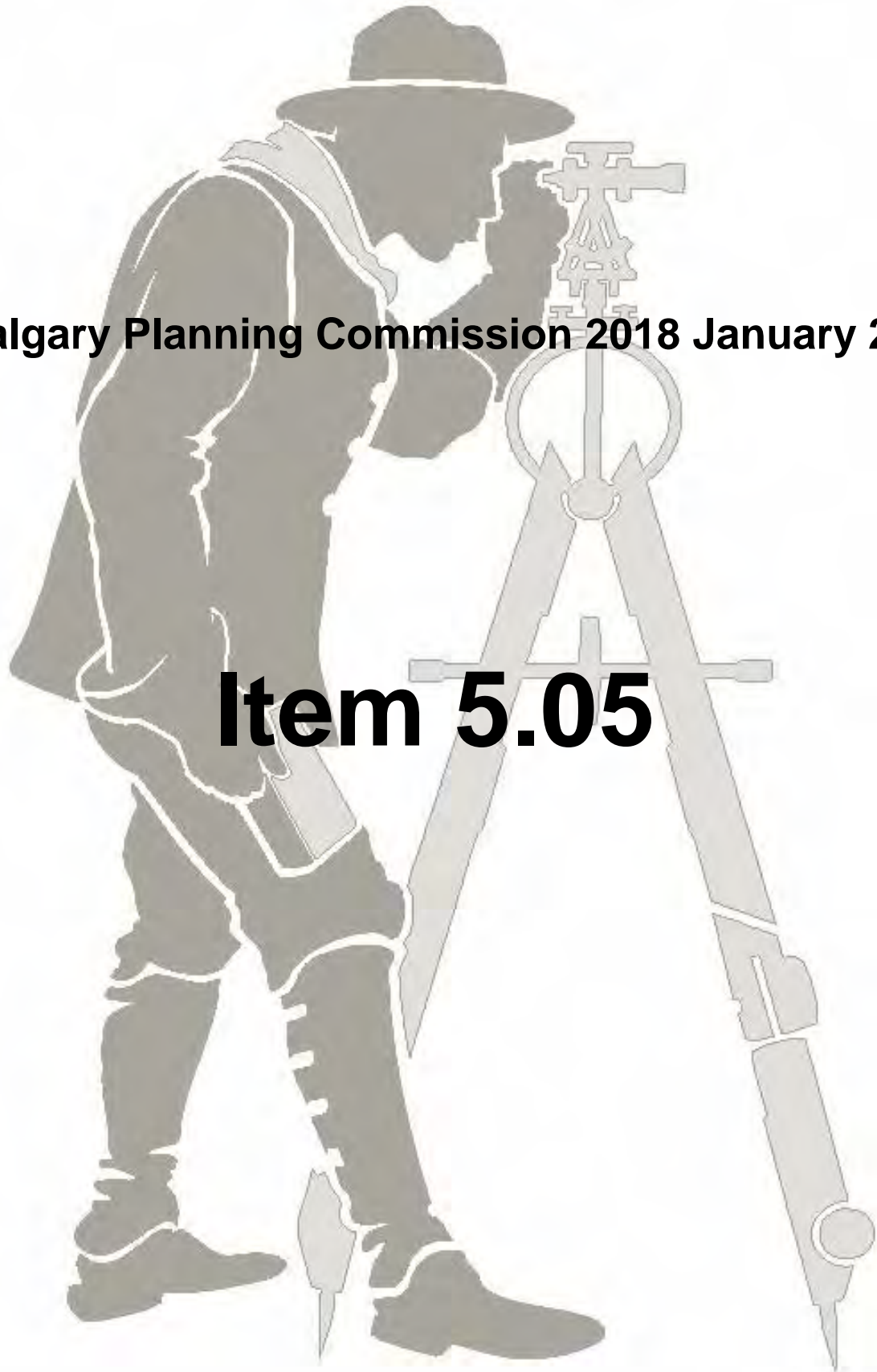
██████████
@SageHillPres



Before printing this e-mail please consider if it is necessary to do so

Calgary Planning Commission 2018 January 25

Item 5.05



LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

EXECUTIVE SUMMARY

This land use amendment seeks the redesignation of a corner parcel in the residential community of Altadore to accommodate the development of new rowhouses. The proposed amendment is from Residential-Contextual Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District. R-CG is a residential designation that primarily accommodates rowhouses but also allows for single detached, side-by-side, and duplex homes that may include a secondary suite. The Applicant intends to build a four unit rowhouse, however an application for development permit has not submitted at this time.

An amendment to the South Calgary/Altadore Area Redevelopment Plan (ARP) is required to accommodate the proposed land use amendment. The amendment changes the land use policy for the site, from “Residential Conservation” to “Residential Low Density” as shown in Map 2 of the South Calgary/Altadore ARP (APPENDIX II).

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment.

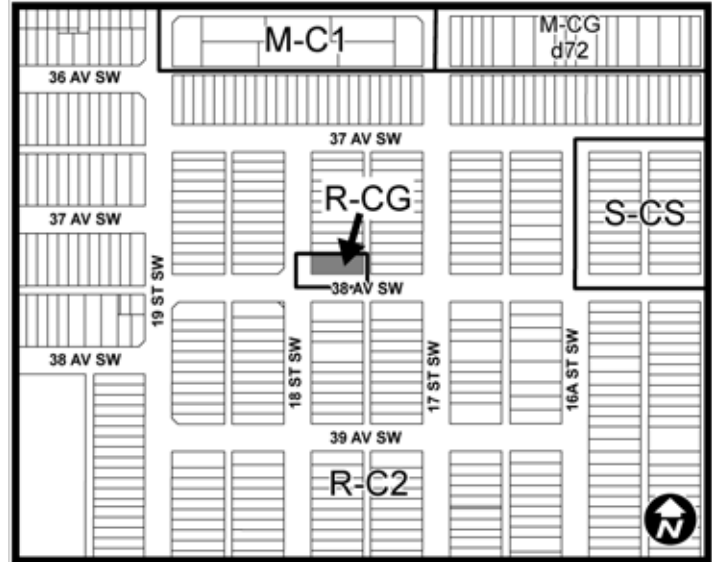
REASON(S) FOR RECOMMENDATION:

The proposed Land Use Amendment complies with the general goals of the Municipal Development Plan to accommodate a moderate increase in density within developed areas, and to maximize the use of existing infrastructure. Rowhouses are an appropriate built-form on this corner site, which helps maintain a residential streetscape, and would be complimentary to the proposal on LOC2017-0291. Furthermore, the proposed redesignation would accommodate multi-residential development that meets the Location Criteria for Multi-Residential Infill.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

LOCATION MAPS



LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendment to the South Calgary/Altadore Area Redevelopment Plan (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.143 acres \pm) located at 3822 - 18 Street SW (Plan 2901P, Block A, Lots 13 and 14) from Residential-Contextual Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

Applicant:

Willix Developments

Landowner:

Willix Developments Ltd

PLANNING EVALUATION

SITE CONTEXT

The subject parcel is located in the community of Altadore in the southwest quadrant of the city. The site is currently developed with a single detached dwelling with a rear detached single garage that is accessed from 38 Avenue SW. Located at the south-west corner of 38 Avenue SW and 18 Street SW, the site is situated within a neighbourhood comprising primarily of single and semi-detached dwelling units.

| Altadore | |
|------------------------------------|---------|
| Peak Population Year | 2015 |
| Peak Population | 9 867 |
| 2017 Current Population | 6 795 |
| Difference in Population (Number) | -3 072* |
| Difference in Population (Percent) | -31%* |

* A portion of Altadore was subtracted in 2016 to create the community of Garrison Woods. As a result, the population of Altadore will be much higher in years prior to 2016. A more accurate indication of population growth or decline within the newly defined boundaries for Altadore would result from data collected for 2018.

LAND USE DISTRICTS

Current Land Use District

The Residential – Contextual Two Dwelling (R-C2) District is intended to accommodate existing residential development in the form of duplex, semi-detached and single detached dwellings in developed areas of the City.

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW**

MAP 5C

Proposed Land Use District

The application initially proposed Multi-Residential – Contextual Grade-Oriented (M-CG) District to accommodate a multi-residential development. However, in response to several concerns raised by the community and area residents, the application was amended to pursue the The Residential – Grade-Oriented Infill (R-CG) District.

The Residential – Grade-Oriented Infill (R-CG) District is intended to do the following:

- accommodate existing residential development;
- accommodate grade-oriented development in the form of rowhouse, duplex, semi-detached and cottage housing clusters;
- accommodates secondary and backyard suites within new and existing residential development;
- provide flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time; and
- accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

The Residential – Grade Oriented infill (R-CG) District would provide additional low density infill housing forms along with potential for secondary suites, at a maximum density of 75 units per hectare. A moderate increase in density would occur upon redesignation and redevelopment of the subject parcel in the form of a four unit rowhouse.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

The subject parcel is located within the Developed Residential Area - Inner City of the MDP, which supports moderate intensification that respects the community context. In general, these policies encourage redevelopment of the Inner City area in a way that is similar in scale and built form to existing development and contributes to a greater variety of housing types overall. The MDP also encourages higher residential densities in areas that are well serviced by existing infrastructure, public amenities, and transit.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

The proposed Residential – Grade-Oriented infill (R-CG) District allows for development that is sensitive to the existing context and conforms to the relevant policies of the MDP. The subject site is located on a corner parcel, and will allow for a greater variety of housing forms in the area. For this reason, the proposed redesignation is found to comply with the general goals of the MDP while providing sound rationale to accommodate the minor amendment to the South Calgary/Altadore Area Redevelopment Plan.

South Calgary/Altadore Area Redevelopment Plan (ARP)

The subject parcel is located within the area designated “Residential Conservation”.

Section 2.2(a) provides “Conservation and Infill” policies for the subject parcel and surrounding areas. The intent of this policy is as follows:

- improve existing neighbourhood quality and character;
- permitting low profile infill development compatible with surrounding dwellings;
- existing structures in good repair should be conserved;
- structures in poor repair should be rehabilitated or replaced; and
- narrow lot (7.5 metres or 25 foot) infill dwellings should be of a design that would encourage families with children to move into them.

The proposed redesignation of the subject parcel to Residential – Grade-Oriented Infill (R-CG) District would continue to comply with this policy. The land use amendment would provide for infill development with moderate increase in density, allowing for a variety of appropriate low profile housing forms, compatible with surrounding dwellings. Rowhouses are not available in the Residential – Contextual Two Dwelling District (R-C2).

Minor Amendment to South Calgary/Altadore ARP

The redesignation of the parcel to Residential – Grade-Oriented Infill (R-CG) District would result in a site specific minor amendment to Map 2 (Land Use Policy) of the South Calgary/Altadore ARP from “Residential Conservation” to “Residential Low Density” (APPENDIX II).

Location Criteria for Multi-residential Infill

In 2014, Council implemented ‘Location Criteria for Multi-Residential Infill’ to provide a more specific tool for the review of land use amendment applications in the Developed Areas and associated amendments to local area plans. The proposed land use generally aligns with several of the location criteria for multi-residential infill development in low density residential areas. The table in APPENDIX IV provides a summary of alignment with the criteria. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP, to assist in determining the appropriateness of an application in the local context.

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW**

MAP 5C

TRANSPORTATION NETWORKS

The subject site is located approximately 260 metres away from the Southbound 13 bus stop with service to a bus hub at Mount Royal University. This bus route also links to the 72, 73, 181, 182, and 306 BRT bus lines. Bus route 306 is a Primary Transit route, and is approximately 2.75 kilometres away from the site. The site is also approximately 280 metres away from an eastbound 13 bus stop with service to the downtown core, and LRT.

The subject site is a corner lot, with lane access, and no parking restrictions. There is an existing curb cut on 38 Avenue SW requires closure and rehabilitation at the time of redevelopment. Access will be required from the lane. Parking is to be provided at the time of development permit, and will need to meet all the required specifications and bylaw requirements for parking stall metrics.

UTILITIES & SERVICING

A portion of the proposed development lies within the Elbow River flood fringe. A time of development application, site design and flood mitigation will be assessed.

Water main is available along 18 Street SW. The existing water system can support the proposed development. No upgrade is needed. The Sanitary sewer is available along 18 Street SW. If the proposed density is over 55 person/ha, and the proposed Peak Wet Weather Flow (PWWF) has exceeded 1L/s, provide a Sanitary servicing study at development permit stage.

No storm sewer is immediately available to service the site. The storm sewer can be extended westerly along 38 Avenue SW at the expense of the developer. Use of a dry well will be determined at the development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

ENVIRONMENTAL SUSTAINABILITY

Sustainability measures will be evaluated at the development permit stage.

GROWTH MANAGEMENT

The proposed amendments do not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW**

MAP 5C

PUBLIC ENGAGEMENT

Community Association Comments

The Marda Loop Community Association provided objections by way of letters submitted for the proposal for M-CG on 2017 November 17 and subsequently in response to the proposal for R-CG on 2017 December 27 (APPENDIX III). Additionally, 93 letters in opposition to the original M-CG proposal were received, followed by 39 letters in opposition to the proposal for R-CG. One (1) letter of support was also received. The common concerns among the letters are as follows:

- Potential of the subject parcel to house four (4) units along with 4 secondary suites.
- Potential increase of demand for on-street parking.
- Rowhousing is seen as not compatible with adjacent developments.
- Multi-residential location criteria are too broad and would allow multi-family development in significant portions of low density areas.

Administration reviewed the concerns raised by various residents and community stakeholders and believes the proposed redesignation continues to be appropriate. The only impact projected would be the loss of on-street parking stalls for immediate visitors. Community-wide parking issues are mitigated by the adoption of a Residential Parking permit zone through the Calgary Parking Authority. As stated previously in the report, the subject parcel is not located within a Residential Parking permit zone. Furthermore, the site is found to sufficiently satisfy the location criteria for multi-residential infill, which is typically used as criteria to assess appropriateness of multi-residential development in land use districts of greater density than this proposed redesignation.

Citizen Engagement

The Applicant sent information handouts to immediate neighbours within a two block radius of the proposed amendments, and have been regularly communicating with the Community Association to advise them about the application. The Applicant presented their amendments to the Community Association in December 2017. While a development permit package is not submitted at this time, the Applicant intends to share their design concept on 2017 January 29 through an open house.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

APPENDIX I

APPLICANT'S SUBMISSION

Applicant's Planning Analysis

Design Intent and Context

The subject parcels are located in the community of South Calgary / Altadore and consist of 0.058 ha each. We are proposing 2 four-unit rowhouse developments with front doors facing 38 AV SW, 18 ST SW and 17 ST SW, and with vehicular access from the adjacent lane. All parking would be provided on-site, at grade, in an enclosed garage.

The current land use designation in both parcels is (R-C2) Residential – Contextual One / Two Dwelling District which allows for duplex, single and semi-detached dwellings. To support the proposed development, we are applying to change the designation from R-C2 to R-CG Residential – Grade-Oriented Infill District.

Like R-C2, the R-CG District is a Low Density Residential District intended for grade-oriented development and does not support multi-residential uses. The Land Use Bylaw explains that the R-CG District:

accommodates grade-oriented development in the form of Rowhouse Buildings, Duplex Dwellings, Semi-detached Dwellings and Cottage Housing Clusters, accommodates Secondary Suites and Backyard Suites with new and existing residential development, provides flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time, and accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

In addition, the site has specific attributes which make it ideal for rowhouse development:

- It is less than 5 minutes' walk from the bus stops along 16 ST SW.
- The site is less than 5 minutes' walk from Kiwanis Park.
- The site is less than 5 minutes' walk from commercial establishments along 16 ST SW.
- The site is less than 10 minutes' walk from Dr. Oakley School.
- The site is less than 15 minutes' walk from River Park.
- The site is less than 5 minutes' drive from commercial establishments along 33 AV SW.
- The site is less than 5 minutes' drive from Glenmore Athletic Park.
- The site is less than 10 minutes' drive from Mt Royal University.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

Policies (and Variations) Municipal Development Plan (MDP)

The proposed project fundamentally meets the goals of the Municipal Development Plan.

This City policy encourages more housing options in established communities, more efficient use of infrastructure, and more compact built forms in locations with direct easy access to transit, shopping, schools and other community services.

The MDP encourages modest redevelopment of the Established Area. (3.5.3 (a.)) The proposed development modestly intensifies the use of the land to a density more appropriate for a central Calgary community.

The MDP requires that “ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street.” (2.4.2 (b.)). By having all vehicular access from the adjacent lane, and unit entries facing 38 AV SW, the pedestrian realm along 17 ST SW & 18 ST SW is protected.

South Calgary / Altadore Area Redevelopment Plan (ARP)

The ARP recommends that this site be of a Conservation/ Infill use. Though not located on a collector, our parcels on 38th AV are within one block of 19th ST, a designated collector. Further, those familiar with the area recognize that 38th AV functions as a collector.

The ARP states:

The intent of the conservation and infill policy is to improve existing neighbourhood quality and character while permitting low profile infill development that is compatible with surrounding dwellings. Existing structures in good repair should be conserved, while structures in poor repair should be rehabilitated or replaced. Narrow lot (7.5 metres/25 foot) infill dwellings should be of a design that would encourage families with children to move into them.

Our proposal will allow a four unit rowhouse on each parcel, each having direct access to grade. Each unit will have two upstairs bedrooms plus one basement bedroom, 3 1/2 baths, and a private at-grade amenity space, and would be perfect for families. The proposed building is of a scale compatible with the single detached and semi-detached housing in the area.

Bylaw (and Relaxations)

The proposed development is at a preliminary stage of design. At this point, relaxations are not anticipated. Should any be proposed in the future, these will be within the intent of City policy.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

Engagement

We will be engaging the community and ward councillor thoroughly in the coming weeks to explain the change in course we're taking with these applications, and how R-CG will provide appropriate redevelopment. We will keep the Development Authority approved of this engagement as it unfolds.

To conclude, the proposed land use amendment will allow development in the form of a Rowhouse Building which is consistent with City policies, and the best use of this parcel. For these reasons, we respectfully request the support of The Development Authority, Calgary Planning Commission and Council for our application.

If you have any questions regarding this application, please contact us at your convenience.
Best regards,

Willix Developments
P.O. Box 415 Stn M
Calgary AB
T2P-2J1, Canada

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C


APPENDIX II

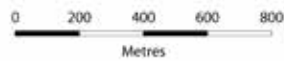
PROPOSED AMENDMENT TO THE SOUTH CALGARY/ALTADORE
AREA REDEVELOPMENT PLAN

- (a) Delete the existing Map 2 entitled "Land Use Policy" and replace with the revised Map 2 entitled "Land Use Policy", as follows:

Map 2
Land Use Policy

Legend

-  Study Area Boundary
-  King Edward School Site
-  Residential Conservation
-  Residential Low Density
-  Residential Medium Density
-  Community Mid-Rise
-  Local Commercial
-  General Commercial
-  Open Space



This map is conceptual only. No measurements of distances or areas should be taken from this map.



LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

APPENDIX III

COMMUNITY ASSOCIATION LETTER



3130 16 Street SW
Calgary, AB, T2T 4G7

December 27, 2017

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Grc@calgary.ca

Attn: Ryan Hall, ryan.hall@calgary.ca

SENT BY EMAIL

Dear Mr. Hall:

RE: Community Association Feedback for LOC2017-0290 & LOC2017-0291

Thank you for providing us the opportunity to offer feedback on these applications that reflect the vision of the Marda Loop Communities Association (MLCA). Further to our letter dated November 6, 2017, the following comments have been written with consideration towards what is best for our communities and our city.

These applications have been amended and now propose redesignations from Residential – Contextual One/Two Dwelling (R-C2) to Residential – Grade-Oriented Infill (R-CG). It is our understanding that development permit applications will be submitted in February 2018 and will propose 4-unit townhomes oriented along 38th Avenue SW on each parcel. The garages would be detached with access from the lane.

The MLCA Planning & Development Committee continues to encourage developers to engage with us and neighbours of proposed developments proactively, prior to application. Proactive communication allows time for facilitating constructive dialogue. The applicant attended the MLCA Planning & Development Committee meeting on December 18, 2017. The outcome of the meeting was a commitment by the applicant to host an open house in mid-January 2018 once they have made further progress on their proposed building designs. Additionally, they will meet with the MLCA Planning & Development Committee and the directly adjacent neighbours in the second half of January 2018 to address minimizing the impacts of the proposed developments (including but not limited to shadowing, privacy, and massing). Although future engagement is planned, it has been a challenge for residents to understand what is being proposed during the request for comment period for these applications.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 2 of 3

To date, the Committee has received written objections from approximately 36 community residents regarding the amended applications (R-CG). These objections are in addition to the 101 objections received for the initial applications (M-CG). We would note that the concerns and opposition expressed in response to the M-CG applications would, in our opinion, also apply to the R-CG applications.

Concerns expressed by neighbours include (but are not limited to):

- Geographic area features resulting in severe privacy and shadowing impact on adjacent neighboring properties.
- Significant negative pressure on parking and traffic.
- Inappropriate contextual fit for the community (this zoning does not belong at this location).
- Rezoning in this location does not align with the Calgary Municipal Development Plan (MDP).
- Rezoning does not align with the South Calgary/Altadore Area Redevelopment Plan (ARP).
- The property does not meet the location criteria for multi-residential infill either in physical proximity or purpose.
- Issues identified in the R-CG Monitoring Report M-2017-034.

The Committee has concerns that this type of development does not fit contextually within the surrounding "residential conservation" area of the community of Altadore as identified in the South Calgary/Altadore ARP. Additionally, while the applicant has committed to meeting the bylaw requirements of the R-CG district, due to the orientation of the buildings and the larger building envelope permitted in this district there will be significant impacts on the neighbours if these applications are approved.

Furthermore, these parcels do not meet The City's Location Criteria for Multi-Residential Infill. While these locations may be on corner parcels (Criteria 1), are within 400 metres of a transit stop (Criteria 2), and have direct lane access (Criteria 8), failure to achieve the purpose statements of any of the remaining five criteria suggests that these locations are not suitable for multi-residential infill.

- Criteria 3 (within 600 metres of an existing or planned primary transit stop): A SW BRT stop is not planned near these locations, nor do the #7, #107, or #13 transit routes operate at frequencies of 10 minutes or less for extended time periods.
- Criteria 4 (on a collector or higher standard roadway on at least one frontage): 17th Street SW, 18th Street SW, and 38th Avenue SW are local roads at these locations. As such, multi-residential development here would increase traffic on local residential streets.
- Criteria 5 (adjacent to existing or planned non-residential development or multi-unit development): These developments would not serve as a transition of height and massing between lower density uses and larger buildings as there are no larger buildings in the immediate vicinity.

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LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

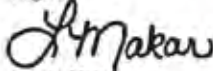
Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 3 of 3

- Criteria 6 (adjacent to or across from an existing or planned open space, park or community amenity): The nearest park is one block away, therefore, the visual impact of the increased height and massing of these developments will not be reduced by the open space, nor will these developments improve the safety of the park.
- Criteria 7 (along or in close proximity to an existing or planned corridor or activity centre): These developments are not in close enough proximity to the 33rd Avenue SW or 14th Street SW Main Streets to act as transitions and/or buffers between lower and higher density development.

While the Committee appreciates that the applicant has reduced the scale of these applications from M-CG to R-CG, it remains the position of the MLCA to strongly oppose these land use redesignation applications. As Altadore continues to re-develop at a fast pace, there are concerns around the lack of a comprehensive review of how and where increasing the density of the community is appropriate. If approved, this would set the precedent for any corner lot in Altadore to be redesignated to multi-residential irrespective of meeting the criteria identified by The City, which is not in the best interest of the community.

If you have any questions regarding these comments, please contact Lauren Makar at your convenience. Thank you in advance for considering these comments when assessing the merits of these applications.

Regards



Lauren Makar
Director - Planning & Development
Marda Loop Communities Association
development@mardaloop.com



Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

Enclosure: Letter from the MLCA – LOC2017-0290 & 0291 CA Feedback 06Nov2017

cc: Evan Woolley, Ward 8 Councillor, The City of Calgary evan.woolley@calgary.ca

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C



3130 16 Street SW
Calgary, AB, T2T 4G7

November 6, 2017

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Circ@calgary.ca

Attn: Ryan Hall, ryan.hall@calgary.ca

SENT BY EMAIL

Dear Mr. Hall:

RE: Community Association Feedback for LOC2017-0290 & LOC2017-0291

Thank you for providing us the opportunity to offer feedback on these applications that reflect the vision of the Marda Loop Communities Association (MLCA). The following comments have been written with consideration towards what is best for our communities and our city.

These applications propose redesignations from Residential – Contextual One/Two Dwelling (R-C2) to Multi-Residential – Contextual Grade-Oriented (M-CG). Based on a phone call with the applicant on October 24, 2017, it is our understanding that development permit applications will be submitted later this year and will propose 4-unit townhomes oriented along 38th Avenue SW on each parcel. The garages would be attached at the front with driveway access from 38th Avenue SW.

The MLCA Planning & Development Committee continues to encourage developers to engage with us and neighbours of proposed developments proactively, prior to application. Proactive communication allows time for facilitating constructive dialogue. The applicant first contacted the Committee on October 18, 2017, well into the brief three-week request for comment period. The Committee is aware of a letter dated October 16, 2017 that was circulated by Wilix to some neighbours. Considering the scope and context of these applications, the Committee feels the community engagement was inadequate.

To date, the Committee has received written objections from approximately 101 community residents. Concerns expressed by neighbours include (but are not limited to): the property does not meet the location criteria for multi-residential infill, rezoning does not align with the South Calgary/Altadore Area Redevelopment Plan, inappropriate contextual fit for the community, privacy and shadowing impact on adjacent neighbouring properties, and additional negative pressure on traffic, parking and utilities.

Marda Loop Communities Association
1100 16 Street SW, Calgary, Alberta T2T 4G7

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

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Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 2 of 3

The Committee has concerns that this type of development does not fit contextually within the surrounding "residential conservation" area of the community of Altadore as identified in the South Calgary/Altadore ARP.

Additionally, although the applicant proposes that these parcels meet The City's Location Criteria for Multi-Residential Infill, the Committee respectfully disagrees. While these locations may be on corner parcels (Criteria 1) and are within 400 metres of a transit stop (Criteria 2), failure to achieve the purpose statements of any of the remaining six criteria suggests that these locations are not suitable for multi-residential infill.

- Criteria 3 (within 600 metres of an existing or planned primary transit stop): A SW BRT stop is not planned near these locations, nor do the #7, #107, or #13 transit routes operate at frequencies of 10 minutes or less for extended time periods.
- Criteria 4 (on a collector or higher standard roadway on at least one frontage): 17th Street SW, 18th Street SW, and 38th Avenue SW are local roads at these locations. As such, multi-residential development here would increase traffic on local residential streets.
- Criteria 5 (adjacent to existing or planned non-residential development or multi-unit development): These developments would not serve as a transition of height and massing between lower density uses and larger buildings as there are no larger buildings in the immediate vicinity.
- Criteria 6 (adjacent to or across from an existing or planned open space, park or community amenity): The nearest park is one block away, therefore, the visual impact of the increased height and massing of these developments will not be reduced by the open space, nor will these developments improve the safety of the park.
- Criteria 7 (along or in close proximity to an existing or planned corridor or activity centre): These developments are not in close enough proximity to the 33rd Avenue SW or 14th Street SW Main Streets to act as transitions and/or buffers between lower and higher density development.
- Criteria 8 (direct lane access): While these sites do have direct lane access, the purpose of the criteria will not be met with the proposed design calling for garage access from 38th Avenue versus the lane.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 3 of 3

The applicant provided examples of other developments that do not meet criteria 4, 5, and/or 6. Upon review, however, we note the following which would indicate that these locations are more suitable for multi-residential development than the locations currently under consideration:

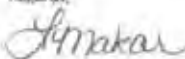
- 2937 & 2915 17th Street SW are within a large area of South Calgary zoned M-C1 and are identified as "residential medium density" in the ARP.
- 3208 20th Street SW zoned R-CG is on a collector road.
- 1631 38th Avenue SW, 3605 20th Street SW, and 3704 19th Street SW are in areas zoned M-C1, are identified as "residential medium density" in the ARP, and are on collector roads.
- 1605 3rd Avenue SW is zoned R-C2, as such we are not clear why this location was included in the application.

The Committee also notes the impact of these applications on the neighbours directly adjacent to the sites on 17th and 18th Streets SW. Due to the proposed orientation of the developments there are shadowing, privacy, and massing issues to consider.

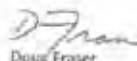
Based on the above, the MICA Planning & Development Committee strongly opposes these land use redesignation applications. As Altadore continues to re-develop at a fast pace, there are concerns around the lack of a comprehensive review of how and where increasing the density of the community is appropriate. If approved, this would set the precedent for any corner lot in Altadore to be redesignated to multi-residential irrespective of meeting the criteria identified by The City, which is not in the best interest of the community.

If you have any questions regarding these comments, please contact Lauren Makar at your convenience. Thank you in advance for considering these comments when assessing the merits of these applications.

Regards,



Lauren Makar
Director - Planning & Development
Marda Loop Communities Association
development@mardaloop.com



Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

cc: Ewan Woolley, Ward 8 Councillor, The City of Calgary ewan.woolley@calgary.ca

LAND USE AMENDMENT
 ALTADORE (WARD 8)
 NORTH OF 28 AVENUE SW AND EAST OF 18 STREET SW

MAP 5C

APPENDIX IV

LOCATION CRITERIA FOR MULTI-RESIDENTIAL INFILL

| Criteria | Assessment |
|--|---|
| Located on a Corner Parcel. | Yes Parcel is located on the south-west corner of 38 Avenue SW and 18 Street SW. |
| Within 400 metres of a transit stop. | Yes Parcel is 200 metres from westbound from Route 13 bus stop. |
| Within 600 metres of an existing or planned primary transit stop or station. | No Parcel is more than 600 metres away from nearest primary transit stop. |
| On a collector or higher standard roadway on at least one frontage. | No Nearest collector is 19 Street SW. |
| Adjacent to existing or planned non-residential development or multi-dwelling development. | No Parcel adjacent to single-detached housing to east. |
| Adjacent to or across from existing or planned open space or park or community amenity. | No Nearest park is 150 meters away. |
| Along or in close proximity to an existing or planned corridor or activity centre. | Yes Parcel is 550 meters from the 33 Avenue SW commercial street |
| Direct lane access. | Yes Parcel is currently serviced by direct lane access. |

A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is mounted on a tripod and has a circular level at the top. The surveyor is holding a pencil in his right hand, which is positioned near the instrument's base. The entire scene is rendered in a dark grey silhouette against a light background.

Calgary Planning Commission 2018 January 25

Item 5.06

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

EXECUTIVE SUMMARY

This Land Use Amendment seeks the redesignation of a corner parcel in the residential community of Altadore to accommodate the development of new rowhouses. The proposed amendment is from Residential-Contextual Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District. R-CG is a residential designation that primarily accommodates rowhouses but also allows for single detached, side-by-side, and duplex homes that may include a secondary suite. The Applicant intends to build a four unit rowhouse, however an application for development permit is not submitted at this time.

An amendment to the South Calgary/Altadore Area Redevelopment Plan (ARP) is required to accommodate the proposed land use amendment. The amendment changes the land use policy for the site, from “Residential Conservation” to “Residential Low Density” as shown in Map 2 of the South Calgary/Altadore ARP (APPENDIX II).

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment.

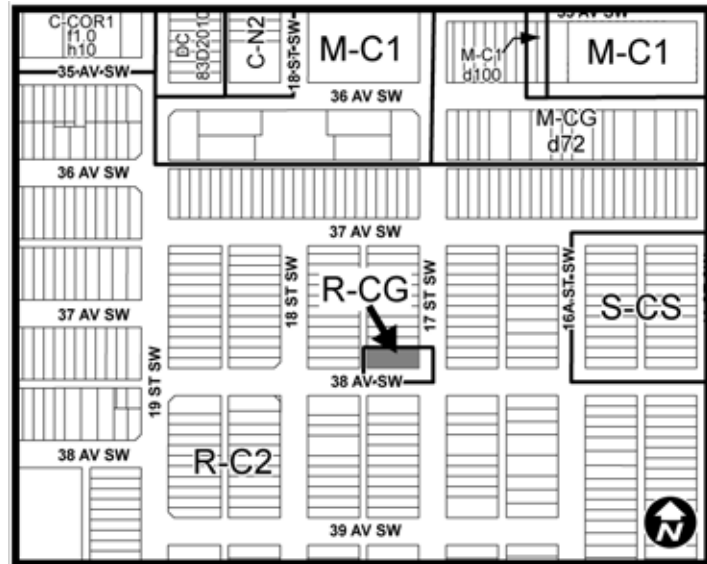
REASON(S) FOR RECOMMENDATION:

The proposed Land Use Amendment complies with the general goals of the Municipal Development Plan to accommodate a moderate increase in density within developed areas, and to maximize the use of existing infrastructure. Rowhouses are an appropriate built-form on this corner site, which helps maintain a residential streetscape, and would be complimentary to the proposal on LOC2017-0290. Furthermore, the proposed redesignation would accommodate multi-residential development that meets the Location Criteria for Multi-Residential Infill.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

LOCATION MAPS



LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendment to the South Calgary/Altadore Area Redevelopment Plan (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.143 acres \pm) located at 3823 - 17 Street SW (Plan 2901P, Block A, Lots 11 and 12) from Residential-Contextual Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Applicant:

Willix Developments

Landowner:

Willix Developments Ltd

PLANNING EVALUATION

SITE CONTEXT

The subject parcel is located in the community of Altadore in the southwest quadrant of the city. The site is currently developed with a single detached dwelling with a rear detached single garage that is accessed from 38 Avenue SW. Located at the south-west corner of 38 Avenue SW and 17 Street SW, the site is situated within a neighbourhood comprising primarily of single and semi-detached dwelling units.

| Altadore | |
|------------------------------------|---------|
| Peak Population Year | 2015 |
| Peak Population | 9 867 |
| 2017 Current Population | 6 795 |
| Difference in Population (Number) | -3 072* |
| Difference in Population (Percent) | -31%* |

* A portion of Altadore was subtracted in 2016 to create the community of Garrison Woods. As a result, the population of Altadore will be much higher in years prior to 2016. A more accurate indication of population growth or decline within the newly defined boundaries for Altadore would result from data collected for 2018.

LAND USE DISTRICTS

Current Land Use District

The Residential – Contextual Two Dwelling (R-C2) District is intended to accommodate existing residential development in the form of duplex, semi-detached and single detached dwellings in developed areas of the City.

Proposed Land Use District

The Residential – Grade-Oriented Infill (R-CG) District is intended to do the following:

- accommodate existing residential development;

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

- accommodate grade-oriented development in the form of rowhouse, duplex, semi-detached and cottage housing clusters;
- accommodates secondary and backyard suites within new and existing residential development;
- provide flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time; and
- accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

The Residential – Grade Oriented infill (R-CG) District would provide additional low density infill housing forms along with potential for secondary suites, at a maximum density of 75 units per hectare. A moderate increase in density would occur upon redesignation and redevelopment of the subject parcel in the form of a four unit rowhouse.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

The subject parcel is located within the Developed Residential Area - Inner City of the MDP, which supports moderate intensification that respects the community context. In general, these policies encourage redevelopment of the Inner City area in a way that is similar in scale and built form to existing development and contributes to a greater variety of housing types overall. The MDP also encourages higher residential densities in areas that are well serviced by existing infrastructure, public amenities, and transit.

The proposed Residential – Grade-Oriented infill (R-CG) District allows for development that is sensitive to the existing context and conforms to the relevant policies of the MDP. The subject site is located on a corner parcel, and will allow for a greater variety of housing forms in the area. For this reason, the proposed redesignation is found to comply with the general goals of the MDP while providing sound rationale to accommodate the minor amendment to the South Calgary/Altadore Area Redevelopment Plan.

South Calgary/Altadore Area Redevelopment Plan (ARP)

The subject parcel is located within the area designated “Residential Conservation”.

Section 2.2(a) provides “Conservation and Infill” policies for the subject parcel and surrounding areas. The intent of this policy is as follows:

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW**

MAP 5C

- improve existing neighbourhood quality and character;
- permitting low profile infill development compatible with surrounding dwellings;
- existing structures in good repair should be conserved;
- structures in poor repair should be rehabilitated or replaced; and
- narrow lot (7.5 metres or 25 foot) infill dwellings should be of a design that would encourage families with children to move into them.

The proposed redesignation of the subject parcel to Residential – Grade-Oriented Infill (R-CG) District would continue to comply with this policy. The land use amendment would provide for infill development with moderate increase in density, allowing for a variety of appropriate low profile housing forms, compatible with surrounding dwellings. Rowhouses are not available in the Residential – Contextual Two Dwelling District (R-C2).

Minor Amendment to South Calgary/Altadore ARP

The redesignation of the parcel to Residential – Grade-Oriented Infill (R-CG) District would result in a site specific minor amendment to Map 2 (Land Use Policy) of the South Calgary/Altadore ARP from “Residential Conservation” to “Residential Low Density” (APPENDIX II).

Location Criteria for Multi-residential Infill

In 2014, Council implemented ‘Location Criteria for Multi-Residential Infill’ to provide a more specific tool for the review of land use amendment applications in the Developed Areas and associated amendments to local area plans. The proposed land use generally aligns with several of the location criteria for multi-residential infill development in low density residential areas. The table in APPENDIX IV provides a summary of alignment with the criteria. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP, to assist in determining the appropriateness of an application in the local context.

TRANSPORTATION NETWORKS

The subject site is located approximately 260 metres away from the Southbound 13 bus stop with service to a bus hub at Mount Royal University. This bus route also links to the 72, 73, 181, 182, and 306 BRT bus lines. Bus route 306 is a Primary Transit route, and is approximately 2.75kilometres away from the site. The site is also approximately 280 metres away from an eastbound 13 bus stop with service to the downtown core, and LRT.

The subject site is a corner lot, with lane access, and no parking restrictions. There is an existing curb cut on 38 Avenue SW requires closure and rehabilitation at the time of redevelopment. Access will be required from the lane. Parking is to be provided at the time of Development Permit, and will need to meet all the required specifications and bylaw requirements for parking stall metrics.

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW**

MAP 5C

UTILITIES & SERVICING

The water main is available along 17 Street SW. The existing water system can support the proposed development without the need for an upgrade.

The sanitary sewer is available at 17 Street SW. If the density is over 55 person/ha and the proposed Peak Wet Weather Flow (PWWF) has exceeded 1L/s, provide a Sanitary servicing study at Development Permit stage.

The storm sewer is available at 38 Avenue SW. The storm sewer needs to extend to service the site at the expense of the developer. Other comments including unit release rate or the use of a drywell will be provided at development permit stage.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

ENVIRONMENTAL SUSTAINABILITY

Sustainability measures will be evaluated at the development permit stage.

GROWTH MANAGEMENT

The proposed amendments do not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

PUBLIC ENGAGEMENT

Community Association Comments

The Marda Loop Community Association provided objections by way of letters submitted for the proposal for M-CG on 2017 November 17 and subsequently in response to the proposal for R-CG on 2017 December 27 (APPENDIX III). Additionally, 93 letters in opposition to the original M-CG proposal were received, followed by 39 letters in opposition to the proposal for R-CG. One (1) letter of support was also received. The common concerns among the letters are as follows:

- Potential of the subject parcel to house four (4) units along with 4 secondary suites.
- Potential increase of demand for on-street parking.
- Rowhousing is seen as not compatible with adjacent developments.

**LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW**

MAP 5C

- Multi-residential location criteria are too broad and would allow multi-family development in significant portions of low density areas.

Administration reviewed the concerns raised by various residents and community stakeholders and believes the proposed redesignation continues to be appropriate. The only impact projected would be the loss of on-street parking stalls for immediate visitors. Community-wide parking issues are mitigated by the adoption of a Residential Parking permit zone through the Calgary Parking Authority. As stated previously in the report, the subject parcel is not located within a Residential Parking permit zone. Furthermore, the site is found to sufficiently satisfy the location criteria for multi-residential infill, which is typically used as criteria to assess appropriateness of multi-residential development in land use districts of greater density than this proposed redesignation.

Citizen Engagement

The Applicant sent information handouts to immediate neighbours within a two block radius of the proposed amendments, and have been regularly communicating with the Community Association to advise them about the application. The Applicant presented their amendments to the Community Association in December 2017. While a development permit package is not submitted at this time, the Applicant intends to share their design concept on 2017 January 29 through an open house.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

APPENDIX I

APPLICANT'S SUBMISSION

Applicant's Planning Analysis

Design Intent and Context

The subject parcels are located in the community of South Calgary / Altadore and consist of 0.058 ha each. We are proposing 2 four-unit rowhouse developments with front doors facing 38 AV SW, 18 ST SW and 17 ST SW, and with vehicular access from the adjacent lane. All parking would be provided on-site, at grade, in an enclosed garage.

The current land use designation in both parcels is (R-C2) Residential – Contextual One / Two Dwelling District which allows for duplex, single and semi-detached dwellings. To support the proposed development, we are applying to change the designation from R-C2 to R-CG Residential – Grade-Oriented Infill District.

Like R-C2, the R-CG District is a Low Density Residential District intended for grade-oriented development and does not support multi-residential uses. The Land Use Bylaw explains that the R-CG District:

accommodates grade-oriented development in the form of Rowhouse Buildings, Duplex Dwellings, Semi-detached Dwellings and Cottage Housing Clusters, accommodates Secondary Suites and Backyard Suites with new and existing residential development, provides flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time, and accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

In addition, the site has specific attributes which make it ideal for rowhouse development:

- It is less than 5 minutes' walk from the bus stops along 16 ST SW.
- The site is less than 5 minutes' walk from Kiwanis Park.
- The site is less than 5 minutes' walk from commercial establishments along 16 ST SW.
- The site is less than 10 minutes' walk from Dr. Oakley School.
- The site is less than 15 minutes' walk from River Park.
- The site is less than 5 minutes' drive from commercial establishments along 33 AV SW.
- The site is less than 5 minutes' drive from Glenmore Athletic Park.
- The site is less than 10 minutes' drive from Mt Royal University.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Policies (and Variations) Municipal Development Plan (MDP)

The proposed project fundamentally meets the goals of the Municipal Development Plan.

This City policy encourages more housing options in established communities, more efficient use of infrastructure, and more compact built forms in locations with direct easy access to transit, shopping, schools and other community services.

The MDP encourages modest redevelopment of the Established Area. (3.5.3 (a.)) The proposed development modestly intensifies the use of the land to a density more appropriate for a central Calgary community.

The MDP requires that “ground and lower levels of developments should demonstrate a strong relationship to the human scale and contribute positively to the public realm and street.” (2.4.2 (b.)). By having all vehicular access from the adjacent lane, and unit entries facing 38 AV SW, the pedestrian realm along 17 ST SW & 18 ST SW is protected.

South Calgary / Altadore Area Redevelopment Plan (ARP)

The ARP recommends that this site be of a Conservation/ Infill use. Though not located on a collector, our parcels on 38th AV are within one block of 19th ST, a designated collector. Further, those familiar with the area recognize that 38th AV functions as a collector.

The ARP states:

The intent of the conservation and infill policy is to improve existing neighbourhood quality and character while permitting low profile infill development that is compatible with surrounding dwellings. Existing structures in good repair should be conserved, while structures in poor repair should be rehabilitated or replaced. Narrow lot (7.5 metres/25 foot) infill dwellings should be of a design that would encourage families with children to move into them.

Our proposal will allow a four unit rowhouse on each parcel, each having direct access to grade. Each unit will have two upstairs bedrooms plus one basement bedroom, 3 1/2 baths, and a private at-grade amenity space, and would be perfect for families. The proposed building is of a scale compatible with the single detached and semi-detached housing in the area.

Bylaw (and Relaxations)

The proposed development is at a preliminary stage of design. At this point, relaxations are not anticipated. Should any be proposed in the future, these will be within the intent of City policy.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Engagement

We will be engaging the community and ward councillor thoroughly in the coming weeks to explain the change in course we're taking with these applications, and how R-CG will provide appropriate redevelopment. We will keep the Development Authority apprised of this engagement as it unfolds.

To conclude, the proposed land use amendment will allow development in the form of a Rowhouse Building which is consistent with City policies, and the best use of this parcel. For these reasons, we respectfully request the support of The Development Authority, Calgary Planning Commission and Council for our application.

If you have any questions regarding this application, please contact us at your convenience.
Best regards,

Willix Developments
P.O. Box 415 Stn M
Calgary AB
T2P-2J1, Canada

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

APPENDIX II

PROPOSED AMENDMENT TO THE SOUTH CALGARY/ALTADORE
AREA REDEVELOPMENT PLAN

- (a) Delete the existing Map 2 entitled "Land Use Policy" and replace with the revised Map 2 entitled "Land Use Policy", as follows:

Map 2
Land Use Policy

Legend

-  Study Area Boundary
-  King Edward School Site
-  Residential Conservation
-  Residential Low Density
-  Residential Medium Density
-  Community Mid-Rise
-  Local Commercial
-  General Commercial
-  Open Space



This map is conceptual only. No measurements of distances or areas should be taken from this map.



LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

APPENDIX III

COMMUNITY ASSOCIATION LETTER



3130 16 Street SW
Calgary, AB, T2T 4G7

December 27, 2017

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Circ@calgary.ca

Attn: Ryan Hall, ryan.hall@calgary.ca

SENT BY EMAIL

Dear Mr. Hall,

RE: Community Association Feedback for LOC2017-0290 & LOC2017-0291

Thank you for providing us the opportunity to offer feedback on these applications that reflect the vision of the Marda Loop Communities Association (MLCA). Further to our letter dated November 6, 2017, the following comments have been written with consideration towards what is best for our communities and our city.

These applications have been amended and now propose redesignations from Residential – Contextual One/Two Dwelling (R-C2) to Residential – Grade-Oriented Infill (R-CG). It is our understanding that development permit applications will be submitted in February 2018 and will propose 4-unit townhomes oriented along 38th Avenue SW on each parcel. The garages would be detached with access from the lane.

The MLCA Planning & Development Committee continues to encourage developers to engage with us and neighbours of proposed developments proactively, prior to application. Proactive communication allows time for facilitating constructive dialogue. The applicant attended the MLCA Planning & Development Committee meeting on December 18, 2017. The outcome of the meeting was a commitment by the applicant to host an open house in mid-January 2018 once they have made further progress on their proposed building designs. Additionally, they will meet with the MLCA Planning & Development Committee and the directly adjacent neighbours in the second half of January 2018 to address minimizing the impacts of the proposed developments (including but not limited to shadowing, privacy, and massing). Although future engagement is planned, it has been a challenge for residents to understand what is being proposed during the request for comment period for these applications.

Marda Loop Communities Association
10000 16 Street SW, Calgary, AB T2T 4G7
403-243-1111 | www.marda.ca

R. Hall

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 2 of 3

To date, the Committee has received written objections from approximately 36 community residents regarding the amended applications (R-CG). These objections are in addition to the 101 objections received for the initial applications (M-CG). We would note that the concerns and opposition expressed in response to the M-CG applications would, in our opinion, also apply to the R-CG applications.

Concerns expressed by neighbours include (but are not limited to):

- Geographic area features resulting in severe privacy and shadowing impact on adjacent neighboring properties.
- Significant negative pressure on parking and traffic.
- Inappropriate contextual fit for the community (this zoning does not belong at this location).
- Rezoning in this location does not align with the Calgary Municipal Development Plan (MDP).
- Rezoning does not align with the South Calgary/Altadore Area Redevelopment Plan (ARP).
- The property does not meet the location criteria for multi-residential infill either in physical proximity or purpose.
- Issues identified in the R-CG Monitoring Report M-2017-034.

The Committee has concerns that this type of development does not fit contextually within the surrounding "residential conservation" area of the community of Altadore as identified in the South Calgary/Altadore ARP. Additionally, while the applicant has committed to meeting the bylaw requirements of the R-CG district, due to the orientation of the buildings and the larger building envelope permitted in this district there will be significant impacts on the neighbours if these applications are approved.

Furthermore, these parcels do not meet The City's Location Criteria for Multi-Residential Infill. While these locations may be on corner parcels (Criteria 1), are within 400 metres of a transit stop (Criteria 2), and have direct lane access (Criteria 8), failure to achieve the purpose statements of any of the remaining five criteria suggests that these locations are not suitable for multi-residential infill.

- Criteria 3 (within 600 metres of an existing or planned primary transit stop): A SW BRT stop is not planned near these locations, nor do the #7, #107, or #13 transit routes operate at frequencies of 10 minutes or less for extended time periods.
- Criteria 4 (on a collector or higher standard roadway on at least one frontage): 17th Street SW, 18th Street SW, and 38th Avenue SW are local roads at these locations. As such, multi-residential development here would increase traffic on local residential streets.
- Criteria 5 (adjacent to existing or planned non-residential development or multi-unit development): These developments would not serve as a transition of height and massing between lower density uses and larger buildings as there are no larger buildings in the immediate vicinity.

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LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

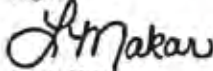
Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 3 of 3

- Criteria 6 (adjacent to or across from an existing or planned open space, park or community amenity): The nearest park is one block away, therefore, the visual impact of the increased height and massing of these developments will not be reduced by the open space, nor will these developments improve the safety of the park.
- Criteria 7 (along or in close proximity to an existing or planned corridor or activity centre): These developments are not in close enough proximity to the 33rd Avenue SW or 14th Street SW Main Streets to act as transitions and/or buffers between lower and higher density development.

While the Committee appreciates that the applicant has reduced the scale of these applications from M-CG to R-CG, it remains the position of the MLCA to strongly oppose these land use redesignation applications. As Altadore continues to re-develop at a fast pace, there are concerns around the lack of a comprehensive review of how and where increasing the density of the community is appropriate. If approved, this would set the precedent for any corner lot in Altadore to be redesignated to multi-residential irrespective of meeting the criteria identified by The City, which is not in the best interest of the community.

If you have any questions regarding these comments, please contact Lauren Makar at your convenience. Thank you in advance for considering these comments when assessing the merits of these applications.

Regards



Lauren Makar
Director - Planning & Development
Marda Loop Communities Association
development@mardaloop.com



Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

Enclosure: Letter from the MLCA – LOC2017-0290 & 0291 CA Feedback 06Nov2017

cc: Evan Woolley, Ward 8 Councillor, The City of Calgary evan.woolley@calgary.ca

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C



3130 16 Street SW
Calgary, AB, T2T 4G7

November 6, 2017

Circulation Control
Planning & Development #8201
PO Box 2100 Station M
Calgary, AB, T2P 2M5
Email: CPAG.Circ@calgary.ca

Attn: Ryan Hall, ryan.hall@calgary.ca

SENT BY EMAIL

Dear Mr. Hall:

RE: Community Association Feedback for LOC2017-0290 & LOC2017-0291

Thank you for providing us the opportunity to offer feedback on these applications that reflect the vision of the Marda Loop Communities Association (MLCA). The following comments have been written with consideration towards what is best for our communities and our city.

These applications propose redesignations from Residential – Contextual One/Two Dwelling (R-C2) to Multi-Residential – Contextual Grade-Oriented (M-CG). Based on a phone call with the applicant on October 24, 2017, it is our understanding that development permit applications will be submitted later this year and will propose 4-unit townhomes oriented along 38th Avenue SW on each parcel. The garages would be attached at the front with driveway access from 38th Avenue SW.

The MLCA Planning & Development Committee continues to encourage developers to engage with us and neighbours of proposed developments proactively, prior to application. Proactive communication allows time for facilitating constructive dialogue. The applicant first contacted the Committee on October 18, 2017, well into the brief three-week request for comment period. The Committee is aware of a letter dated October 16, 2017 that was circulated by Wilix to some neighbours. Considering the scope and context of these applications, the Committee feels the community engagement was inadequate.

To date, the Committee has received written objections from approximately 101 community residents. Concerns expressed by neighbours include (but are not limited to): the property does not meet the location criteria for multi-residential infill, rezoning does not align with the South Calgary/Altadore Area Redevelopment Plan, inappropriate contextual fit for the community, privacy and shadowing impact on adjacent neighbouring properties, and additional negative pressure on traffic, parking and utilities.

Marda Loop Communities Association
1111 16 Street SW, Calgary, AB T2T 4G7

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 2 of 3

The Committee has concerns that this type of development does not fit contextually within the surrounding "residential conservation" area of the community of Altadore as identified in the South Calgary/Altadore ARP.

Additionally, although the applicant proposes that these parcels meet The City's Location Criteria for Multi-Residential Infill, the Committee respectfully disagrees. While these locations may be on corner parcels (Criteria 1) and are within 400 metres of a transit stop (Criteria 2), failure to achieve the purpose statements of any of the remaining six criteria suggests that these locations are not suitable for multi-residential infill.

- Criteria 3 (within 600 metres of an existing or planned primary transit stop): A SW BRT stop is not planned near these locations, nor do the #7, #107, or #13 transit routes operate at frequencies of 10 minutes or less for extended time periods.
- Criteria 4 (on a collector or higher standard roadway on at least one frontage): 17th Street SW, 18th Street SW, and 38th Avenue SW are local roads at these locations. As such, multi-residential development here would increase traffic on local residential streets.
- Criteria 5 (adjacent to existing or planned non-residential development or multi-unit development): These developments would not serve as a transition of height and massing between lower density uses and larger buildings as there are no larger buildings in the immediate vicinity.
- Criteria 6 (adjacent to or across from an existing or planned open space, park or community amenity): The nearest park is one block away, therefore, the visual impact of the increased height and massing of these developments will not be reduced by the open space, nor will these developments improve the safety of the park.
- Criteria 7 (along or in close proximity to an existing or planned corridor or activity centre): These developments are not in close enough proximity to the 33rd Avenue SW or 14th Street SW Main Streets to act as transitions and/or buffers between lower and higher density development.
- Criteria 8 (direct lane access): While these sites do have direct lane access, the purpose of the criteria will not be met with the proposed design calling for garage access from 38th Avenue versus the lane.

LAND USE AMENDMENT
ALTADORE (WARD 8)
NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

Community Association Feedback for LOC2017-0290 & LOC2017-0291
Page 3 of 3

The applicant provided examples of other developments that do not meet criteria 4, 5, and/or 6. Upon review, however, we note the following which would indicate that these locations are more suitable for multi-residential development than the locations currently under consideration:

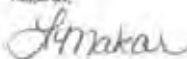
- 2937 & 2915 17th Street SW are within a large area of South Calgary zoned M-C1 and are identified as "residential medium density" in the ARP.
- 3208 20th Street SW zoned R-CG is on a collector road.
- 1631 38th Avenue SW, 3605 20th Street SW, and 3704 19th Street SW are in areas zoned M-C1, are identified as "residential medium density" in the ARP, and are on collector roads.
- 1605 3rd Avenue SW is zoned R-C2, as such we are not clear why this location was included in the application.

The Committee also notes the impact of these applications on the neighbours directly adjacent to the sites on 17th and 18th Streets SW. Due to the proposed orientation of the developments there are shadowing, privacy, and massing issues to consider.


Based on the above, the MICA Planning & Development Committee strongly opposes these land use redesignation applications. As Altadore continues to re-develop at a fast pace, there are concerns around the lack of a comprehensive review of how and where increasing the density of the community is appropriate. If approved, this would set the precedent for any corner lot in Altadore to be redesignated to multi-residential irrespective of meeting the criteria identified by The City, which is not in the best interest of the community.

If you have any questions regarding these comments, please contact Lauren Makar at your convenience. Thank you in advance for considering these comments when assessing the merits of these applications.

Regards,



Lauren Makar
Director - Planning & Development
Marda Loop Communities Association
development@mardaloop.com



Doug Fraser
President
Marda Loop Communities Association
president@mardaloop.com

cc: Ewan Woolley, Ward 8 Councillor, The City of Calgary ewan.woolley@calgary.ca

LAND USE AMENDMENT
 ALTADORE (WARD 8)
 NORTH OF 38 AVENUE SW AND WEST OF 17 STREET SW

MAP 5C

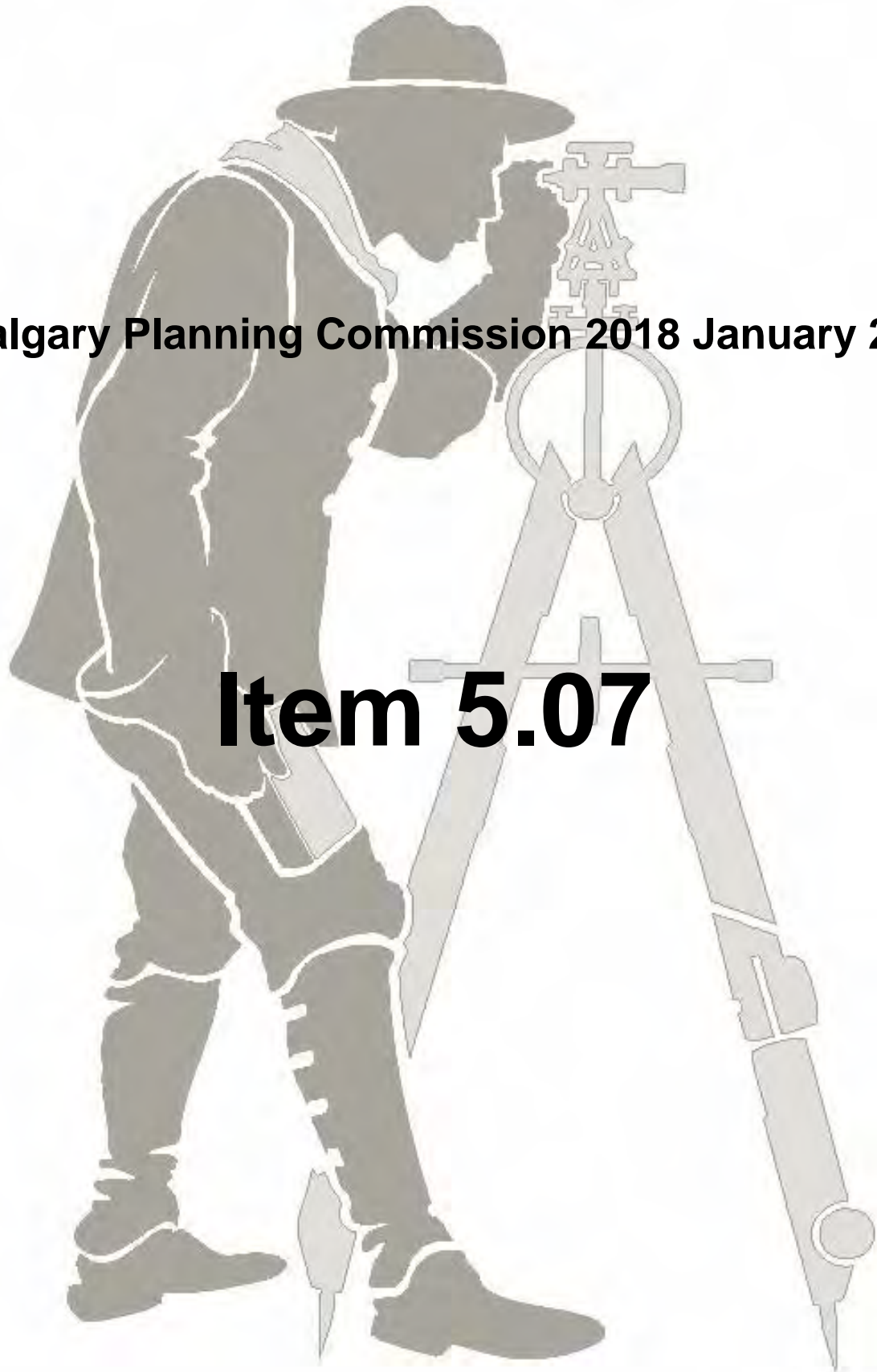
APPENDIX IV

LOCATION CRITERIA FOR MULTI-RESIDENTIAL INFILL

| Criteria | Assessment |
|--|---|
| Located on a Corner Parcel. | Yes Parcel is located on the south-east corner of 38 Avenue SW and 17 Street SW. |
| Within 400 metres of a transit stop. | Yes Parcel is 150 metres from westbound from Route 13 bus stop. |
| Within 600 metres of an existing or planned primary transit stop or station. | No Parcel is more than 600 metres away from nearest primary transit stop. |
| On a collector or higher standard roadway on at least one frontage. | No Nearest collector is 19 Street SW. |
| Adjacent to existing or planned non-residential development or multi-dwelling development. | No Parcel adjacent to single-detached housing to east. |
| Adjacent to or across from existing or planned open space or park or community amenity. | No Nearest park is 100 meters away. |
| Along or in close proximity to an existing or planned corridor or activity centre. | Yes Parcel is 600 meters from the 33 Avenue SW commercial street |
| Direct lane access. | Yes Parcel is currently serviced by direct lane access. |

Calgary Planning Commission 2018 January 25

Item 5.07



**POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE**

MAP 15C

EXECUTIVE SUMMARY

This Land Use Amendment application seeks to redesignate three existing commercial units in a single storey building from Centre City Multi-Residential High Rise District (CC-MH) to Centre City Multi-Residential High Rise Support Commercial District (CC-MHX) to allow for commercial uses. The units comprise an office, convenience store and a small retail store. The current commercial uses are legal non-conforming and would not be allowed should an existing tenant vacate a unit without immediate replacement with a new tenant. This application seeks to provide certainty of use for the commercial units in future.

An amendment to the Beltline Area Redevelopment Plan is required to accommodate this Land Use Amendment application.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment.

REASON(S) FOR RECOMMENDATION:

The current CC-MH District does not allow commercial uses and the existing units have been operating as legal non-conforming uses. The land use redesignation corrects an issue created by the adoption of the Land Use Bylaw 1P2007, which no longer allowed commercial units on the subject site. Redesignation of the three commercial units to CC-MHX would enable conformance to the Land Use Bylaw. This minor housekeeping amendment would allow for support commercial uses on the subject site. The proposal conforms to relevant policies of the Municipal Development Plan and Beltline Area Redevelopment Plan as amended.

POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE

MAP 15C

LOCATION MAPS



POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE

MAP 15C

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendment to the Beltline Area Redevelopment Plan (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm 0.14 acres \pm) located at 111 – 14 Avenue SE (Plan 9611585, Units 7, 8 and 114) from Centre City Multi-Residential High Rise District (CC-MH) **to** Centre City Multi-Residential High Rise Support Commercial District (CC-MHX).

POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE

MAP 15C

Applicant:

CityTrend

Landowner:

Spring Creek Development Corporation
1376743 Alberta Ltd (Jay C.P. Damen,
Jane M. Hoffman)

PLANNING EVALUATION

SITE CONTEXT

The subject site is located at the corner of 14 Avenue SE and Centre Street SE in the Beltline. The site is part of a parcel which is currently developed with a single storey commercial building and a seven-storey multi-residential building under the same condominium plan. There is no direct access between the commercial and residential units. The three units proposed to be redesignated are all located in the single storey commercial building. The three existing commercial units are small in scale and have existed for many years in the community.

LAND USE DISTRICTS

Centre City Multi-Residential High Rise Support Commercial Districts (CC-MHX) are primarily residential with a limited range of support commercial uses, restricted in size and location within the building. Administration finds the redesignation of the existing commercial units to CC-MHX appropriate as it will bring them into conformance with the Land Use Bylaw.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Beltline Area Redevelopment Plan (ARP)

The subject site is included in the Primarily Residential Area of the Beltline ARP. The proposed CC-MHX District fits well with the intent of the area which is to allow “primarily residential with a limited range of uses including support commercial uses, restricted in size and location within the building”. The subject site, however, is not situated along the street frontages identified for non-residential uses as shown on Map 3B of the Beltline ARP.

In order to bring the proposal into alignment with the Beltline ARP, it is necessary to make an amendment to Map 3B – Non-Residential Use Policy Area of the ARP to include the subject site in the Non-Residential Use Policy Area.

**POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE**

MAP 15C

TRANSPORTATION NETWORKS

There is an on street loading zone along 14 Ave SE and parking restriction (2 hour maximum) on 14 Avenue SE and Centre Street SE adjacent to the site. A Traffic Impact Assessment (TIA) or parking study was not requested for this application. However, upon future redevelopment of the parcel, the applicant will be required to submit a TIA in support of the development.

UTILITIES & SERVICING

Public water, sanitary and storm mains exist within the adjacent public rights-of-way. Services exist from public mains to the existing site and development. Should changes to servicing be required at the time of future development, services shall be to the satisfaction of Water Resources.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required, as there are no known environmentally related concerns associated with the lands and/or proposed scope. The lands and existing development are located within the Overland Flow Zone flood risk area as per current Council approved flood maps. At the time of future development, proposed development shall conform with current Land Use Bylaw flood regulations.

ENVIRONMENTAL SUSTAINABILITY

Sustainability measures will be evaluated at the development permit stage.

GROWTH MANAGEMENT

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

PUBLIC ENGAGEMENT

Community Association Comments

A letter of no objection was received from the Beltline Neighbourhoods Association, following discussions with the applicant.

**POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE**

MAP 15C

Citizen Comments

No comments received by CPC Report submission date.

Public Meetings

No public meetings were held.

POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE

MAP 15C

APPENDIX I

APPLICANT'S SUBMISSION

2.0 Planning Evaluation

This application proposes the redesignation of three commercial units in an existing building from CC-MH to CC-MHX. The current district does not allow for commercial uses. The units to be redesignated are occupied by Hoffman Law, Centre Point Grocery and Calgary Coin.

A multi-residential building and a single storey commercial building currently exist on the site. There is no direct access between the existing residential and commercial units. Redevelopment of the site is not proposed.

A pre-application meeting was held prior to the submission of this application – PE2017-00638.

Site Context

The site is located in the Beltline community of the Centre City. It is located at the corner of 14 Avenue SE and Centre Street.

Proposed Land Use District

We are seeking to redesignate the site to the Centre City Multi-Residential High Rise Commercial Support District (CC-MHX).

Commercial units have existed on this site for many years, providing services to local residents. Currently, an office, convenience store and small retail store are located on the subject property. Though commercial uses are existing, they are not allowed in the current CC-MH District. They have been operating as legal non-conforming uses. Should one of the current tenants move out and not be immediately replaced by a new tenant the commercial use would cease to be allowed.

This land use redesignation seeks to provide the owners of the commercial units certainty of use and allow them greater flexibility in the future. Additionally, the proposed CC-MHX District would provide local residents with small-scale commercial uses in an existing commercial building and would not create any negative impacts.

Site Characteristics

The subject site is located on a corner parcel. The surrounding properties have a mix of uses including CC-MH and Direct Control Districts based in R-5 and RM-7. There is an existing multi-residential building on site. However, there is no direct access between the commercial and residential units. The subject commercial units form part of the residential complex and are under the same condominium plan.

**POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE**

MAP 15C

Public Engagement

CITYTREND met with the Victoria Park Business Improvement Area and discussed the application with Councillor Woolley prior to our submission. The proposal has also been discussed with the condo board. We will continue to engage with the Councillor's office, Community Association, Victoria Park BIA and adjacent neighbours on the application as it progresses.

Conclusion

The proposed redesignation addresses a long-standing issue on the site following the adoption of 1P2007. The redesignation to CC-MHX is appropriate to the context where there is currently a mix of commercial and residential uses.

- The proposed District of CC-MHX provides supportive uses to nearby residents
- The uses included in the proposed District are limited in impact and size.
- The proposed redesignation falls in line with the policies included in the Municipal Development Plan
- The units to be redesignated include the corner unit, following the policies included in the Beltline ARP for the location of non-residential uses
- Commercial uses have existed on the site for many years without issue in the existing single storey commercial building
- The land use redesignation fixes an issue created by the adoption of 1P2007, which no longer allowed commercial units on the subject site
- The adoption of this land use redesignation will provide the owners of the commercial units certainty of use and greater flexibility for users into the future

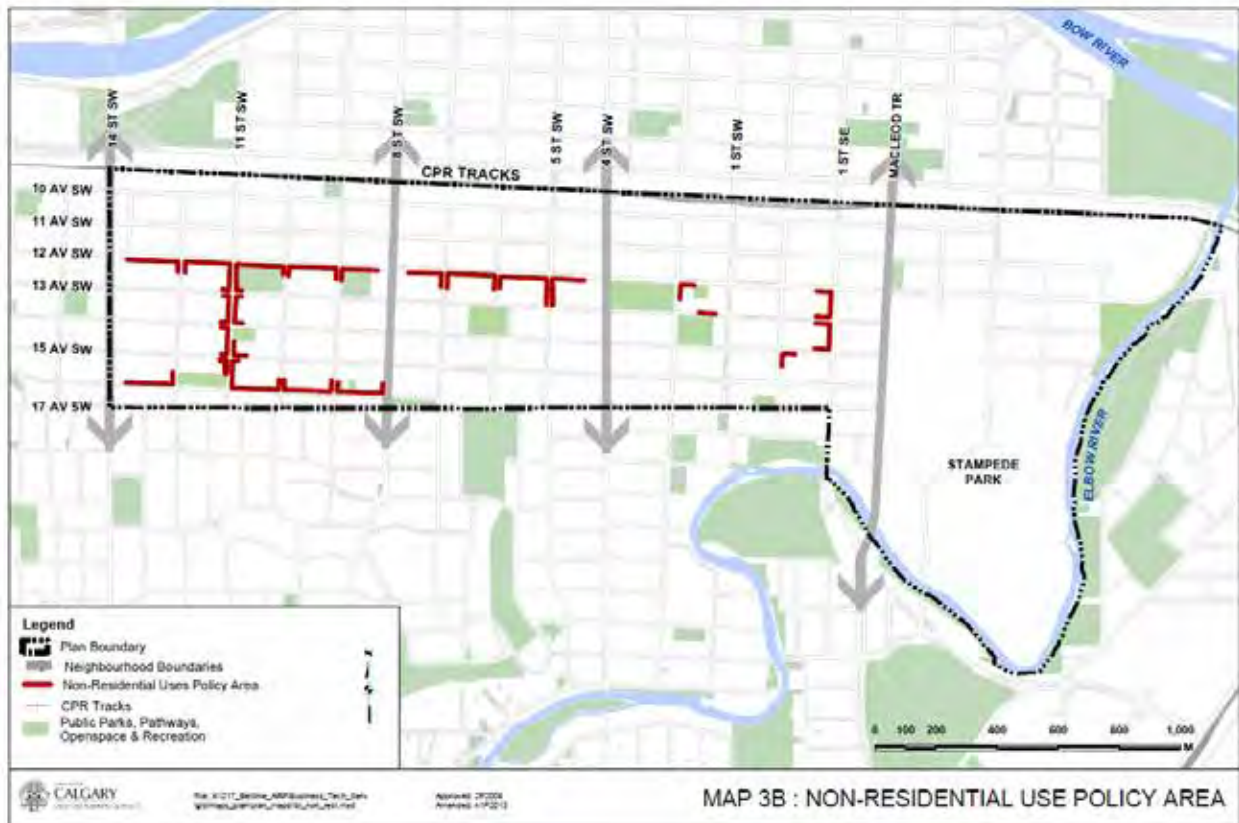
POLICY AMENDMENT AND LAND USE AMENDMENT
BELTLINE (WARD 11)
14 AVENUE SE AND CENTRE STREET SE

MAP 15C

APPENDIX II

PROPOSED AMENDMENT TO THE BELTLINE
AREA REDEVELOPMENT PLAN

- (a) Delete the existing Map 3B entitled “Non-Residential Use Policy Area” and replace with the revised Map 3B entitled “Non-Residential Use Policy Area”, as follows:



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is a large, complex piece of equipment with a circular level and a vertical axis. The entire scene is rendered in a dark grey silhouette against a light background.

Calgary Planning Commission 2018 January 25

Item 5.08

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

EXECUTIVE SUMMARY

This land use amendment application seeks to redesignate a residential parcel in the community of Glenbrook from the Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for Secondary Suites in the existing Semi-detached Dwelling. To Administration’s knowledge there are two existing suites located on the parcel and the application was submitted as a result of a complaint.

The existing R-C2 district does not allow Secondary Suites in a semi-detached dwelling. The definition of Secondary Suite in the Land Use Bylaw states that Secondary Suite must be located on the same parcel as a Contextual Single Detached Dwelling or a Single Detached Dwelling unless in the R-CG district. In the R-CG district, Secondary Suites must be located on the same parcel as a Dwelling Unit contained in a Contextual Semi-detached Dwelling, Contextual Single Detached Dwelling, Rowhouse Building, Semi-detached Dwelling, or a Single Detached Dwelling. Redesignation to R-CG will provide the additional development rights necessary to legalize the existing Secondary Suites. The process to legalize the two existing Secondary Suites is subject to this land use application and future development permit and building permits.

PREVIOUS COUNCIL DIRECTION

None

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

REASON(S) FOR RECOMMENDATION:

The proposed land use amendment complies with the general goals of the Municipal Development Plan to accommodate a moderate increase in density within developed areas, and to maximize the use of existing infrastructure. The proposed R-CG district provides additional development rights necessary to legalize the existing Secondary Suites in the Semi-detached Dwelling while ensuring future redevelopment on the parcel to be compatible with the character of the existing neighborhood. The proposed land use district was designed to be implemented in proximity to or directly adjacent to low-density residential development by offering flexibility with regards to building setbacks and building height that facilitate a diversity of grade-oriented housing types.

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

LOCATION MAPS



LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares \pm (0.14 acres \pm) located at 3340 - 41 Street SW (Plan 7884GK, Block 61, Lot 20) from Residential - Contextual Two Dwelling (R-C2) District **to** Residential - Grade-Oriented Infill (R-CG) District.

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

Applicant:

Maria Livaditis

Landowner:

Maria Livaditis
Billy Koutselas

PLANNING EVALUATION

SITE CONTEXT

The subject parcel is located in the community of Glenbrook, on the north east corner of 41 Street SW and 33 Avenue SW. The site is currently developed with a semi-detached dwelling with two-vehicle surface parking area that is accessed from the lane. The site is situated within a neighborhood comprised primarily of single detached and semi-detached dwellings.

According to data from the City of Calgary's 2017 Civic Census, the following table identifies Glenbrook's peak population and year, current population and the population amount and percentage difference between the peak and current populations.

| Glenbrook | |
|------------------------------------|--------|
| Peak Population Year | 1982 |
| Peak Population | 7,674 |
| 2017 Current Population | 7,209 |
| Difference in Population (Number) | - 465 |
| Difference in Population (Percent) | - 6.1% |

LAND USE DISTRICTS

Current Land Use District

The Residential – Contextual Two Dwelling (R-C2) District is intended to accommodate residential development in the form of duplex, semi-detached and single detached dwellings in developed areas of the City.

Proposed Land Use District

The Residential – Grade-Oriented Infill (R-CG) District is intended to accommodate grade-oriented development in the form of Rowhouse Buildings, duplex dwellings, semi-detached dwelling and Cottage Housing Clusters in close proximity or directly adjacent to low-density residential development. The district also accommodates Secondary Suites and Backyard Suites with new and existing residential development. The district offers flexibility with regards to building setbacks and building height that facilitate integration of a diversity of grade-oriented

**LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW**

MAP 12W

housing types over time. The redesignation of the subject parcel would result in a moderate increase in density. At the maximum permitted density of 75 units per hectare, this site could accommodate up to 4 units.

This land use redesignation was submitted as a result of a complaint for the two existing illegal Secondary Suites located within the semi-detached dwelling. The intent of the redesignation is to legalize the existing Secondary Suites. The definition of Secondary Suites in the Land Use Bylaw only permits Secondary Suites in a Single Detached or Contextual Single Detached Dwelling except in the R-CG district where it allows Secondary Suites to be on the same parcel as a Dwelling Unit contained in a semi-detached dwelling.

The process to legalize the two existing Secondary is subject to this land use application and future development permit and building permit. Approval of this land use application does not constitute approval of a specific development type, but rather it allows R-CG development to be considered via the development permit process.

There is no concurrent or pending development permit associated with this application.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

The subject parcel is located within the Residential Developed Established Area of the Municipal Development Plan, which supports moderate intensification that respects the community context. In general, these policies encourage redevelopment of the Established Area in a way that is similar in scale and built form to existing development and contributes to a greater variety of housing types overall. The MDP also encourages higher residential densities in areas that are well service by existing infrastructure, public amenities and transit.

The proposed Residential – Grade Oriented Infill (R-CG) District allows for development that is sensitive to the existing context and conforms to the relevant policies of the MDP. The subject site is adjacent to low-density residential development, is on a corner parcel, and will allow for greater variety of housing forms in the area.

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

Local Area Plan

The parcel is located within the Low-Density Residential area of the Glamorgan – Glenbrook Design Brief, which was adopted by Council in 1971. The design brief establishes a basic framework for where the different types of development take place in the area.

The proposed redesignation of the subject parcel to Residential – Grade Oriented Infill (R-CG) District is in alignment with the Low-Density Residential identified in the design brief. The proposed district allows for moderate density increase while maintaining the low-density residential intent of the neighbourhood.

Location Criteria for Multi-residential Infill

In 2014, Council implemented 'Location Criteria for Multi-Residential Infill' to provide a more specific tool for the review of land use amendment applications in the Developed Areas. The criteria are not meant to be applied in the absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP, to assist in determining the appropriateness of an application to the local context. The proposed land use generally aligns with several of the location criteria for multi-residential infill development in low-density residential areas. The list of criteria and assessment of the parcel is as follows:

| Location Criteria for Multi-Residential infill | Assessment |
|---|---|
| On a corner parcel | Yes: Parcel is located at the north-east corner of 41 Street SW and 33 Avenue SW |
| Within 400 metres of a transit stop | Yes: Parcel is within 400m of the westbound Route 13 bus stop. |
| Within 600 metres of a transit stop on the Primary Transit Network | Yes: Parcel is approximately 525m from both northbound and southbound of #306 BRT bus stops that provide services to Westbrook LRT station and Mount Royal University. |
| On a collector or higher standard roadway on at least one frontage | No: Parcel is approximately 350m from Richmond Road SW and 360m from 37 Street SW |
| Adjacent to existing or planned non-residential development or multi-unit development | No: Parcel is adjacent to low-density residential and is approximately 270m from a multi-residential development. |

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

| | |
|---|--|
| Adjacent to or across from an existing or planned open space, park or community amenity | No: Parcel is approximately 100m from an open space and 370m from the Glenbrook Community Association Rink |
| Along or in close proximity to an existing or planned corridor or activity centre | No: Parcel is approximately 400m from the Glamorgan Shopping Centre located at the southwest corner of Richmond Road SW and 37 Street SW. |
| Served by direct lane access | Yes: Parcel has direct lane access |

TRANSPORTATION NETWORKS

The subject parcel is a corner lot with access from the lane to two existing parking stalls. There is surface capacity on the east side of the parcel to accommodate additional parking spaces to support Secondary Suites or any form of R-CG development. The site is not within a Residential Parking zone and there are currently no on street parking restrictions.

The subject parcel is located approximately 525 metres from both a Northbound and Southbound route 306 bus stop (which is a primary Transit line). The Northbound route provides service to the Westbrook LRT station, and the Southbound route provides service to Mount Royal University, and a bus hub with access to bus routes 13, 72, 73, and 181.

UTILITIES & SERVICING

The site is within the Glendale pressure zone for water services. The existing system is able to support the proposed land use. No upgrade is required at this time.

In the event of an R-CG re-development, a storm extension or construction of drywell to contain storm runoff on site will be required.

Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.

ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

ENVIRONMENTAL SUSTAINABILITY

Building sustainability measures will be evaluated at the development permit stage.

GROWTH MANAGEMENT

The proposed amendments do not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

PUBLIC ENGAGEMENT

Community Association Comments

Administration received a letter of opposition from the Glenbrook Community Association (APPENDIX II).

Concerns stated are summarized as follows:

- Community Association sees no reason to re-zone the property from R-C2 to R-CG as the existing property fits under the R-C2 designation.
- The applicant asked for R-C2s in the applicant submission.

Administration reviewed the comments made by the Glenbrook Community Association and considers the proposed redesignation to be appropriate. The proposal conforms to relevant policies of the Municipal Development Plan. The proposed R-CG district provides the additional development rights necessary to legalize the existing Secondary Suites.

The existing R-C2 district does not provide the same development rights as R-CG district. Secondary Suites in an R-C2 district are not allowed in Semi-detached Dwelling. However, Secondary Suites can be located on the same parcel as a Dwelling Unit contained in a semi-detached dwelling in an R-CG district. In addition, the R-CG district allows grade-oriented development in the form of Rowhouse Buildings, duplex dwellings, semi-detached dwelling and Cottage Housing Clusters.

Citizen Comments

No comments were received by the CPC report submission date.

Public Meetings

No public meeting were held by the Applicant or Administration.

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

APPENDIX I

APPLICANT'S SUBMISSION

To whom it may concern

We, the owners of 3340/3342 – 41 ST SW Calgary AB, T3E 3L2, would like to apply for the legalization of the secondary suites in said property. For this reason we wish to have our property approved for re-zoning to an R-CG land use. We would like to comply with all and any necessary steps in this process, so that we can increase the value of our property as well as create a safe and legal environment for those living on the lower levels.

Our building is a 2 story with four, completely separate suites. Specifically and as per a recent City inspection, they have separate entrance, hot water tanks, furnaces, electric panels, thermostat controls, and interconnected smoke detectors and do not require any structural changes of any kind. The house is located on a large corner lot with ample green space. There is a double driveway at the east side of the building off the back alley, with additional street parking. The property is located within very short distances to major amenities such as shopping centers, community halls, grocery stores and parks. As well as being within a 10 minute drive to downtown, it is situated in very close proximity to transit which puts it within minutes to Mount Royal University.

Thank you for your consideration.

Sincerely,

Maria Livaditis

LAND USE AMENDMENT
GLENBROOK (WARD 6)
41 STREET SW AND 33 AVENUE SW

MAP 12W

APPENDIX II

COMMUNITY ASSOCIATION LETTER



**GLENBROOK COMMUNITY
ASSOCIATION**

3524 – 45 Street S.W.

Calgary, Alberta

T3E 3V2

Phone: (403) 249-6664

Fax: (403) 686-0884

Email: Glenbrook.community@shaw.ca

15 November 2017

LOC2017-0295

After review of this application The Glenbrook Community Association sees no reason to re-zone this property to R-CG from R-C2. As we read the application, this property will fit under the existing R-C2 designation, plus the applicant asks for a R-C2 designation in their application letter.

Yours truly

Murray Ost
President

A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the telescope of the instrument. The instrument is a large, complex piece of machinery with a circular base and a vertical column. The text "Calgary Planning Commission 2018 January 25" is overlaid on the image.

Calgary Planning Commission 2018 January 25

Item 5.09

**POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW**

MAP 22W

EXECUTIVE SUMMARY

The proposal is for a land use amendment to develop an acreage property in the community of West Springs. The application contemplates a future 30 lot subdivision of single detached dwellings and a Municipal Reserve dedication, accommodated through the R-1s and S-SPR land use districts. The proposed increase in density requires an amendment to the West Springs Area Structure Plan.

PREVIOUS COUNCIL DIRECTION

None

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment.

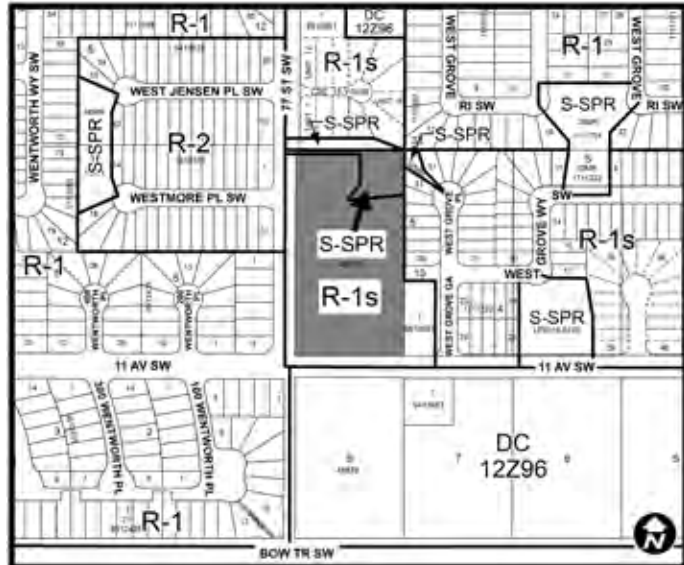
REASON(S) FOR RECOMMENDATION:

The proposed land use and policy amendments allow for a development that through its built form and density, complements the existing context of the area.

POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

LOCATION MAPS



POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendments to the West Springs Area Structure Plan (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 1.93 hectares \pm (4.77 acres \pm) located at 7750 - 11 Avenue SW (Plan 4587S, Block 9) from DC Direct Control District to Residential – One Dwelling (R-1s) District and Special Purpose – School, Park and Community Reserve (S-SPR) District.

POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

Applicant:

WSP Canada

Landowner:

Richard Press
Jennifer Press

PLANNING EVALUATION

SITE CONTEXT

The site is in the south-east corner of West Springs and is approximately 2 hectares in area. It currently contains a single residence. While this site has remained as a residential acreage, the surrounding sites to the east and west have been developed in alignment with the densities of the existing Area Structure Plan. The current land use designation and West Springs Area Structure Plan (ASP) policy do not allow for development beyond 7.4 to 12.4 units per hectare (3-5 units per acre).

The application seeks approval to develop the site at a density of 15.5 units per hectare (6.3 units per acre). The applicant is proposing approximately 30 single detached residential units for the 2 hectare parcel, therefore requiring both a Land Use Amendment and an amendment to the West Springs Area Structure Plan to enable this slight density increase.

| West Springs | |
|------------------------------------|--------|
| Peak Population Year | 2017 |
| Peak Population | 10,031 |
| 2017 Current Population | 10,031 |
| Difference in Population (Number) | 0 |
| Difference in Population (Percent) | 0% |

LAND USE DISTRICTS

The existing Direct Control land use district is essentially a holding district that only allows for limited development. To accommodate the proposed density, a Land Use Amendment to the Residential – One Dwelling (R-1s) District is required. To allow for the dedication of municipal reserve, a portion of the site will also be redesignated to Special Purpose – School, Park and Community Reserve (S-SPR) District. Together the two new land use designations will provide for a planned subdivision that aligns with the adjacent development.

**POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW**

MAP 22W

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

West Springs Area Structure Plan (ASP)

An amendment to the West Springs Area Structure Plan (ASP) is required to accommodate the proposed development. Currently the ASP assigns a land use category of "Standard Density Infill" to this parcel. This allows a maximum density of 7.4 to 12.4 units per hectare (3 to 5 units per acre). With the proposed 30 units, the gross developable density of the application is 15.5 units per hectare (6.29 units per acre). A change to the land use map in the ASP to the "Urban Development" category (5-7 units per acre/12.4-17.3 units per hectare) is therefore necessary. No other ASP policies require amendment.

Please note that there are two maps attached for the proposed ASP amendment (APPENDICES II and III). An unrelated major amendment to the West Springs Area Structure Plan (M-2017-023) is scheduled to proceed to Council Public Hearing on 2018 February 20, in advance of this item's appearance at Council. Should Council approve the major amendment, the ASP map in APPENDIX III is applicable. Should Council not approve the major amendment, or in any situation where the subject application (LOC2017-0213) precedes the major amendment to Council, then the ASP map in APPENDIX II is applicable.

TRANSPORTATION NETWORKS

The development will be served by a single through road that connects 77 Street SW with 11 Avenue SW. No Transportation Impact Assessment was required for the proposed level of density.

UTILITIES & SERVICING

The developer will be responsible for providing all deep services through the tentative plan. These services are available from 77 Street SW or 11 Avenue SW.

ENVIRONMENTAL ISSUES

A Phase I Environmental Site Assessment was submitted with the application. No significant issues were identified.

POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

GROWTH MANAGEMENT

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

PUBLIC ENGAGEMENT

Community Association Comments

At the time of CPC report submission, no comments from the West Springs/Cougar Ridge Community Association had been received.

Citizen Comments

Five letters were received from adjacent residents. Concerns centred on the impacts of increased traffic, potential for speeding on new streets, and the proposed road configuration of the new development impacting adjacent sites with street lights. As the expected increase in traffic for this development is small, Administration did not believe that any changes to the application were warranted.

Public Meetings

In October 2017, information was provided at engagement sessions that presented multiple applications ongoing in the community. Comments specific to this application expressed similar concerns as those from individual resident letters and were reviewed and assessed in the same manner.

POLICY AMENDMENT AND LAND USE AMENDMENT
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX I

APPLICANT'S SUBMISSION

This application relates to a 1.93 ha (4.77 acre) parcel of land in the community of West Springs (the "Site"). The development proposal is to enable a 30-lot detached residential development, including road widening and a full dedication of Municipal Reserve land. Currently designated Direct Control Bylaw 12Z96 for rural residential uses, a land use amendment to the Residential - One Dwelling District (R-1s) is sought for the Site.

Community character is mostly established, and the proposed development will be in keeping. The Site is the final piece to be developed in a row of similarly sized properties situated north of 11 Avenue SW. Except for land south of 11 Avenue SW, planning approvals and/or development of adjacent properties has been completed. In accordance with the West Springs ASP, development in the vicinity of the Site is predominantly detached residential development pursuant to the R-1 or R-2 Districts and similar in character to the proposed development.

The Site is identified as "Standard Density Infill Land Use Policy Area" in the West Springs ASP. This area allows for traditional suburban residential densities, with a density range of 7.4 - 12.4 units per gross developable acre ("GDA"). The development proposal is for a higher density of 15.5 units per GDA. This density provides a balanced approach between maintaining the community's establishing character with a similar housing typology, and those policies of the MDP seeking higher densities. An amendment to the West Springs ASP is required to accommodate the additional density.

A dedication of Municipal Reserve to 10% of the developable land area is proposed. The park will provide a local amenity space, as well as create a needed multi-use trail connection between two Municipal Reserve parcels on adjacent properties. There are no wetlands on the property, or environmentally significant areas.

The developer has met or been in contact with the owners of contiguous properties to the north and east of the Site, as well as the West Springs Cougar Ridge Community Association and Coach Ridge Community Association. Through these contacts no objections to the development proposal have been identified at the time of this report.

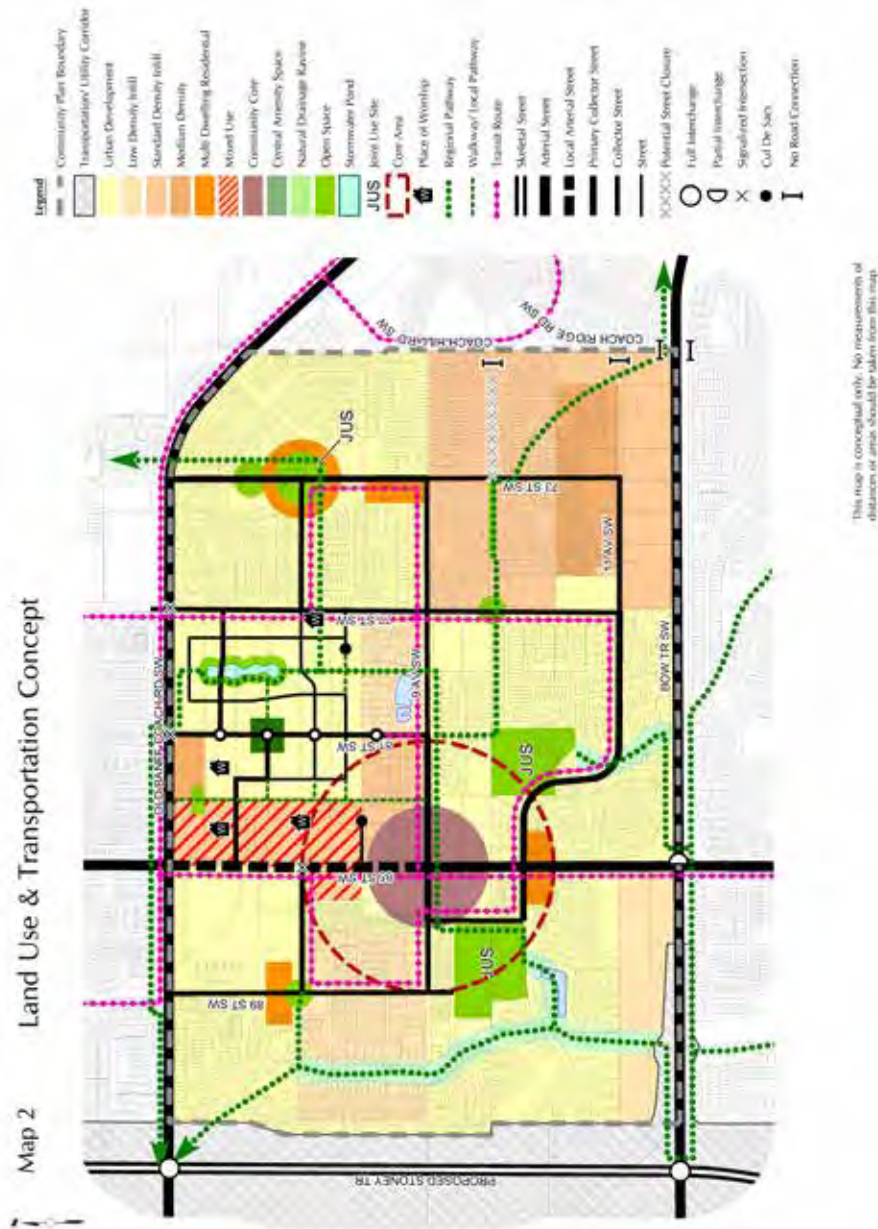
POLICY AMENDMENT AND LAND USE AMENDMENT
 WEST SPRINGS (WARD 6)
 11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX II

PROPOSED POLICY AMENDMENT TO THE WEST SPRINGS
 AREA STRUCTURE PLAN

- (a) Delete the existing Map 2 entitled “Land Use & Transportation Concept” and replace with the revised Map 2 entitled “Land Use & Transportation Concept as follows:

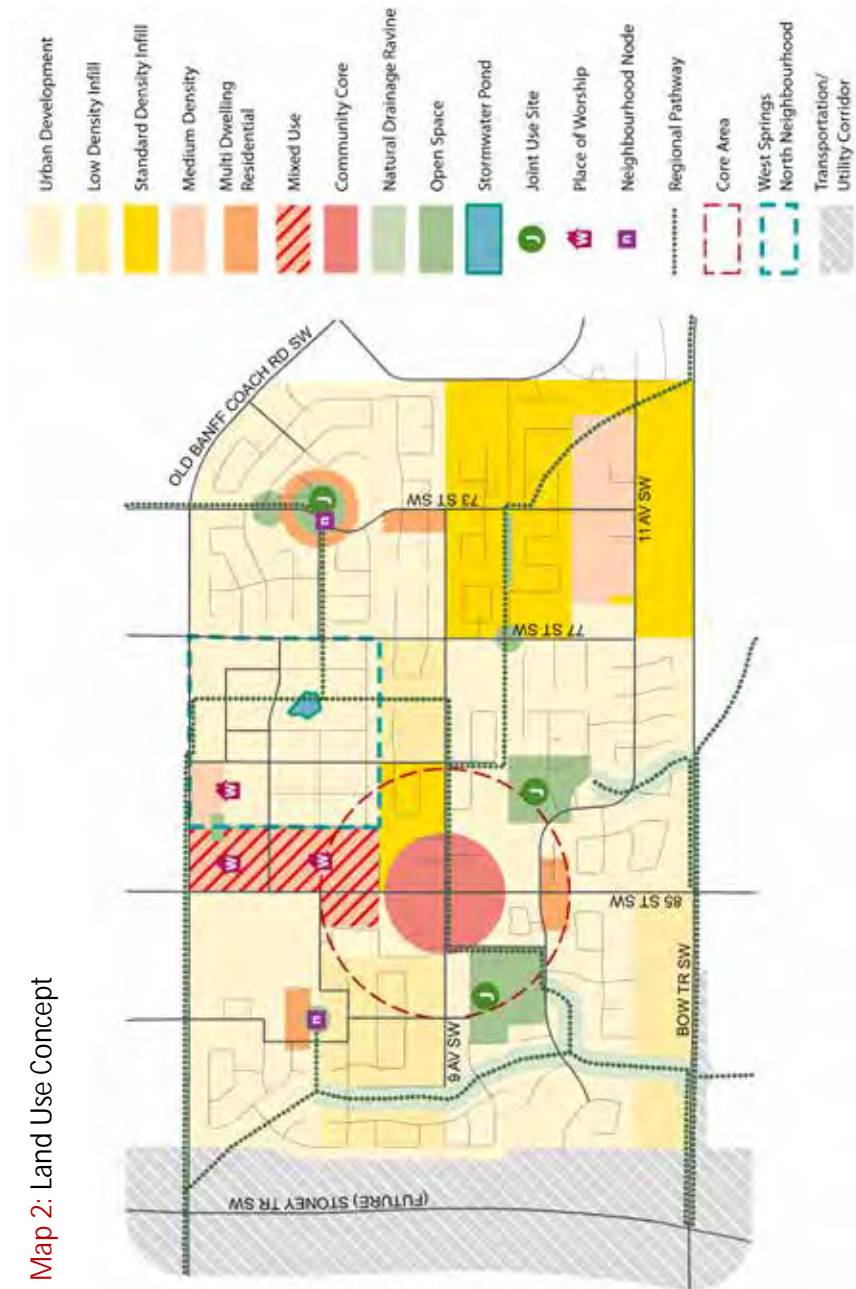


POLICY AMENDMENT AND LAND USE AMENDMENT
 WEST SPRINGS (WARD 6)
 11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX III

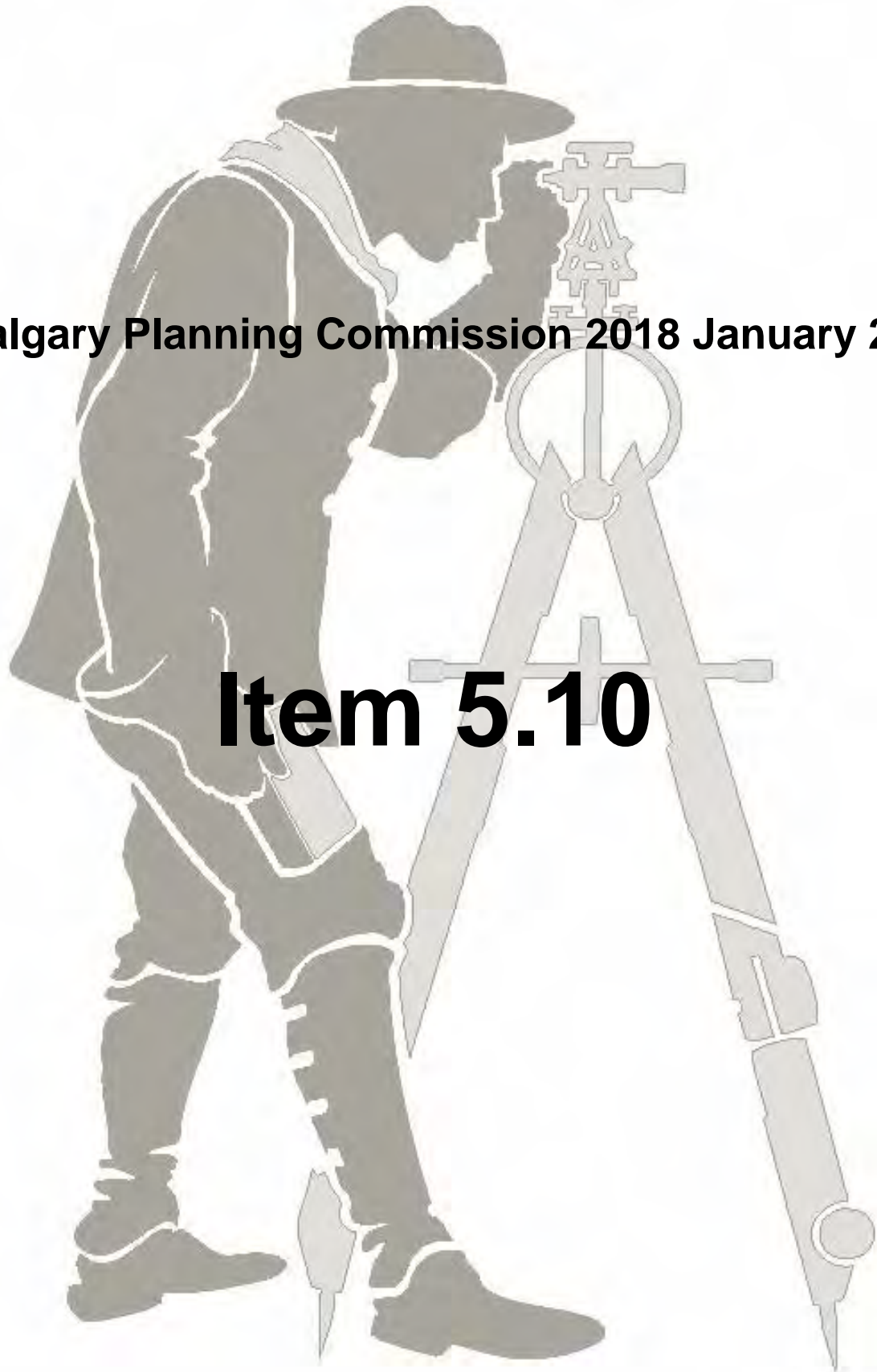
If Bylaw 15P2018 is passed at Council on 2018 February 20 the map this map will become the new West Springs Area Structure Plan Land Use Map, replacing the one in APPENDIX II.



Map 2: Land Use Concept

Calgary Planning Commission 2018 January 25

Item 5.10



**OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW**

MAP 22W

EXECUTIVE SUMMARY

The proposal is for an outline plan to develop an acreage property in the community of West Springs to a higher density. The application contemplates a future 30 lot subdivision of single detached dwellings and a Municipal Reserve dedication, accommodated through the R-1s and S-SPR land use districts. The increase in density requires an amendment to the West Springs Area Structure Plan.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission **APPROVE** the proposed Outline Plan.

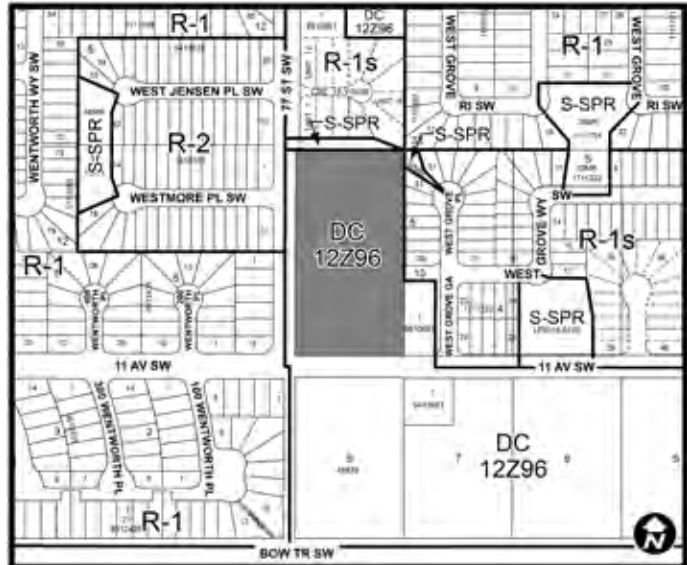
REASON(S) FOR RECOMMENDATION:

The proposed outline plan allows for a development that through its built form and density, complements the existing context of the area.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

LOCATION MAPS



OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Calgary Planning Commission **APPROVE** the proposed Outline Plan to subdivide 1.93 hectares \pm (4.769 acres \pm), with conditions (APPENDIX I).

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

Applicant:

WSP Canada

Landowner:

Richard Press
Jennifer Press

Address

7750 - 11 Avenue SW

Legal

Plan 4587S, Block 9

PLANNING EVALUATION

SITE CONTEXT

The site is in the south-east corner of West Springs and is approximately 2 hectares in area. It currently contains a single residence. While this site has remained as a residential acreage, the surrounding sites to the east and west have been developed in alignment with the densities of the existing Area Structure Plan.

| West Springs | |
|------------------------------------|--------|
| Peak Population Year | 2017 |
| Peak Population | 10,031 |
| 2017 Current Population | 10,031 |
| Difference in Population (Number) | 0 |
| Difference in Population (Percent) | 0% |

LAND USE DISTRICTS

The existing Direct Control land use district is essentially a holding district that only allows for limited development. To accommodate the proposed density, a land use amendment to Residential – One Dwelling (R-1s) District is required. To allow for the dedication of municipal reserve, a portion of the site will also be redesignated to Special Purpose – School, Park and Community Reserve (S-SPR) District. Together the two new land use designations will provide for a planned subdivision that correlates well with the adjacent development.

**OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW**

MAP 22W

DENSITY

The application proposes a density of 15.5 units per gross developable hectare, all in the form of single detached dwellings. The lands to the east, assigned with a Medium Density land use category in the ASP, have been developed recently to a density of approximately 18 units per hectare. The adjacent lands to the west were largely developed about 15 years ago and were developed in line with the Urban Density category of the ASP (12.4 to 17.3 units per hectare). The proposed density for this development is therefore very much in line with the surrounding developments.

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

West Springs Area Structure Plan (ASP)

An amendment to the West Springs Area Structure Plan (ASP) is required to accommodate the proposed development. Currently the ASP assigns a land use category of "Standard Density Infill" to this parcel. This allows a maximum density of 7.4 to 12.4 units per hectare (3 to 5 units per acre). With the proposed 30 units, the gross developable density of the application is 15.5 units per hectare (6.29 units per acre). A change to the land use map in the ASP to the "Urban Development" category (5-7 units per acre/12.4-17.3 units per hectare) is therefore necessary. No other ASP policies require amendment.

SUBDIVISION DESIGN

The proposed single road that accesses the site will present front-drive garages on both sides of the street. Average lot size is anticipated to be approximately 440 square metres.

A municipal reserve dedication is located at the north end of the plan area and upon development of the relevant parcels, will be consolidated into a larger park space.

TRANSPORTATION NETWORKS

The development will be served by a single through road that connects 77 Street SW with 11 Avenue SW, ensuring the ability to connect through the site for both pedestrians and vehicles. No Transportation Impact Assessment was required for the proposed level of density.

**OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW**

MAP 22W

UTILITIES & SERVICING

The developer will be responsible for providing all deep services through the tentative plan. These services are available from 77 Street SW or 11 Avenue SW.

ENVIRONMENTAL ISSUES

A Phase I Environmental Site Assessment was submitted with the application. No significant issues were identified.

PUBLIC ENGAGEMENT

Community Association Comments

At the time of CPC report submission, no comments from the West Springs/Cougar Ridge Community Association had been received.

Citizen Comments

Five letters were received from adjacent residents. Concerns centred on the impacts of increased traffic, potential for speeding on new streets, and the proposed road configuration of the new development impacting adjacent sites with street lights. As the expected increase in traffic for this development is small, Administration did not believe that any changes to the application were warranted.

Public Meetings

In October 2017, information was provided at engagement sessions that presented multiple applications ongoing in the community. Comments specific to this application expressed similar concerns as those from individual resident letters and were reviewed and assessed in the same manner.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX I

CONDITIONS OF APPROVAL

1. A uniform screening fence (with gates where appropriate) of high quality material requiring minimum maintenance, be provided at the developer's expense where required along the east edge of 77 Street SW from 11 Avenue SW to Westmore Mr SW; the design of such fence shall be to the satisfaction of the Approving Authority.
2. Existing buildings that are to be removed are to be done so prior to endorsement of the instrument.
3. With each Tentative Plan, the developer shall submit a density phasing plan indicating the intended phasing of Subdivision within the Outline Plan area and the projected number of dwelling units within each phase and demonstrating compliance with minimum required densities in accordance with the Area Structure Plan.
4. Prior to affected Tentative Plan approval (and submission of construction drawings), the proposed community and street names be submitted to the satisfaction of the Subdivision Authority.
5. All existing access to the affected properties in the area shall be maintained or alternative access be constructed at the developer's expense.

Development Engineering:

6. The parcels shall be developed in accordance with the development restriction recommendations outlined in the following report(s):
 - Geotechnical Report "West Springs Development – 11 Avenue & 77 Street SW", prepared by McIntosh Lalani Engineering Ltd. (File No ML 8239), dated July 10, 2017.
7. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
8. Prior to endorsement of any Tentative Plan/prior to release of a Development Permit, execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

-
9. Off-site levies, charges and fees are applicable. Contact the Subdivision Development Coordinator, Calgary Approval Coordination for further information at 403-268-6739 or email urban@calgary.ca.
 10. Make satisfactory cost sharing arrangements with Bri-Mor for its proportionate share of the Oil/Grit separator installed/constructed in downstream that was paid for and/or constructed by Bri-Mor under West Springs Phase 4 as you will be benefiting from the OGS installed by that developer.
 11. **Prior to endorsement of any Tentative Pan/prior to release of a Development Permit**, make satisfactory cost sharing arrangements with Bri-Mor Property Management Ltd. for part cost of the existing underground utilities and surface improvements installed in 11 Ave SW, that were constructed by Bri-Mor Property Management Ltd. Under West Springs, Phase 4 (2015-064).
 12. **Prior to endorsement of any Tentative Pan/prior to release of a Development Permit**, make satisfactory cost sharing arrangements with Dundee Developments Corporation for part cost of the existing underground utilities installed in 77 Street SW, that were constructed by Dundee Development Corporation under West Springs, Phase 1 (1999-0003).
 13. Make repayment arrangements with The City of Calgary for part cost of the existing paving for the west 2 lanes of 77 Street SW (5.75 metres), that were constructed by Dundee Development Corporation under West Springs, Phase 1 (1999-0003).

Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
 14. Make repayment arrangements with The City of Calgary for part cost of the existing sidewalk, curbs and gutters, including catch basins and leads on the west side of 77 Street SW, that were constructed by Dundee Development Corporation under West Springs, Phase 1 (1999-0003).

Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
 15. Make repayment arrangements with The City of Calgary for part cost of the existing underground utilities, paving (5.75 metres), sidewalk, curbs and gutters and street lighting installed in 77 Street SW that were constructed by Dundee Development Corporation under West Springs, Phase 4 (2013-0006) subdivision (and financed through Roads Program 204 (432715)).

Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

16. The Developer shall make payment to the City for their share of the East Springbank Servicing Study.

Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.

17. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
- a) Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b) Construct the underground utilities and surface improvements along and within the plan area.
 - c) Construct the onsite and offsite storm water management facilities (wet pond, wetlands, etc) to service the plan area according to the most current City of Calgary Standard Specifications Sewer Construction, Stormwater Management and Design Manual and Design Guidelines for Subdivision Servicing.
 - d) Construct the MSR/MR within the plan area.

Transportation:

18. No direct vehicular access shall be permitted to or from 77 St SW or 11 Av SW and a restrictive covenant shall be registered on all affected lots **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.
19. All roads and intersections shall be designed to the appropriate City standard and to the satisfaction of the Director of Transportation Planning.
20. The developer shall be responsible for all offsite improvement costs to satisfy pedestrian to transit connectivity, including but not limited to a standard bus apron at the bus zone located on 77 ST.
21. In conjunction with the construction of the road network for this development, transit stops shall be provided to the satisfaction of the Director, Transit.
22. **Prior to Release of any permits or Permissions to Construct**, the Developer shall enter into a Construction Access Roads Agreement with Roads Maintenance.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

-
23. **Prior to approval of the affected Tentative Plan**, the developer shall submit scale (1:500) drawings showing the geometry of and vehicle templating of all proposed bus route corners.

Parks:

24. Construct all pathway routes within and along the boundaries of the plan area according to Parks' *Development Guidelines and Standard Specifications – Landscape Construction* (current version), including setback requirements, to the satisfaction of the Director, Parks.
25. Rehabilitate all portions of the Municipal Reserve lands along the boundaries of the plan area that are damaged as a result of this development, all to the satisfaction of the Director, Parks.
26. **Prior to approval of the first tentative plan or stripping and grading permit** (whichever comes first), it shall be confirmed that grading of the development site will match the existing grades of adjacent parks and open space (MR and/or ER), with all grading confined to the private property, unless otherwise approved by Parks.
27. **Prior to endorsement of the subject tentative plan**, Landscape Construction Drawings for the proposed Municipal Reserve lands are to be submitted to Parks for review and approval.

OUTLINE PLAN
 WEST SPRINGS (WARD 6)
 11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX II

SUBDIVISION DATA SHEET

| | HECTARES | ACRES |
|-----------------------------|----------|-------|
| GROSS AREA OF PLAN | 1.93 | 4.77 |
| LESS: ENVIRONMENTAL RESERVE | 0 | 0 |
| LESS: LAND PURCHASE AREA | 0 | 0 |
| NET DEVELOPABLE AREA | 1.93 | 4.77 |

| LAND USE (Residential) | HECTARES | ACRES | # OF LOTS | # OF UNITS (FOR MULTI RESIDENTIAL) |
|---------------------------|----------|-------|-----------|--|
| R-1s | 1.33 | 3.29 | 30 | |
| Total Residential | 1.33 | 3.29 | 30 | |
| (Non-Residential) | | | | |
| S-SPR | 0.19 | 0.48 | 1 | |

| | HECTARES | ACRES | % OF NET AREA |
|--------------------|----------|-------|---------------|
| ROADS (CREDIT) | 0.40 | 0.99 | 20.7 |
| PUBLIC UTILITY LOT | 0 | 0 | 0 |

| RESERVES | HECTARES | ACRES | % OF NET AREA |
|-----------------|----------|-------|---------------|
| MR | 0.19 | 0.48 | 10 |
| MSR | | | |
| MR (NON-CREDIT) | | | |

APPENDIX III

APPLICANT'S SUBMISSION

6 REASONS FOR APPROVAL

This application relates to a 1.93 ha (4.77 acre) parcel of land in the community of West Springs (the "Site"). The development proposal is to enable a 30-lot detached residential development, including road widening and a full dedication of Municipal Reserve land. Currently designated Direct Control Bylaw 12Z96 for rural residential uses, a land use amendment to the Residential - One Dwelling District (R-1s) is sought for the Site.

Community character is mostly established, and the proposed development will be in keeping. The Site is the final piece to be developed in a row of similarly sized properties situated north of 11 Avenue SW. Except for land south of 11 Avenue SW, planning approvals and/or development of adjacent properties has been completed. In accordance with the West Springs ASP, development in the vicinity of the Site is predominantly detached residential development pursuant to the R-1 or R-2 Districts and similar in character to the proposed development.

The Site is identified as "Standard Density Infill Land Use Policy Area" in the West Springs ASP. This area allows for traditional suburban residential densities, with a density range of 7.4 - 12.4 units per gross developable acre ("GDA"). The development proposal is for a higher density of 15.5 units per GDA. This density provides a balanced approach between maintaining the community's establishing character with a similar housing typology, and those policies of the MDP seeking higher densities. An amendment to the West Springs ASP is required to accommodate the additional density.

A dedication of Municipal Reserve to 10% of the developable land area is proposed. The park will provide a local amenity space, as well as create a needed multi-use trail connection between two Municipal Reserve parcels on adjacent properties. There are no wetlands on the property, or environmentally significant areas.

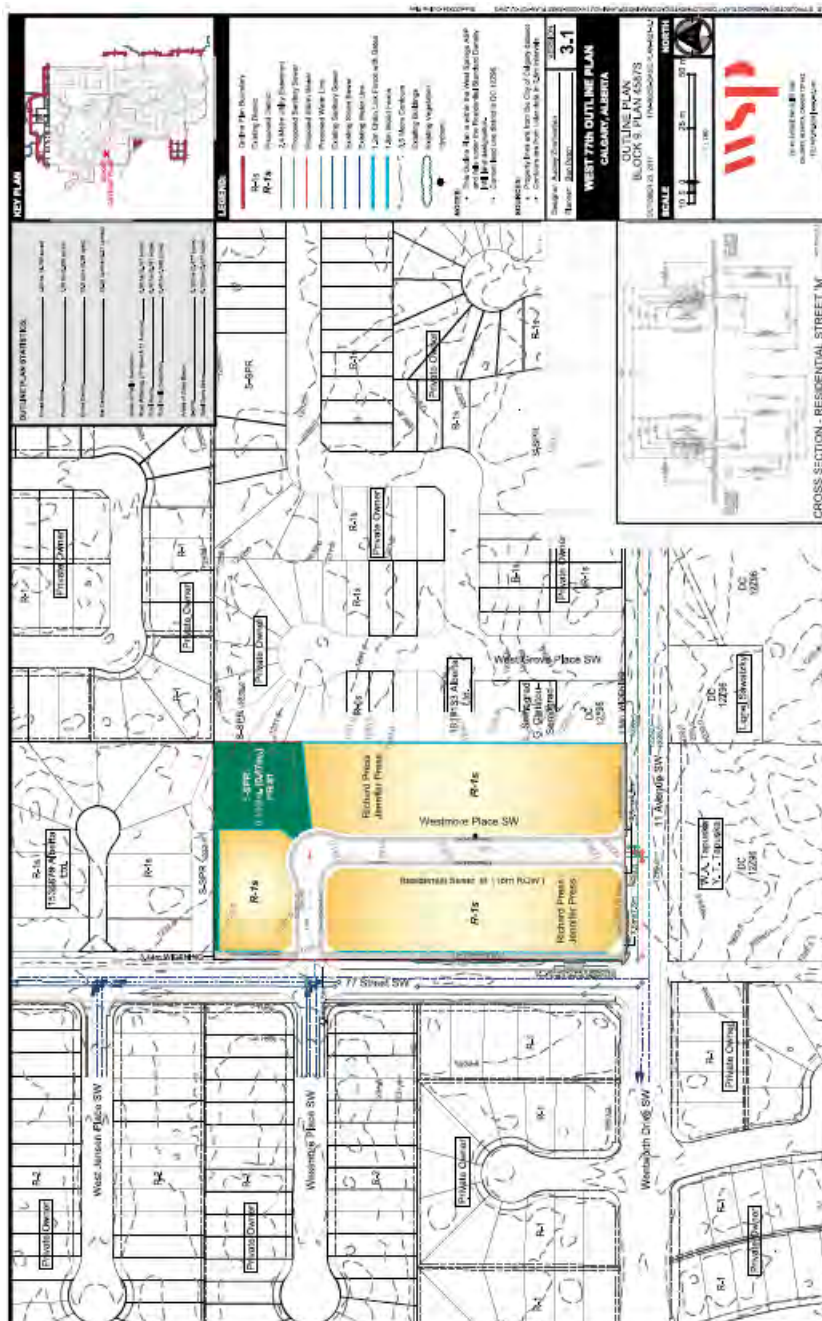
The developer has met or been in contact with the owners of contiguous properties to the north and east of the Site, as well as the West Springs Cougar Ridge Community Association and Coach Ridge Community Association. Through these contacts no objections to the development proposal have been identified at the time of this report.

OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX IV

OUTLINE PLAN

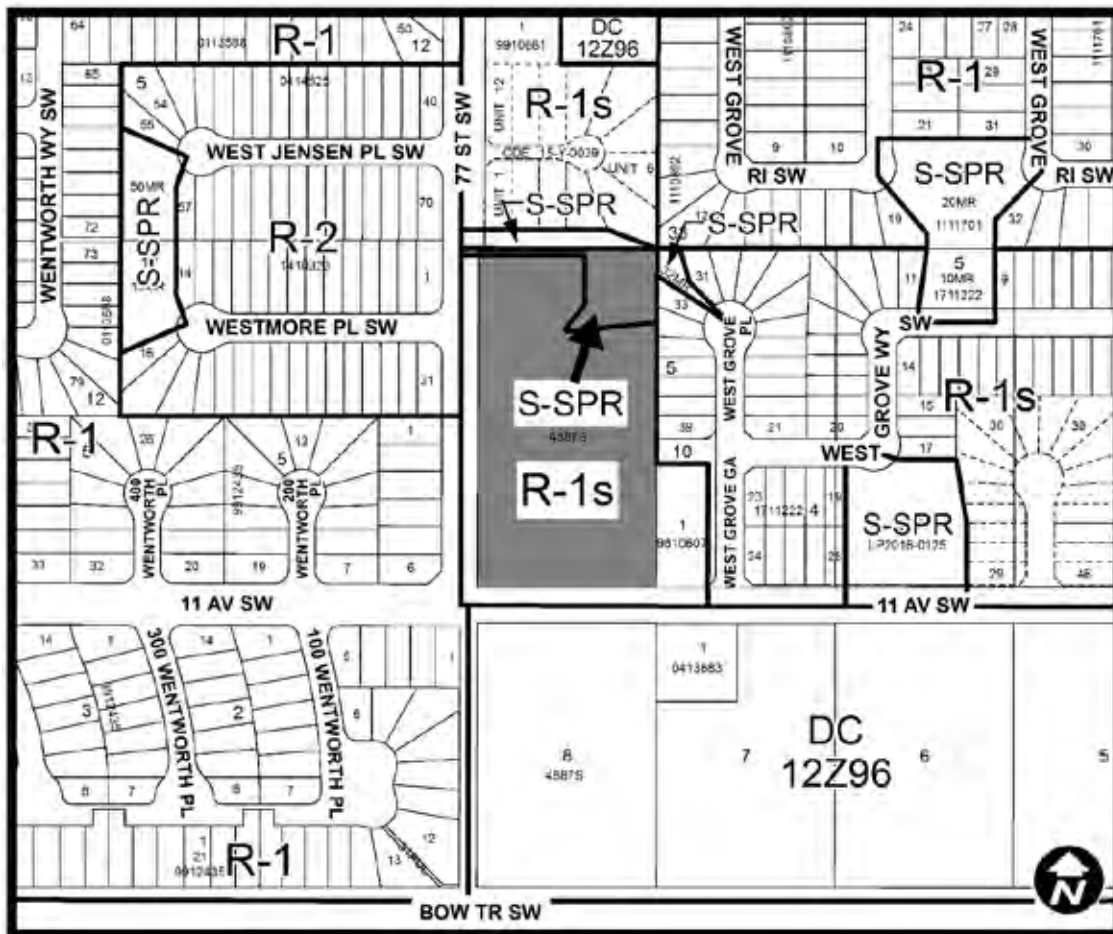


OUTLINE PLAN
WEST SPRINGS (WARD 6)
11 AVENUE SW EAST OF 77 STREET SW

MAP 22W

APPENDIX V

PROPOSED LAND USE DISTRICT MAP



A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a transit instrument. The surveyor is looking through the instrument's eyepiece. The transit instrument is a large, complex piece of equipment with a circular level and a vertical axis. The entire scene is rendered in a dark grey silhouette against a light background.

Calgary Planning Commission 2018 January 25

Item 5.11

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

EXECUTIVE SUMMARY

This development permit application proposes a new three-to-five storey Multi-Residential Development with Live Work Units along 69 Avenue SW in the community of Kingsland. The proposal is for a total of 110 units and underground parkade, on 11 contiguous laned parcels (the site). A number of public realm improvements have been included in this Development Permit.

The site is currently designated as Residential-Contextual One / Two Dwelling (R-C2) District. A related Land Use Amendment application (LOC2015-0095 / Bylaw 50D2016) seeks to redesignate the site to Multi-Residential – Contextual Low Profile (M-C1) District. A decision on this Development Permit cannot be rendered prior to Council approving LOC2015-0095 / Bylaw 50D2016. The Bylaw is pending second and third reading upon conditional approval of this Development Permit application. Second and third reading of Bylaw 50D2016 must be completed by 2018 April 11 otherwise the proposed Bylaw will lapse and be abandoned.

PREVIOUS COUNCIL DIRECTION

ADOPT, Moved by Councillor Pincott, Seconded by Councillor Carra, that the Calgary Planning Commission Recommendations contained in Report CPC2016-057 be adopted, **after amendment**, as follows:

That Council:

1. ADOPT the proposed redesignation of 0.61 hectares ± (1.50 acres ±) located at 617, 623, 627, 631, 635, 639, 643, 703, 707, and 715 – 69 Avenue SW (Plan 1754HK, Block 2, Lots 1 to 11) from Residential – Contextual One / Two Dwelling (R-C2) District to Multi-Residential - Contextual Low Profile (M-C1) District, in accordance with Administration's Recommendation; and,
2. Give **first** reading to the proposed Bylaw 50D2016; **and**
3. **WITHHOLD second and third readings of Bylaw 50D2016 until the Calgary Planning Commission has conditionally approved the development permit application.**

CARRIED

INTRODUCE, Moved by Councillor Pincott, Seconded by Councillor Carra, that Bylaw 50D2016, Being a Bylaw of The City of Calgary to Amend that Land Use Bylaw 1P2007 (Land Use Amendment LOC2015-0095), be introduce and read a first time.

CARRIED

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

ADMINISTRATION'S RECOMMENDATION(S)

2018 January 25

That Calgary Planning Commission:

1. **RECEIVE AND ACCEPT** this report and appendices for information; and
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, **APPROVE** Development Permit DP2016-2458 with conditions (APPENDIX II), subject to Council giving second and third reading of Bylaw 50D2016.

REASON(S) FOR RECOMMENDATION:

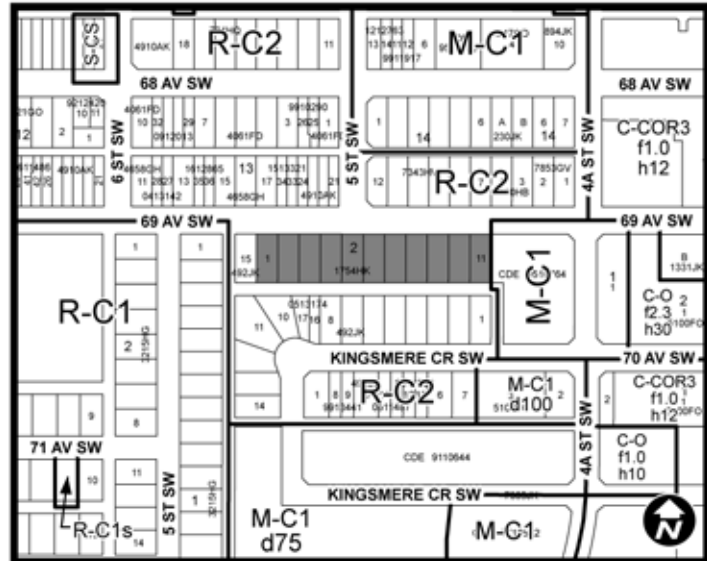
The proposal which allows for intensification of the site, aligns with applicable policies of the Municipal Development Plan.

The proposed low-profile, medium-density development is consistent with the low to medium residential density character of the area.

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MAP 28S

LOCATION MAPS



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
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(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION

That Calgary Planning Commission:

1. **RECEIVE AND ACCEPT** this report and appendices for information; and,
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, **APPROVE** Development Permit DP2016-2458 with conditions (APPENDIX II), subject to Council giving second and third reading of the Bylaw 50D2016.

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

Applicant:

Sturgess Architecture

Landowner:

James T F Lee
Jean Libin
Janl Developments Ltd

Address

617 - 69 Avenue SW
623 - 69 Avenue SW
627 - 69 Avenue SW
631 - 69 Avenue SW
635 - 69 Avenue SW
639 - 69 Avenue SW
643 - 69 Avenue SW
703 - 69 Avenue SW
707 - 69 Avenue SW
711 - 69 Avenue SW
715 - 69 Avenue SW

Legal

Plan 1754HK; Block 2; Lot 11
Plan 1754HK; Block 2; Lot 10
Plan 1754HK; Block 2; Lot 9
Plan 1754HK; Block 2; Lot 8
Plan 1754HK; Block 2; Lot 7
Plan 1754HK; Block 2; Lot 6
Plan 1754HK; Block 2; Lot 5
Plan 1754HK; Block 2; Lot 4
Plan 1754HK; Block 2; Lot 3
Plan 1754HK; Block 2; Lot 2
Plan 1754HK; Block 2; Lot 1

PLANNING EVALUATION

INTRODUCTION

This development permit application is for a multi-residential building that contains 110 units (104 dwelling units and six Live Work Units) and an underground parkade. The development sits on 11 contiguous laned parcels with a total area of 0.61 hectares (1.50 acres). Consolidation of the 11 parcels is a condition of this development permit.

SITE CONTEXT

The site is located in the community of Kingsland along the south side of 69 Avenue SW. The immediate surrounding development and land use context is comprised of Semi-detached Dwellings in the Residential - Contextual One / Two Dwelling (R-C2) District. There is a three-storey multi-residential development to the immediate east of the site. Multi-residential land uses and developments exist within a 100 metre radius from the site under the Multi-Residential - Contextual Low Profile (M-C1) District.

The site is located approximately 200 meters west of Macleod Trail S and 400 meters east of Elbow Drive SW both of which are part of the Primary Transit Network.

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MAP 28S

According to the data from The City of Calgary 2017 census, shown on the table below, the population in the community of Kingsland has declined by 13 percent since it achieved its peak population in 1971. Increasing density through development that allows for sensitive intensification could help stabilize this population decline.

| Kingsland | |
|---------------------------------------|-------|
| Peak Population Year | 1971 |
| Peak Population | 5,341 |
| 2017 Current Population | 4,667 |
| Different in Population (Number) | -674 |
| Difference in Population (Percentage) | -13% |

LAND USE DISTRICTS

The development has been designed in accordance with the proposed Multi-Residential – Contextual Low Profile (M-C1) District. The M-C1 District is intended to be in close proximity or adjacent to low density residential development. It allows for multi-residential development that has a higher number of dwelling units and traffic generation than low density residential dwellings. It provides for Multi-Residential Developments in a variety of built forms with low heights and medium density. The M-C1 District also allows for varied building height and front setback areas in a manner that reflects the immediate context.

The proposed development generally meets the rules of the M-C1 District with supported relaxations.

SITE CHARACTERISTICS

The site is generally flat with no significant grade variation and is occupied by 11 semi-detached dwellings. An unpaved lane exists in the rear which will be paved, as part of the proposed development. Private mature coniferous trees are located in the north area, along 69 Avenue SW. Private mature deciduous trees exist at the rear.

Power poles and power lines exist along the north side of the lane. As part of the proposed development, the power poles will be relocated to facilitate access to proposed garages and the underground parkade. The new poles will be taller and the power lines will be raised to address separation requirements between the wires and the building. No poles will be added along the south side of the lane.

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MAP 28S

LEGISLATION & POLICY

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

The site is located in the Developed Residential-Established area in Map 1: Urban Structure of the MDP. This area is characterized primarily by residential communities containing a mix of low and medium density housing, with support retail in relatively close proximity. In addition, the site is located approximately 200 metres from Macleod Trail S which is part of the Primary Transit Network and a Main Street Corridor.

The proposal advances the overall MDP policy direction to build a more compact city with an efficient use of land that enhances the vitality and character of local neighborhoods while promoting a variety of housing options.

Policies specific to the typology of the area included in section 3.5.1 (a) support moderate intensification in a form and nature that respects the scale and character of the neighbourhood. Furthermore, section 3.5.3 (c) notes that new development in Established Areas should incorporate appropriate densities, a mix of land uses and a pedestrian-friendly environment to support an enhanced Base or Primary Transit Network.

City-wide policies included in section 2.2.2 (b) support the increase in density in proximity to the Primary Transit Network. And, as a way to create stronger residential neighbourhoods. Section 2.2.5 (a, b) encourage higher residential densities and redevelopment that is similar in scale and built form, and that increases the mix of housing types; and, supports development and redevelopment that provides a broader range of housing choices in local communities to help stabilize population declines and supports the demographic needs of communities.

Local Area Plan

There is no Local Area Plan for Kingsland.

Land Use Bylaw 1P2007 (LUB)

The proposed development generally complies with the rules of the M-C1 District. As shown on the Bylaw Discrepancy table below, there are a few relaxations that have been considered acceptable, including relaxation to the maximum building height and density allowed under the M-C1 District.

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MAP 28S

| Bylaw Relaxations | | |
|---|--|--|
| Regulation | Standard | Provided |
| 592 Building Setbacks | (1) Unless otherwise referenced in subsection (2), the minimum building setback from a property line shared with a street is the greater of: (a) the contextual multi-residential building setback; | Plans indicate the north setback to the first storey windows is 4.19m (-2.31m), the setback to the upper balconies is 3.56m (-2.94m) and the setback to the second and third floor is 3.2m (-3.30m). <i>Relaxation is supported.</i> <i>The setback to the building wall is 5.0m. There are window wells and bay windows projections / cantilevered sections into the north setback that add to the architectural richness of the development (break up a large blank wall).</i> |
| 549 Projections Into Setback Areas | (1) Unless otherwise referenced in subsections (2), (3), (4), (5), (6), and (7), a building or air conditioning units must not be located in any setback area. | Due to the location of the proposed building within the north setback area this rule is non-compliant. <i>Relaxation is supported.</i> <i>Rational appears above (592). No air-conditioning units are shown in the setback area.</i> |
| | (5) window wells may project a maximum of 0.8m into any setback area. | Plans indicate the north window wells project 2.74m (+1.94m) into the setback area. <i>Relaxation is supported.</i> <i>The proposed window wells, in the form of landscaped terracing, allows for natural light into the basement of the development.</i> |
| 594, 13 Building Height and Cross Section | (1) Unless otherwise referenced in subsections (2) and (3), the maximum building height is 14.0 m | Plans indicate the building will exceed 14.0m in height on all four elevations. This is due to the residential mid-block building. <i>Relaxation is supported.</i> <i>Generally, the proposed building is lower than the maximum building height of 14.0m. The mid-block building is approximately 16.0m and this represents only 4.5 percent of the overall development. The strategically located mid-block building enhances the design and breaks up the horizontal mass of the development.</i> |

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MAP 28S

| | | |
|-------------------|---|--|
| 590 Density | (1) The maximum density for parcels designated M-C1 District is 148 UPH. | <p>Plans indicate a density of 179 UPH (+31 UPH) - 110 Units (+20 units over allowable density).</p> <p><i>Relaxation is supported.</i></p> <p><i>The development includes a variety of unit types and sizes (bachelor suites, one, two and three bedroom units). The inclusion of smaller units increases the number of units but not the overall massing of the development. Having a variety of housing options is consider appropriate.</i></p> <p><i>There were preliminary discussions on this matter at the Land Use Amendment application stage. As part of this development, a design exercise provided by the Applicant shows that if the smaller units merged, the overall density count could be reduced by 20 units to achieve the maximum density allowed under M-C1; however, the massing of the building would not change.</i></p> |
| 557 Amenity Space | (8) Private amenity space must: (b) have no minimum dimensions of less than 2.0 metres. | <p>Plans indicate dimensions on the first level patios of 1.50m (-0.5m), the third level of 0.85m (-1.15m) and the fourth level of 1.05m (-0.95m).</p> <p><i>Relaxation is supported.</i></p> <p><i>Main level terraces range from 8.8 sq.m. – 14.8 sq.m, third level balconies range from 1.4 sq.m. – 8.2 sq.m. and fourth level balconies/terraces range from 3.7 sq.m.-11.7 sq.m. Additional common amenity space is provided within the courtyard and at the main building entrance.</i></p> |
| | (9) Common amenity space: (c) must have a contiguous area of not less than 50.0 square metres, with no dimension less than 6.0 metres; | <p>Plans indicate the minimum dimension of the courtyard is 4.0m (-2.0m). This has not been considered as common amenity space.</p> <p><i>Relaxation is supported.</i></p> <p><i>The distance between building walls is approximately 10.0m. The common amenity space is 4m wide and the private amenity spaces occupies the balance. The courtyard includes sitting areas where residents can interact.</i></p> |

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MAP 28S

| | | |
|--|---|---|
| <p>557 Amenity Space</p> | <p>(2) A patio may be located in a setback area between a street-oriented multi-residential building and a property line shared with a street.</p> | <p>Plans indicate patios in the north setback area although the development does not qualify as Street-Oriented Multi-residential building.</p> <p><i>Relaxation is supported.</i> <i>The proposal is not considered street oriented development as the individual accesses are not within 4.5m from the property line that shares the street. However, including patios in the setback area is considered appropriate. The patios allow for natural light into the units located in the basement.</i></p> |
| <p>Parking Stalls</p> | <p>Total site requirement: 152 parking stalls</p> | <p>Plans indicate 148 (-4) parking stalls on site.</p> <p><i>Relaxation is supported.</i> <i>Six tandem parking are provided but not considered under the Land Use Bylaw. The subject site is located approximately 200 metre from Macleod Trail S which is part of the Primary Transit Network.</i></p> |
| <p>550 General Landscaped Area Rules</p> | <p>(6) Unless otherwise referenced in subsections (7) and (8), all areas of a parcel, except for those portions specifically required for motor vehicle access, motor vehicle parking stalls, loading stalls, garbage facilities, or any purpose allowed by the Development Authority, must be a landscaped area.</p> <p>(7) All setback areas adjacent to a street or another parcel, except for those portions specifically required for motor vehicle access, must be a landscaped area.</p> | <p>Due to the location of the building within the North setback area these rules are non-compliant.</p> <p><i>Relaxation is supported.</i> <i>Rational appears above (592).</i></p> |
| <p>551 Specific Rules for Landscaped Areas</p> | <p>(3) The maximum hard surfaced landscaped area is: (b) 40.0 % of the required soft landscape area, in all other cases.</p> | <p>Plans indicate a total hard landscaped area of 2249.95m² (+1267.23m²) or 91.58% (+51.58%) of the total landscaped area.</p> <p><i>Relaxation is supported.</i> <i>Permeable pavers, proposed as a finishing material in the courtyard, are not considered soft landscaping</i></p> |

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MAP 28S

SITE LAYOUT & BUILDING DESIGN

This development permit proposes a multi-residential development with a total gross building area of 9,980 square meters. The proposal increases the number of units from the 22 existing units to 110 units in a variety of unit types and sizes.

The building siting and design has been influenced by four key elements:

1. adjacent existing development and neighbourhood character;
2. existing mature coniferous trees in front of the development;
3. “T” intersection at the front of the site along 69 Avenue SW; and,
4. the existing rear lane.

The premise of the design is to create building blocks along the east-west axis of the site with a central open space or courtyard. The building blocks along the courtyard are approximately 10 metres high (3 to 3.5 storey). One and three bedroom stacked units are located on the north side and two bedrooms townhouse style units are on the south side of the courtyard. All units around the courtyard area have dual frontage (courtyard - street or courtyard - lane), which allows for natural light and cross ventilation.

The five storey mid-block building is approximately 16 meters high and has been designed to provide a terminus point at the “T” intersection of 69 Avenue SW and 5 Street SW. Fifth Street SW is an important pedestrian link to Chinook Mall and the Chinook LRT Station, which is located 1.3 kilometres northeast of the site.

The mid-block building breaks up the development’s horizontal massing, enriches the building’s architectural form and frames the main entrance to the building. Apartment style units with a variety of unit sizes (bachelor suites, one and two bedroom dwelling units) are included. The south portion is stepped back and generally, balconies face east and west to address concerns related to overlooking into neighbours’ backyards. Live Work Units are located on the main floor and are directly accessed from the lane and the lobby area.

Balconies are provided throughout the development to soften the building façade and courtyard, and provide eyes on the street. The building colour scheme includes three shades of grey with primary colours used as accent colours in balconies and soffits. Wheelchair ramps have been included throughout the development.

Public Realm Improvements

As a result of public engagement and the collaborative approach between the Applicant, the Kingsland Community Association and Administration, improvements to the public realm have been provided as part of this Development Permit. All improvements will be at the expense of the developer. Details are as follows:

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MAP 28S

- New two metre wide public sidewalk along 69 Avenue SW from the intersection of 4A Street SW to the lane located west of the site. This is added to improve the pedestrian experience along 69 Avenue SW.
- Existing lane will be paved. The extent of the lane paving is from 69 Avenue SW to Kingsmere Crescent SW. This will support additional traffic in the lane and provide for a better interface with the proposed units facing the lane.
- Curb extension at the “T” intersection of 69 Avenue SW and 5 Street SW. This improvement was considered after the community open house as a traffic calming feature and a way to improve the pedestrian experience (crossing) at the intersection.
- Existing trees along 69 Avenue SW will remain, except for those that will be removed to allow for the building’s main entrance. Mature trees are an important feature for the area.

DESIGN REVIEW

A representative from the City Wide Urban Design team was part of the internal review process. Concerns were raised in relation to enhancing the street wall, setbacks and lane interface. Rationale and the resulting revision to the proposal were deemed appropriate.

On 2017, June 28, the proposed development was considered by Urban Design Review Panel (UDRP). The panel provided comments and suggestions for improvements to the building’s entranceways and the courtyard experience. The UDRP supported the proposed design, including the Bylaw relaxations. The UDRP comments, together with the applicant’s response to these suggestions are included in APPENDIX V. The applicant’s rationale and the resulting additional revisions were deemed appropriate.

CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED)

The proposed development has considered the basic principles of CPTED. The main building access is clearly defined, well-lit and legible. To improve visibility and safety, the two trees located next to this area will be removed. Gates are proposed in the main entrance and all secondary entrances to the building, to increase safety for residents.

The building design differentiates private areas from public areas through controlled accesses. Glazing and balconies are included throughout the building, adding new eyes on the street and the lane. The courtyard is a common amenity area which includes sitting opportunities to support residents’ interaction.

**DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S

LANDSCAPING

The landscaping design has been carefully considered to add visual appeal and aesthetics throughout the development.

As mentioned previously, there are 20 private spruce trees along 69 Avenue SW. Most trees will remain in place except for two trees that will be removed to allow for the building's main entrance. The location of the trees influenced the building's siting and the front setback. The private mature deciduous trees existing at the rear of the site will be removed to allow for the development.

The front setback area is comprised of grassed areas and concrete walkways leading to the entrance of those units facing the street. A generous open space with concrete finishing is located by the main entrance of the building. Along the front of the building, the landscape plan considers sunken individual landscaping terracing (accessed only by each individual dwelling unit), and low-raised planters.

In the courtyard area, planters, high enough to provide for the planting requirement and privacy, are considered. Planters and low gates define the private amenity spaces for the units that have direct access from the courtyard. Benches are located between the private and the common amenity spaces to provide gathering opportunities for residents.

To add character to the lane, planting areas occupied by small trees are considered in between garage doors. The tree type identification is a Prior to Release condition of this application and will conform to the planting design outlined by ENMAX Power Corporation to avoid conflicts with utility lines.

Permeable pavers are proposed as a finishing material for the courtyard floor. High quality concrete finishing is considered on all other common areas throughout the development.

SITE ACCESS & TRAFFIC

A Transportation Impact Assessment (TIA) was completed in 2015 as part of the Land Use application. The TIA concludes that the existing intersections in the vicinity of the site will operate at a similar level of service post-development.

Pedestrian access to the site is from 69 Avenue SW. Vehicular access to the site and the underground parkade is via a rear lane parallel to 69 Avenue SW. Access to the lane is from 69 Avenue SW and Kingsmere Crescent SW.

Sixty-ninth Avenue SW is classified as a collector roadway with sidewalks on both sides of the street. The site is located approximately of 440 metres walking distance from the route 3 bus stop on Elbow Drive SW; 460 metres walking distance from the route #81 bus stop on Macleod Trail SW; and, 300 metres walking distance from the route 47 bus stop on Glenmore Trail SW. The closest LRT station is the Chinook Station, located 1.3 kilometres from the subject site.

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MAP 28S

Elbow Drive SW and Macleod Trail S are both part of the Primary Transit Networks in the Calgary Transportation Plan. A signed bicycle route is located on 6 Street SW (north of 69 Avenue SW) and on 5 Street SW (south of 69 Avenue SW) which connects to the pathway system on Glenmore Trail SW and Heritage Drive SW.

PARKING

The proposed development requires a total of 152 parking stalls. The total parking stalls provided on site is 148 stalls (not including six tandem stalls). As per the Bylaw discrepancy table the development is deficient 4 parking stalls. Parking stalls are located in an underground parkade and in 20 private garages, both with direct access from the lane. The parkade includes visitor, universal access and regular parking stalls. A parking study was not required for this application.

Bicycle parking stalls class 1 (located in secured or controlled area) and class 2 (located in unsecured or uncontrolled area) provided exceed the Bylaw requirements (total of 95 surplus bicycle parking stalls). Bicycle parking stalls - class 1 are in the parkade. Bicycle parking stalls - class 2 are proposed by the main building entrance and by the lobby area.

The site is located in a Residential parking permit zone, with parking restrictions on 69 Avenue SW (permit parking only 07:00 – 17:30). The proposed multi-residential development will not qualify for parking permits.

UTILITIES & SERVICING

As a condition of this development permit, a section of the existing 150 millimetre watermain along 69 Avenue SW is required to be upgraded at the Developer's expense. Water connection is available from 69 Avenue SW once the main is upgraded to a 200 millimetre diameter.

Storm sewers are available to service the site from 5 Street SW.

Sanitary sewer mains are available to service the site from 69 Avenue SW.

A Sanitary Servicing Study (SSS) for the proposed development has been submitted by the Applicant, and approved by Water Resources.

ENVIRONMENTAL ISSUES

A Phase I Environmental Site Assessment for the site was requested by Environmental and Safety Management. Based on the provided information in the Curtis Geo Solutions Inc report dated January 2017, no further investigation was recommended.

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MAP 28S

ENVIRONMENTAL SUSTAINABILITY

The site and building design supports sustainable features. Consideration has been given to creating an environmentally conscious design. Strategies for achieving an environmentally conscious site and building include:

- Compact development through increased density in close proximity of transit, allows for more efficient use of infrastructure.
- Opportunities for adding more units (and doors) on the street and the lane.
- Units with dual frontage and operable windows allow for natural light and cross ventilation.
- Surplus dedication of bicycle parking stalls for the public and residents of the development promote alternate transportation modes.
- Low water planting and native species used for landscaping support water efficient practice.
- Use of permeable materials and inclusion of a cistern to control stormwater run-off.
- Selection of environmentally preferable materials and minimization of waste during construction.
- Solar Photovoltaic (PV) System proposed in the roof to partially offset onsite energy requirements.
- Enhancements to public realm around the development support a safer pedestrian experience.
- Retention of existing mature trees.

PUBLIC ENGAGEMENT

Community Association Comments

A collaborative approach between the Applicant, Administration, and the Kingsland Community Association board (KCA) was key during the review of this Development Permit. The KCA initially submitted a letter of opposition detailing a number of concerns and issues.

As part of the community engagement plan, prepared by the Applicant, a number of meetings took place where these concerns were discussed and solutions explored. Revisions to the development include a stepped back rear portion of the mid-block building, the colour of the building was revised, and rear lane paving was included. The Applicant's rationale and the resulting revisions were accepted. After the re-circulation process, the KCA submitted a letter in support of the development (see APPENDIX IV).

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MAP 28S

Citizen Comments

Three letters from nearby residents were received in August 2016. The letters highlighted residents' concerns related to property maintenance, disruption expected during construction and height of the proposed mid-block building.

Public Meetings

As part of the community engagement plan prepared by the Applicant, an Applicant led Community Open House occurred in June 2016. Administration attended the Open House to answer any process and policy related questions.

The feedback received from the residents was shared with the City and options were jointly explored. Follow up correspondence shared with the residents who attended the open house included development and design rationale and listed revisions to the development including the curb extensions at the intersection of 69 Avenue SW and 5 Street SW. A summary of the comments received and responses are included in APPENDIX VI. No follow up questions or concerns were received by the time this report was finalized.

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
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 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



45 Avenue Development
 1111 Avenue SW
 Calgary, Alberta T2P 0K1
 Phone: (403) 243-1111
 Fax: (403) 243-1112
 Email: info@45avenue.com

| | |
|------------|-------------|
| DATE | 1/16/2018 |
| DRAWN BY | J. BURGHERS |
| CHECKED BY | J. BURGHERS |
| SCALE | AS SHOWN |

DP 0.2

DESIGN RATIONALE

This project aims to increase density and support economic vitality in the neighborhood through the provision of a variety of unit types. The project is also designed to support the long-term vision of the City and the surrounding neighborhood to provide a mix of housing options for a diverse demographic of the population. This project will also provide an opportunity for a new multi-story building to be constructed in the neighborhood, which will help to increase the density of the neighborhood and provide a variety of unit types.

The proposed development consists of a 14-story high-rise building with a mix of unit types and amenities. The building is designed to be a high-quality, modern multi-residential development that will provide a variety of unit types and amenities. The building is designed to be a high-quality, modern multi-residential development that will provide a variety of unit types and amenities. The building is designed to be a high-quality, modern multi-residential development that will provide a variety of unit types and amenities.

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Prepared by:
 J. BURGHERS



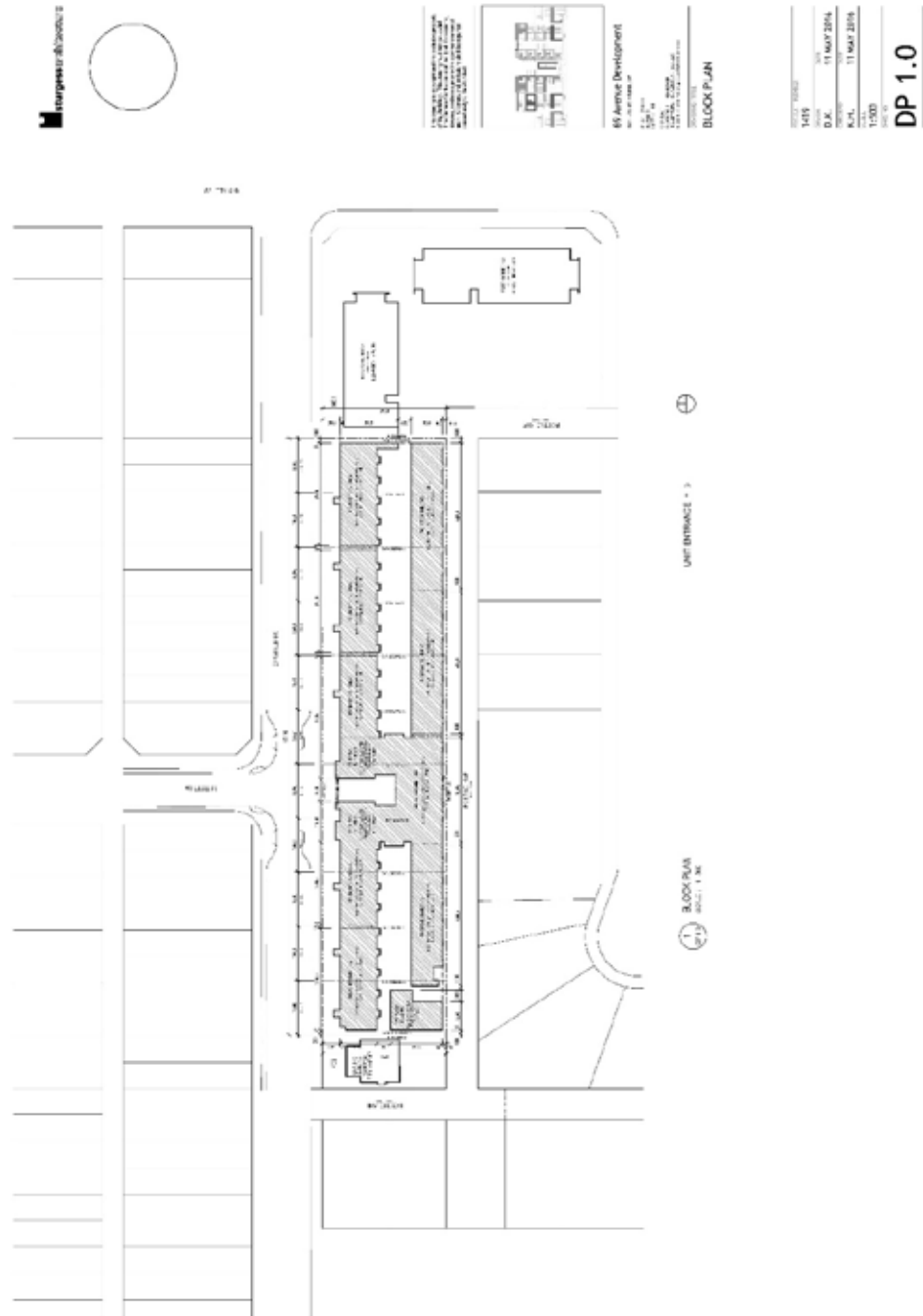
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 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S



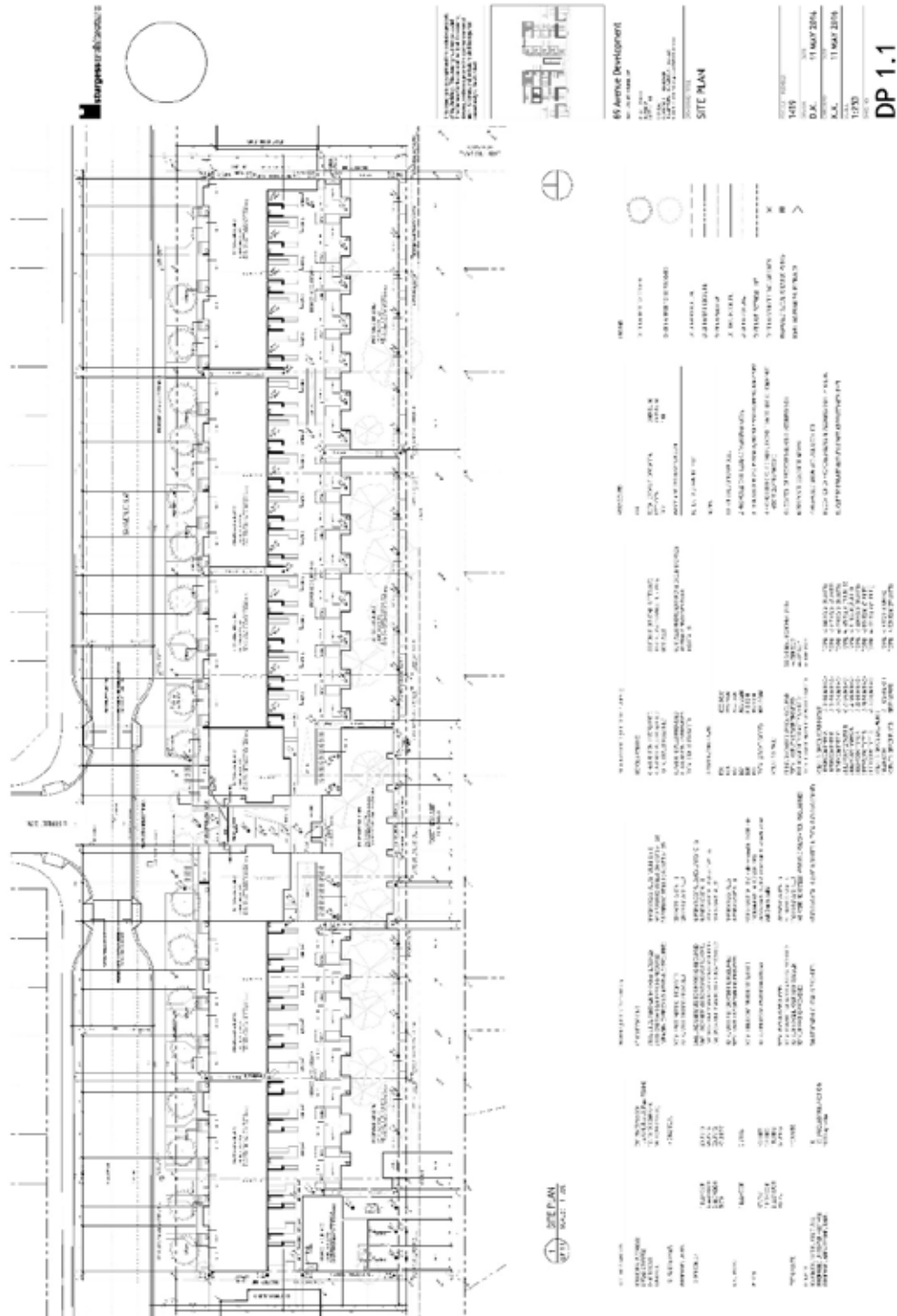
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MAP 28S



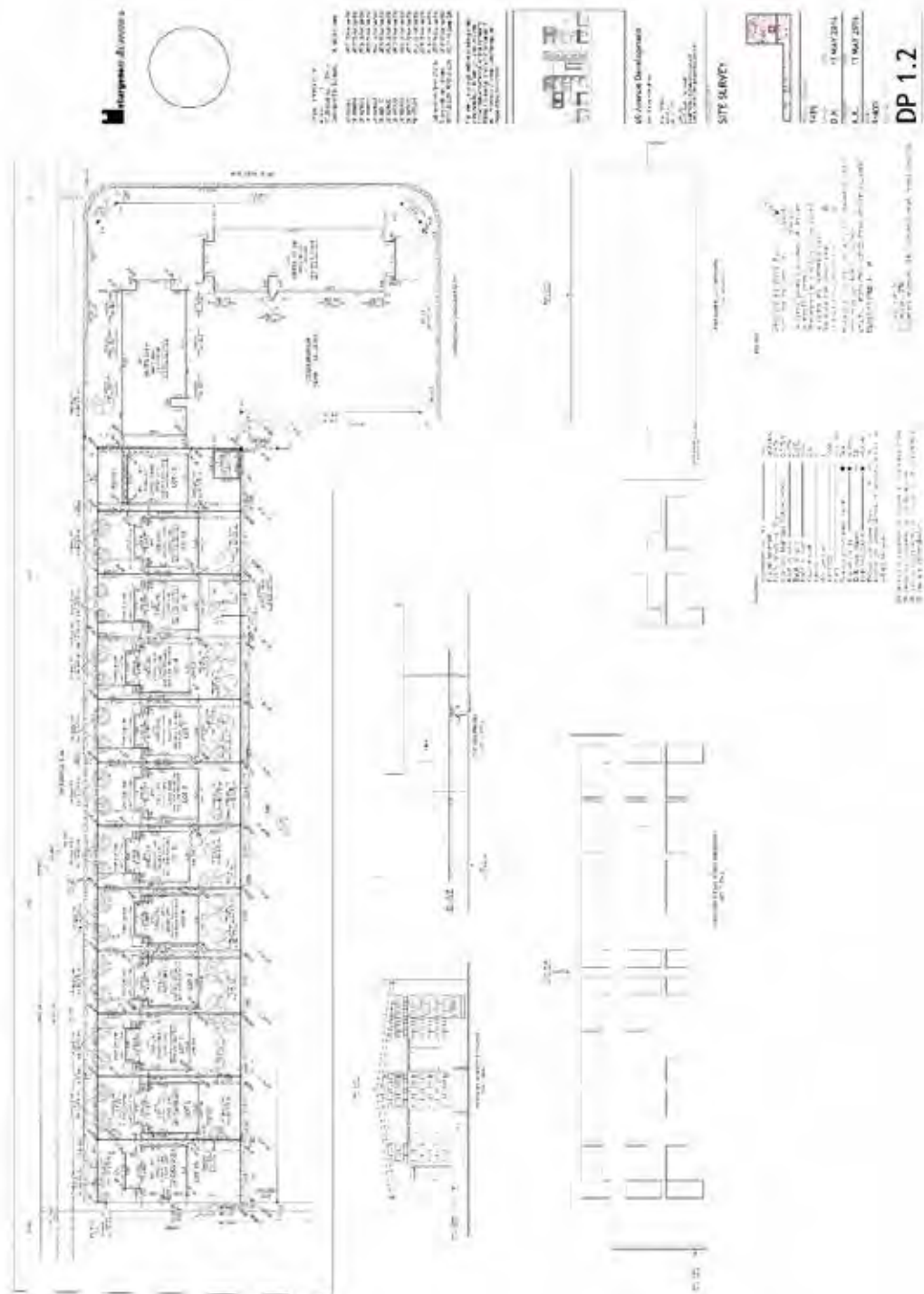
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 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



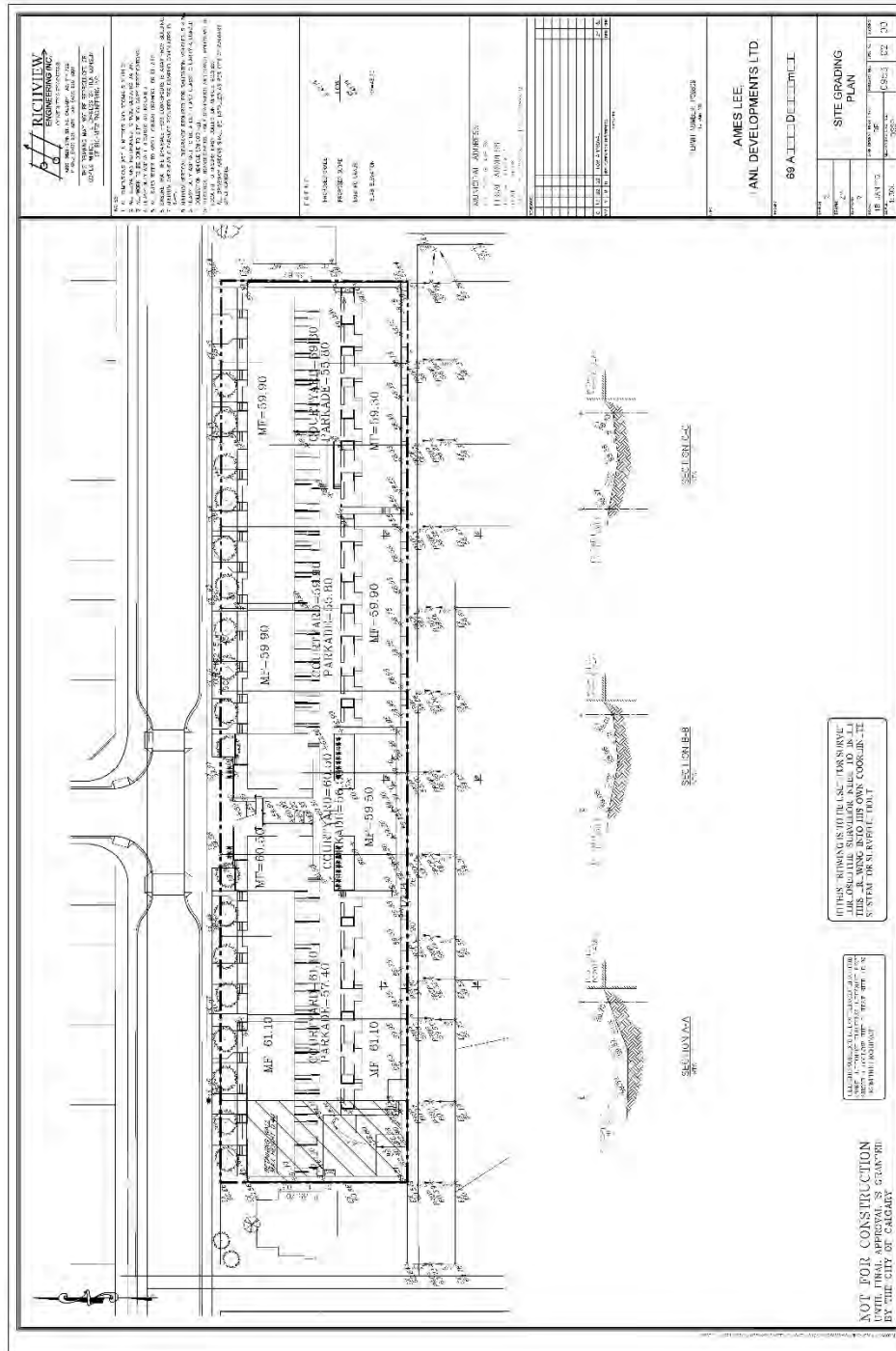
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MAP 28S



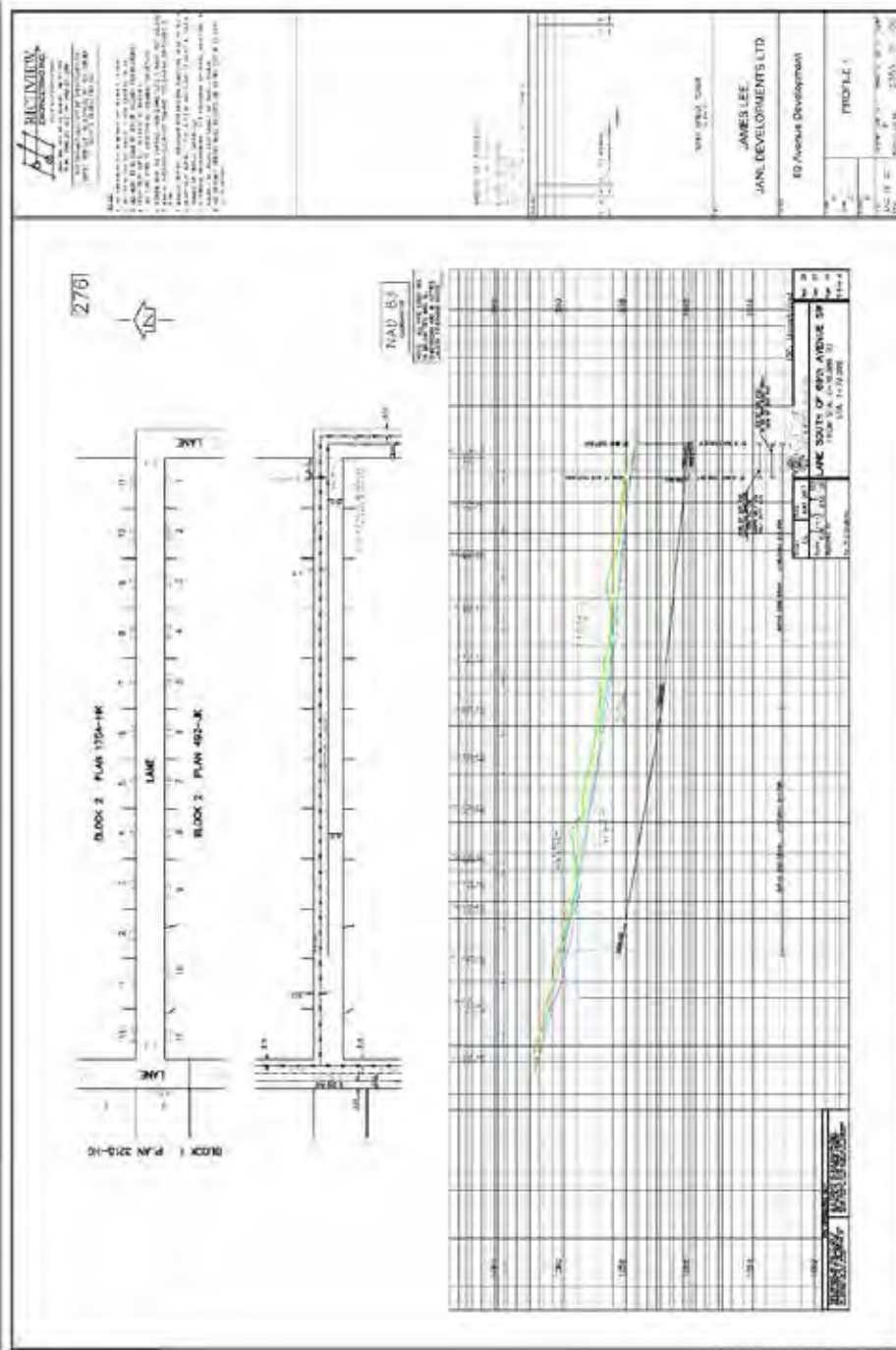
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 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



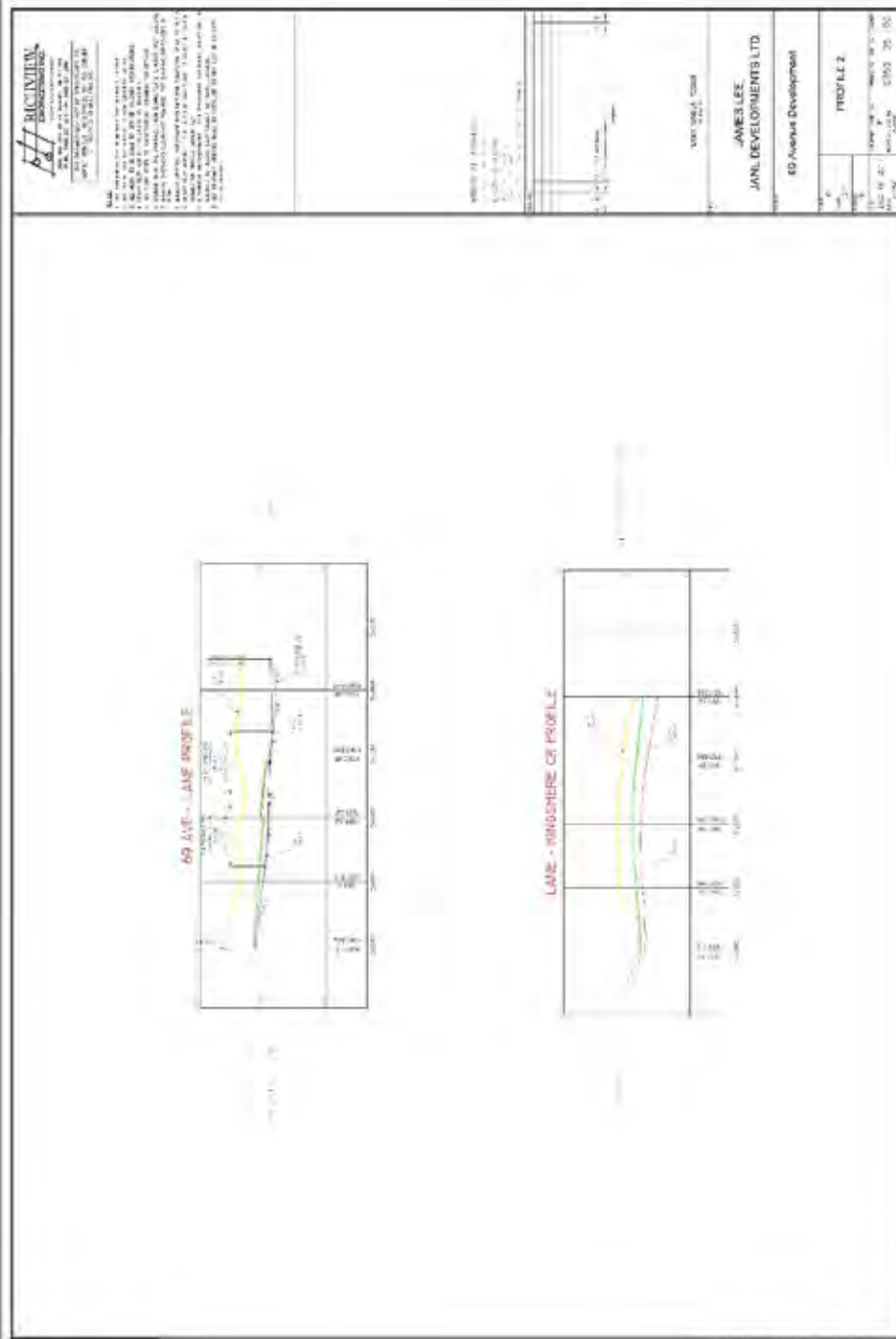
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ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

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 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

BASSETT ASSOCIATES
 LANDSCAPE ARCHITECTURE INC.
 2500 17TH AVENUE SW, SUITE 100
 CALGARY, ALBERTA T2M 0K6
 TEL: (403) 243-8888
 WWW.BASSETTASSOCIATES.COM

Professional Engineer
 License No. 12345
 Name: [Name]

Professional Architect
 License No. 67890
 Name: [Name]

DATE: 12/15/2017
 PROJECT: MULTI-RESIDENTIAL
 DEVELOPMENT PERMIT
 APPLICATION NO. DP2016-2458
 PLAN 17240R

DATE: 12/15/2017
 PROJECT: MULTI-RESIDENTIAL
 DEVELOPMENT PERMIT
 APPLICATION NO. DP2016-2458
 PLAN 17240R

DATE: 12/15/2017
 PROJECT: MULTI-RESIDENTIAL
 DEVELOPMENT PERMIT
 APPLICATION NO. DP2016-2458
 PLAN 17240R

Detail 26-101-25: Tree/Shrub Planting

The diagram shows a cross-section of a tree/shrub planting. It includes a root ball with soil, a layer of mulch, and an irrigation system with a drip emitter. Labels include: Root Ball, Soil, Mulch, Irrigation System, Drip Emitter, and Watering Schedule. A table to the right lists: SPECIES: [Species], SIZE: [Size], PLANTING DATE: [Date], and other details.

26-101-25

Detail 26-101-26: Tree/Shrub Planting

The diagram shows a cross-section of a tree/shrub planting. It includes a root ball with soil, a layer of mulch, and an irrigation system with a drip emitter. Labels include: Root Ball, Soil, Mulch, Irrigation System, Drip Emitter, and Watering Schedule. A table to the right lists: SPECIES: [Species], SIZE: [Size], PLANTING DATE: [Date], and other details.

26-101-26

1 TREE PLANTING DETAIL

DATE: 12/15/2017

26-101-26

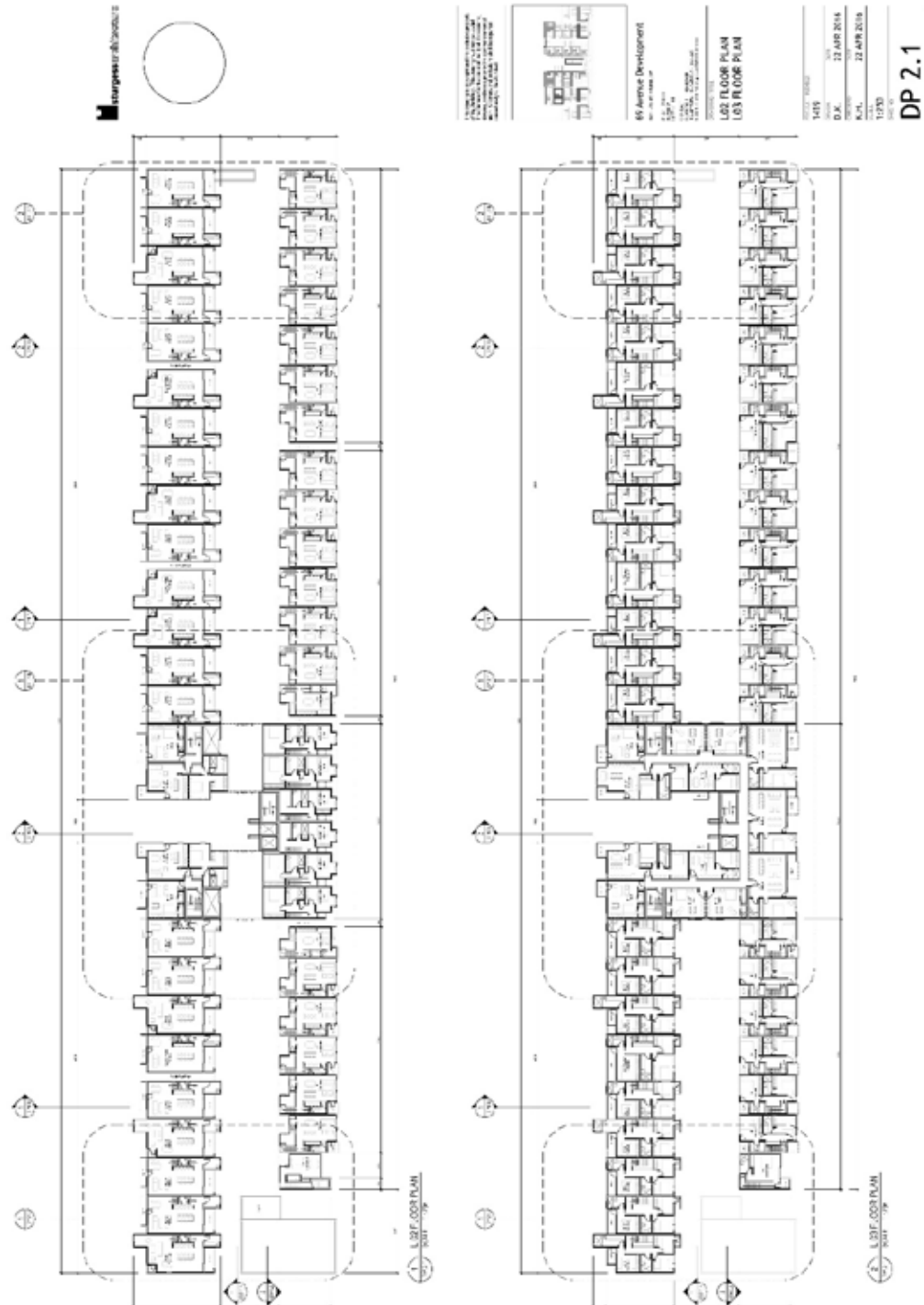
2 SHRUB PLANTING DETAIL

DATE: 12/15/2017

26-101-25

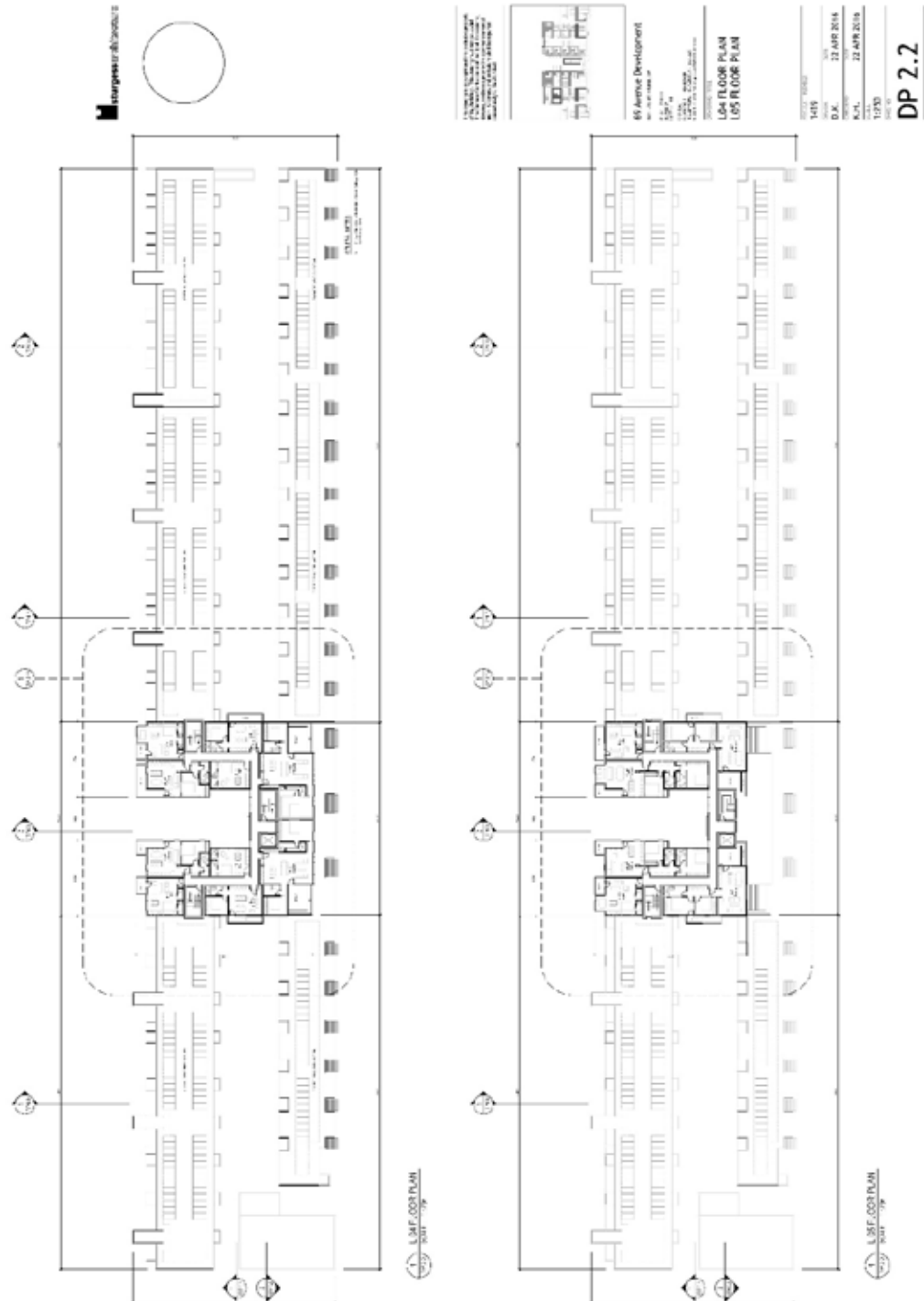
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KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



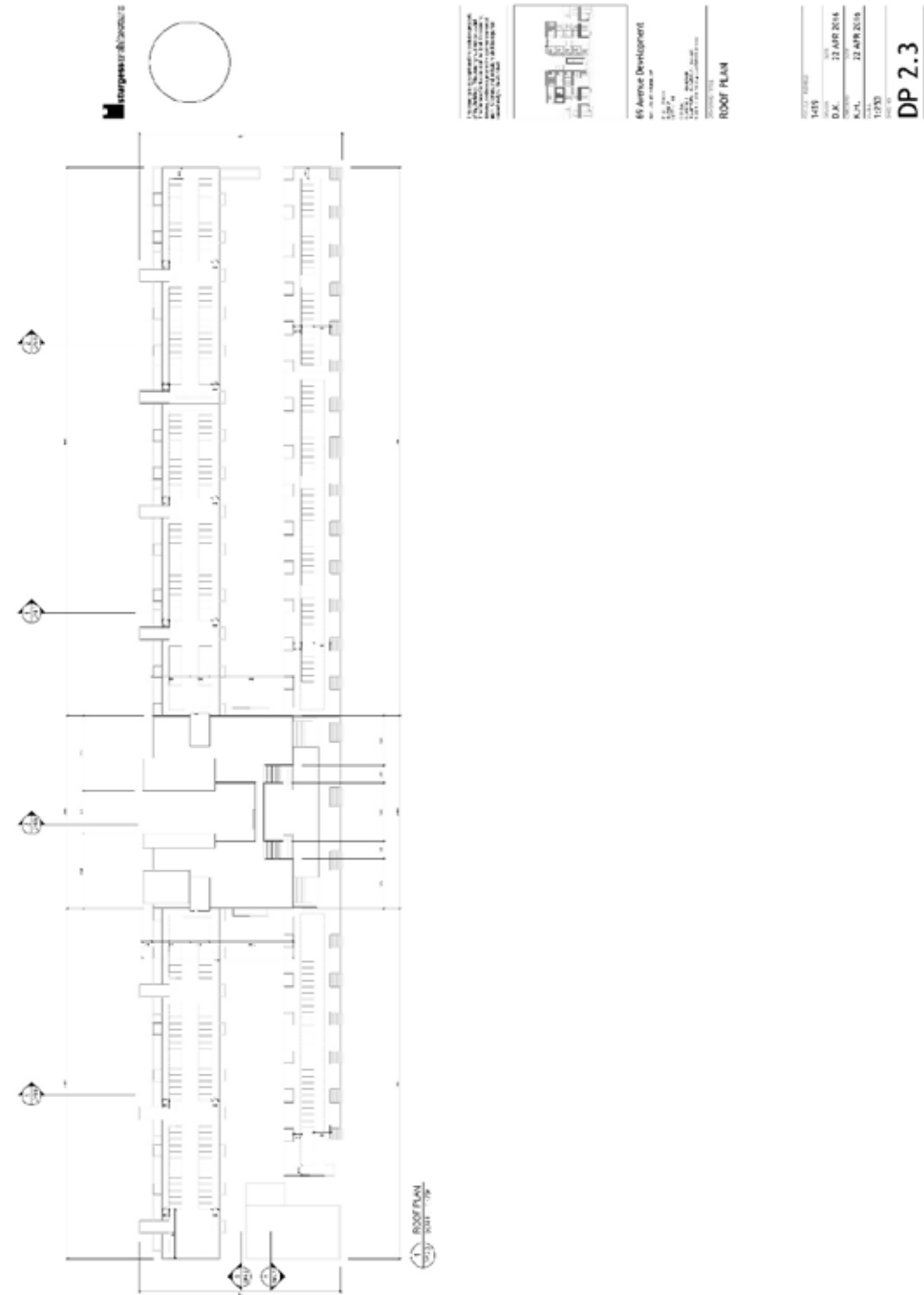
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 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



66 Avenue Development
1000 66 Avenue SW, Calgary, Alberta T2M 4K6
604.271.1111
www.66avenue.com

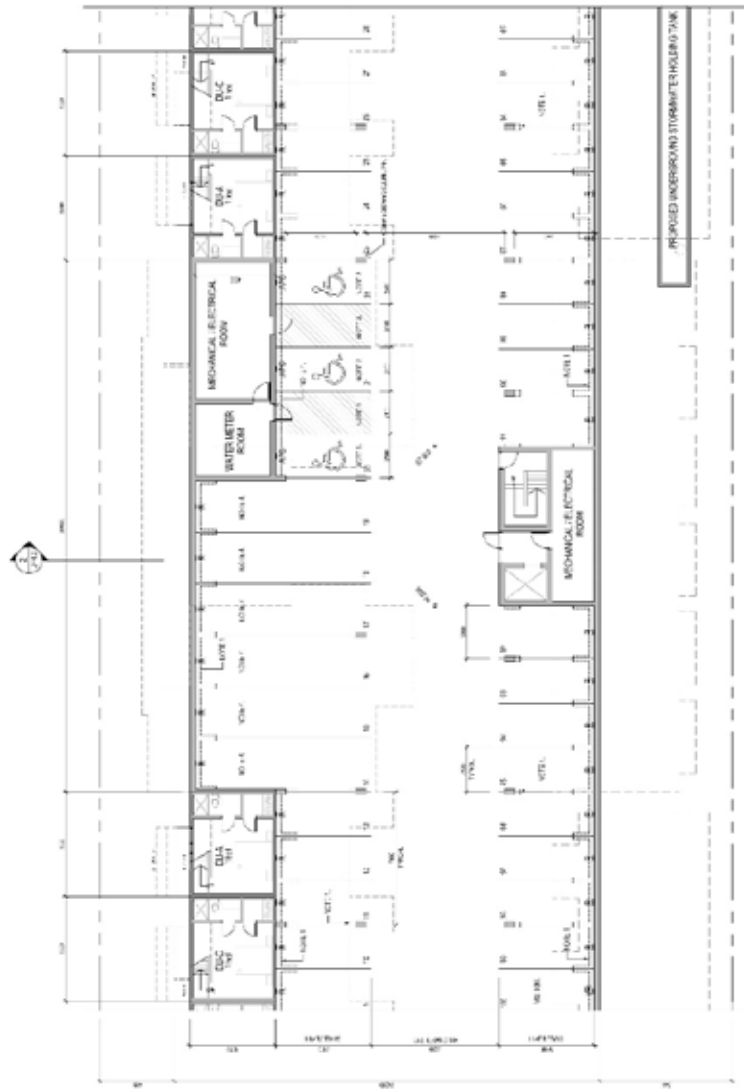
PROPOSED DEVELOPMENT

66 Avenue Development
1000 66 Avenue SW, Calgary, Alberta T2M 4K6
604.271.1111
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PROPOSED DEVELOPMENT

66 Avenue Development
1000 66 Avenue SW, Calgary, Alberta T2M 4K6
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| | |
|---------|-------------|
| DATE | 2016 |
| REV | 1 |
| D.K. | 22 APR 2016 |
| R.C. | 22 APR 2016 |
| U.S. | 22 APR 2016 |
| T.1000 | |
| SCALE | 1:1000 |
| PROJECT | DP 2.5 |



POPULAR FLOOR PLAN
2016-11-10

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

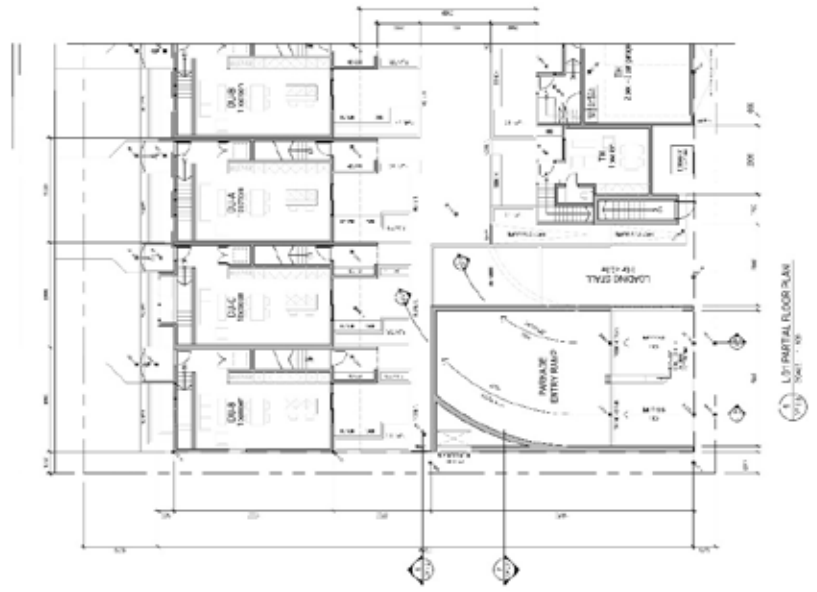
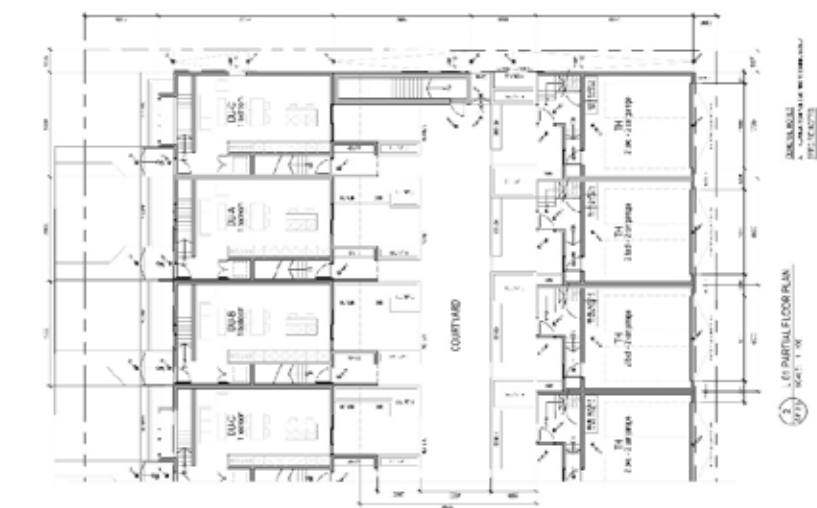
 

69 Avenue Development
1000 69 Avenue SW, Calgary, Alberta T2M 4K6
604.271.1100
www.69avenue.com

LOT 1 PARTIAL FLOOR PLAN

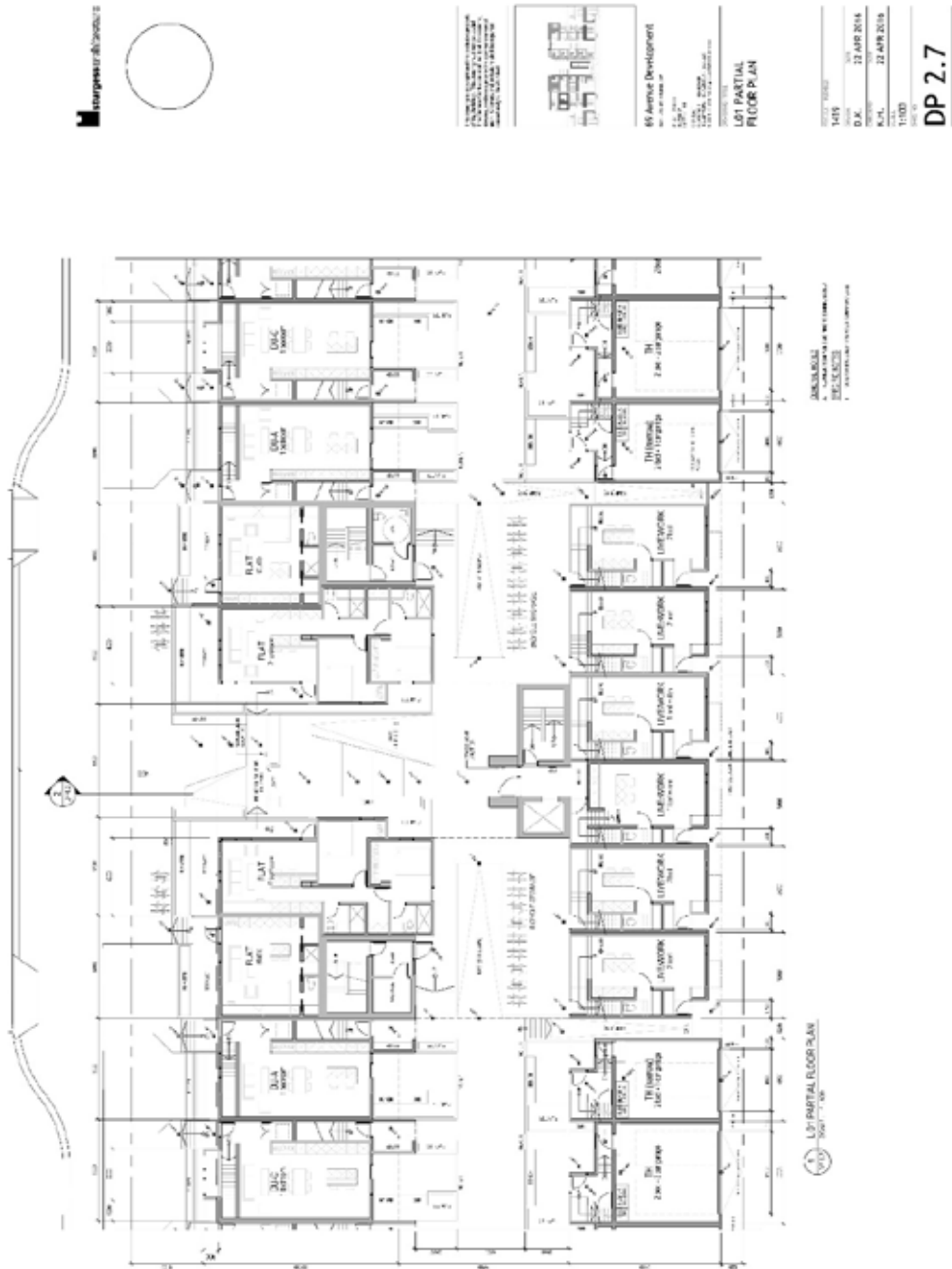
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| R.A. | 22 APR 2016 |
| U.S.D. | |
| U.S.D. | |

DP 2.6



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



69 Avenue Development
100 PARTIAL FLOOR PLAN
1:1000

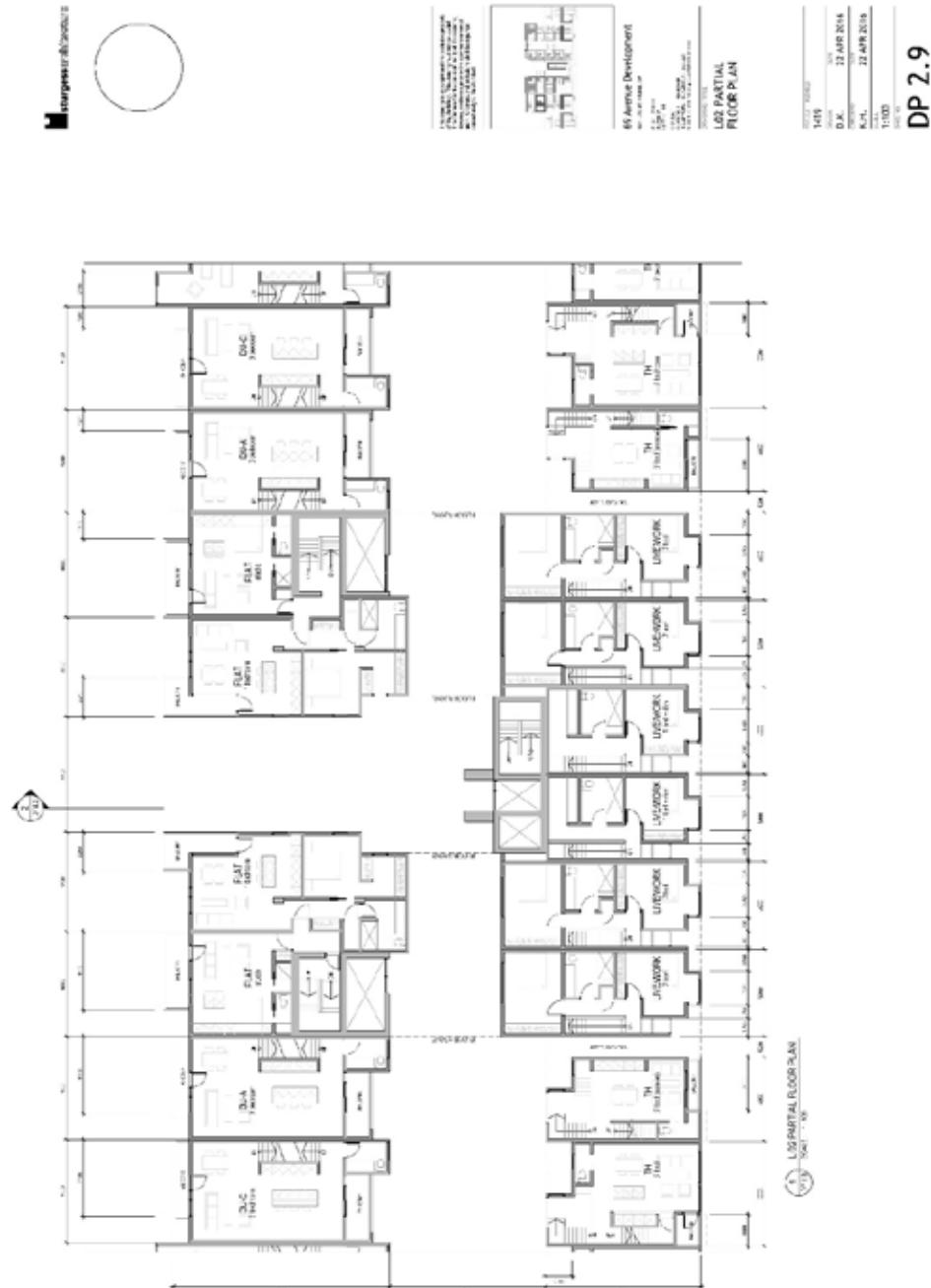
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DP 2.8



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DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



69 Avenue Development
 100' x 100' x 100'

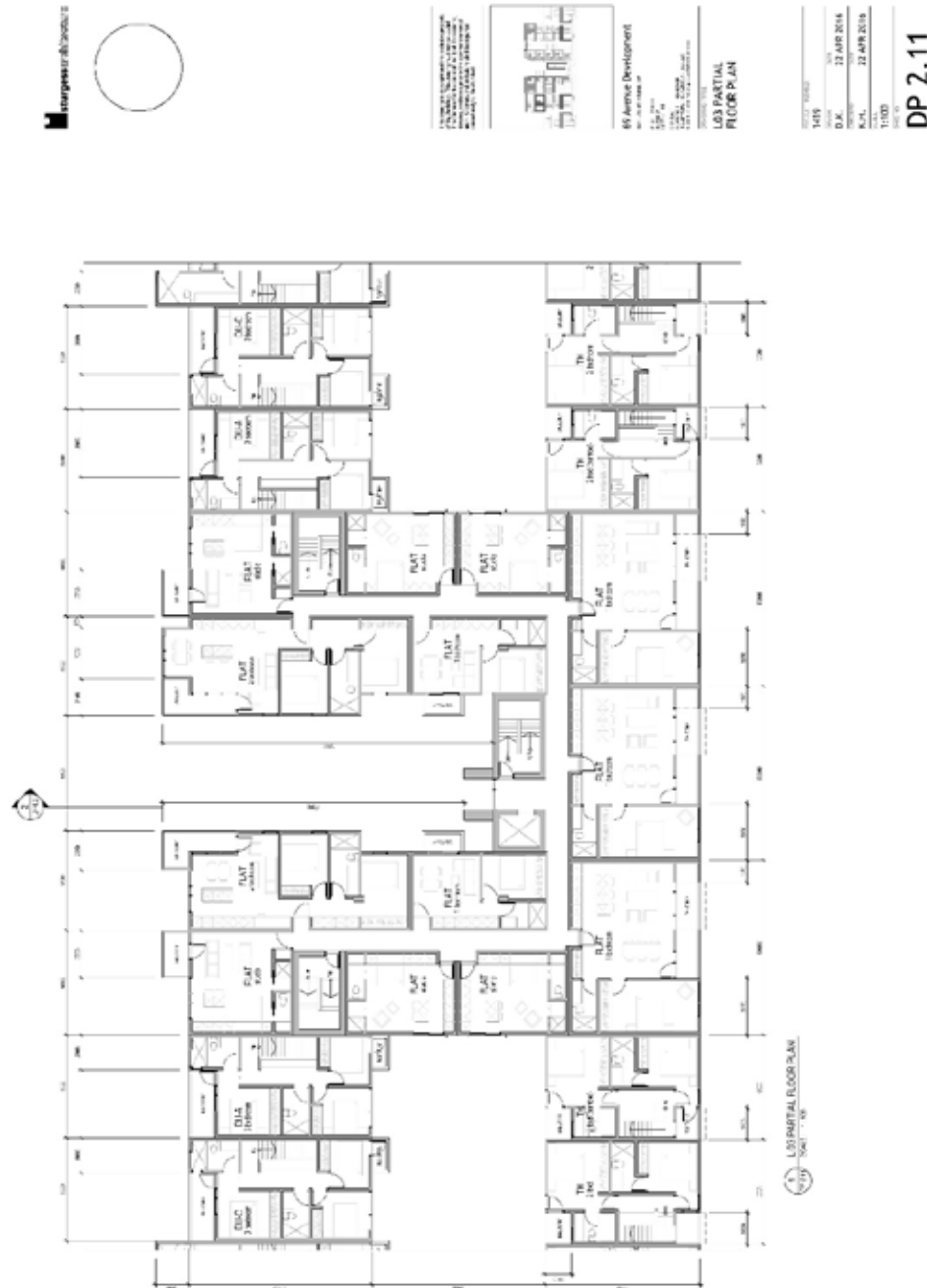
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| DATE | 12 APR 2018 |
| DATE | 22 APR 2018 |
| DATE | 22 APR 2018 |
| DATE | 11/00 |

DP 2.10



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

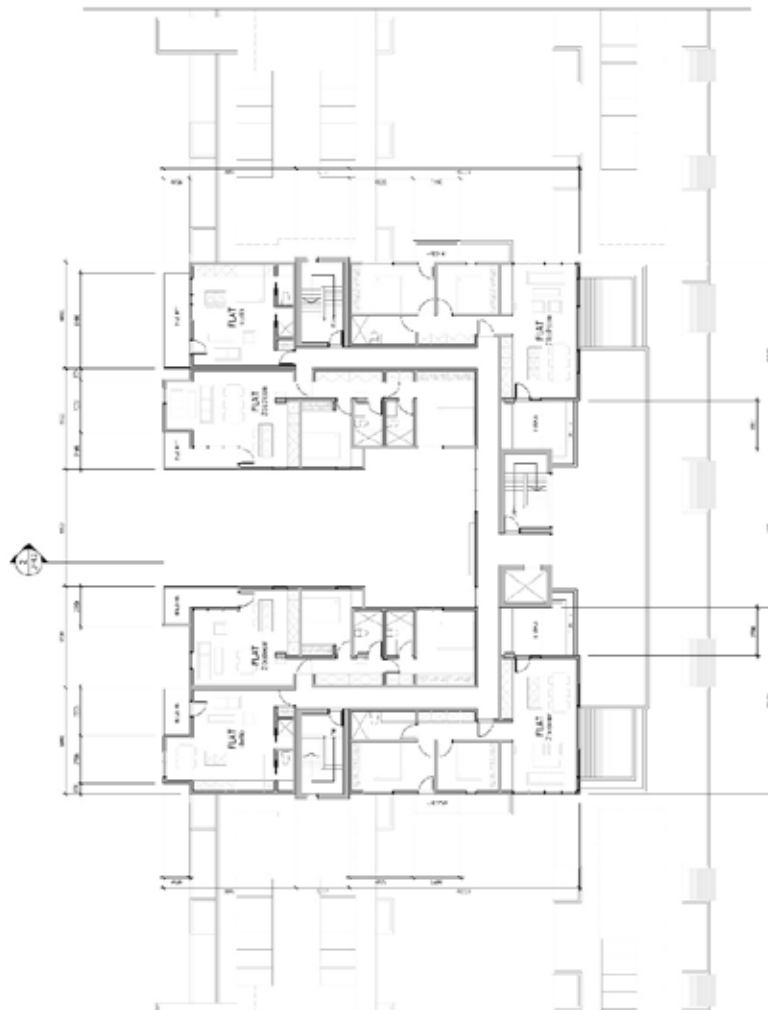


66 Avenue Development
1700 Macleod Trail S, Suite 100
Calgary, Alberta T2G 2M9
Tel: (403) 243-1111
Fax: (403) 243-1112



66 Avenue Development
1700 Macleod Trail S, Suite 100
Calgary, Alberta T2G 2M9
Tel: (403) 243-1111
Fax: (403) 243-1112

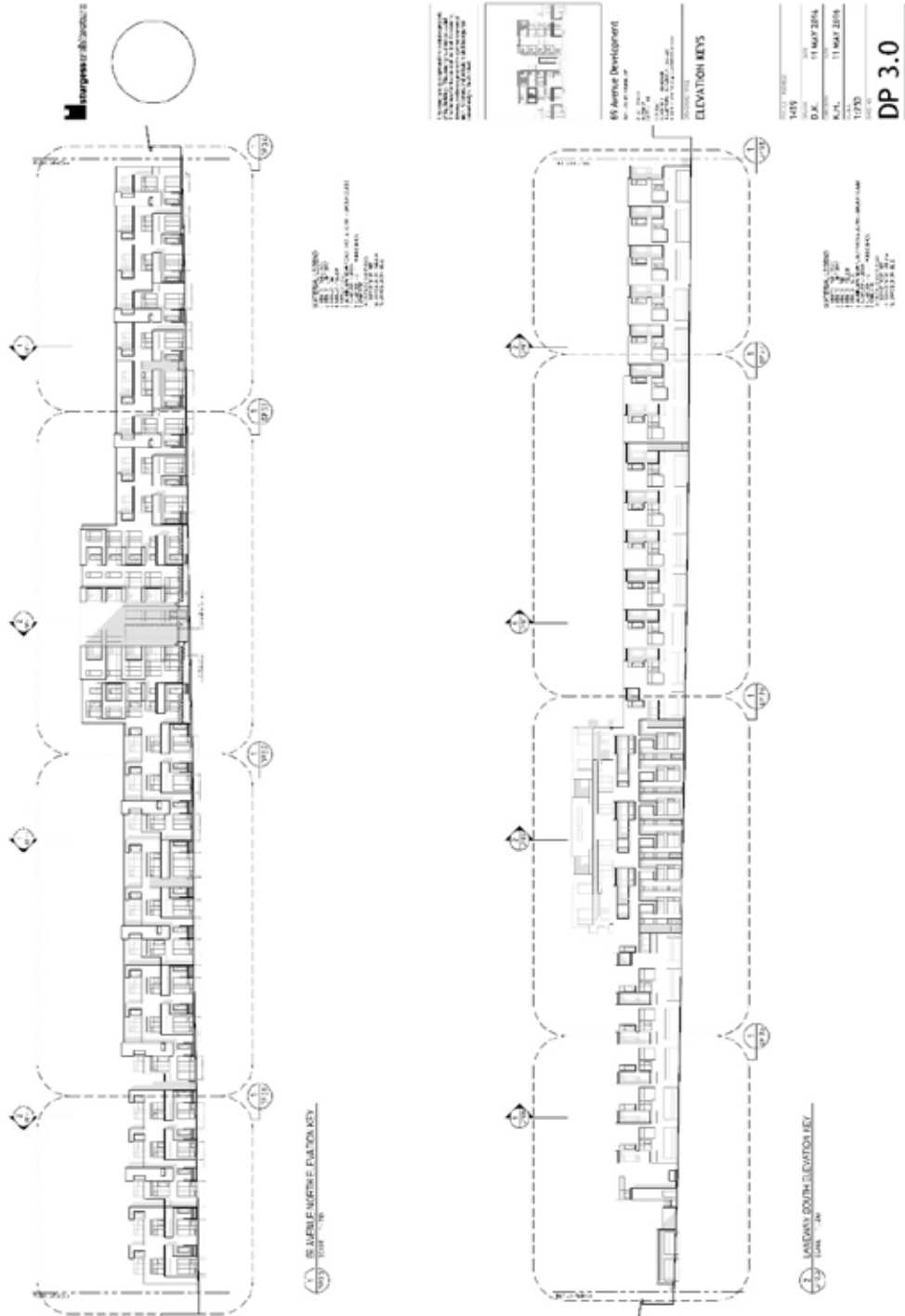
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| SCALE | 1:1000 |
| PROJECT NO. | DP 2.13 |



LIVE PARTIAL FLOOR PLAN
1/10

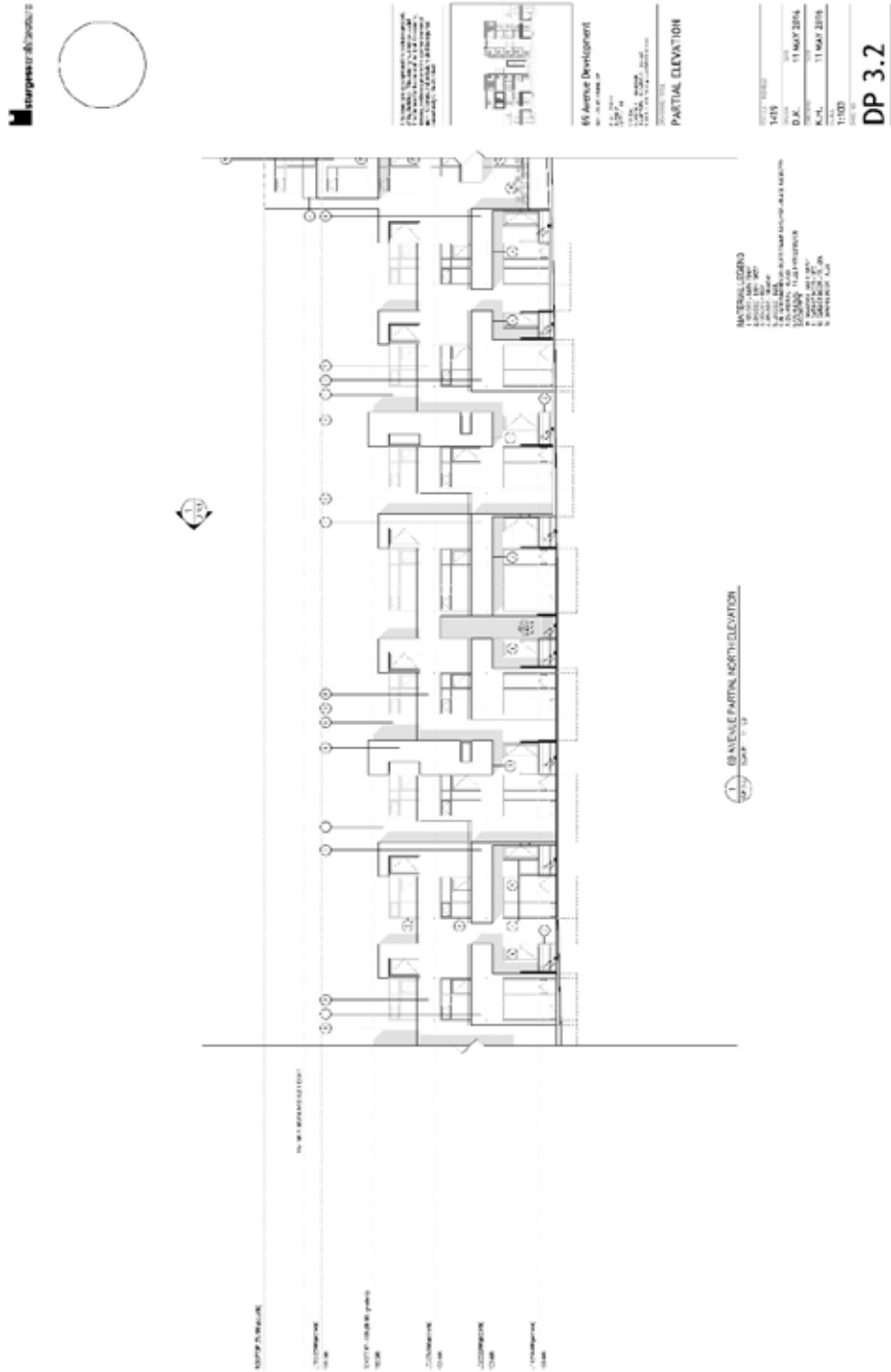
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DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



66 Avenue Development
1111 66 Avenue SW, Calgary, Alberta T2C 0A8
66 Avenue Development
1111 66 Avenue SW, Calgary, Alberta T2C 0A8
PARTIAL ELEVATION

1:1000
11 MAY 2016
11 MAY 2016
DP 3.3



MATERIAL LEGEND
1. BRICK
2. CONCRETE
3. GLASS
4. METAL
5. STONE
6. WOOD

69 AVENUE PARTIAL NORTH ELEVATION
SCALE: 1/8" = 1'-0"

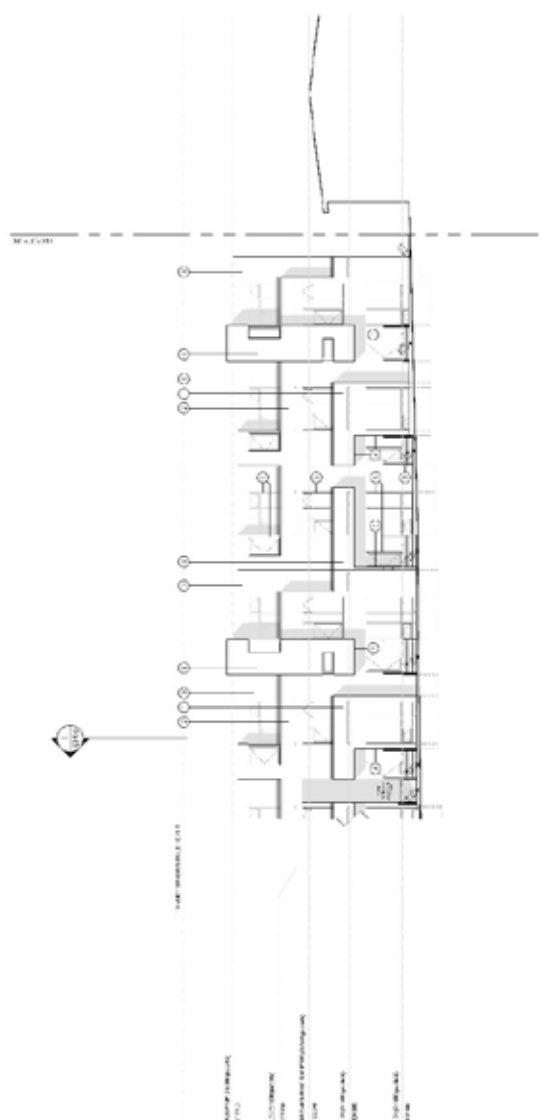
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 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



66 Avenue Development
 66 Avenue SW, Calgary, Alberta
 2,500 sq. ft.
 1,200 sq. ft. live/work unit
 1,300 sq. ft. commercial space
 1,000 sq. ft. parking
 1,000 sq. ft. landscaping
 1,000 sq. ft. site work

PROJECT NUMBER: 1417
 DATE: 11 MAY 2014
 D.R.C.: 11 MAY 2014
 R.C.L.: 11 MAY 2014
 T: 1:000
 DP 3.4

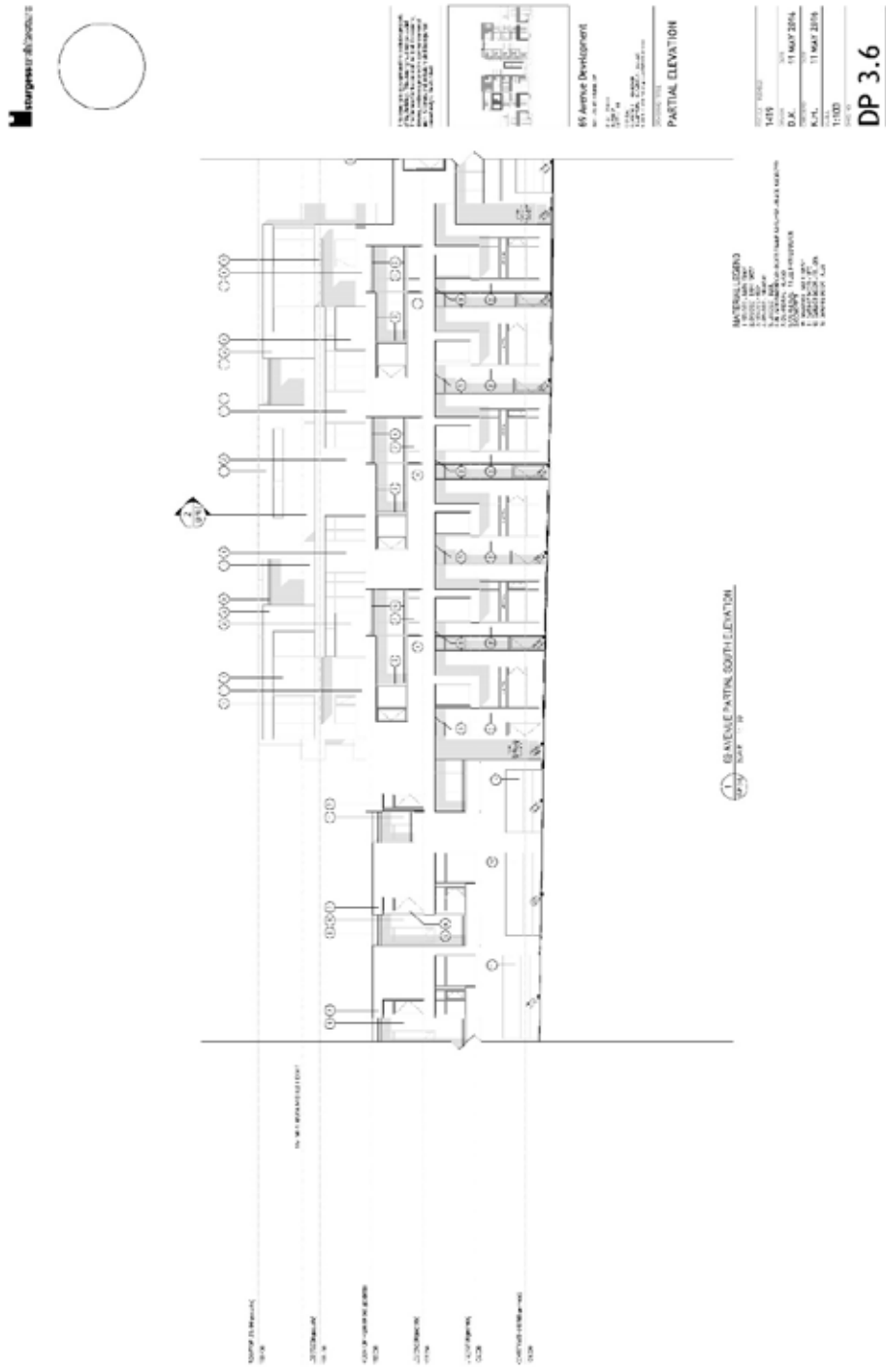


- MATERIAL LEGEND
- 1. BRICK
 - 2. CONCRETE
 - 3. GLASS
 - 4. METAL
 - 5. WOOD
 - 6. ASPHALT
 - 7. GRASS
 - 8. LANDSCAPING

69 AVENUE PARTIAL NORTH ELEVATION
 MAP 28S

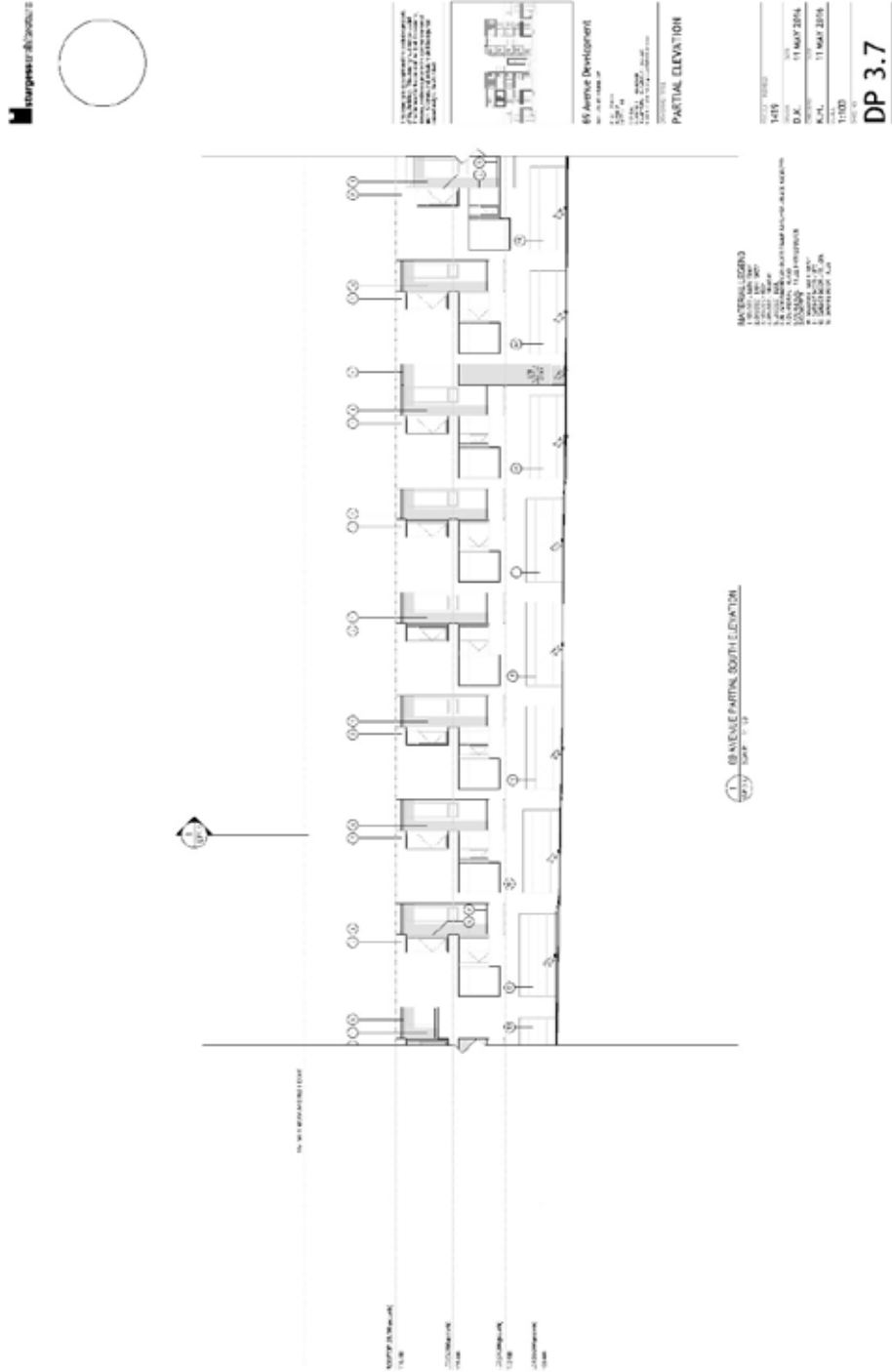
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DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



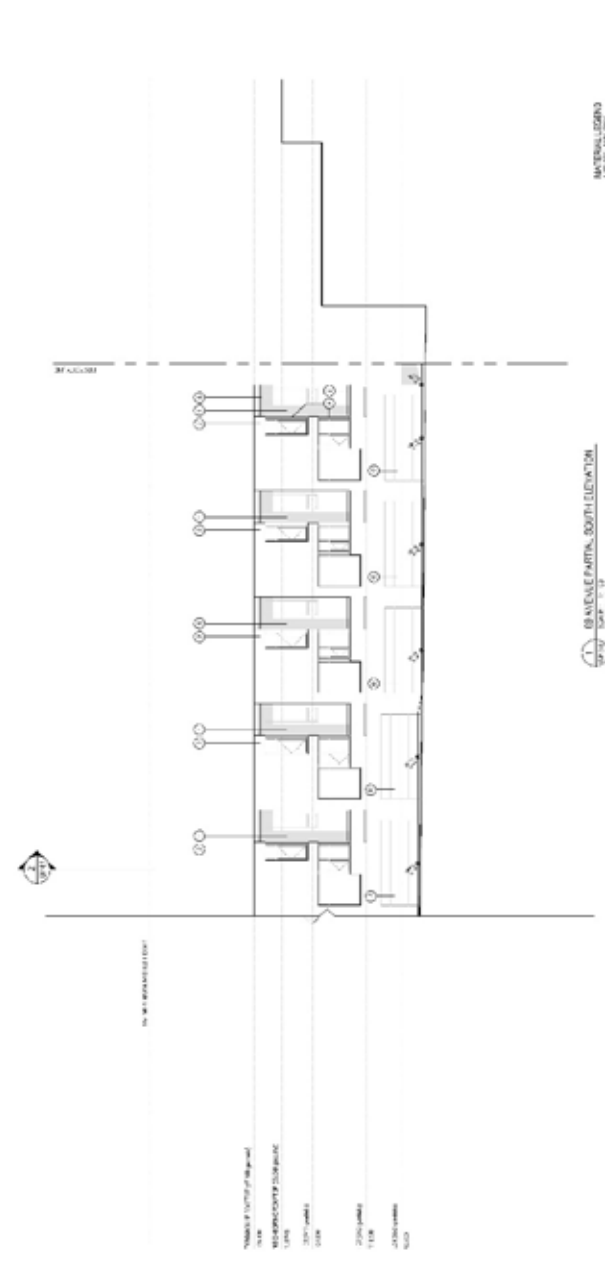
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 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



66 Avenue Development
 66 Avenue SW, Calgary, Alberta
 2.5.0000
 2.5.0000
 2.5.0000
 2.5.0000
 PARTIAL ELEVATION

| | |
|---------------|-------------|
| DATE | 11 MAY 2014 |
| D.R. | 11 MAY 2014 |
| R.D. | 11 MAY 2014 |
| T.O. | 11 MAY 2014 |
| SCALE | 1:1000 |
| DP 3.8 | |

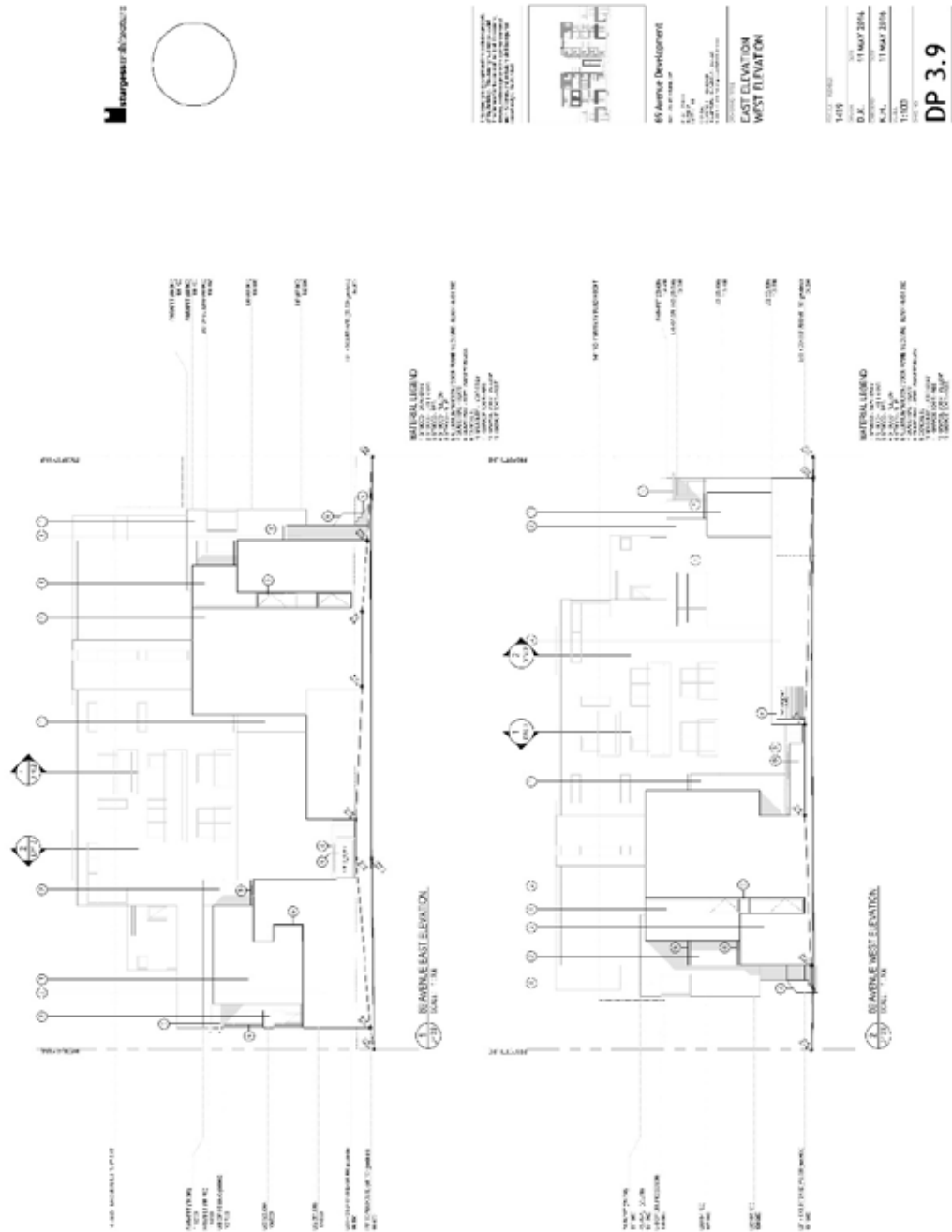


- MATERIAL LEGEND
- 1. BRICK
 - 2. CONCRETE
 - 3. METAL PANEL
 - 4. GLASS
 - 5. STAINLESS STEEL
 - 6. ALUMINUM
 - 7. POLYURETHANE
 - 8. POLYURETHANE
 - 9. POLYURETHANE
 - 10. POLYURETHANE

69 AVENUE PARTIAL, SOUTH ELEVATION

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



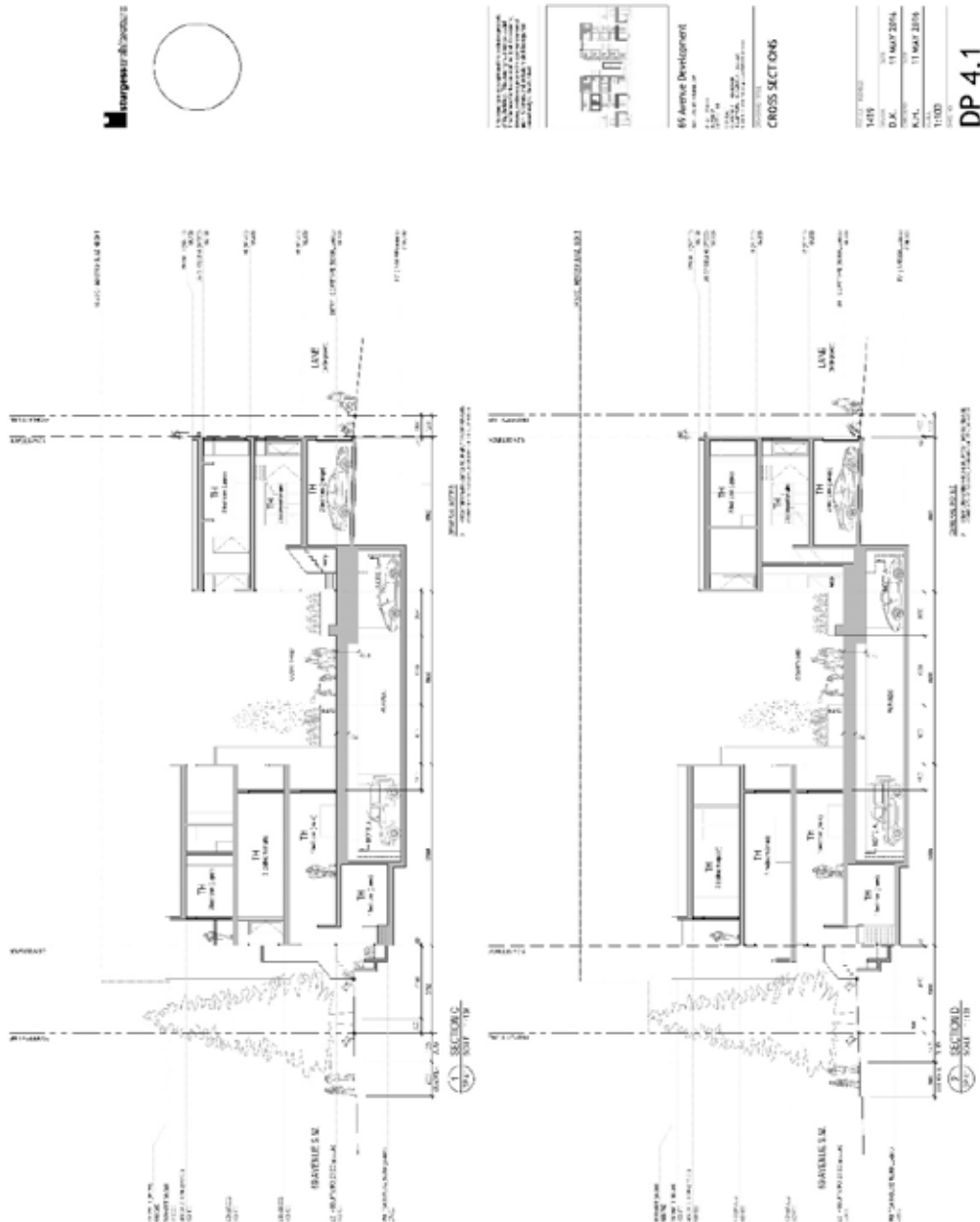
DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



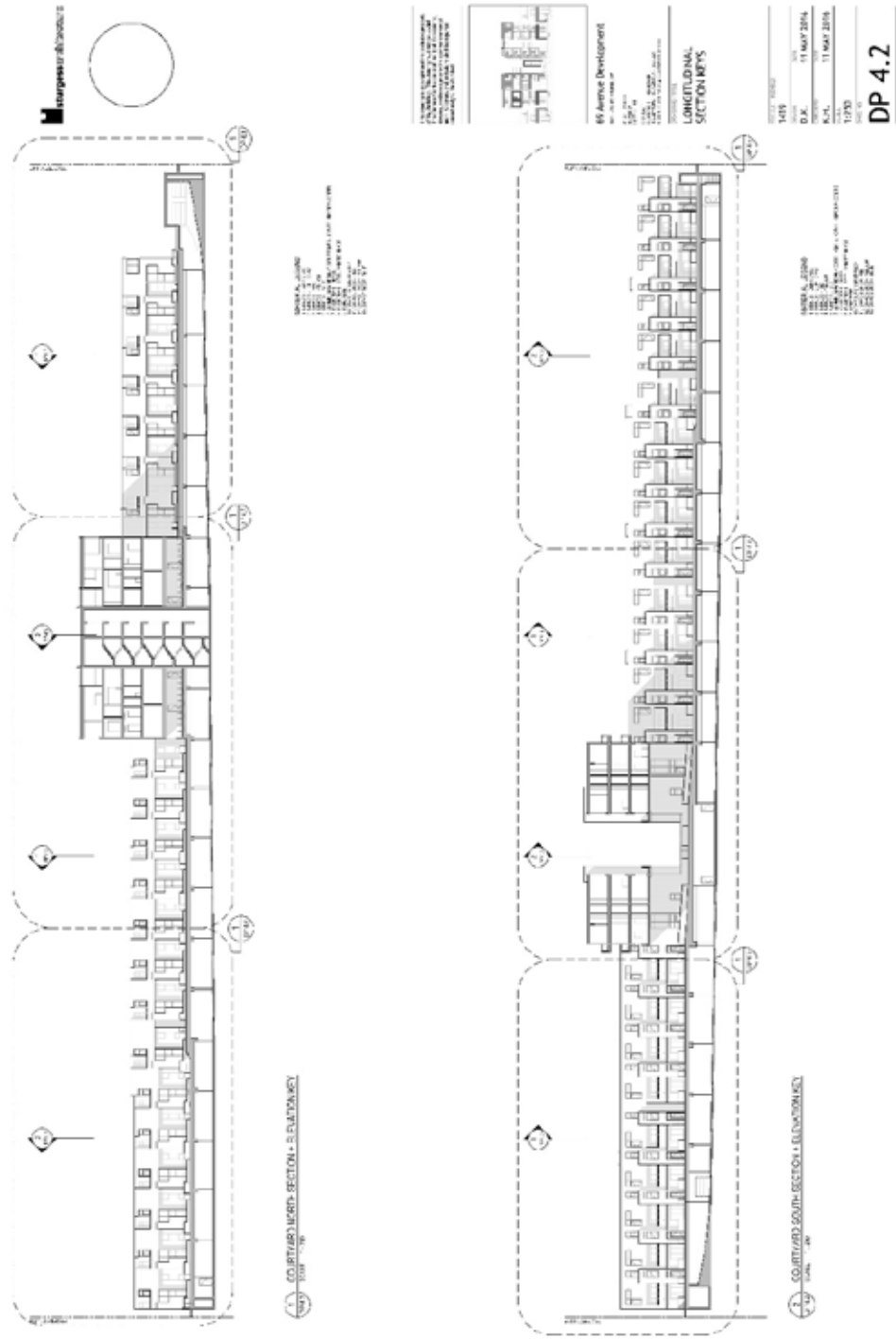
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DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

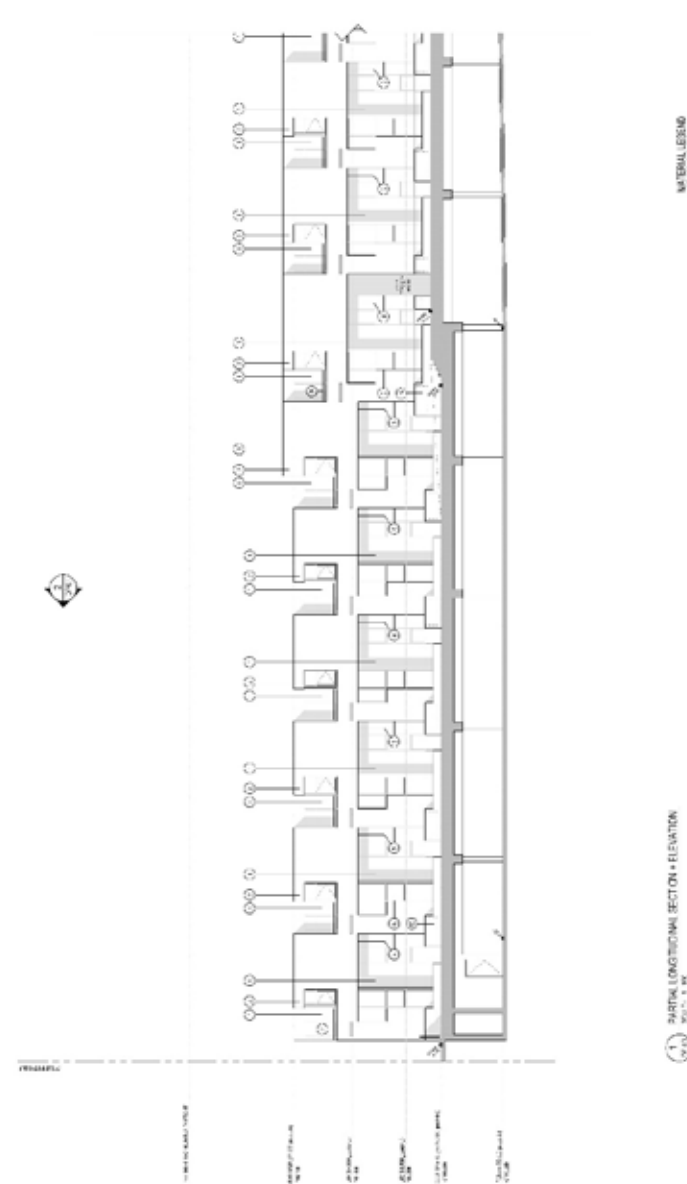


69 Avenue Development
 1000 sq. ft.
 1000 sq. ft.
 1000 sq. ft.
 1000 sq. ft.

PARTIAL LONGITUDINAL SECTION + ELEVATION

| | |
|-------|-------------|
| DATE | 11 MAY 2016 |
| D.R. | 11 MAY 2016 |
| R.D. | 11 MAY 2016 |
| SCALE | 1:1000 |

DP 4.3



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

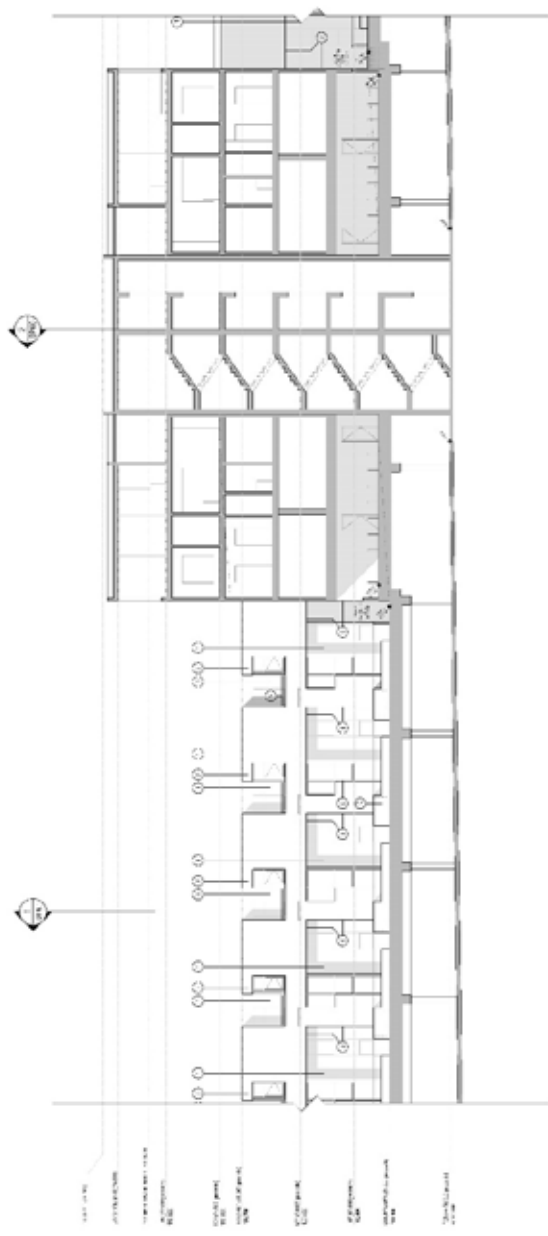


69 Avenue Development
 11000 69 Avenue SW, Calgary, Alberta T2C 0A8
 403.243.3333
 www.69avenue.com

PARTIAL LONGITUDINAL SECTION + ELEVATION

| | |
|------------|-------------|
| DATE | 11 MAY 2014 |
| DRAWN BY | 11 MAY 2014 |
| CHECKED BY | 11 MAY 2014 |
| SCALE | 1:1000 |

DP 4.4



MATERIAL LEGEND

- 1. CONCRETE
- 2. BRICK
- 3. GYPSONUM BOARD
- 4. GYPSONUM BOARD WITH FINISH
- 5. GYPSONUM BOARD WITH FINISH AND PAINT
- 6. GYPSONUM BOARD WITH FINISH AND PAINT AND GLASS
- 7. GYPSONUM BOARD WITH FINISH AND PAINT AND GLASS AND METAL
- 8. GYPSONUM BOARD WITH FINISH AND PAINT AND GLASS AND METAL AND WOOD

PARTIAL LONGITUDINAL SECTION + ELEVATION
 11000 69 AVENUE SW

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

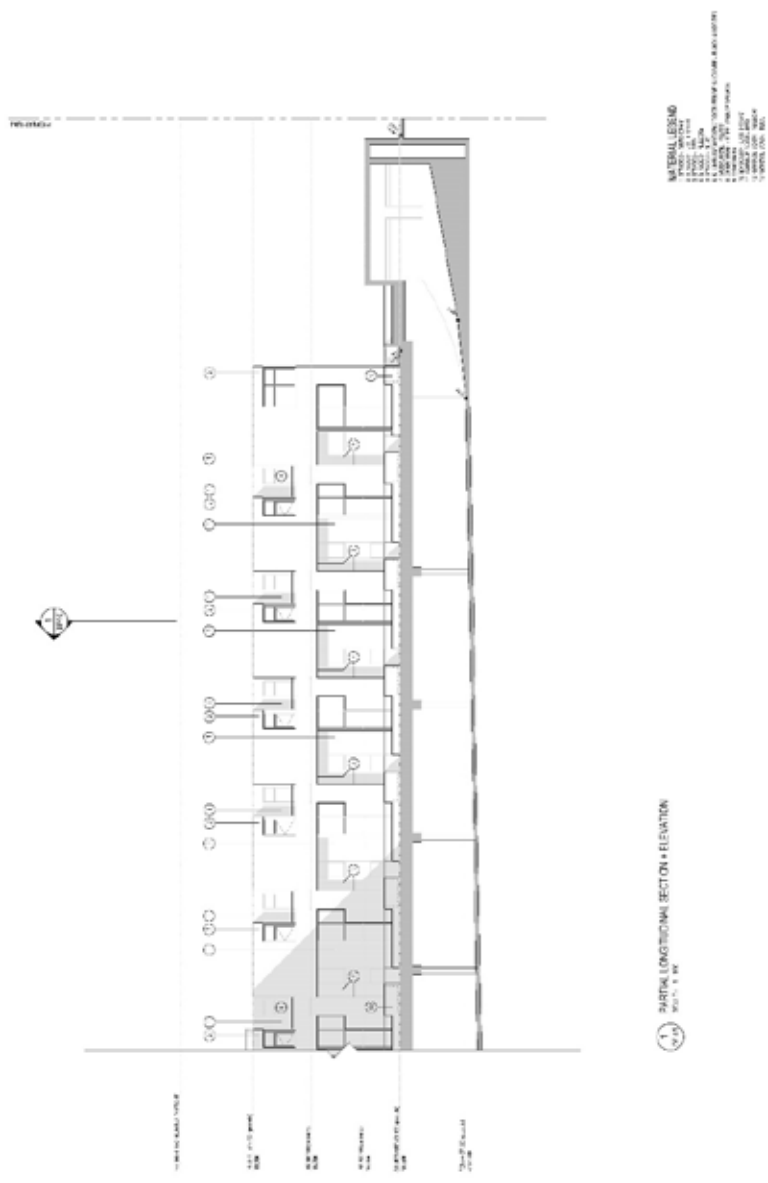


69 Avenue Development
 11000 69 Avenue SW, Calgary, AB T2C 0A8
 403.243.3222
 www.69avenue.com

PARTIAL LONGITUDINAL SECTION + ELEVATION

| | |
|---------|-------------|
| DATE | 11 MAY 2016 |
| DRAWN | 11 MAY 2016 |
| CHECKED | 11 MAY 2016 |
| SCALE | 1:1000 |

DP 4.5



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

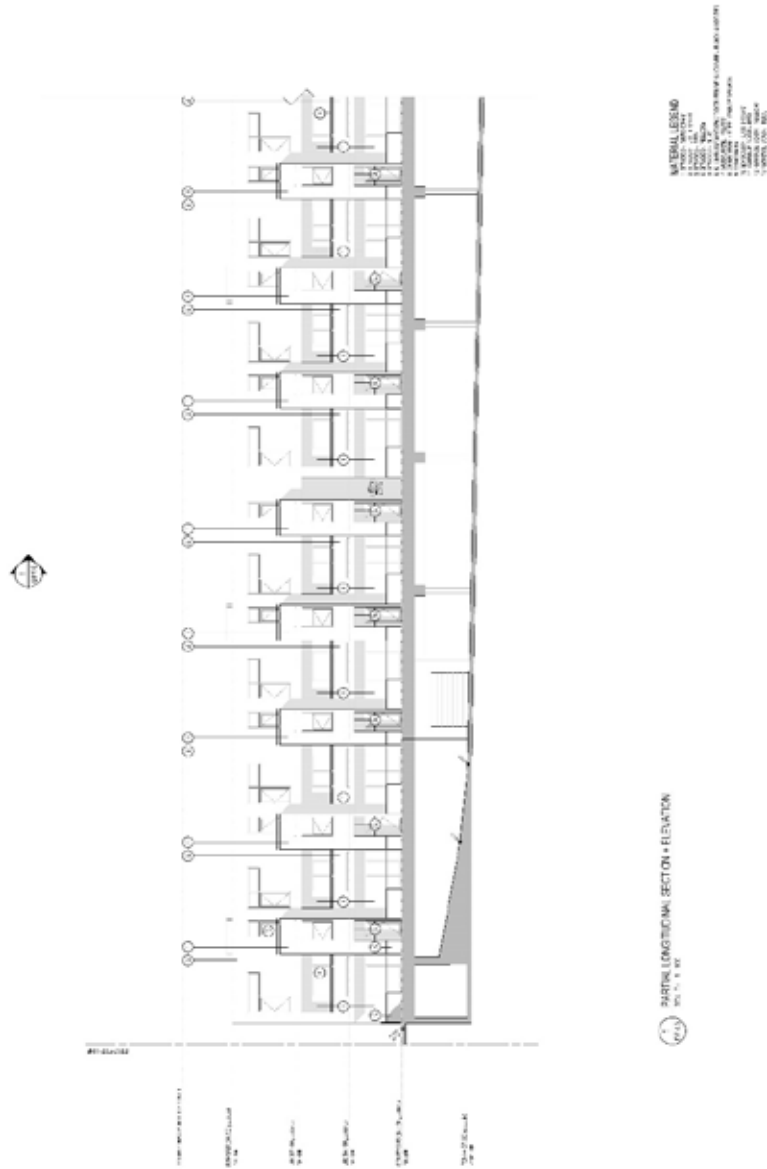
MAP 28S



69 Avenue Development
 1:1000
 11 MAY 2016

PARTIAL LONGITUDINAL SECTION + ELEVATION

DP 4.6



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

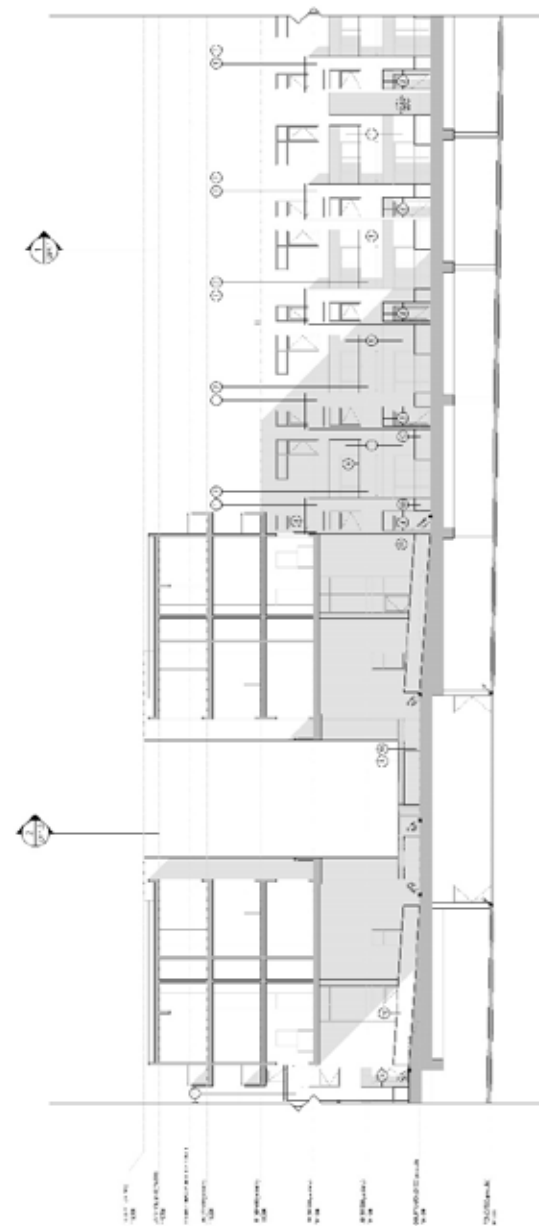
 


66 Avenue Development
1111 11th Ave SW
Calgary, Alberta T2K 0K4
Tel: (403) 243-1111
Fax: (403) 243-1112
www.mcm.ca

PARTIAL LONGITUDINAL SECTION + ELEVATION

| | |
|---------|-------------|
| DATE | 11 MAY 2016 |
| DRAWN | 11 MAY 2016 |
| CHECKED | 11 MAY 2016 |
| SCALE | 1:1000 |

DP 4.7



MATERIAL LEGEND

- 1. CONCRETE
- 2. BRICK
- 3. GLASS
- 4. METAL PANELS
- 5. WOOD PANELS
- 6. INSULATION
- 7. ROOFING
- 8. FLOORING
- 9. FINISHES
- 10. MECHANICAL
- 11. ELECTRICAL
- 12. PLUMBING

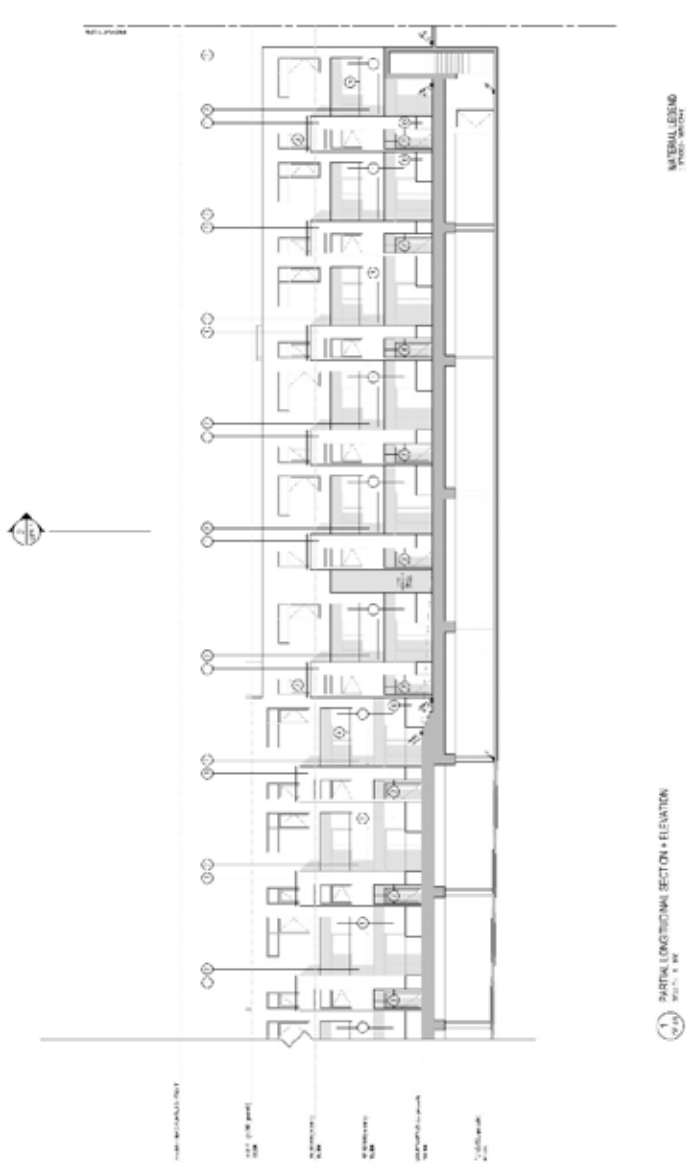
PARTIAL LONGITUDINAL SECTION + ELEVATION
SCALE: 1:1000

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



69 Avenue Development
 69 Avenue SW, Calgary, AB T2C 1S7
 5,500 sq. ft.
 2016-2017
 11 MAY 2016
 1:1000
 DP 4.8



MATERIAL LEGEND
 1. BRICK
 2. STUCCO
 3. CONCRETE
 4. GLASS
 5. METAL
 6. WOOD
 7. ASPHALT
 8. PAINT
 9. TERRAZZO
 10. GRANITE

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



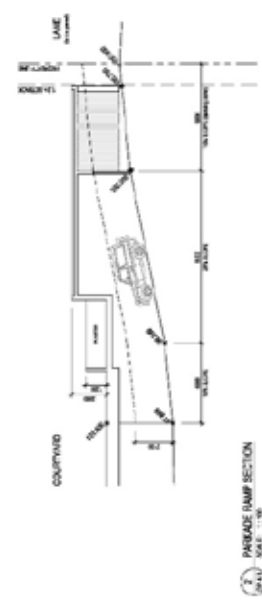
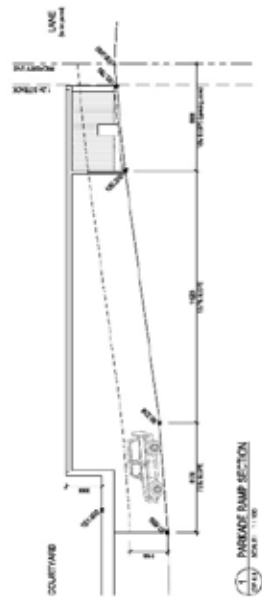
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69 Avenue Development
 69 Avenue SW
 Calgary, Alberta T2C 1S8
 PROJECT NO: 2016-2458
 DRAWING NO: 2016-2458-01
 DATE: 11 MAY 2016

ISSUED FOR:
 PARADE RAMP
 SECTIONS

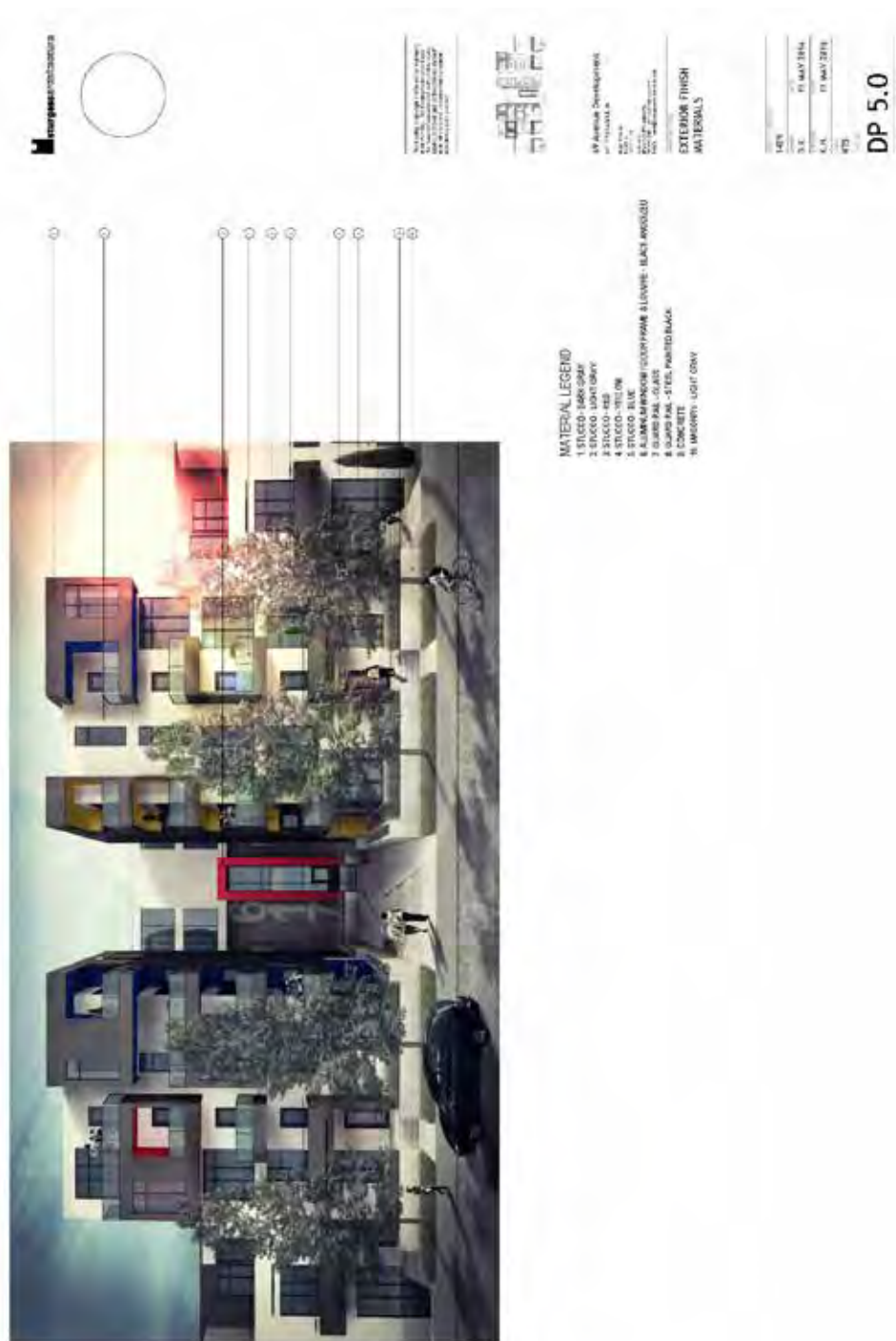
| | |
|------|-------------|
| DATE | 11 MAY 2016 |
| BY | E.K. |
| DATE | 11 MAY 2016 |
| BY | K.H. |
| DATE | 11 MAY 2016 |
| BY | K.H. |
| DATE | 11 MAY 2016 |
| BY | K.H. |

DP 4.9



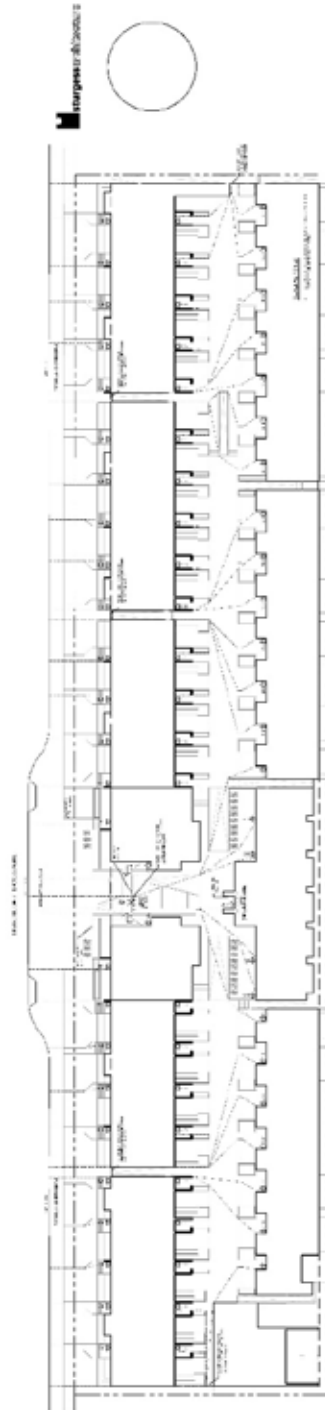
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 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



FIRE ACCESS PLAN
DATE: 1/25/18

69 Avenue Development
1000 69 Avenue SW, Calgary, Alberta T2C 1S7
604-241-1111
www.69avenue.com

69 Avenue Development
1000 69 Avenue SW, Calgary, Alberta T2C 1S7
604-241-1111
www.69avenue.com

FIRE ACCESS PLAN

| | |
|------|-------------|
| DATE | 1/25/18 |
| REV | 001 |
| D.K. | 22 APR 2016 |
| R.C. | 22 APR 2016 |
| T.P. | 1/25/18 |

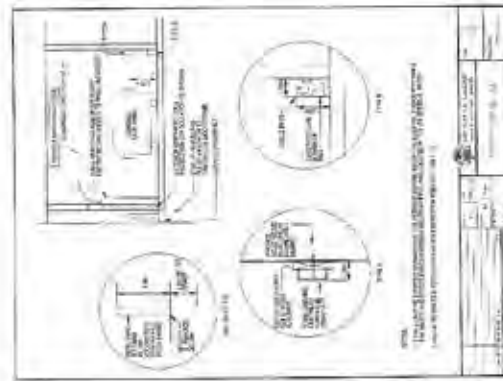
DP 6.0

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

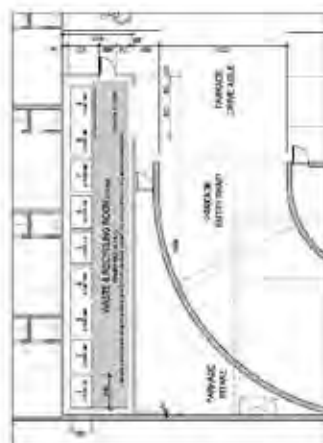
MAP 28S



| | |
|---------------|-------------|
| DATE | 11 MAY 2018 |
| BY | 11 MAY 2018 |
| REVISION | 11 MAY 2018 |
| NO. OF ISSUES | |
| DP 6.1 | |



MUNICIPAL WASTE & RECYCLING
MAP 28S



WASTE ENCLOSURE ROOM
FURNITURE
UTILITY ROOM
WASTE ENCLOSURE ROOM
FURNITURE
UTILITY ROOM

MUNICIPAL WASTE & RECYCLING
MAP 28S

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

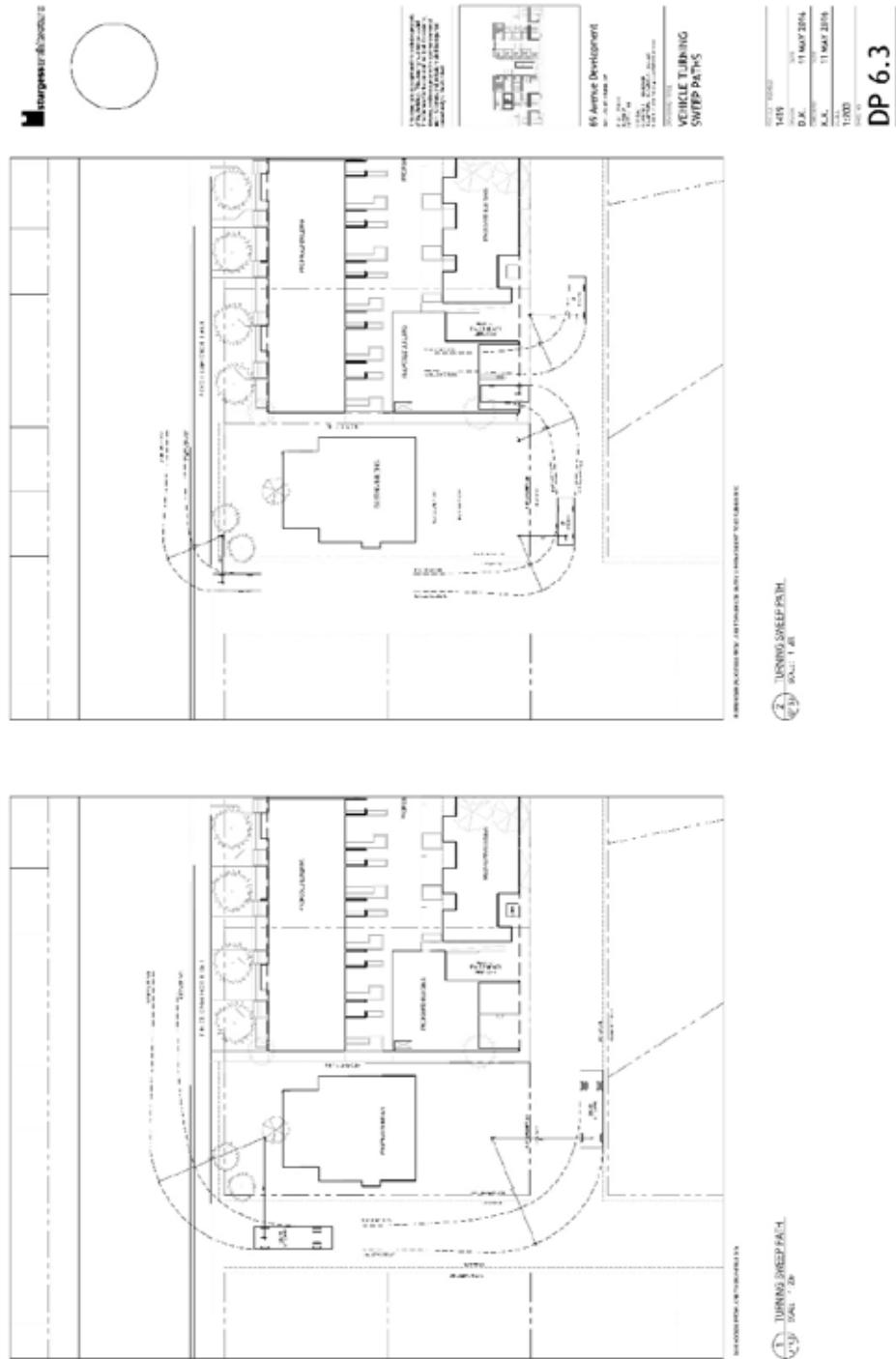
The image contains several technical drawings and specifications for a development permit. At the top left is the logo for 'Municipal Development Department'. Below it is a circular site plan. To the right of the site plan is a drawing of a building facade with a sign, labeled '48 Avenue Development'. Further right is a table with the following information:

| | |
|----------|-------------|
| DATE | 11 MAY 2016 |
| D.F.C. | 11 MAY 2016 |
| D.C. | 11 MAY 2016 |
| AS NOTED | |

Below the table is the text 'DP 6.2'. To the right of the site plan is a drawing of a parking storage detail, labeled 'PARKING STORAGE DETAIL'. Below that is a drawing of a bicycle storage detail, labeled 'BICYCLE STORAGE AND LIGHTING SPEC'. To the right of the site plan is a drawing of a utility traverse, labeled 'UTILITY TRVERSE'. Below that is a drawing of a flat base storage spec, labeled 'FLAT BASE STORAGE SPEC'. To the right of the site plan is a drawing of a hanging bike storage spec, labeled 'HANGING BIKE STORAGE SPEC'. At the bottom left is a drawing of a hanging bike storage spec, labeled 'HANGING BIKE STORAGE SPEC'.

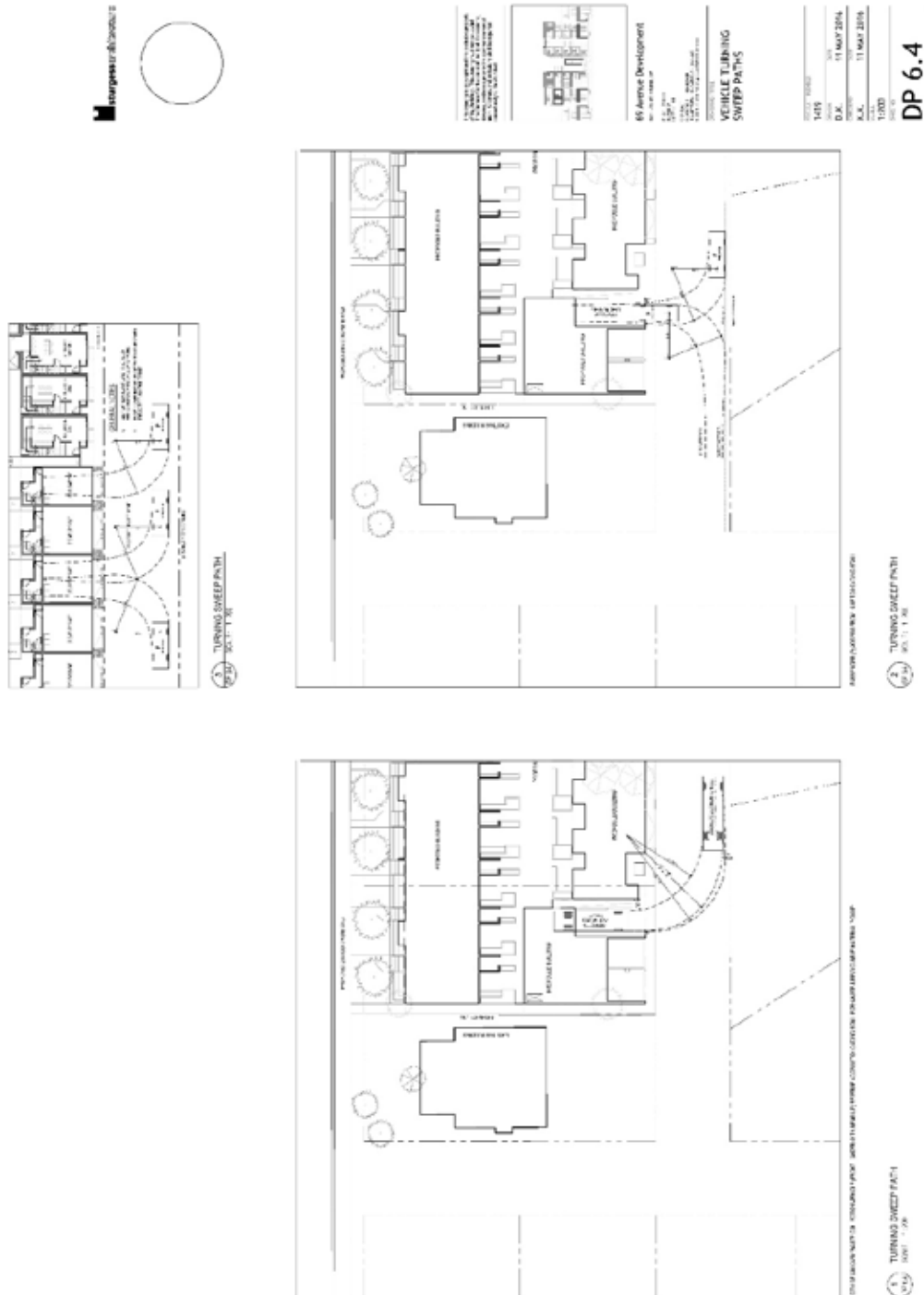
DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

integrated architecture



3000-04 - REVISED FOR DP
2017-01-10 - 10:00 AM
1. 69 AVENUE SW WEST OF MACLEOD TRAIL S
2. 69 AVENUE SW WEST OF MACLEOD TRAIL S
3. 69 AVENUE SW WEST OF MACLEOD TRAIL S
4. 69 AVENUE SW WEST OF MACLEOD TRAIL S
5. 69 AVENUE SW WEST OF MACLEOD TRAIL S
6. 69 AVENUE SW WEST OF MACLEOD TRAIL S
7. 69 AVENUE SW WEST OF MACLEOD TRAIL S
8. 69 AVENUE SW WEST OF MACLEOD TRAIL S
9. 69 AVENUE SW WEST OF MACLEOD TRAIL S
10. 69 AVENUE SW WEST OF MACLEOD TRAIL S

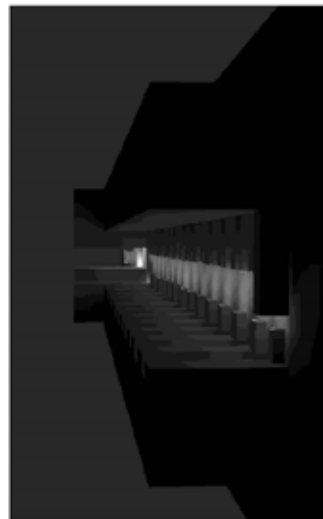
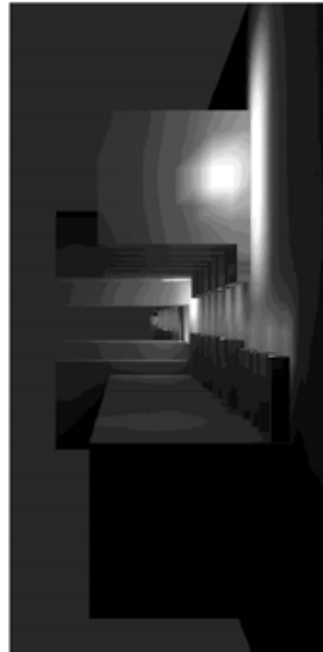


3000-04 - REVISED FOR DP
2017-01-10 - 10:00 AM
1. 69 AVENUE SW WEST OF MACLEOD TRAIL S
2. 69 AVENUE SW WEST OF MACLEOD TRAIL S
3. 69 AVENUE SW WEST OF MACLEOD TRAIL S
4. 69 AVENUE SW WEST OF MACLEOD TRAIL S
5. 69 AVENUE SW WEST OF MACLEOD TRAIL S
6. 69 AVENUE SW WEST OF MACLEOD TRAIL S
7. 69 AVENUE SW WEST OF MACLEOD TRAIL S
8. 69 AVENUE SW WEST OF MACLEOD TRAIL S
9. 69 AVENUE SW WEST OF MACLEOD TRAIL S
10. 69 AVENUE SW WEST OF MACLEOD TRAIL S

PHOTOGRAPHIC
IMAGES

PROJECT NAME
PROJECT NUMBER
CLIENT
DATE
DRAWN BY
CHECKED BY
DATE
SCALE
PROJECT NO.

DPES 2.0



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

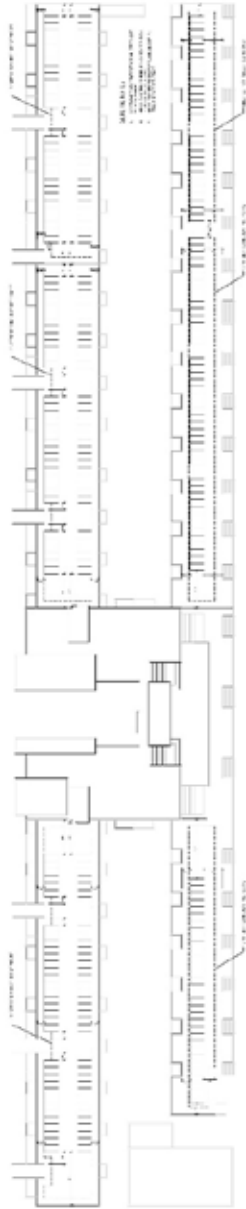


66 Avenue Development
1111 66 Avenue SW, Calgary, Alberta T2C 0A8
604-271-1111
www.66avenue.com

PROPOSED SOLAR
PANEL LOCATIONS

| | |
|---------|-------------|
| DATE | 2016 |
| REV | 001 |
| DRAWN | 22 APR 2016 |
| CHECKED | 22 APR 2016 |
| DATE | 2016 |
| REV | 001 |
| DATE | 2016 |
| REV | 001 |

DPE 1.0



DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

APPENDIX II

CONDITIONS OF APPROVAL

Prior to Release Requirements

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

Planning:

1. Submit a total of four (4) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. One (1) of the plan set(s) shall highlight all of the amendments.
- b. Three (3) detailed written response(s) to the Conditions of Approval document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. Amend plans to address conflicts and provide a clearance letter(s) from ENMAX Corporation indicating that:
 - Conflicts, identified in the ENMAX Corporation letter dated September 12, 2016, have been satisfied (power poles and power line re-location, sufficient space for pad-mounted transformer). Letter received on October 27, 2017 indicates that a work order was created to resolve the problem, and that the conflict is resolvable.
 - Conflicts, identified in the ENMAX Corporation letter dated December 04, 2017, have been satisfied (trees conflict with the existing overhead power lines, and location of new transformer).
3. Submit detailed lighting information on shielding, mounting details and height for the proposed development pursuant to Part 3, Division 4 of Land Use Bylaw 1P2007. Ensure that lighting does not extend beyond the boundaries of the site to any adjacent residential areas or major roadways. Ensure lighting is provided by all building entrances.

**DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S

Development Engineering:

4. Consolidate the subject parcels. Submit a copy of the registered plan and certificate of title, confirming the consolidation of subject parcels onto a single titled parcel, to the Development Engineering Generalist.
5. The existing 150mm watermain along 69 Avenue SW is required to be upgraded, at the Developer's expense, to 200mm and connected to the existing 200mm from both west and east sides per the following sketch.



Water connection is available from 69 Avenue SW once the main has been upgraded to a 200mm.

The requirement to construct public infrastructure, watermains, and service connections within City rights-of-way requires the developer to execute an Indemnification Agreement to the satisfaction of the Manager of Growth Management.

To initiate circulation and approval of the Work that will form part of the Indemnification Agreement, submit construction drawings online using your existing VISTA account at calgary.ca/vista. At the time of submission of the construction drawings, the following items shall also be submitted:

**DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S

- a. An 8 1/2" x 11" site plan indicating the construction boundaries.
 - b. Indicate who will be party to the agreement, provide contact information and a certificate of title for adjacent lands associated with the construction of the infrastructure.
 - c. A detailed description of the Work that will form part of the Indemnification Agreement (cubic metres of asphalt and/or concrete, pipe diameters for sanitary, storm and watermains and their respective lengths in linear metres) within the City right of way.
 - d. A detailed cost estimate for the scope of Work including GST prepared by the contractor.
6. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact developmentservicing2@calgary.ca for additional details.

For further information, refer to the following:

Design Guidelines for Development Site Servicing Plans

http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf

Development Site Servicing Plans CARL (requirement list)

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

7. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrolgist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

**DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

8. After approval of the development permit but prior to release of the development permit, the property owner shall execute an Off-Site Levy Agreement. This is required for the payment of off-site levies pursuant to Bylaw 2M2016. The off-site levy is based on a 2016 development approval date.

Should payment be made prior to release of the development permit, an Off-Site Levy Agreement won't be required.

For further information, contact Subdivision Development Coordinator, Calgary Approvals Coordination at 403-268-6739 or email offsitelevy@calgary.ca.

Transportation:

9. Amend the plan to provide the following basic geometry detail to the curb extension design: radii, tangent etc. For collector streets, a minimum pavement width of 7.0m between curb extensions (LG to LG) should be maintained (6.73m and 6.8m proposed). Due to the reduced street width, the corner radius on a curb extension may need to be larger and must be checked with vehicle turning templates.
10. Amend the plan to indicate bollards or other suitable protection device for the wood power poles in the lane adjacent to all garage entrances/aprons. Detail to include sweep path (TYP) to show impact to garage access. Type of barrier and extent of protection to be determined as part of the indemnification process for the required road construction and lane paving.
11. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Construction of new 2.0 m sidewalks adjacent to 69 Avenue SW,
- b. Construction of 6 new wheelchair ramps,
- c. Construction of new asphalt lane paving from 69 Avenue SW along the existing lane adjacent to the property and to Kingsmere Crescent,
- d. Rehabilitation of existing sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel, and

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- e. Construction of traffic calming (curb extension) at the intersection of 69 Avenue SW and 5 Street SW.
 - f. Relocation of power poles in the lane
 - g. Relocation of existing street light
12. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to 69 Avenue SW.
13. Provide a letter acknowledging the revised signage on 69 Avenue SW and back lane is required due to the development. The developer shall be responsible for these costs at their sole expense. Contact the Traffic Engineer (403-268-3520) a minimum 8 weeks prior to occupancy to arrange for the necessary signage. Traffic will invoice for the work at the time of installation.

Parks:

No comments.

Permanent Conditions

The following permanent conditions shall apply:

Planning:

- 14. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
- 15. No changes to the approved plans shall take place unless authorized by the Development Authority.
- 16. A Development Completion Permit shall be issued **before development is occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.

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17. A lighting system to meet a minimum of 54 LUX for the parkade with a uniformity ratio of 4:1 on pavement shall be provided. A minimum of 22 LUX shall be provided in any common areas.
18. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
19. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.
20. Handicapped parking stalls shall be located as shown on the approved plans released with this permit. Handicap parking stall(s) shall be clearly designated, signed and located close to the entrance of the building with barrier-free accessibility.
21. Fascia signage shall be placed only in the designated sign area as indicated on the approved plans. Any damage to the building face, as a result of the sign installation or removal, shall be repaired to the satisfaction of the Development Authority.
22. Loading and delivery shall take place in the designated loading stall as shown on the approved plans and shall, at no time, impede the safety of pedestrian movements and use of the parking lot.
23. The garbage enclosure shall be kept in a good state of repair at all times and the doors shall be kept closed while the enclosures are not actively in use for delivery or removal of refuse.
24. All areas of soft landscaping shall be provided with an underground sprinkler irrigation system as identified on the approved plans.

Development Engineering:

25. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

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For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: www.calgary.ca/ud (under publications).

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

26. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
 - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
 - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
27. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
28. The development shall be built and operated in functional compliance with the Stormwater Management measures outlined for the parcel.
29. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
30. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.

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31. As the development application has not met the standards in either the current City of Calgary standards – “Development Reviews: Design Standards for the Storage and Collection of Recyclable Materials and Waste” or the current Waste and Recycling Bylaw, the development may not be eligible to receive collection service from The City of Calgary.
32. **To obtain a final estimate**, contact Subdivision Development Coordinator, Calgary Approvals Coordination at 403-268-6739 or email offsitelevy@calgary.ca, **after approval of the Development Permit**. Payment shall be made for the off-site levies pursuant to Bylaw 2M2016, **after approval of the Development Permit but prior to issuance of a Development Completion Permit or any occupancy of the building**.
- Only certified cheques or bank drafts made payable to the City of Calgary are acceptable.
 - Mail or Drop off payment to 3rd Floor – Permit pick up counter, Municipal Building, 800 Macleod Trail SE, Calgary, AB, T2P 2M5. Attention: Subdivision Officer, Subdivision Development Mail Code #8032.
 - Reference the development permit number and provide contact information for the person(s) providing the payment.

Should payment be made prior to release of the development permit, an Off-Site Levy Agreement won't be required.

For further information, contact Subdivision Development Coordinator, Calgary Approvals Coordination at 403-268-6739 or email offsitelevy@calgary.ca.

Transportation:

33. No direct vehicular access is permitted to or from 69 Avenue SW.
34. Construction drawings, for the public street and lane improvements, may be required at the Indemnification Agreement stage.
35. The approved driveway(s) required for this development must be constructed to the ramp grades as shown on the approved Development Permit plans. Negative sloping of the driveway within the City Right-of-way is not acceptable. If actual grades do not match the approved grades, the developer/owner shall be responsible for all costs to remove and reconstruct the entire driveway ramp in accordance with approved grades.
36. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.

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37. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.

Parks:

No comments.

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APPENDIX III

APPLICANT STATEMENT – DESIGN RATIONAL

This project aims to accommodate and support diverse population growth in the neighborhood through the provision of a variety of unit types. The project is also designed to support the long term vision of the City, with well designed, thoughtful development contributing to increased density. The proposed development consists of studio units, 1 bedroom units, 2 bedroom units, and 3 bedroom units providing a housing opportunity for a greater demographic of the population. This varied unit mix and density also allows for more reasonably priced market units, thus expanding the potential housing ownership market within the city.

The proposed development primarily consists of 1-bed and 3-bed dwelling units fronting 69 Avenue and townhouses along the Lane, separated by an internal courtyard amenity space. These dwelling units are setback to achieve a number of goals: to retain the row of existing coniferous trees that have a significant presence along 69th Avenue, to respect the adjacent existing development and neighborhood character through appropriate height and massing, to preserve the residential "rhythm" of the street by having front doors and massing of individual units address the street. In addition to respecting the architectural context, these design elements reflect many of the adjacent community resident's wishes gathered from earlier conversations with our client.

To recognize the intersection of 5th Street and 69th Avenue, we propose a taller building with a central entry courtyard that acts as the main building entry for the development. The dwelling units and townhouses vary in height from two (2) to three (3) storeys, whereas the apartment block is proposed as a 16 metre tall building. The limited footprint of the apartment block, coupled with the height of the existing coniferous trees will have little shadow effect on the neighbors to the north across 69th Avenue.

Further to this project's contextual respect to the adjacent existing and potential development, it meets the fundamental goals of the Municipal Development Plan: planning for long-term sustainability, diverse population, adaptability through unit mix to the needs of residents, good urban design initiatives, opportunity for sustainable transportation connectivity, building on an existing site, thus protecting the natural environment, and the design of this building is based on sustainable design principals.

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APPENDIX IV

COMMUNITY ASSOCIATION LETTER



505 78th Avenue SW
Calgary, Alberta T2V 0T3
Bus: 403.255.1400
Fax: 403.255.1407
Email:
admin@kingslandcommunity.ca

Dec 12, 2017

Lisette Burga Gherzi (Community Planning – South)
Planning, Development and Assessment #8201
The City of Calgary, 5th Floor, Municipal Building, 800 Macleod Trail SE
PO Box 2100, Station M, Calgary, AB Canada T2P 2M5

Re: DP2016-2458 – 617 69 Av SW R-C2 / M-C1 (110 Units)

Dear Ms. Burga Gherzi,

Kingsland Community Association (KCA) reviewed the re-circulated package for the multi-residential development application and supports the project. KCA appreciated the genuine engagement with the applicant, architect and Development Authority as the DP progressed. KCA supported the project at the Open House in June 2017 as a result, and now look forward to a successful project completion.

KCA prefers for the construction to be completed in one phase, and also will support a two phased approach if there are no significant incremental bylaw discrepancies as a result.

KCA kindly requests the file manager to notify the KCA Planning Director of non-conforming aspects of the application when that information is available.

Thank you for the opportunity to provide input on this application.

Best Regards,

Darren MacDonald

Darren MacDonald
Planning Director
Kingsland Community Association

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APPENDIX V

URBAN DESIGN REVIEW PANEL COMMENTS

| | | |
|--------------------------------|--|---------------------------|
| Date: | June 28, 2017 | |
| Time: | 2:30 pm | |
| Panel Members: | Present: | Absent: |
| | Janice Liebe (chair) | Brian Horton (vice-chair) |
| | Robert LeBlond | Bruce Nelligan |
| | Chad Russill | Philip Vandermeay |
| | Yogeshwar Navagrah | |
| | Terry Klassen | |
| Advisor: | David Down, Chief Urban Designer | |
| Application number: | DP2017-2458 | |
| Municipal address: | 617,623,627,631,635,639,643,703,707,711,715 69 Av SW | |
| Community: | Kingsland | |
| Project description: | Multi-residential development; Live Work Units; Sign - Class B | |
| Review: | first | |
| File Manager: | Lisette Burga Ghersi | |
| City Wide Urban Design: | Lothar Wiwjorra | |
| Applicant: | Sturgess Architecture | |
| Architect: | Dalton Kaun | |
| Owner: | James Lee | |
| Ranking: | Endorse with (minor) Comment | |

Summary

This application represents a thoughtful and considered response to the urban context in which it sits. The documentation and presentation provided to the Panel was comprehensive and clearly illustrated the intent behind the design which supports, compliments and adds to the surrounding neighbourhood. Of particular note is the effort to provide a diverse unit mix that will be attractive to a wide range of residents, and the courtyard typology that will create a community "microcosm" for the residents of the development. The Panel supports the request for relaxation of the maximum units per hectare as the development includes a high ratio of small unit types of diverse type. The Panel encourages CPC to favourably consider developments that meet the intent for densification, provide a mix of unit type for affordability and choice and are a positive architectural response to the context. This is in line with the "fundamental goals of the Municipal Development Plan; planning for long term sustainability, diverse population, adaptability through unit mix to the needs of the residents" *(quote from the applicant's submission) The Panel supports the request for relaxation of the maximum building height for 4.5% of the building. The Panel feels that the central "tower" element that is designed as a termination of 5th Street is a positive urban design response. The project demonstrates that the impact of shadow and overlook to adjacent sites is relatively insignificant.

Applicant Response

(November 15, 2017)

Noted.

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| Urban Vitality | | | |
|---|---|---|---------|
| | Topic | Best Practice | Ranking |
| 1 | Retail street diversity | Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided. | N/A |
| 2 | Retail street transparency, porosity | Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows. | N/A |
| 3 | Pedestrian-first design | Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience. | Support |
| UDRP Commentary | | | |
| Applicant response <i>Noted.</i> | | | |
| 4 | Entry definition / legibility | Entry points are clear and legible | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |
| 5 | Residential multi-level units at grade | Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use. | Support |
| UDRP Commentary This project is exemplary in this category. All faces of the development have residential doors at street level, inclusive of the street face, lane face and both sides of the internal courtyard. | | | |
| Applicant Response <i>Noted.</i> | | | |
| 6 | At grade parking | At grade parking is concealed behind building frontages along public streets. | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |
| 7 | Parking entrances | Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first. | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |

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| 8 | Other | Live work units | Support |
| UDRP Commentary | | | |
| The Panel strongly supports the inclusion of Live Work units within the development. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i> | | | |
| | Topic | Best Practice | Ranking |
| 9 | LRT station connections | Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas. | N/A |
| 10 | Regional pathway connections | Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas. | N/A |
| 11 | Cycle path connections | Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade. | Support |
| UDRP Commentary | | | |
| Bicycle storage is reasonably convenient within the parkade level. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 12 | Walkability - connection to adjacent neighbourhoods / districts / key urban features | Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use. | Support |
| UDRP Commentary | | | |
| The 69th Avenue sidewalk has been widened to 2m and the large existing street trees are to remain. The Panel strongly encourages the development by the City of pedestrian crosswalks mid-block on 69th with a traffic lights as proposed by the Applicant. As 69th is a busy traffic corridor - creating opportunities for better, safer pedestrian connection north south to unite the neighbourhood would be a benefit. | | | |
| Applicant Response | | | |
| <i>Curb extensions with marked pedestrian crossing have been added at the intersection of 69 Avenue SW and 5 Street SW.</i> | | | |
| 13 | Pathways through site | Provide pathways through the site along desire lines to connect amenities within and beyond the site boundaries. | Support |
| UDRP Commentary | | | |
| The courtyard concept provides for through block connection east west and north south in several locations for residents. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |

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| 14 | Open space networks and park systems | Connects and extends existing systems and patterns. | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |
| 15 | Views and vistas | Designed to enhance views to natural areas and urban landmarks. | Support |
| UDRP Commentary | | | |
| There are no defined urban landmarks in the vicinity. The proposal is considerate of overlook to adjacent residential areas. | | | |
| Applicant Response <i>Noted.</i> | | | |
| 16 | Vehicular interface | | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |
| 17 | Other | Tower elevations facing the courtyard | Comment |
| UDRP Commentary | | | |
| The elevations of the tower facing the internal courtyard do not seem to be as strong as some of the other elevations. They are in a significant position and could offer greater complexity in massing or design in support of the overall concept. | | | |
| Applicant Response | | | |
| <i>The east and west elevations of the mid-block have been articulated through the use of Juliet balconies and typical balconies. The glazing locations have also been articulated in order to add visual interest to these facades.</i> | | | |
| Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i> | | | |
| | Topic | Best Practice | Ranking |
| 18 | Massing relationship to context | Relationship to adjacent properties is sympathetic | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |
| 19 | Massing impacts on sun shade | Sun shade impacts minimized on public realm and adjacent sites | Support |
| UDRP Commentary | | | |
| Applicant Response <i>Noted.</i> | | | |

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| 20 | Massing orientation to street edges | Building form relates / is oriented to the streets on which it fronts. | Support |
| | UDRP Commentary | | |
| | Applicant Response | | |
| | <i>Noted.</i> | | |
| 21 | Massing distribution on site | | Support |
| | UDRP Commentary | | |
| | Applicant Response | | |
| | <i>Noted.</i> | | |
| 22 | Massing contribution to public realm at grade | Building form contributes to a comfortable pedestrian realm at grade | Support with comment |
| | UDRP Commentary | | |
| | The north south connections from the courtyard to the street or laneway could be emphasized. The proposed width of the connections is very narrow and if they can be widened it would benefit the project. The Panels also commented that using these connections to provide a more substantive break in the street wall for complexity of massing would be of benefit. The architecture of the facade should reinforce these "doorways" for clarity and legibility. | | |
| | Applicant Response | | |
| <i>The north and south connections to the street and lane will be emphasized through an increase in ceiling height and/or lighting. This will significantly help to reinforce these doorways for clarity and legibility.</i> | | | |
| 23 | Other | Courtyard Typology | Support with comment |
| | UDRP Commentary | | |
| | This was the most significant topic raised by the Panel. The courtyard concept is strongly supported. A few recommendations were offered in an effort to strengthen the proposal. 1) Planting of trees on the south side of the courtyard may improve their long term success due to better water retention. 2) Consider using planters for food production. It was noted that this would be dependent upon the condominium agreement, but the applicant was receptive to the concept. 3) The addition of low fences and gates to define the private boundary of the interior residential patios would encourage greater use and occupancy of these space as it creates a sense of "ownership" and better defines the private space without decreasing visibility and engagement "across the fence". 4) Consider the design of the central space to encourage lingering, active use and community. The space at the moment is unoccupied by furnishings or other elements that suggest how it is to be occupied. Ideas that were floated included a community table, a fire pit, children's play area etc. The Panel would like to see this space defined in a way that supports a communal use beyond that of a circulation corridor. | | |
| | Applicant Response | | |
| <i>(1) Our landscape consultant, Bassett Associates, have recommended the spring snow flowering crab trees within the courtyard be placed on the north side of the courtyard. With the trees in the north planters they will get more sunlight, as well, the planters will have irrigation so there shouldn't be any concerns for short term success.</i> | | | |

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| | <p>(2) <i>The notion of food production within the courtyard has been considered but will ultimately be a decision made by the active condominium corporation.</i></p> <p>(3) <i>Low fences and gates have been added to all courtyard amenity spaces. This will certainly create a stronger sense of ownership towards these spaces and will better define the space.</i></p> <p>(4) <i>The intent of the courtyard is to provide a blank canvas for residents of the building to activate in several ways noted above. It is Sturgess Architectures intent that the building residents determine the best use for courtyard based on the current building demographics and define the space as they wish.</i></p> | | |
|---|---|--|---------|
| <p>Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i></p> | | | |
| Topic | Best Practice | Ranking | |
| 24 | Safety and security | CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances. | Support |
| UDRP Commentary | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 25 | Pedestrian level comfort - wind | Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions. | Support |
| UDRP Commentary | | | |
| As the applicant has designed several developments of this typology an in Calgary, a conversation with Urban Design regarding the success or mitigation strategies used would suffice to conclude this comment. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 26 | Pedestrian level comfort - snow | Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions. | Support |
| UDRP Commentary | | | |
| As the applicant has designed several developments of this typology an in Calgary, a conversation with Urban Design regarding the success or mitigation strategies used would suffice to conclude this comment. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 27 | Weather protection | Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages. | Support |
| UDRP Commentary | | | |
| The renderings seem to include weather cover at all entries. This is to be confirmed with Urban Design. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |

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| | | | |
|--|-----------------------------------|---|----------------------|
| 28 | Night time design | | Uncertain |
| UDRP Commentary | | | |
| The lighting design is to be confirmed and accepted by/with Urban Design. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 29 | Barrier free design | Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps. | Support with comment |
| UDRP Commentary | | | |
| The ramp indicated in the center of the courtyard may feel more integral to the design if reduced to a 5% or lesser slope. Perhaps it could be widened for the full width of the courtyard, or moved to one side for a single wider stair vs a bisected stair (recognizing that access to some of the residential patios would need study). The lesser slope removes the requirement for handrails and is easier to manoeuvre. | | | |
| Applicant Response | | | |
| <i>Increasing the ramp on the east courtyard to span the width of the courtyard will impede access to the units and walkways adjacent to the north and south. The slope of the ramps indicated could be lessened to provide a slope closer to 5%.</i> | | | |
| 30 | Winter city | Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity. | Support |
| UDRP Commentary | | | |
| The project has demonstrated consideration for sun angles and access to daylight throughout the year. Double sided unit layouts are beneficial to the requirement to maximize access to daylight. | | | |
| Applicant Response | | | |
| <i>Noted.</i> | | | |
| 31 | Other | unused | |
| Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i> | | | |
| Topic | | Commentary | Ranking |
| 32 | Waste / recycling | | Support |
| 33 | Enmax (Power) / Atco (Gas) | This item was not discussed. The applicant is to resolve this item to the satisfaction of Urban Design. | Uncertain |
| 34 | Transformer / switchgear | This item was not discussed. The applicant is to resolve this item to the satisfaction of Urban Design. | Uncertain |
| 35 | Exhaust / intake | This item was not discussed. The applicant is to resolve this item to the satisfaction of Urban Design. | Uncertain |
| 36 | Electrical vaults | This item was not discussed. The applicant is to resolve this item to the satisfaction of Urban Design. | Uncertain |
| 37 | Loading | | Support |
| 38 | Fire truck access | | Support |
| 39 | Other | | |

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
 DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
 (FASCIA SIGN)
 KINGSLAND (WARD 11)
 ON 69 AVENUE SW WEST OF MACLEOD TRAIL S

MAP 28S

APPENDIX VI

**SUMMARY OF COMMENTS FROM KINGSLAND RESIDENTS
 (FROM OPEN HOUSE – June 2017)**

The tables below are a summary of the main concerns and positive comments expressed by the 24 residents who attended the Applicant led Open House in June 2017.

| Concerns Raised in the comments | | Positives Raised in the comments | |
|--|----|---|---|
| Traffic | 12 | Looks Better/Pretty Design/ Contemporary Design | 8 |
| Parking / visitors Parking | 6 | Paved Back Alley | 2 |
| Height | 6 | Laneway work spaces | 2 |
| Landlords | 5 | Courtyard | 2 |
| Density | 3 | Keeping trees | 2 |
| Decrease in property value | 3 | Articulated Facade | 1 |
| Privacy | 3 | Varied Units | 1 |
| Noise | 3 | Underground parking | 1 |
| Impact of construction | 2 | Revitalization/ Improvements to the community | 1 |
| Proximity to school | 2 | | |
| Setback | 1 | | |
| Loss of trees | 1 | | |
| Loss of one way streets | 1 | | |
| Increase in the amount of affordable housing | 1 | | |

The following is the response to the main concerns raised. This response was sent to the residents in December 2017. This response included rationale and design changes that were considered to address concerns.

Traffic

Concern:

Additional number of vehicles expected in the area as well as speeding along 69 Avenue SW.

Response:

The original application included a Transportation Impact Assessment (TIA), which is a study that look at the traffic patterns and volume generation and its relation with existing infrastructure. The TIA was received, reviewed and accepted by The City. The study concluded that the additional traffic that is expected to be generated by the development can be supported by existing infrastructure. Lane paving was a recommendation that resulted from the TIA. Applicant agreed to pave the lane as one of the public realm improvements for the development.

To address speeding concerns along 69 Ave SW, curb extensions on both sides of 69 AV SW and 5th Street SW intersection are being proposed. The curb extensions will narrow the carriageway to encourage slower traffic and provide for safer pedestrian crossing. This is another public realm improvement included in the development.

**DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
ON 69 AVENUE SW WEST OF MACLEOD TRAIL S**

MAP 28S

Parking / visitors parking

Concern:

Parking shortage around the proposed development and in the community.

Response:

Land Use Bylaw 1P2007 required parking stalls for residents and visitors of the proposed development will be provided within the site. On-street parking, even if not part of the parking counts, is also available. Loading stall for the development is provided within the site.

Height

Concern:

Proposed additional height on the central portion of the development which allows for 5 storeys.

Response:

Majority of the proposed building is below the maximum building height allowed under the M-C1 District (14 metres / 4 storeys). Approximately 4.5 percent of the proposed development exceeds the maximum building height and is strategically located at the intersection of 69 Avenue SW and 5 Street SW. A shadow study completed by the applicant, and reviewed by The City, shows that the shadow cast for the proposed development including the central portion, does not exceed the shadow cast produced by the existing trees along 69 Avenue SW. In addition, the proposed central portion of the building enhances the building's main entrance and adds architectural richness to the development.

Landlord

Concern:

Property owners' up keeping and sites tidiness

Response:

This concern is not directly related to the application. If there are any concerns related to the up keeping of the properties, please contact 311 and refer to the Community Standards Bylaw 5M2004.

<http://www.calgary.ca/CA/city-clerks/Documents/Legislative-services/Bylaws/5M2004-CommunityStandards.pdf>

Density

Concern:

Proposed number of units (110)

Response:

The proposed development includes a variety of dwelling unit types (bachelor suites; one, two, and three bedroom units). A detailed design brief was provided by the Applicant highlighting the fact that if the number of units was to be reduced, the building form/mass would not change. Bachelor suites would be converted into one or two bedroom units. The study also argues that including a range of unit types provides affordable solutions for different demographics.

DEVELOPMENT PERMIT – NEW: MULTI-RESIDENTIAL
DEVELOPMENT, LIVE WORK UNIT (1 BUILDING), SIGN - CLASS B
(FASCIA SIGN)
KINGSLAND (WARD 11)
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MAP 28S

The Municipal Development Plan (MDP) supports moderate intensification in a form and nature that respects the scale and character of the neighbourhood. , in addition the MDP encourages higher residential densities and redevelopment that increases the mix of housing choices. The proposed development is located approximately 200 metre from Macleod Trail South which is a Main Street Corridor (as identified by the MDP) and is supported by primary transit. Parking requirements are also met on site.

Decrease of Property Value

Concern:

Potential loss in property value due to proposed development

Response:

Property value is not a planning matter that can be considered during the planning process.

Privacy

Concern:

Privacy in backyards

Response:

The rear area of the fourth and fifth floors of the central volume has been stepped back to mitigate this concern. Generally speaking, redevelopment occurs on parcels of different sizes, orientations, and distance from adjacent properties. Proposed development tends to be in different building form and scale. There are no practical, reasonable and enforceable design standards that have been devised to allow the full use of land whilst guaranteeing complete privacy for every neighbour. Planning does review and ensure a reasonable degree of privacy is respected.

Noise

Concern:

Development will increase noise levels in the community

Response:

The City has Community Standards Bylaw which includes the noise bylaw to address this type of concerns in communities. Contact 311 with any noise concern and refer to the Community Standards Bylaw 5M2004

<http://www.calgary.ca/CA/city-clerks/Documents/Legislative-services/Bylaws/5M2004-CommunityStandards.pdf>

A silhouette of a surveyor wearing a wide-brimmed hat and a jacket, standing next to a large transit instrument. The surveyor is looking through the instrument's eyepiece. The instrument is a classic surveying tool with a circular level and a vertical staff.

Calgary Planning Commission 2018 January 25

Item 5.12

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

EXECUTIVE SUMMARY

This application, known as 'Ellistown', covers an area of 11.37 hectares \pm (28.09 acres \pm) of residual undeveloped land in the community of Red Carpet. The application proposes to redesignate the subject lands to three different land use districts:

- M-CGd60 district (8.77 hectares), allowing for a maximum of 526 townhouses up to 12 metres in height (approximately two to three storeys);
- C-COR1f3.0h16 district (0.8 hectares), allowing for commercial or mixed use buildings up to 16 metres in height (approximately four storeys); and
- M-H1h18d155 district (1.8 hectare), allowing for multi-residential apartment buildings with up to 279 units and up to 18 metres in height (approximately five storeys).

An outline plan which proposes a concept plan for the development has been reviewed in conjunction with this application. This application also includes a proposed closure of 0.04 hectares \pm (0.09 acres \pm) undesignated portion of road right-of-way located within the plan area.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION'S RECOMMENDATION

2018 January 25

That Calgary Planning Commission recommends **APPROVAL** of the proposed Road Closure and Land Use Amendment.

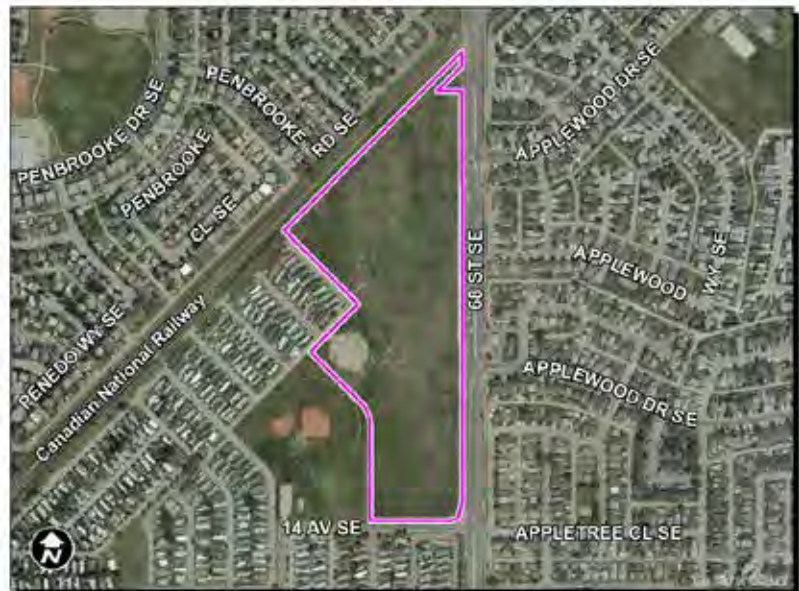
REASON(S) FOR RECOMMENDATION:

The proposal is consistent with applicable policies of the Municipal Development Plan (MDP). Supportive public infrastructure is in place and the site is located in close proximity to public parks and the regional pathway system. The proposed land use districts allow for higher density residential development and small scale commercial development that support the MDP's goals for a more compact urban form. In addition, the proposed land uses allow for development of a complete community with a range of housing that exceeds density requirements.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

LOCATION MAPS



Road Closure Map



Land Use Amendment Map



ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION

1. Recommend that Council **ADOPT**, by bylaw, the proposed closure of 0.04 hectares \pm (0.09 acres \pm) of road (Plan 0614409, Area A) adjacent to 825, 841, 901 – 68 Street SE, with conditions (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 11.37 hectares \pm (28.09 acres \pm) located at 809, 825, 841 and 901 – 68 Street SE and the closed road (Plan 3270AG, Block Z; Plan 3270AG, Block 1, Lots 1 and 2; Plan 1612085, Block 8, Lot 1; Plan 0614409, Area A) from Special Purpose – Future Urban Development (S-FUD) and Undesignated Road Right-of-Way to Multi-Residential – Contextual Grade-Oriented (M-CGd60) District, Multi-Residential – High Density Low Rise (M-H1h18d155) District, and Commercial – Corridor 1 f3.0h16 (C-COR1f3.0h16) District.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

Applicant:

Casola Koppe

Landowner:

Lansdowne Equity Ventures Ltd
The City of Calgary

PLANNING EVALUATION

SITE CONTEXT

The subject lands are located on the west side of 68 Street SE, to the north of 17 Avenue SE, in the community of Red Carpet. The land comprises four parcels, as well as an undesignated road right-of-way that is proposed to be closed as part of this application. Collectively, these lands make up 11.37 hectares \pm (28.09 acres \pm), with the lands bound:

- to the north by the Canadian National (CN) Railway line and residential community of Penbrooke Meadows;
- to the east by 68 Street SE, and the residential community of Applewood Park;
- to the south by the Mountview Mobile Home Park; and
- to the west by a different mobile home park on lands owned by Lansdowne, as well as Mountview Park, the CN Railway line, and the residential community of Penbrooke Meadows.

The site is relatively flat with little vegetation.

The proposed road closure with this application is for a portion of the 68 Street SE right-of-way, adjacent to 825, 841 and 901 68 Street SE. The road right-of-way is approximately 0.04 hectares (0.09 acres) in size and is not built or used to access the site. Two new site access points along 68 Street SE and a third access along 14 Avenue SE are proposed for the outline plan. The closed road right-of-way will be included in the boundary of the proposed land use amendment and associated outline plan.

According to data from The City of Calgary 2017 census, the population of Red Carpet peaked in 2006 with a decline of ten percent since that time. The current unit mix in Red Carpet is 44 percent apartments and 56 percent mobile homes. Table 1 below summarizes population trends in the community of Red Carpet.

Table 1: Population Trends in the Community of Red Carpet

| Red Carpet | |
|------------------------------------|-------|
| Peak Population Year | 2006 |
| Peak Population | 1,777 |
| 2017 Current Population | 1,605 |
| Difference in Population (Number) | -172 |
| Difference in Population (Percent) | -10% |

ROAD CLOSURE AND LAND USE AMENDMENT
 RED CARPET (WARD 9)
 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

LAND USE DISTRICTS

With the exception of the undesignated road right-of-way, the subject lands are currently designated Special Purpose – Future Urban Development (S-FUD) District. This application proposes to redesignate the plan area to three different land use districts:

- Multi-Residential Contextual Grade-Oriented (M-CGd60) District
 - The proposed M-CGd60 district is intended to allow for multi-residential development within townhouse building forms that have units with direct grade access; and
 - The site would have development potential for a maximum building height of 12 metres (approximately three storeys), and a maximum density of 60 residential units per hectare (up to 526 residential units).

- Multi-Residential High Density Low Rise (M-H1h18d155) District
 - The proposed M-H1h18d155 District allows for multi-residential development in a variety of forms; and a limited range of small-scale, complementary retail and commercial uses on the main floors of multi-residential buildings; and
 - The site would have development potential for a maximum building height of 18 metres (approximately five storeys), and a maximum density of 155 residential units per hectare (up to 279 residential units), with a maximum floor area ratio (FAR) of 4.0.

- Commercial Corridor 1 (C-COR1f3.0h16) District
 - The C-COR1f3.0h16 designated area is intended to allow for small to mid-scale local commercial development with active frontages,
 - The site would have opportunities for commercial uses on the ground floor of buildings, with the option for residential and/or office uses on the upper floors; and
 - The site would have development potential for a maximum building height of 16 metres (approximately three storeys), with a maximum floor area ratio (FAR) of 3.0.

The proposed land use districts and information on density, FAR, and height are summarized in Table 2 below.

Table 2: Proposed Land Use Districts

| Land Use District | Area (ha) | Minimum # of residential units | Maximum # of units | Anticipated # of units | Maximum Height (m) | Maximum FAR |
|-------------------|-----------|--------------------------------|--------------------|------------------------|--------------------|-------------|
| M-CGd60 | 8.77 | - | 526 | 516 | 12 | N/A |
| M-H1h18d155 | 1.80 | 270 | 279 | 270 | 18 | 4 |
| C-COR1f3.0h16 | 0.80 | - | - | 50 | 16 | 3 |
| Total | 11.37 | - | - | 836 | - | - |

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (2009)

The subject site is located within a Developed Residential - Established Area typology of the Municipal Development Plan (MDP) (Map 1: Urban Structure). Established Areas are comprised of residential communities that were planned and developed between the 1950s and 1990s. They are primarily residential communities containing a mix of low and medium density housing that support commercial retail in relatively close proximity. The road network in these areas is often a blend of modified-grid and curvilinear.

General land use policies for Developed Residential Areas encourage the retention of housing stock and moderate intensification in a form and nature that respects the scale and character of these neighbourhoods. Redevelopment within predominantly multi-residential areas should be compatible with the established pattern of development, consider appropriate transitions between adjacent areas and allow for a variety of residential housing typologies to meet the needs of present and future populations.

Redevelopment should support the revitalization of local communities by adding population and a mix of commercial and service uses with supporting mobility policies to encourage development of high-quality pedestrian and cycling connections and well-integrated parking solutions.

The proposed land use redesignation supports the MDP in many ways, including:

- Allowing for densification in a readily serviced, developed area (section 2.1.1.g);
- Providing a mix of residential, commercial and service uses (section 3.5.1);
- Optimizing use of existing infrastructure and services (section.2.1.4.a);
- Allowing for development of a similar scale and built form in existing low density areas but with an increased mix of low density housing types (such as Townhouse, Rowhouses). (section 2.2.5.a);
- Respecting the existing low density residential by proposing compatible development and transitioning to multi-residential development (section 2.3.2.b);
- Creating a walkable environment. The outline plan proposes effective pedestrian and cyclist connections within the site and to adjacent communities. (section 3.5.1.f); and
- Allowing for a range of housing types to help stabilize population decline and to support the demographic needs of the population (section 2.2.5.b).

Section 2.2.5.e of the MDP states that for large development sites over 1.0 hectare in the Developed Area, a comprehensive plan is required. To properly evaluate this application, a comprehensive plan, consistent with the requirement of the MDP was submitted. The associated outline plan is the comprehensive plan that addresses key planning issues

**ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

associated with the development of the site, including site layout, urban design, integration with surrounding community, transportation and engineering issues, connectivity and circulation. The outline plan recommended for approval is the result of an extensive design exercise that Administration, the Urban Design Team, Urban Design Review Panel, conducted with the applicant to refine the initial design submitted in 2015 June.

Local Area Plan - Marlborough Design Brief (adopted by Council 1971, revised 1974)

The subject site is referenced in the Marlborough Design Brief. It should be noted that this Design Brief is presently being reviewed in conjunction with the upcoming report on plans for 2018 local area plan work. The Design Brief is a non-statutory plan adopted by Council in 1971 to provide planning policy context for subdivision and development within the boundary of the Marlborough Sector. The Marlborough Sector is bound by the Trans-Canada Highway, 17 Avenue SE, and Stoney Trail. The majority of this plan area has been developed into the existing communities of Marlborough, Penbrooke Meadows, and Forest Lawn/Forest Heights, with the exception of the subject site. The subject site is recognized as being part of the “remaining uncommitted area in Section 14E” within the plan, which identifies the area as intended for a mobile home park, with a recommended maximum density of 22 units per hectare (8.9 units per acre). The plan also identifies that a local convenience shopping facility will be permitted as part of a mobile home park development. This proposal will exceed the recommended density at approximately 32.4 units per hectare (13.1 units per acre). The impact of this development on local services and city infrastructure has been considered as part of the application review.

TRANSPORTATION NETWORKS

A Transportation Impact Assessment (TIA) was submitted for the proposed land use amendment and outline plan. The scope of the TIA was local, with a focus on adjacent intersections. 68 Street SE is an arterial with a posted speed of 60 kilometers per hour. Applewood Drive SE (North and South leg) and 14 Avenue SE are collector roads with a posted speed of 50 kilometers per hour. The outline plan concept have three access points, including two along 68 Street SE at Applewood Drive SE north and Applewood Drive SE south, and a third proposed access along 14 Avenue SE at the southwest corner of the site. The accesses along 68 Street SE will become the west leg to the existing three-legged intersection. The access along 14 Avenue SE will form the north leg of the intersection with the Mountview Mobile Home Park driveway. To accommodate build out of the site, intersection improvements will be required at the intersection of 68 Street SE and Applewood Drive SE (north and south intersections). Within the development, private roads with custom cross sections are proposed.

**ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Public Transit

The Applewood Park and Penbrooke Meadows communities are established with access to transit services that provide good transportation options throughout the area, as well as, the rest of the City. Additionally, the site is north of 17 Avenue SE, which is designated as part of the Primary Transit Network with future bus rapid transit.

UTILITIES & SERVICING

Sanitary sewer mains are available to service the site to the north, opposite the CN Railway line, and to the south, along 14 Avenue SE. Water mains to tie into the existing Applewood system are located on Applewood Drive SE in the east, and at the 14 Avenue SE system to the south.

Offsite water mains and sanitary sewer extensions will be required at the development permit stage, and will be at the expense of the developer. Storm sewers are presently available to service the site. The subject site is within the boundary of the Western Irrigation District drainage catchment, and is therefore subject to stormwater volume control measures.

A Staged Master Drainage Plan (SMDP) and a Sanitary Servicing Study (SSS) for the plan area have been submitted and approved by Water Resources.

ENVIRONMENTAL ISSUES

Review of this application identified the need to assess concerns related to the railway and to assess potential impacts related to the historical stockpiling of soil on the subject site. In 2015, a Phase II ESA was completed and the soil results from boreholes in the vicinity of the rail line and the stockpiled soil met the applicable criteria. Considering the soil results, groundwater impacts were not suspected and no further investigation was recommended.

ENVIRONMENTAL SUSTAINABILITY

The proposed land use amendment supports the MDP sustainability policies by developing in the Developed Established Area. The site is located near amenities, employment, parks, and transit. The supporting outline plan includes a site layout that provide pathways to encourage walking and cycling within the site and to adjacent communities. Environmental sustainability will also be evaluated by Administration when individual development permits are processed.

GROWTH MANAGEMENT

The subject site is not located within an area covered by an existing Growth Management Overlay. The proposed land use amendment does not require additional capital infrastructure investment and therefore, no growth management concerns have been identified.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

PUBLIC ENGAGEMENT

Community Association Comments

There is currently no Community Association established for the community of Red Carpet. Given the site proximity to the Applewood Park and Penbrooke Meadows community, the application was circulated to both of these Community Associations on August 2016. The Penbrooke Meadows Community Association did not provide any comments on this proposal. The Applewood Park Community Association reviewed the initial proposal and provided comments, their comments are included in APPENDIX IV of this report and are summarized as follows:

- Concern with the proposed density and potential traffic increase in the area;
- Question if traffic signals will be installed on 68 Street SE at the north access and timing for this local improvement;
- Concern with the lack of green space available for families;
- Suggested playground or other active programming space for the proposed open space for families;
- Question about duration of construction; and
- Question about how transit services will be affected by the proposed development.

Administration also contacted both Community Associations on 2017 August 11 prior to the 2017 August 29 public information session and on 2017 December 11 to determine if they had additional comments on this application prior to development of a recommendation for Calgary Planning Commission. No response was received.

Citizen Comments

Administration received three letters from adjacent residents. The letters expressed both support for and concerns with the proposed development. The comments are summarized as follows:

- Support for the addition of commercial services within the community and local community services within walking distance;
- Support for the addition of affordable and senior housing;
- Support for the development of 'unused' land;
- The wait time for the signalized intersection at 68 Street and 17 Avenue SE is very long;
- Concerns with potential traffic increase in the area as a result of site development;
- Question if the adjacent mobile home parks will be subject to future redevelopment; and
- Concern with the closure of 68 Street SE and requested additional information on the proposed road closure location.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

Public Meetings

On 2017 August 29, Administration held an information session at the Penbrooke Meadows Community Association, to share details of the updated application with community members.

The goals of the information session were to:

- Inform the public of the proposed development and the City's planning processes;
- Answer any questions the public may have about the development, with City representatives and the applicant on hand; and
- Collect feedback from the public about the proposed development through a 'What We Heard Report'.

A communications plan was developed to inform the community about the project and the information session. The methods used to inform the public included:

- Large bold signs placed at high-traffic intersections in the community;
- Post card mail outs to surrounding area residents;
- Twitter and Facebook advertisement campaigns;
- Sending emails to the community members subscribed to our email list, sharing on-going project information and engagement details on the project webpage; and
- Regular and on-going communications with the Community Associations.

There were 68 people in attendance at this session and 27 feedback forms received.

What We Heard

Citizens provided a range of comments, with the main themes and areas for concern identified by community members as follows:

- The number of units being proposed is too much density and it will further impact traffic in the area. For example; citizens are concerned by the traffic volume on 68 Street SE as is and feeling that this will worsen.
- Some citizens feel that the proposed heights of the buildings are too high for the area and do not fit in with the surrounding community
- Some citizens would like to see better pedestrian connections through the site and in the surrounding areas
- Some citizens are supportive of future development and feel it will bring revitalization to the area

The What We Heard Report with verbatim comments can be found in APPENDIX V.

ROAD CLOSURE AND LAND USE AMENDMENT
 RED CARPET (WARD 9)
 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

What We Did

The What We Heard Report from the 2017 August Information Session, was shared with the applicant and the key changes to the concept plan and responses to community concerns are noted below.

| Issue identified | Applicant response |
|-------------------------|--|
| Pedestrian Connectivity | <ul style="list-style-type: none"> · A clear separation has been created between pedestrian and vehicular modes of transportation. Plans have been designed to create clear connectivity into, and throughout the site. The internal and external connectivity provide an organized system including primary and secondary road network and sidewalks for pedestrian, cycling; · Better walkability and connections provided throughout the site, including a three metre wide pathway on the north side of Ellisborough Boulevard to accommodate cyclists; and · The orientation of built form has been changed to reinforce the streets by fronting the commercial and residential units to the street. |
| Open Space | <ul style="list-style-type: none"> · The open space is now more prominent and central in the development. |
| Building Heights | <ul style="list-style-type: none"> · To address building height concerns, low-density buildings will be placed along the perimeter of the site to create a buffer for the apartment-type buildings. These will also be located further away from the main arteries (68 Street and 14 Avenue SE); and · The applicant's goal is to create a site that will incorporate a variety of housing types, including five storey apartment buildings and two to three storey townhomes, all the while ensuring an appealing flow with the surrounding communities and minimal obstruction of views. |
| Traffic | <ul style="list-style-type: none"> · A Transportation Impact Assessment (TIA) was submitted for the proposed development and reviewed by Transportation Planning; and · To accommodate build out of the site, intersection improvements will be required at the intersection of 68 Street SE and Applewood Drive SE (north and south intersection). |

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX I

APPLICANT'S SUBMISSION



#240, 5010 Richard Road SW
Calgary T3E 6L1

December 18, 2017

Christine Leung
Senior Planner
South Team, Community Planning

Subject: Ellistown Development: An Introduction from the Developer

On behalf of Slokker Canada West, I'd like to thank you for the time and attention you have provided to the Ellistown Development and for the considered team comments shared by Corporate Planning and Applications Group (CPAG).

While we have provided an updated outline plan and a comprehensive response in the attached submission, we also wanted to include an overview of the enhancements we have made to the original plan. These changes are a result of our ongoing consultation with the City, public engagement with the Applowood and Penbrooke Community, and our desire to make a positive contribution, vitalizing an important area in east Calgary.

The right mix of housing and amenities

As with any development, we know it is important to find a reasonable balance between density and amenities in a manner that is appropriate and suitable within the greater context of community and neighbourhoods. Since there is an extensive stock of medium to high density condos in the surrounding area, Slokker Canada West has reworked the original high-density proposal for this site. This earlier proposal of 912 homogeneous apartment style units with 80,000 square feet of retail space has now been revised as a more balanced mixed unit design consisting of a greater variety of housing types, and a smaller neighbourhood centred retail area.

Within the revised outline plan you will find the proposal is for predominantly low to medium density town homes with some complementary medium to high density apartment-style condos. We have also greatly reduced the retail footprint. The current proposal provides for 836 units and 20,000 square feet of retail with opportunities for additional housing and/or professional space above.

With an overall density set slightly above 71.42 UPH, the overall density is respective of neighbouring parcels while providing a housing type with a level of affordability not readily available in this area.

The retail component is important as it will promote walkable communities and provide essential amenities for the neighbourhood – while not duplicating what is available at the nearby East Hills development.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

Enhancements improve walkability and neighbourhood character

Slokker Canada West worked through suggestions from the City to develop the following design enhancements in the proposed area:

1. Moving the commercial area so that it has exposure to 68th Street. This provides better accessibility and allows for more desirable streetscapes in the interior spaces
2. Creating street oriented homes within the site that enhances walkability of the site
3. The current plan calls for a village-style community that enhances the overall neighbourhood character. Predominantly townhomes, Ellistown will draw homeowners to the area, creating a community aspect that has been lacking.
4. Elliston Park and the nearby baseball diamond are community gems. The enhancements to the Ellistown plan will create a more usage of those Calgary facilities and contribute to healthy living in the area.

A commitment to quality and design excellence

At Slokker Canada West, we are committed to delivering quality that meets or exceeds current standards. Our homes in Ellistown will reflect the quality standards of our past projects. As per the City's requirements, these townhomes will respect existing community character while fostering distinctiveness. We are pleased that our projects showcase modern color schemes and quality exterior finishes.

(Please see Attachment A – Slokker Canada West Past Projects.)

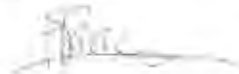
Honouring the heart of the community

We are creating a vibrant family community at Ellistown. In fact, we are working with a theme that focuses on families – from the beautiful town homes to the overall plan, including the pedestrian connection and the central area of the village connecting to commercial amenities, to the baseball diamond and nearby Elliston Park.

To help share our family focus theme, we want to honour the Ellis family – a family who have lived in this part of Calgary since the original Shepard S. Ellis homesteaded in 1912. Five generations of the Ellis family have lived in and around Elliston Park since then. Because of the family's rich history with Calgary and the surrounding area, we want to honour the Ellis family.

Thank you for taking the time to read this introduction. We are pleased to present the updated outline plan to the Corporate Planning and Applications Group (CPAG).

Respectfully,



Peter Paauw
President
Slokker Canada West



Committed to building inviting communities

At [Slokker Canada West](#) we are proud of the quality homes we've built in the Calgary area. The examples shown here highlight the nature of the homes we would build in the Ellistown Development.



ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX II

PROPOSED ROAD CLOSURE CONDITIONS

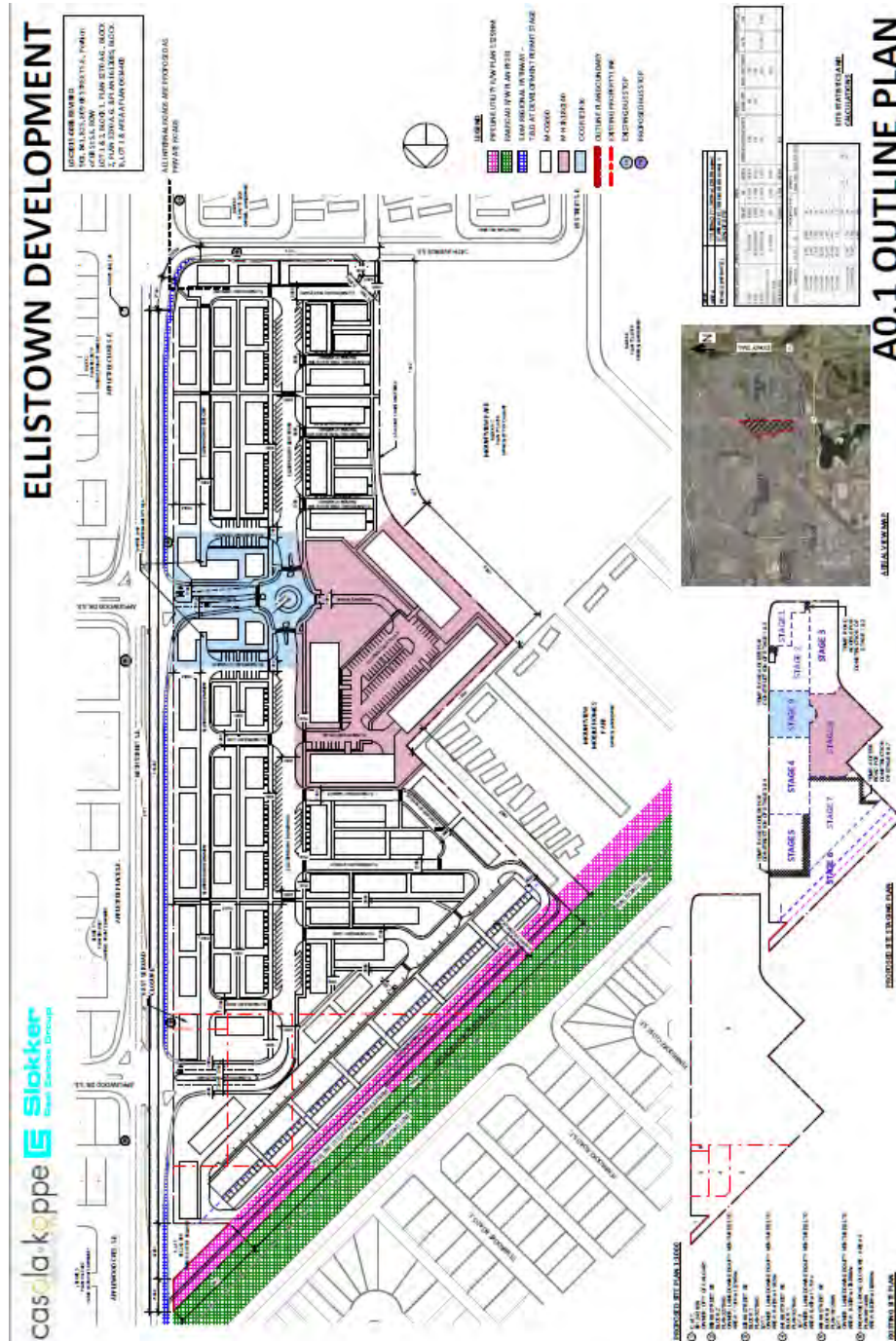
1. The applicant is responsible for all costs associated with the road closure, including all necessary physical construction, removal, rehabilitation, utility relocation, etc.
2. That protection and/or relocation of any utilities will be at the applicant's expense and to the appropriate standards.
3. Any utility right-of-ways are to be provided to the satisfaction of the Development Authority and the City Solicitor.
4. Utility easements are to be provided as required, with a utility right-of-way plan and an accompanying easement document is to be registered concurrently with the subdivision.
5. That the closed road right-of-way be consolidated with the adjacent lands.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX III

PROPOSED OUTLINE PLAN



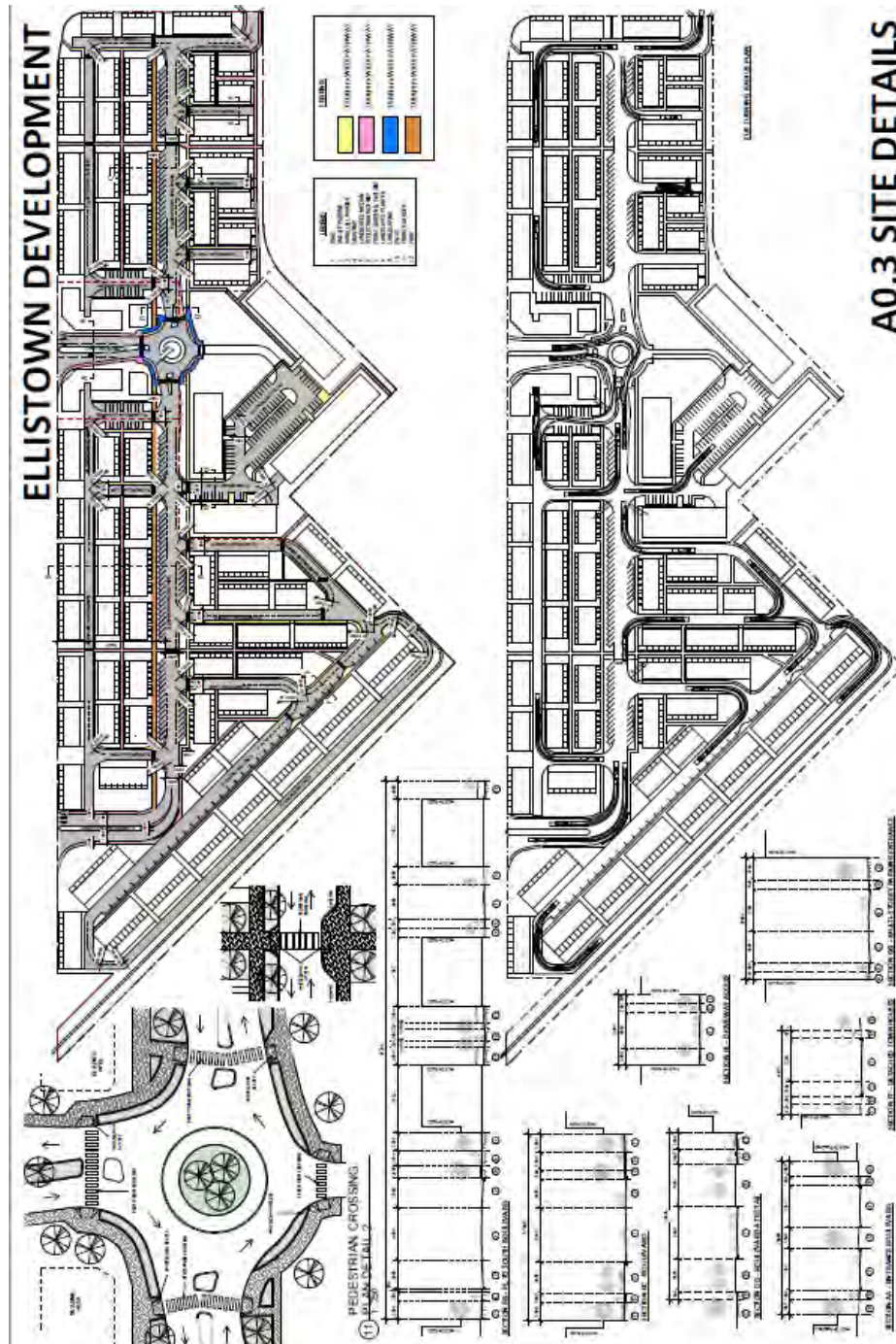
ELLISTOWN DEVELOPMENT

A0.1 OUTLINE PLAN

casla koppe **Slokker**
Urban Services Group

ROAD CLOSURE AND LAND USE AMENDMENT
 RED CARPET (WARD 9)
 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

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ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

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APPENDIX IV

COMMUNITY ASSOCIATION LETTER

From: Steve [mailto: [REDACTED]]
Sent: Sunday, August 14, 2016 9:00 PM
To: CPAG Circ
Cc: Andrew Sutherland; Janet Rivard; [REDACTED]; Ron Motteram; Sherry Desjardins; Bob Rivard; Deborah Loewen
Subject: Request for comment on application. File Number LOC2015-0085. Comments from Applewood Park Community Association.

To: Circulation Control
Planning, Development & Assessment # 8201 Regarding File Number LOC2015-0085.

The Applewood Park Community Association board has the following comments and concerns regarding the development application with the above file number:

The board is concerned with how the increased population density will affect traffic on 68 Street and will there be traffic lights installed on 68 Street at the north entrance / exit into and out of Applewood Park. Will the lights be installed before construction on the project begins?

We would anticipate that many of the residents of Ellistone Village will be young families and therefore we are concerned about the amount of green space available, and how much of it will be dedicated to playground space with playground equipment for children as well as soccer fields and baseball diamonds. There is at this time a fairly heavy usage of the playground facilities in Applewood Park with many of the people using the facilities coming from out of the area.

What will be the duration of the construction period?

How will busing be affected, particularly the 305 express and the 45?

Thank you very much for your time.

Sincerely,
Steve Ayrheart,
President Applewood Park Community Association

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX V

WHAT WE HEARD REPORT



**Ellis Town Land-use and
Outline Plan Application**

Stakeholder Report Back: What we Heard
September 2017

Project overview

The City of Calgary received a proposal (file number LOC2015-0085) from Cosola Koppe Architects, representing the developer and landowners, to redesignate (rezone) the subject site.

The subject site is an 11.4 hectare parcel of residual undeveloped land in the community of Red Carpet. The property abuts the west side of 68 Street SE, in the area north of 17 Avenue SE.

The proposal is to change the allowable uses and development form for this property and close a portion of an adjacent road right-of-way. An Outline Plan showing the concept plan for the development is included as part of the application. The application allows for:

- Under the M-CG District (8.8 hectares), a maximum of 494 townhouses limited to 12 metre in height (about 2-3 storeys);
- Under the C-COR1f3.0h16 District (0.7 hectares), mixed-use and commercial buildings (e.g. commercial storefronts with apartments or offices above), up to 16 metres (about 4 storeys);
- Under the M-H1f3.0h18d215 District, apartment buildings (276 units) up to 18 metres (about 5 storeys); and
- the uses listed in the proposed C-COR1, M-H1 and M-CG designations.

Engagement overview

On August 29, 2017 the City held an information session at the Penbrooke Community Association, to share details of the application with community members and project members and the developer were on hand to answer any questions they had. There were 68 people in attendance.

What we heard

At this event, attendees were given the opportunity to share their thoughts on the application with the project team by filling out a comment form. We received 27 completed comment forms.

Citizens provided a diversity of comments and the main themes and areas for concern identified by community members were as follows:

- Citizens are concerned that the number of units being proposed is too much density and it will further impact traffic in the area. For example; citizens are concerned by the traffic volume on 68th Street and feel that this will worsen.
- Some citizens feel that the proposed heights of the buildings are too high for the area and do not fit in with the surrounding community

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

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Ellis Town Land-use and Outline Plan Application

Stakeholder Report Back: What we Heard
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- Some citizens would like to see better pedestrian connections through the site and in the surrounding areas
- Some citizens are supportive of future development and feel it will bring revitalization to the area
- For a verbatim listing of all the input that was provided, please see the [Verbatim Responses](#) section.

Next steps

Citizen feedback provides Administration and Council with valuable local knowledge of the community and the proposed development area. Administration will be sharing this report with the applicant and once Administration is ready to make their recommendation for the application, this input will also be used in reports provided to Calgary Planning Commission and City Council.

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E



Ellis Town Land-use and
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Stakeholder Report Back: What we Heard
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Verbatim Comments

Content is captured as it was provided by citizens. No edits have been made unless there was personal information or offensive language which is removed with an indication that this has happened.

| |
|---|
| Know City wants densitybut ? - this is too much. 2 to 4 stories only. Have no 5 storeys in the area. Senior population growing - stairs are hard to climb. Develop some unique places with 1 floor townhomes or with 2 suites, one for a caregiver (NOT a renter). Consider your commercial area through the perspective of a "gathering" place so programming can occur. Need some lot lots as I assume families are also the target. Make sure lots of parking underground & safe for children. Do not like the word "burrough" - too English, we are very cultural diverse. Please keep me informed [personal info removed]. |
| I hope this development goes ahead, so long as the density is not too high. |
| Signalize the intersection of 68th St and 14th Ave. Intersection is unsafe for everybody, including cars, transit, buses. Plus parking along 14 Ave - makes roadway too narrow, like a one lane roadway. |
| 68 St traffic is a serious problem now at such hours. If this is built, traffic will be a real serious problem. I don't see any plans to help (ie. Widening 68 St, extend Memorial drive then to Stoney or put under 17 Ave. No senior facilities ie. Housing. No special bus lanes from 17 ave into Ellistown. |
| Feel anything over 4 storeys high will not fit into the area & will be out of place. Traffic flow on 68th St can be heavy now anytime of day. Increased traffic will just add to the problem. If turning left from abbercove DR SE onto 68th is hard enough now. Feel that the number of proposed units is way to high for this area. Need to consider schooling for families in all those units. |
| There is no doubt that the field could use something put there however adding more people in a tight space is a little ridiculous. Why not a sportsplex or a library? Why not take care of the people in the existing communities? That piece of land next to a park could be used more wisely, again promote healthy living build an ice rink, skate park, rock wall. Somewhere people can go without having to travel far from home, send their kids and feel safe knowing where they are. As well a road on 14th is unwise with how many people park on that road nowadays why are you trying to make a situation worse. Money may make our world spin but isn't time to take care of what's existing instead? So putting low income housing is not what any of the local residents want. Put it out by East Hills. We do not want or need crime. put into our neighbourhood. Another issue is the fact that we have Global Fest. not enough security this year. With tons of people having lack of respect for the people in their homes now add more? You all need to sit and rethink the use of this space! |
| I think it is a Great Idea & very overdue! The weed filled eye-sore does nothing to promote our neighbourhoods. A big park would have been great, but ideally condos, retail & green space makes sense. I do hope green space is visible more than buildings but still nice to see something being done! |
| Any buildings should not exceed the height of present buildings adjacent to or across from them. Other than that - have at er! |

ROAD CLOSURE AND LAND USE AMENDMENT
 RED CARPET (WARD 9)
 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E



**Ellis Town Land-use and
 Outline Plan Application**

Stakeholder Report Back: What we Heard
 September 2017

| |
|--|
| <p>This is a much needed development - beautiful actually and it would make living in the NE/SE feel less like we are "the poor end of the city". But, would someone <u>please</u> extend the sidewalk on the north side of 17 Ave SE so a person can walk, bikes, use their motorized scooter etc. to go from Applewood, Ellisborough, the trailers parks to Easthills Mall without walking in mud or being bogged down where one sidewalk ends and another one begins.</p> |
| <p>It seems really nice. My only concern is how accurate. The pictures are showing. Flat roofed condos. In our winter climate a sloped roof is preferred.</p> |
| <p>Looks okay so far.</p> |
| <p>Would like to see some single family stand alone houses. Not liking the idea of 5 story units, would prefer nothing over 3 stories.</p> |
| <p>Traffic on 68 St is congested now. Adding all these units will create even reater problem. I say no to the project. Resolve the 68 St first and then go for this proposal.</p> |
| <p>Can see a lot of traffic conjestion already on 68 E to 17 to Eastport shopping - Costco, Walmart, banks, Theater. Only 1 left turn at light on 68 & 17 so at rush hour lots of traffic now. Will be much more conjested with another 900 or so people.</p> |
| <p>We live in Penbrooke trailer park. Some of the concerns we have is there are only one entrance intot he park on 17 Ave SE at 68 St SE. If there was ever an emergency we would have major concerns. We also need lights at the corner of 17 Ave SE at 62 St. It would be nice if they would open up 14 Ave SE into our park or off 68 St SE.</p> |
| <p>We don't need more people in the area it will be crowded we thought it is going to be comercial (like Superstore or No Frills) & would suggest a senior community type (Bungalow style villas)</p> |
| <p>Worn idea. No no no building. I need commercial or build something for old people like bungalow. Not 2000 pepole in the area to crowded for this area.</p> |
| <p>Very worried about increased traffic on 14 Ave & 65 St. 14 Ave - is a major bus route and already there are many people parking on both sides during the day. Makes road a little narrow, even worse in winters when there are snowbanks. During both rush hours there is an increase of traffic cutting through on both 65 St & 14 Ave.</p> |
| <p>Live at [personal info removed] limited access to property is restricted to one way off of 17 Ave next 7-11. I have had three neighbors in car accidents this year alone! What can be done mayor's office and area councillor don't care.</p> |
| <p>Looking forward to new neighbours!</p> |
| <p>I, as a resident of Applewood, I strongly oppose the rezoning of land. The fact that existing structures (ie trailer park will be an eyesore to future development I believe is what is pushing this review ahead. That, in itself, is a shameful situation. The fact of the matter is there are many, many units already located w in a small radius. As it is, 17th Avenue is cluttered with condominiums, & I am mindful of the further condo development that will be undertaken shortly east of both street. How many condos are too many?? I believe the space should be left zoned as is. We do need another 1,300 units w in such a small area. I do not mind stating that any decision in favour of this rezoning will impact my vote @ election time.</p> |

ROAD CLOSURE AND LAND USE AMENDMENT
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E



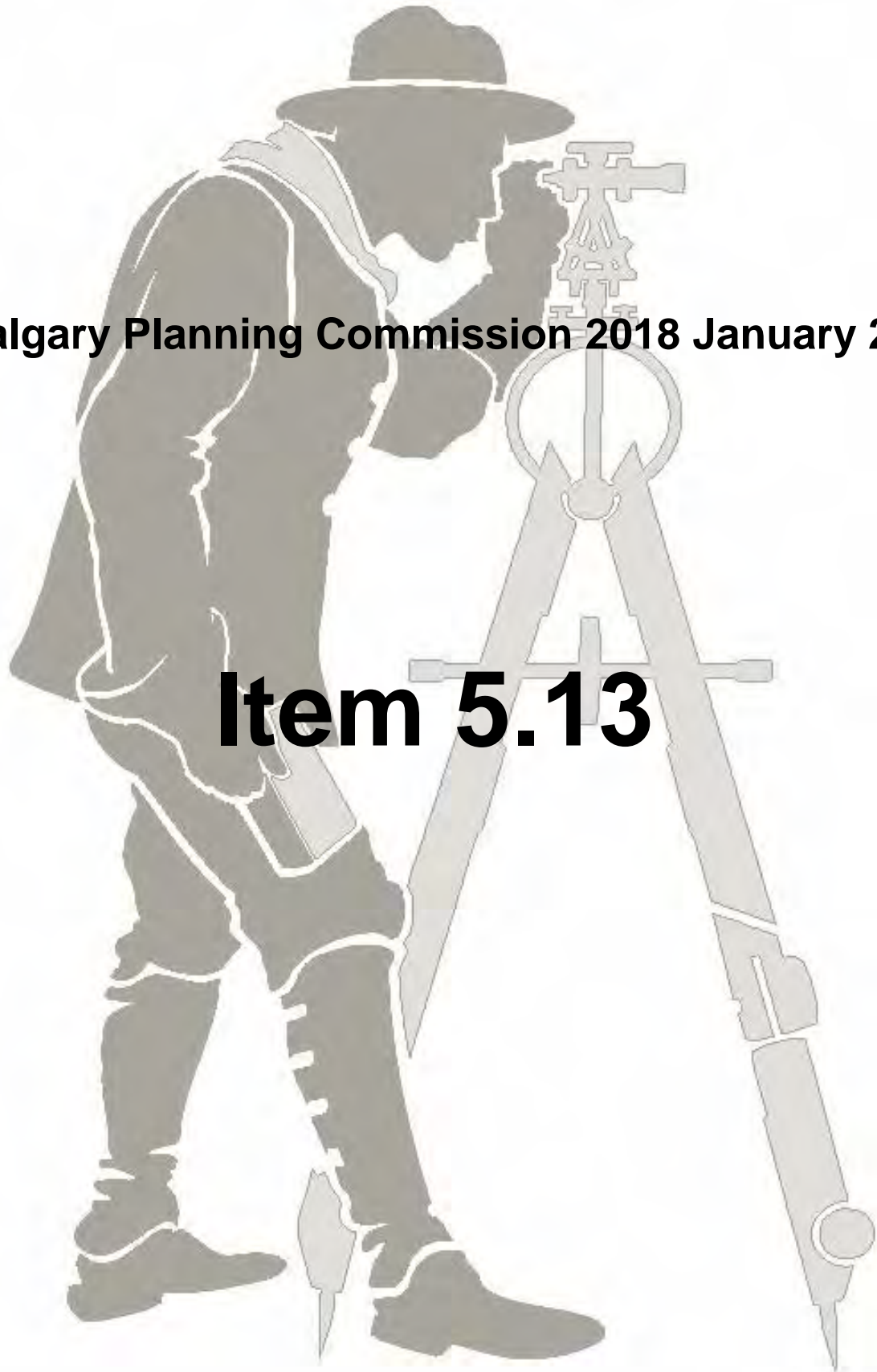
**Ellis Town Land-use and
Outline Plan Application**

Stakeholder Report Back: What we Heard
September 2017

| |
|---|
| Not happy - need less population - more green space. Amount of traffic on 14 Ave is very concern. ? Needs to everyone - more studies are needed! No access to 14 ave / traffic lights on 14 flow of traffic needs to improve. |
| Really needs more planting & is lacking in looks - at least the complexes on 17 Ave across from the dump have better appeal. This looks like I expect - ghetto. Enough of cementing everything. |
| I am a resident of Heritage Estates. I have one concern, about foot traffic or automobile traffic, increasing due to new project. I do not wish to see extra folks in my neighbourhood that do not live there. I feel there would be a lack of respect for the homes and property of the residents. Please consider separating the two developments. Thank you. |
| I think Mountview Mobile Home park will make a nice neighbour. |

Calgary Planning Commission 2018 January 25

Item 5.13



**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

EXECUTIVE SUMMARY

The Ellistown outline plan covers an area of 11.37 hectares ± (28.09 acres ±) of residual undeveloped land in the community of Red Carpet. The plan area is currently designated Special Purpose – Future Urban Development (S-FUD) District and comprises an undesignated portion of road right-of-way. The associated land use amendment and road closure have been reviewed in conjunction with this outline plan application.

This outline plan proposes a comprehensively designed, mixed-use development that will be serviced by private roads that are connected to adjacent public streets and existing developed communities. A neighbourhood commercial area is located at the main entrance to the proposed development, which will allow for opportunities for local commercial uses on the ground floor of the buildings with opportunities for residential and/or office uses on upper floors. Two multi-residential land use areas are proposed to allow for townhouse and apartment building forms.

The residential districts are expected to accommodate 836 residential units. Given the proposed street design and land use framework, it is anticipated that the lands will develop with street-oriented buildings along a main boulevard through the site and have building with frontages along the main entrance road to establish a gateway into the development. The outline plan also proposes effective pedestrian and cyclist connections within the site and to adjacent communities.

PREVIOUS COUNCIL DIRECTION

None.

ADMINISTRATION'S RECOMMENDATION

2018 January 25

That Calgary Planning Commission **APPROVE** the proposed Outline Plan.

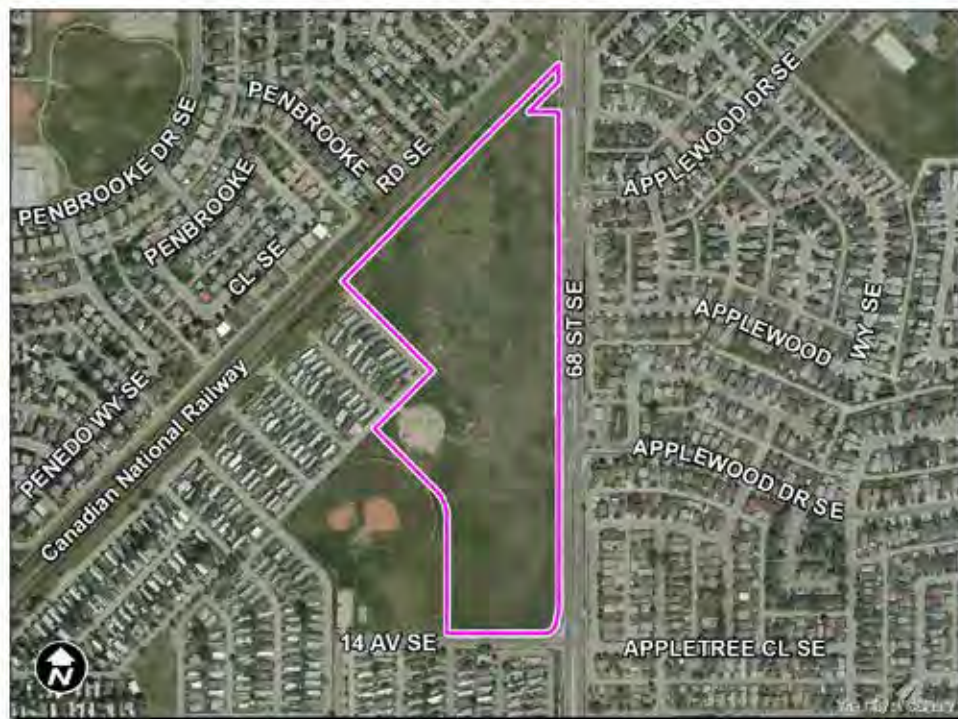
REASONS FOR RECOMMENDATION:

The proposal is consistent with the applicable policies of the Municipal Development Plan (MDP). Supportive public infrastructure is in place, and the site is located in close proximity to public parks and the regional pathway system. The proposed land use district allows for higher density residential development and small scale, local support commercial uses that support the MDP's goals of compact urban form. The plan provides a functional site design, complemented by an effective road network and amenity space system. The area exceeds density requirements, and provides a range of housing types along with commercial areas and amenity space.

OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

LOCATION MAPS



OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION

Recommend that Calgary Planning Commission **APPROVE** the proposed outline plan to subdivide 11.37 hectares \pm (28.09 acres \pm), with conditions (APPENDIX I).

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Applicant:

Casola Koppe

Landowner:

Lansdowne Equity Ventures Ltd
The City of Calgary

Address

809 - 68 Street SE
825 - 68 Street SE
841 - 68 Street SE
901 - 68 Street SE
Closed Road

Legal Description

Plan 3270AG, Block Z
Plan 3270AG, Block 1, Lot 1
Plan 3270AG, Block 1, Lot 2
Plan 1612085, Block 8, Lot1
Plan 0614409, Area A

PLANNING EVALUATION

SITE CONTEXT

The subject lands are located on the west side of 68 Street SE, to the north of 17 Avenue SE, in the community of Red Carpet. The land comprises four parcels, as well as an undesignated road right-of-way that is proposed to be closed as part of the associated land use amendment. The closed road right-of-way will be integrated into the proposed outline plan area. Collectively, these lands make up 11.37 hectares \pm (28.09 acres \pm), with the lands bound:

- to the north by the Canadian National (CN) Railway line and residential community of Penbrooke Meadows;
- to the east by 68 Street SE, and the residential community of Applewood Park;
- to the south by the Mountview Mobile Home Park; and
- to the west by a different Mobile Home Park on lands owned by Lansdowne, as well as Mountview Park, the CN Railway line, and the residential community of Penbrooke Meadows.

The site is relatively flat with little vegetation.

According to data from The City of Calgary 2017 census, the population of Red Carpet peaked in 2006 with a decline of ten percent since that time. The current unit mix in Red Carpet is 44 percent apartments and 56 percent mobile homes. Table 1 below summarizes population trends in the community of Red Carpet.

Table 1: Population Trends in the Community of Red Carpet

| Red Carpet | |
|------------------------------------|-------|
| Peak Population Year | 2006 |
| Peak Population | 1,777 |
| 2017 Current Population | 1,605 |
| Difference in Population (Number) | -172 |
| Difference in Population (Percent) | -10% |

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

LAND USE DISTRICTS

In concurrence with the proposed outline plan is the road closure and land use amendment application that proposes to redesignate the subject lands from Special Purpose – Future Urban Development (S-FUD) District and undesignated road right-of-way to three different land use districts:

- Multi-Residential Contextual Grade-Oriented (M-CGd60) District
 - The proposed M-CGd60 district is intended to allow for multi-residential development within townhouse building forms that have units with direct grade access; and
 - The site would have development potential for a maximum building height of 12 metres (approximately three storeys), and a maximum density of 60 residential units per hectare (up to 526 residential units).

- Multi-Residential High Density Low Rise (M-H1h18d155) District
 - The proposed M-H1h18d155 District allows for multi-residential development in a variety of forms; and a limited range of small-scale, complementary retail and commercial uses on the main floors of multi-residential buildings; and
 - The site would have development potential for a maximum building height of 18 metres (approximately five storeys), and a maximum density of 155 residential units per hectare (up to 279 residential units), with a maximum floor area ratio (FAR) of 4.0.

- Commercial Corridor 1 (C-COR1f3.0h16) District
 - The C-COR1f3.0h16 designated area is intended to allow for small to mid-scale local commercial development with active frontages,
 - The site would have opportunities for commercial uses on the ground floor of buildings, with the option for residential and/or office uses on the upper floors; and
 - The site would have development potential for a maximum building height of 16 metres (approximately three storeys), with a maximum floor area ratio (FAR) of 3.0.

The proposed land use districts and information on density, FAR, and height are summarized in Table 2 below.

Table 2: Proposed Land Use Districts

| Land Use District | Area (ha) | Minimum # of residential units | Maximum # of units | Anticipated # of units | Maximum Height (m) | Maximum FAR |
|-------------------|-----------|--------------------------------|--------------------|------------------------|--------------------|-------------|
| M-CGd60 | 8.77 | - | 526 | 516 | 12 | N/A |
| M-H1h18d155 | 1.80 | 270 | 279 | 270 | 18 | 4 |
| C-COR1f3.0h16 | 0.80 | - | - | 50 | 16 | 3 |
| Total | 11.37 | - | - | 836 | - | - |

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

LEGISLATION & POLICY

South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (2009)

The subject site is located within a Developed Residential - Established Area typology of the Municipal Development Plan (MDP) (Map 1: Urban Structure). Established Areas are comprised of residential communities that were planned and developed between the 1950s and 1990s. They are primarily residential communities containing a mix of low and medium density housing that support commercial retail in relatively close proximity. The road network in these areas is often a blend of modified-grid and curvilinear.

General land use policies for Developed Residential Areas encourage the retention of housing stock and moderate intensification in a form and nature that respects the scale and character of these neighbourhoods. Redevelopment within predominantly multi-residential areas should be compatible with the established pattern of development, consider appropriate transitions between adjacent areas and allow for a variety of residential housing typologies to meet the needs of present and future populations.

Redevelopment should support the revitalization of local communities by adding population and a mix of commercial and service uses with supporting mobility policies to encourage development of high-quality pedestrian and cycling connections and well-integrated parking solutions.

The proposed outline plan supports the MDP in many ways, including:

- Allowing for densification in a readily serviced, developed area (section 2.1.1.g);
- Providing a mix of residential, commercial and service uses (section 3.5.1);
- Optimizing use of existing infrastructure and services (section.2.1.4.a);
- Allowing for development of a similar scale and built form in existing low density areas but with an increased mix of low density housing types (such as Townhouse, Rowhouses). (section 2.2.5.a);
- Respecting the existing low density residential by proposing compatible development and transitioning to multi-residential development (section 2.3.2.b);
- Creating a walkable environment. The outline plan proposes effective pedestrian and cyclist connections within the site and to adjacent communities. (section 3.5.1.f); and
- Allowing for a range of housing types to help stabilize population decline and to support the demographic needs of the population (section 2.2.5.b).

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

This outline plan also addresses key planning issues associated with the development of the site, including site layout, urban design, integration with surrounding community, transportation and engineering issues, connectivity and circulation. The outline plan submitted is consistent with the MDP, section 2.2.5.e, which states that for large development sites over 1.0 hectare in the Developed Area, a comprehensive plan is required.

Local Area Plan - Marlborough Design Brief (adopted by Council 1971, revised 1974)

The subject site is referenced in the Marlborough Design Brief. It should be noted that this Design Brief is presently being reviewed in conjunction with the upcoming report on plans for 2018 local area plan work. The Marlborough Design Brief is a non-statutory plan adopted by Council in 1971 to provide planning policy context for the subdivision and development within the boundary of the Marlborough Sector. The Marlborough Sector is bound by the Trans-Canada Highway, 17 Avenue SE, and Stoney Trail. The majority of this plan area has been developed into the existing communities of Marlborough, Penbrooke Meadows, and Forest Lawn/Forest Heights, with the exception of the subject site. The subject site is recognized as being part of the “remaining uncommitted area in Section 14E” within the plan, which identifies the area as intended for a mobile homes park with local service commercial.

DENSITY

The Marlborough Design Brief recommends a maximum density of 22 units per hectare (8.9 units per acre) in the “remaining uncommitted area in Section 14 E” of the plan. This proposal proposes an anticipated total of 836 residential units. The unit counts equate to an anticipated density of 32.4 units per hectare (13.1 units per acre). The impact of this development on local services and city infrastructure has been considered as part of the application review.

SUBDIVISION DESIGN

The comprehensively designed mixed-use development will be serviced by private roads. A development concept has been established for the plan area to facilitate the review of future subdivision and development permit applications.

Residential Development

Within the outline plan area, there are two land use areas identified for Multi-Residential Development with an anticipated 836 units proposed in townhouse and apartment form. Given the proposed street design and land use framework, it is anticipated that the lands will develop with street-oriented buildings along the main boulevard. A mix of laned and laneless units, with and without private garages are anticipated within the M-CG district. The final design of these multi-residential buildings will be determined at the development permit stage.

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Commercial Development

The commercial area is proposed at an entrance to the site, at the intersection of Applewood Drive and 68 Street SE. The commercial area is approximately 0.8 hectare in size with exposure onto 68 Street SE, providing visibility and access for the future commercial development. The concept for the commercial buildings shows units fronting onto the adjoining streets, with any surface parking in the rear. The C-COR1f3.0h16 district allows for the development of a local grocery store, small restaurants, retail and consumer service uses, and other support commercial uses. There is also potential to locate residential units or offices above the ground floor.

Municipal Reserve

Municipal Reserves were previously dedicated for subdivided parcels over 0.8 hectares in the area. However, 809 68 Street SE (Plan 3270AG) is required to dedicate Municipal Reserve in the amount of 10% of the net developable land or the equivalent as cash-in-lieu. On 2016 September 1, it was determined by the Joint Use Coordinating Committee (JUCC), that this Municipal Reserve land would be dedicated as cash-in-lieu.

Urban Design Review Panel (UDRP)

UDRP reviewed this outline plan on 2017 May 3 and again on 2017 June 28. UDRP comments from Presentation 1 and Presentation 2, together with the applicant's response are provided in APPENDIX VI. The panel recommended further review at the 2017 May 3 meeting with concerns regarding the orientation and frontages of residential and commercial units along the main boulevard and at the gateway location which was dominated by driveways and parking. The applicant provided design updates to address the panel's commentary, which resulted in an endorsement with comments on 2017 June 28. Administration believes that the proposal has effectively addressed the panel's comments. The outline plan recommended for approval is the result of an extensive design exercise that Administration, the Urban Design Team, Urban Design Review Panel, conducted with the applicant to refine the initial design submitted in 2015 June.

TRANSPORTATION NETWORKS

A Transportation Impact Assessment (TIA) was submitted for the proposed land use amendment and outline plan. The scope of the TIA was local, with a focus on adjacent intersections. 68 Street SE is an arterial with a posted speed of 60 kilometers per hour. Applewood Drive SE (North and South leg) and 14 Avenue SE are collector roads with a posted speed of 50 kilometers per hour. The proposed outline plan concept will have three access points, including two along 68 Street SE at Applewood Drive SE north and Applewood Drive SE south, and a third along 14 Avenue SE at the southwest corner of the site. The accesses along 68 Street SE will become the west leg to the existing three-legged intersection. The access along 14 Avenue SE will form the north leg of the intersection with the Mountview Mobile Home Park driveway. To accommodate build out of the site, intersection improvements will be required at the intersection of 68 Street SE and Applewood Drive SE (north and south intersections). Within the development, private roads with custom cross sections are proposed.

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Public Transit

The Applewood Park and Penbrooke Meadows communities are established with access to transit services that provide good transportation options throughout the area, as well as, the rest of the City. Additionally, the site is north of 17 Avenue SE, which is designated as part of the Primary Transit Network with future bus rapid transit.

Active Modes Connections

The proposed site is expected to provide effective barrier free pedestrian connections between the site, 68 Street SE, 14 Avenue SE and 17 Avenue SE. A shared pathway exists along the east side of 68 Street SE and a regional pathway is proposed along the west side, which will provide primary access to the Applewood Park community and 17 Avenue SE. The CN Railway to the north and west of the site proposes the biggest active mode obstacle to any major destination that exist to the west of the site. The plan identifies a pathway network throughout the site, ensuring good pedestrian connectivity and walkability between public spaces (railway crossings, public parks), and land uses.

UTILITIES & SERVICING

Sanitary sewer mains are available to service the site to the north, opposite the CN Railway line, and to the south, along 14 Avenue SE. Water mains to tie into the existing Applewood system are located on Applewood Drive SE in the east, and at the 14 Avenue SE system to the south.

Offsite water mains and sanitary sewer extensions will be required at the development permit stage and will be at the expense of the developer. Storm sewers are presently available to service the site. The subject site is within the boundary of the Western Irrigation District drainage catchment, and is therefore subject to stormwater volume control measures.

A Staged Master Drainage Plan (SMDP) and a Sanitary Servicing Study (SSS) for the plan area have been submitted and approved by Water Resources.

ENVIRONMENTAL ISSUES

Review of this application identified the need to assess concerns related to the railway and to assess potential impacts related to the historical stockpiling of soil on the subject site. In 2015, a Phase II ESA was completed and the soil results from boreholes in the vicinity of the rail line and the stockpiled soil met the applicable criteria. Considering the soil results, groundwater impacts were not suspected and no further investigation was recommended.

**OUTLINE PLAN
RED CARPET (WARD 9)
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MAP 14E

ENVIRONMENTAL SUSTAINABILITY

The proposed outline plan supports the MDP sustainability policies by developing in the Developed Established Area. The site is located near amenities, employment, parks, and transit. The supporting outline plan includes a site layout that provide pathways to encourage walking and cycling within the site and to adjacent communities. Environmental sustainability will also be evaluated by Administration when individual development permits are processed.

GROWTH MANAGEMENT

The subject site is not located within an area covered by an existing Growth Management Overlay. The proposed land use amendment does not require additional capital infrastructure investment and therefore, no growth management concerns have been identified.

PUBLIC ENGAGEMENT

Community Association Comments

There is currently no Community Association established for the community of Red Carpet. Given the site proximity to the Applewood Park and Penbrooke Meadows community, the application was circulated to both of these Community Associations on August 2016. The Penbrooke Meadows Community Association did not provide any comments on this proposal. The Applewood Park Community Association reviewed the initial proposal and provided comments, their comments are included in APPENDIX VII of this report and are summarized as follows:

- Concern with the increase in density and potential traffic increase in the area;
- Question if traffic signals will be installed on 68 Street SE at the north access and timing for this local improvement;
- Concern with the lack of green space available for families;
- Suggested playground or other active programming space for open space for families;
- Question about duration of construction; and
- Question about how transit services will be affected by the proposed development.

Administration also contacted both Community Associations on 2017 August 11 prior to the 2017 August 29 public information session and on 2017 December 11 to determine if they had additional comments on this application prior to development of a recommendation for Calgary Planning Commission. No response was received.

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Citizen Comments

Administration received three letters from adjacent residents. The letters expressed both support for and concerns with the proposed development. The comments are summarized as follows:

- Support for the addition of commercial services within the community and local community services within walking distance;
- Support for the addition of affordable and senior housing;
- Support for the development of 'unused' land;
- The wait time for the signalized intersection at 68 Street and 17 Avenue SE is very long;
- Concerns with potential traffic increase in the area as a result of site development;
- Question if the adjacent mobile home parks will be subject to future redevelopment; and
- Concern with the closure of 68 Street SE and requested additional information on the proposed road closure location.

Public Meetings

On 2017 August 29, Administration held an information session at the Penbrooke Meadows Community Association, to share details of the updated application with community members.

The goals of the information session were to:

- Inform the public of the proposed development and the City's planning processes;
- Answer any questions the public may have about the development, with City representatives and the applicant on hand; and
- Collect feedback from the public about the proposed development through a 'What We Heard Report' There were 68 people in attendance at this session and we received 27 feedback forms.

A communications plan was developed to inform the community about the project and the information session. The methods used to inform the public included:

- Large bold signs placed at high-traffic intersections in the community;
- Post card mail outs to surrounding area residents;
- Twitter and Facebook advertisement campaigns;
- Sending emails to the community members subscribed to our email list, sharing on-going project information and engagement details on the project webpage; and
- Regular and on-going communications with the Community Associations.

**OUTLINE PLAN
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MAP 14E

There were 68 people in attendance at this session and 27 feedback forms received.

What We Heard

Citizens provided a range of comments, with the main themes and areas for concern identified by community members as follows:

- Citizens are concerned that the number of units being proposed is too much density and it will further impact traffic in the area. For example; citizens are concerned by the traffic volume on 68 Street SE as is and feeling that this will worsen.
- Some citizens feel that the proposed heights of the buildings are too high for the area and do not fit in with the surrounding community
- Some citizens would like to see better pedestrian connections through the site and in the surrounding areas
- Some citizens are supportive of future development and feel it will bring revitalization to the area

The What We Heard Report with verbatim comments can be found in APPENDIX VIII.

What We Did

The What We Heard Report from the 2017 August information session, was shared with the applicant and the key changes to the concept plan and responses to community concerns are noted below.

| Issue identified | Applicant response |
|-------------------------|---|
| Pedestrian Connectivity | <ul style="list-style-type: none">· A clear separation has been created between pedestrian and vehicular modes of transportation. Plans have been designed to create clear connectivity into, and through the site. The internal and external connectivity provide an organized system including primary and secondary road network and sidewalks for pedestrian, cycling;· Better walkability and connections provided throughout the site, including a three metre wide pathway on the north side of Ellisborough Boulevard to accommodate cyclists; and· The orientation of built form has been changed to reinforce the streets by fronting the commercial and residential units to the street. |
| Open Space | <ul style="list-style-type: none">· The open space is now more prominent and central in the development. |

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MAP 14E

| | |
|------------------|---|
| Building Heights | <ul style="list-style-type: none">· To address building height concerns, low-density buildings will be placed along the perimeter of the site to create a buffer for the apartment-type buildings. These will also be located further away from the main arteries (68 Street and 14 Avenue SE); and· The applicant's goal is to create a site that will incorporate a variety of housing types, including five storey apartment buildings and two to three storey townhomes, all the while ensuring an appealing flow with the surrounding communities and minimal obstruction of views. |
| Traffic | <ul style="list-style-type: none">· A Transportation Impact Assessment (TIA) was submitted for the proposed development and reviewed by Transportation Planning; and· To accommodate build out of the site, intersection improvements will be required at the intersection of 68 Street SE and Applewood Drive SE (north and south intersection). |

APPENDIX I

CONDITIONS OF APPROVAL

Planning:

1. Prior to approval of the first tentative plan or development permit, an application for street names shall be approved by Addressing and Data Services.
2. This outline plan does not approve any building locations. Building locations will be determined at the Development Permit stage.
3. Residential development with habitable area shall be setback a minimum of 30 metres from the railway property line.
4. A concurrent development permit will be required if a strata subdivision or bareland condominium is applied for.
5. Consolidation of the parcels will be required prior to applying to strata subdivide the site and prior to the release of any development permit application for the site.
6. Gated communities are not permitted in multi-residential land use districts, in accordance with the General Rules for those districts in the Land Use Bylaw 1P2007. Future development shall maintain direct access to public roads.
7. In conjunction with future phased development permit application(s), a master site plan shall be submitted for the entire site to demonstrate a comprehensive approach to future potential development showing the proposed features, including:
 - a) building footprints;
 - b) major uses;
 - c) parking areas;
 - d) internal roads;
 - e) servicing between parcels;
 - f) access/egress points;
 - g) pedestrian pathway system;
 - h) open space and landscaping; and
 - i) such other information determined necessary by the Development Authority.

The master plan shall be submitted with the first development permit application and updated and resubmitted, as required by the Development Authority, with each subsequent development permit application.

OUTLINE PLAN
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MAP 14E

Development Engineering:

8. This subject plan area is within the boundary of the Western Irrigation District (WID) drainage catchment and subject to stormwater volume control measures. Average annual runoff volume is limited to a net zero increase. Low Impact Development and stormwater source control is recommended. The allowable discharge rate is to be determined by the Staged Master Drainage Plan (SMDP) report.
9. The proposed Watermains are to tie into the existing Applewood system (at either of the two locations on Applewood Drive), in the east and at the 14 Avenue SE system to the south as shown in the drawing below. Watermain extensions will be at the cost of the developer. Watermain connections are as indicated on the attached plan.



10. Sanitary sewer mains are available to service the site. Stages 1, 2, 3, 9, and 10, are to connect to the north at MH#1, to the 250mm main. A CN Rail crossing agreement will be required, and at Development Permit stage, construction must be completed under an Indemnification Agreement with the City.

**OUTLINE PLAN
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MAP 14E

Stages 4, 5, 6, 7 and 8 can make connection to sanitary sewers to the south at MH #21, to the 250 mm main as requested by the applicant. At Development Permit stage, construction must be completed under an Indemnification Agreement with the City.

Sewer main extensions will be at the cost of the developer.

The calculations are based on the following assumptions: The sanitary servicing (>375 mm) was evaluated by Development Planning with an estimate ΔQ_{PWW} of 22.8 L/s to the north and 10.5 L/s to the south based on unit estimates supplied by the Applicant for each of the stages.

There are downstream constraints in the 1050mm Penbrooke Trunk along 60 ST SE and along Hubalta Road SE. The Penbrooke Trunk upgrade is planned for these locations from 2019-2023 so it would be beneficial to coordinate occupancy with this upgrade.

Sanitary sewer main connections are as indicated on the attached plan.



11. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.
12. Separate service connections to a public main shall be provided for each proposed lot (including strata lots).

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

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13. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, execute a Development Agreement. Contact the Subdivision Development Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email urban@calgary.ca.
 14. Off-site levies, charges and fees are applicable.
 15. The owner of the subject site shall make satisfactory cost sharing arrangements with Macalgary Dev. (Alberta) Inc. for part cost of the water main in 68 Street SE adjacent to the site which was installed by Macalgary dev. (Alberta) Inc. through their Applewood, Phase 1 (1989-040) subdivision.
 16. The owner of the subject site shall make satisfactory cost sharing arrangements with Daon Development Corporation Ltd. for part cost of the existing utilities in 68 Street S.E. adjacent to the site, which was installed by Daon Development Corporation Ltd. through their Applewood Park, Phase 1 (1980-049) subdivision.
 17. The owner of the subject site shall make satisfactory cost sharing arrangements with Uniflo Holdings & Dev. Ltd. for part cost of the underground infrastructure and surface improvements in 14 Avenue S.E. adjacent to the site which was installed by Uniflo Holdings & Dev. Ltd. through their Mountview Mobile Home Park (1973-037) subdivision.
 18. The Applicant will remit payment to the City for half collector standard road in 68 Street S.E. along the east boundary of the Development Area.
 19. The Owner of the subject site shall make a repayment to The City of Calgary for part cost of surface improvements in 14 Avenue SE along the south boundary of the development area.
 20. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
 - a. Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
 - b. Construct the underground utilities and surface improvements within the boundary of the plan area.
 - c. Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

**OUTLINE PLAN
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WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Transportation:

21. At the first tentative plan stage, intersection design adjacent to 68 Street, road dedication for right-of-way stubs and curb returns for the signalized intersections are required as per COC Roads design standards for the arterial road standard.
22. Future tentative plans and development permits will indicate the minimum drive aisle distance from 68 Street (Arterial Street) to the next intersection within the site. It is to be achieved at both the north and south proposed intersections adjacent to Applewood Drive SE, and the appropriate design for the access to the site from 14 Avenue SE.
23. Transportation Planning recognizes that the internal road network is intended to be a private road system. With any tentative plan or development permit application related to these lands, plans will be required to indicate the appropriate operation of the private roads and maneuvering capabilities for all Emergency, Waste and Recycling Services, commercial service and client/owner vehicles. This will be required at all dead-end streets, to and from the site at all approved access points and is recommended to be to Complete Streets standards.
24. The configuration of the road network would only be appropriate if the site remains as one parcel or becomes a bareland condo. If a conventional or strata subdivision is proposed in the future, public roads will be required.
25. At the development stage, and should the applicant choose to provide the network as a private road system, access easements may be required to connect to the adjacent parcels for pedestrian and vehicular continuity.
26. Transit service as required to serve the future development will be designed and constructed to the satisfaction of Transportation Planning and at the expense of the developer. The developer /owner will also provide a transit service statement to indicate the method of connecting service to the private site and network.
27. Prior to approval of the first tentative plan or at development permit stage, a noise analysis is to be submitted to and approved by Transportation Planning for the residential development adjacent to 68 Street SE.
28. Prior to approval of the first tentative plan or development permit, performance securities and payment for improvements will be required. Costs associated with driveway accesses (Applewood North, Applewood South & 14 Avenue), signals, sidewalks and transit amenity, pedestrian connections and associated transportation infrastructure will be at the expense of the developer.

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

Costs for the modifications and upgrade to adjoining intersections and roadways will be required as noted but not limited to:

- Signal infrastructure upgrade (100% cost contribution for the 4th leg) at both north and south intersections of 68 Street and Applewood Drive SE.,
- Driveway access Signal infrastructure upgrade (33% cost contribution) for 68 Street and 14 Avenue SE.,
- Turning lanes, road markings, and signage adjacent to the public right of way and roadway connections to the subject development area.

Parks:

29. No backsloping from private lots onto adjacent Municipal Reserve lands will be permitted.
30. Storm water or other drainage from privately-owned parcels onto the Municipal Reserve space parcel is not permitted. Any unauthorized drainage from private parcels onto adjacent park / open space parcels must be resolved to the satisfaction of Calgary Parks and Water Resources and any damage resulting from such drainage will require restoration at the developer's expense. Resolution of drainage issues must be approved by the Parks Development Inspector. Contact the Development Inspector at (403) 268-5325 for an inspection.
31. Construct all Regional Pathway routes within and along the boundaries of the plan area according to Parks' *Development Guidelines and Standard Specifications – Landscape Construction* (current version), including setback requirements, to the satisfaction of Calgary Parks.

OUTLINE PLAN
 RED CARPET (WARD 9)
 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX II

OUTLINE PLAN DATA SHEET

| | HECTARES | ACRES |
|-----------------------------|----------|-------|
| GROSS AREA OF PLAN | 11.37 | 28.08 |
| LESS: ENVIRONMENTAL RESERVE | 0.00 | 0.00 |
| LESS: LAND PURCHASE AREA | 0.00 | 0.00 |
| NET DEVELOPABLE AREA | 11.37 | 11.37 |

| LAND USE (Residential) | HECTARES | ACRES | # OF LOTS | # OF UNITS (FOR MULTI RESIDENTIAL) |
|---------------------------|----------|-------|-----------|--|
| M-CGd60 | 8.77 | 21.67 | - | 516 |
| M-H1h18d155 | 1.80 | 4.44 | - | 270 |
| Total Residential | 10.57 | 26.12 | - | 836 |
| C-COR1f3h16 | 0.80 | 1.98 | - | 50 |

| | HECTARES | ACRES | % OF NET AREA |
|--------------------|----------|-------|---------------|
| ROADS (CREDIT) | 0.00 | 0.00 | 0.00 |
| PUBLIC UTILITY LOT | 0.00 | 0.00 | 0.00 |

| RESERVES* | HECTARES | ACRES | % OF NET AREA |
|-----------|----------|-------|---------------|
| MR | 0.00 | 0.00 | 0.00 |
| MSR | 0.00 | 0.00 | 0.00 |

* 809 68 ST SE (total area 1.131 hectare) owes reserve of 10% of the net developable area. This MR land will be dedicated as cash-in-lieu.

APPENDIX III

APPLICANT'S SUBMISSION



#240, 5010 Richard Road SW
Calgary T3E 6L1

December 18, 2017

Christine Leung
Senior Planner
South Team, Community Planning

Subject: Ellistown Development: An Introduction from the Developer

On behalf of Slokker Canada West, I'd like to thank you for the time and attention you have provided to the Ellistown Development and for the considered team comments shared by Corporate Planning and Applications Group (CPAG).

While we have provided an updated outline plan and a comprehensive response in the attached submission, we also wanted to include an overview of the enhancements we have made to the original plan. These changes are a result of our ongoing consultation with the City, public engagement with the Applewood and Penbrooke Community, and our desire to make a positive contribution, vitalizing an important area in east Calgary.

The right mix of housing and amenities

As with any development, we know it is important to find a reasonable balance between density and amenities in a manner that is appropriate and suitable within the greater context of community and neighbourhoods. Since there is an extensive stock of medium to high density condos in the surrounding area, Slokker Canada West has reworked the original high-density proposal for this site. This earlier proposal of 912 homogeneous apartment style units with 80,000 square feet of retail space has now been revised as a more balanced mixed unit design consisting of a greater variety of housing types, and a smaller neighbourhood-centric retail area.

Within the revised outline plan you will find the proposal is for predominantly low to medium density town homes with some complementary medium to high density apartment-style condos. We have also greatly reduced the retail footprint. The current proposal provides for 836 units and 20,000 square feet of retail with opportunities for additional housing and/or professional space above.

With an overall density set slightly above 71.42 UPH, the overall density is respective of neighbouring parcels while providing a housing type with a level of affordability not readily available in this area.

The retail component is important as it will promote walkable communities and provide essential amenities for the neighbourhood – while not duplicating what is available at the nearby East Hill development.

OUTLINE PLAN
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MAP 14E

Enhancements improve walkability and neighbourhood character

Slokker Canada West worked through suggestions from the City to develop the following design enhancements to the proposed area:

1. Moving the commercial area so that it has exposure to 68th Street. This provides better accessibility and allows for more desirable streetscapes in the interior spaces
2. Creating street oriented homes within the site that enhances walkability of the site
3. The current plan calls for a village-style community that enhances the overall neighbourhood character. Predominantly townhomes, Ellistown will draw homeowners to the area, creating a community aspect that has been lacking.
4. Elliston Park and the nearby baseball diamond are community gems. The enhancements to the Ellistown plan will create a more usage of these Calgary facilities and contribute to healthy living in the area.

A commitment to quality and design excellence

At Slokker Canada West, we are committed to delivering quality that meets or exceeds current standards. Our homes in Ellistown will reflect the quality standards of our past projects. As per the City's requirements, these townhomes will respect existing community character while fostering distinctiveness. We are pleased that our projects showcase modern color schemes and quality exterior finishes.

(Please see Attachment A – Slokker Canada West Past Projects.)

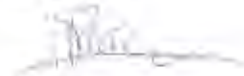
Honouring the heart of the community

We are creating a vibrant family community at Ellistown. In fact, we are working with a theme that focuses on families – from the beautiful town homes to the overall plan, including the pedestrian connection and the central area of the village connecting to commercial amenities, to the baseball diamond and nearby Elliston Park.

To help share our family focus theme, we want to honour the Ellis family – a family who have lived in this part of Calgary since the original Shepard S. Ellis homesteaded in 1912. Five generations of the Ellis family have lived in and around Elliston Park since then. Because of the family's rich history with Calgary and the surrounding area, we want honour the Ellis family.

Thank you for taking the time to read this introduction. We are pleased to present the updated outline plan to the Corporate Planning and Applications Group (CPAG).

Respectfully,



Peter Paauw
President
Slokker Canada West



Committed to building inviting communities

At [Slokker Canada West](#) we are proud of the quality homes we've built in the Calgary area. The examples shown here highlight the nature of the homes we would build in the Ellistown Development.

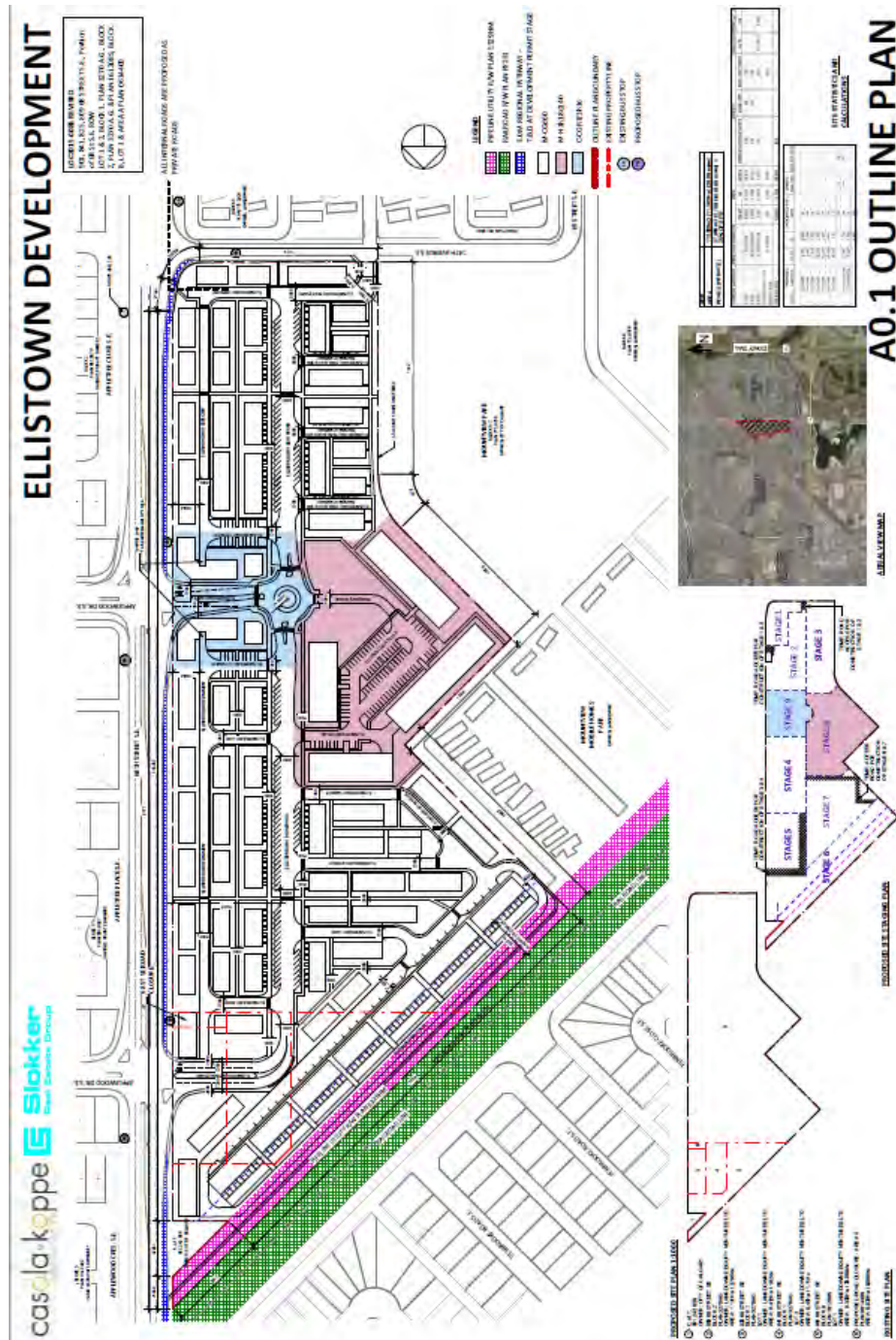


OUTLINE PLAN
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 WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

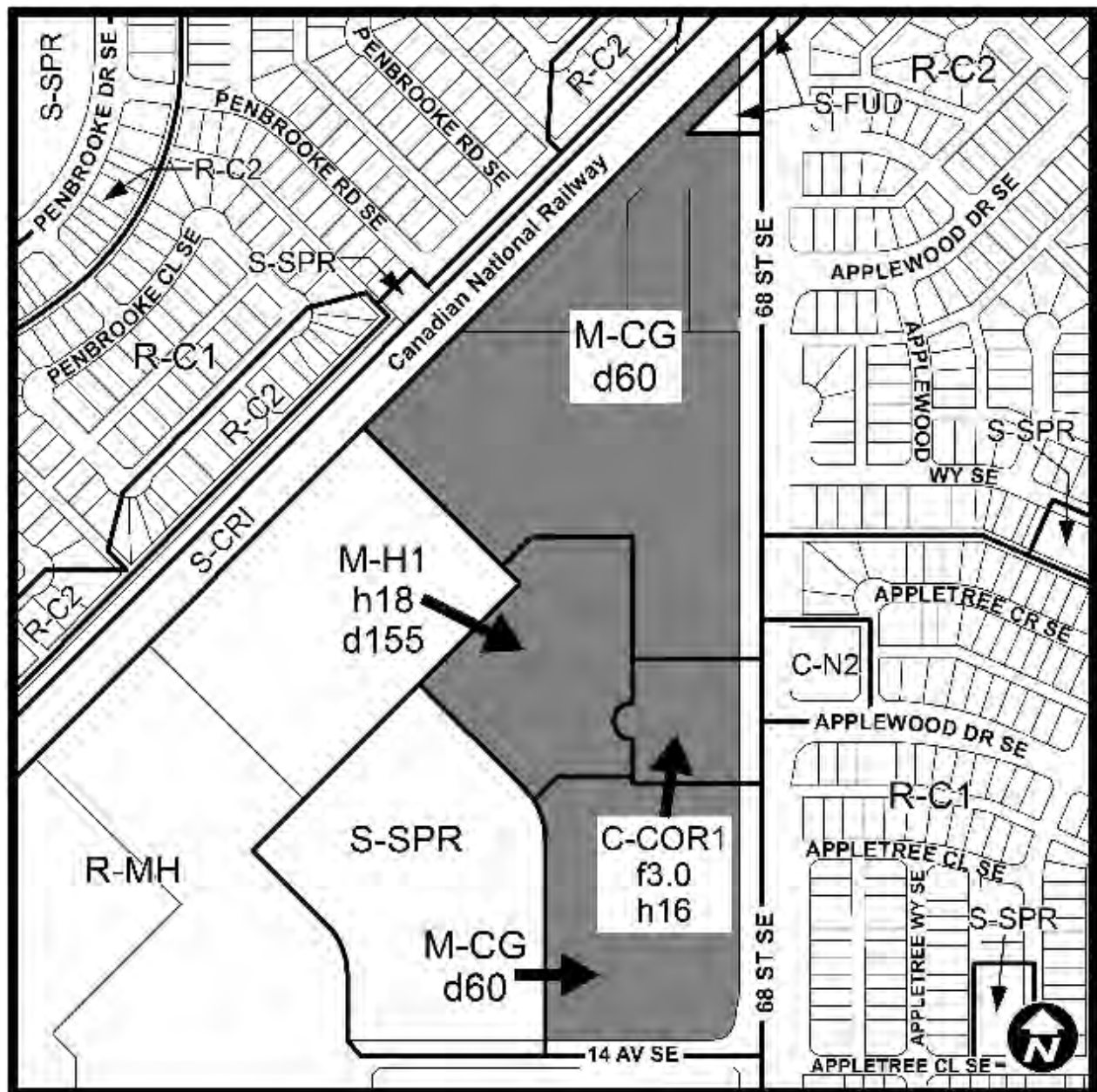
APPENDIX IV

OUTLINE PLAN



APPENDIX V

PROPOSED LAND USE DISTRICT MAP



**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

APPENDIX VI

URBAN DESIGN REVIEW PANEL COMMENTS AND APPLICANT'S RESPONSE

Date: May 3, 2017 and June 28, 2017
Application number: LOC2015-0085
Municipal address: 809, 825, 841, 901 68 Street SE and undesignated road right-of-way adjacent 825, 841, 901 68 Street SE
Community: Red Carpet
Project description: Land Use Amendment, Outline Plan, Road Closure
File Manager: Christine Leung
City Wide Urban Design: Lothar Wiwjorra
Applicant/Architect: Casola Koppe Architect
Owner: Lansdowne Equity Ventures Ltd., City of Calgary
Overall Ranking: Support with Comments – UDRP believes the second review with the applicant has contributed to an improved outcome

General Comments

UDRP appreciated the opportunity to provide initial and second review input to this project at the land use application stage. Administration indicated initial concerns around four main issues including: sound attenuation along 68 Street, walkability, pedestrian vehicle connections to adjacent neighbourhoods and landscaping. UDRP shares Administration's concern about walkability and open space planning along with effective and elegant traffic calming outcomes that support all modes sharing the public realm. Specifically, UDRP's concerns regarding the lack of hierarchy for the street and pathway system and variety of housing products within the development have been addressed appropriately. Key points addressed are the following: Orientation of built form that reinforces the streets by fronting units to the street. This includes fronting retail and housing onto the main entry drive (Ellisborough Gate), reorienting units to face the primary residential street (Ellisborough Boulevard) from the traffic circle on both sides of this street, so that front doors, porches address this main residential street, and include more diverse housing design along this street. Park spaces initially felt residual, now occur more prominent and central in the development. These should be designed to achieve intentional connectivity, use and design to encourage use, and to improve the environment for housing fronting them. Finally, more information is required on specific dimensioning of the public realm from face of building to street that identifies the green space reserve, street tree realm, scale of sidewalks, reserve for patio or other private open space fronting streets etc. There remains a general concern that not enough space is available to provide for the landscape public realm that is illustrated in the precedent images. Please refer to our responses (initial Presentation#1 and subsequent Presentation#2) to categorized comments below:

| Urban Vitality Contribution | | | |
|------------------------------------|--------------------------------|--|---|
| | Topic | Best Practice | Ranking |
| 1 | Retail Street Diversity | Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of streets. Space for patios and cafe seating is provided. | Do not support (Presentation #1) Support with comments (Presentation #2) |
| UDRP Presentation #1 Commentary | | | |

**OUTLINE PLAN
RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

MAP 14E

| | | |
|----------|--|--|
| | <p>The orientation of the retail buildings does not form a sense of entry or "main street". The proposal would be strengthened by having the buildings front the main entry road and traffic circle with rear parking. This retail should be a focus for residents of the area to walk with fronts facing the sidewalks.</p> <p>The current layout has the retail placing a back or side face to the primary circulation. Further, the applicant is requested to seek opportunities to enhance the central gathering area surrounding the CRU's by providing space for patios and café seating. The applicant should provide dimensional study that shows the available sidewalk widths to be preserved, area for street trees, patios etc. to ensure an active street and walkable pedestrian zone.</p> | |
| | Applicant's Response | |
| | <p>The retail buildings have been orientated to address the main entry street as well as the boulevard. Rear parking has been provided with access off the boulevard. Retail and restaurant space will front on to these roads with patios and café seating where appropriate to create an interface with pedestrians. Dimensions have been provided to show the widths of the sidewalks and planters.</p> | |
| | UDRP Presentation #2 Commentary | |
| | <p>UDRP agrees that the retail buildings have been orientated to address the main entry street as well as the boulevard. Rear parking has been provided with access off the boulevard. Retail and restaurant space will front on to these roads with patios and café seating where appropriate to create a walkable interface. Dimensions have been provided to show the widths of the sidewalks and planters.</p> | |
| 2 | Retail street transparency and porosity | <p>Retail street maximizes glazing - 70% and more. Maintains view into and out of retail, avoids display-only windows.</p> |
| | | <p>Support with comments (Presentation #1 and #2)</p> |
| | UDRP Presentation #1 Commentary | |
| | <p>At the DP stage, the Panel would like to see more details regarding the retail frontage area and how it interfaces with 68 Street at Applewood Drive.</p> | |
| | Applicant's Response | |
| | <p>More detail will be provided at DP stage.</p> | |
| 3 | UDRP Presentation #2 Commentary | |
| | <p>See above</p> | |
| | Pedestrian-first design | <p>Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.</p> |
| | | <p>Support with comments (Presentation #1 and #2)</p> |
| | UDRP Presentation #1 Commentary | |
| | <p>At the DP stage, the Panel would like to see more details regarding the public realm along the west side of 68 Street. More specifically, the Panel would like to see details of how the development will enhance transit and pedestrian connectivity along 68 Street SE.</p> | |
| | Applicant's Response | |
| | <p>More detail will be provided at DP stage. Additional pedestrian access points have been created along 68 Street and 14 Avenue to break up the travel distance to transit stops.</p> | |
| | UDRP Presentation #2 Commentary | |
| | <p>More detail will be provided at DP stage. Extra pedestrian access points have been created along</p> | |

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RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE**

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| | 68 Street and 14 Avenue to break up the travel distance to transit stops. UDRP has stated that safe crossings where internal streets and sidewalks intersect, best practice outcomes should accommodate an optimal experience for all modes in the shared public realm of the development. | | |
| 4 | Entry Point Definition/Legibility | Entry points are clear and legible | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | The Panel is interested in reviewing how the applicant designs the main entry from Ellisborough Gate and applies best practices in urban design to clearly define this as the primary entry point. | | |
| | Applicant's Response | | |
| | Landscaping, building orientation, building design and massing will help to define the entrance into the development. The retail buildings are a different typology to the rest of the 68 Street frontage, which is an advantage in achieving this as an entry point. More detail will be provided at DP stage. | | |
| | UDRP Presentation #2 Commentary | | |
| | UDRP agrees the landscaping, building orientation, building design and massing now helps to better define the entrance into the development. The retail buildings are a different typology to the rest of the 68 Street frontage, which is an advantage in achieving this as an entry point. More detail will be provided at DP stage. | | |
| 5 | Residential Multi-Level Units at grade | Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use. | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | Provide dimensional information on the space available from building face to the street, complete with reserve for outdoor patios, landscaping, sidewalk, street trees to ensure an active, walkable and enjoyable public zone. | | |
| | Applicant's Response | | |
| | Site cross sections have been provided on outline plan drawing A0.3 SITE DETAILS. | | |
| | UDRP Presentation #2 Commentary | | |
| | Site sections have been provided for review. | | |
| 6 | At grade parking | At grade parking is concealed behind building frontages along public streets. | Do not support (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | The prevalence of garage doors along the primary street face will result in cars parked on private drives in front of these garage doors – and a resulting "parking lot" feel to the main streets. For this reason, the panel would like an alternate design that places the garages along back lanes rather than along main streets. | | |

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| Applicant's Response | | | |
| The garages have been replaced by pedestrian entrances that lead off a wide sidewalk creating a more interactive edge to the boulevard. Landscaped planters separate the sidewalk from angled street parking. | | | |
| UDRP Presentation #2 Commentary | | | |
| The garages have been replaced by pedestrian entrances that lead off a wide sidewalk creating a more interactive edge to the boulevard. Landscaped planters separate the sidewalk from angled street parking. The layout of street parking must accommodate a sustainable and healthy green canopy in the streetscape. | | | |
| 7 | Parking Entrances | Ramps are concealed as much as possible. Entrances to parking are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first. | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | Provide information on below grade parking entrances for multi-unit residential that complies with the best practice noted. | | |
| | Applicant's Response | | |
| | Approximate entrances to below-grade parking have been shown. A Large parkade will service all four blocks and will most likely need two entry/exit points for vehicles. They have been located away from the visitor parking. More detail will be provided at DP stage when the buildings have been designed and an accurate parking calculation has been done. | | |
| | UDRP Presentation #2 Commentary | | |
| Approximate entrances to below-grade parking have been shown. A Large parkade will service all four blocks and will most likely need two entry/exit points for vehicles. They have been located away from the visitor parking. More detail will be provided at DP stage when the buildings have been designed and an accurate parking calculation has been done. | | | |
| 8 | Other | unused | |
| Urban Connectivity <i>Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian-first environments.</i> | | | |
| | Topic | Best Practice | Ranking |
| 9 | Transit Station Connections | Supports transit use via legible, dedicated pedestrian pathways to bus stops with direct routes. Avoids desire lines / shortcutting through parking areas. | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| The Panel would like more details on how the site will provide strong pedestrian connections to current transit service in the area along 68 Street and 14 Avenue. | | | |

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| Applicant's Response | | | |
| <p>Apart from the two main site entrances along 68 Street, there are three other pedestrian access points along this edge. These form part of the network of pedestrian pathways that circulate throughout the site. These five access points will allow for easy access to the transit service provided along 68 Street. By having these access points, the distance to the transit service is shortened or made more direct. The same idea has been applied to 14 Avenue, where there are two access points that will allow for easy access to the transit service. Upgrades to the existing transit stops will be made and the City is requiring new transit stops to be provided by the Developer along 68 Street. Exact locations will be determined at the DP stage.</p> | | | |
| UDRP Presentation #2 Commentary | | | |
| <p>UDRP recognizes the plan the change that apart from the two main site entrances along 68 Street, there are three other pedestrian access points along the 68 Street edge. These form part of the network of pedestrian pathways that circulate throughout the site. These five access points will allow for easier access to the transit service provided along 68 Street. By having these access points, the distance to the transit service is shortened or made more direct. The same idea has been applied to 14 Avenue, where there are two access points that will allow for easy access to the transit service. Upgrades to the existing transit stops will be made and The City is requiring new transit stops to be provided by the Developer along 68 Street. UDRP understands exact locations will be determined at a later stage.</p> | | | |
| 10 | Regional Pathway Connections | <p>Supports walkability via intentional urban design connections to pathway systems</p> | <p>Potential (Presentation #1)</p> <p>Support with comments (Presentation #2)</p> |
| | UDRP Presentation #1 Commentary | | |
| | <p>Provide detail on how the development will develop a pedestrian and cycling connection to the pedestrian overpass on the south western portion of the development. The panel requests that this connection be continuous along the southwest edge and connect to 68 street as well as to the overpass. There are to be defined connections from within the development to this pathway to promote walkability and cycling. As the overpass provides access to adjacent neighbourhoods, schools and recreational facilities, this pathway is an important feature to be addressed as a primary consideration.</p> | | |
| | Applicant's Response | | |
| | <p>A pathway running along the South West perimeter of the site has been provided. There are three pedestrian access points along this path that connect with the neighbouring park. This path extends along the North West perimeter of the site, along the railway, to connect to 68 Street at the North corner of the site. Other internal pedestrian paths link to this path to enable walkability and a network of paths to cycle on. The pedestrian access points along this path are located on the most direct route linking to the pedestrian overpass.</p> | | |
| UDRP Presentation #2 Commentary | | | |
| <p>A pathway running along the South West perimeter of the site has been provided. There are three pedestrian access points along this path that connect with the neighbouring park. This path extends along the North West perimeter of the site, along the railway, to connect to 68 Street at the North corner of the site. Other internal paths link to the pathway network facilitating walking and cycling.</p> | | | |

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| | The pedestrian access points along this path are located on the most direct route linking to the pedestrian overpass. | | |
| | Cycle Path Connections | Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade. | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | See comments above regarding connections to overpass over the railway tracks and to 68 Street. Future development of the project is to demonstrate bicycle storage facilities. Provide bicycle racks at park spaces within the site. Indicate how safe cycling is provided for along all streets within the development. | | |
| 11 | Applicant's Response | | |
| | Bicycle stalls will be provided at DP stage along with more detailed landscape drawings which will show stalls located at the park. Main pedestrian routes are wide enough for the safe passing of pedestrians and bicycles. | | |
| | UDRP Presentation #2 Commentary | | |
| | UDRP understands that bicycle stalls will be provided at DP stage along with more detailed landscape drawings which will show stalls located at the park. Main pedestrian routes are wide enough for the safe passing of pedestrians and bicycles. | | |
| | Walkability - connection to adjacent neighbourhoods/ districts/ key urban features | Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | The development should provide pedestrian connections with the adjacent development to the southwest. The applicant should also explore options to provide a pedestrian connection along the railway line to connect to the overpass (possibly along the pipeline ROW). This would provide a stronger connection to GW Skene school and St. Peter elementary school which are within walking distance of the site. | | |
| 12 | Applicant's Response | | |
| | Pedestrian connections have been created along the West property line to connect to the neighbouring park as well as to the most direct route to the overpass. Pathways have been provided running parallel to the railway line from 68 Street as well as from 14 Ave, along the property line to two pedestrian connection points. Pathways along the boulevard and the main site entrances also enable pedestrians from 68 Street or Applewood Drive to reach these connection points. | | |
| | UDRP Presentation #2 Commentary | | |
| | Pedestrian connections have been created along the West property line to connect to the neighbouring park as well as to the most direct route to the overpass. Pathways have been provided running parallel to the railway line from 68 Street as well as from 14 Avenue, along the property line to two pedestrian connection points. Pathways along the boulevard and the main site entrances also enable pedestrians from 68 Street or Applewood Drive to reach these connection points. | | |

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| | Pathways through site | Provide pathways through the site along desire lines to connect amenities within and without the site boundaries. | Support with comments (Presentation #1 and #2) |
| UDRP Presentation #1 Commentary | | | |
| The Panel would like more details regarding the pathways between the townhomes. More specifically, the Panel would like to understand how the applicant is going to promote these spaces as public spaces and ensure that the space is activated. Experience has shown that the narrow dimension presented will result in this space feeling proprietary and the pathways not being used. Notes elsewhere in this review ask that the typology be reversed and to make the main streets the primary pedestrian path with the rear of the housing be laneways with parking access. | | | |
| Applicant's Response | | | |
| 13 | In an effort to minimize the amount of asphalt surface on the site, we have not introduced rear parking lanes. The rear pathways between the buildings have been made wider and now connect to individual entrances into private gardens and patios creating a more distinct and inviting path of travel. Trees, shrubs and grass will line these pathways and where they cross over, small amenity nodes will be established. These will be focal points along the path with benches, trellis structures, interesting paving, planters, small water features and other such landscaping furniture/features. Although the boulevard has been designed to be the central spine for both vehicles and pedestrians, it will mainly service pedestrians who are passing through the site or using the retail/ commercial facilities, or the residents of the development who are catching public transport on a daily basis. The rear pedestrian pathways will provide a more park-like experience for people to enjoy leisurely activities such as dog-walking, running, riding bikes, for children to walk along or play around, away from vehicular traffic. They are to be places of moderate community interaction rather than simply channels for pedestrian traffic. These two pathways play different roles. By providing rear lanes for garages or parking, the opportunity for place-making and community is diminished. Lanes, in this case, become dead edges and separate the site into a mere grid for the domination of vehicles. | | |
| UDRP Presentation #2 Commentary | | | |
| UDRP recognizes that, in an effort to minimize the amount of asphalt surface on the site, the applicant has not introduced rear parking lanes. The rear pathways between the buildings have been made wider and now connect to individual dwelling entrances into private gardens and patios creating a more distinct and inviting path of travel. Trees, shrubs and grass will line these pathways and where they cross over, small amenity nodes will be established. These will be focal points along the path with benches, trellis structures, interesting paving, planters, small water features and other such landscaping furniture/features. Although the boulevard has been designed to be the central spine for both vehicles and pedestrians, it will mainly service pedestrians who are passing through the site or using the retail/ commercial facilities, or the residents of the development who are catching public transport on a daily basis. The rear pedestrian pathways will provide a more park-like experience for people to enjoy leisurely activities such as dog-walking, running, riding bikes, for children to walk along or play around, away from vehicular traffic. They are to be places of moderate community interaction rather than simply channels for pedestrian traffic. The two pathways play different roles. By providing rear lanes for garages or parking, the opportunity for placemaking and community is diminished. Lanes, in this case, become dead edges and separate the site into a grid, primarily serving vehicles | | | |

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| | Open Space Networks and Park Systems | Connects and extends existing systems and patterns. | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | The Panel believes there are opportunities to improve the design of open spaces within the development. Emphasis should be placed on providing a more central gathering space with some thought around how the space can be activated. | | |
| | Applicant's Response | | |
| 14 | The park has been relocated to be closer to the centre of the site in a more prominent location. It is easily accessible for residents of the development as well as for neighbouring residents. All pathways along the boulevard and main site entrances meet at the traffic circle and the park. Patios from the retail/commercial buildings as well as from the multi-storey residential buildings will have views of this open landscaped space. The park is to be the main gathering area for the development. Pathways will run through the park, trees, shrubs and flower beds will help to define the open grass space while providing shade for seating, and a playground will encourage family activity. This park will provide opportunity for other informal activities such as yoga on the lawn or even community social events. Other open spaces, such as the smaller amenity nodes will be designed to accommodate smaller gatherings. These will be defined by landscaping and furniture such as benches and tables under trellis structures. | | |
| | UDRP Presentation #2 Commentary | | |
| | UDRP supports the central, more prominent park-like open space. It will be more convenient for residents and the surrounding community. All pathways along the boulevard and main site entrances meet at the traffic circle and the park. Patios from the retail/commercial buildings as well as from the multi-storey residential buildings will have views of this open landscaped space. The park is to be the main gathering area for the development. Pathways will run through the park, trees, shrubs and flower beds will help to define the open grass space while providing shade for seating, and a playground will encourage family activity. This park will provide opportunity for other informal activities such as yoga on the lawn or even community social events. Other open spaces, such as the smaller amenity nodes will be designed to accommodate smaller gatherings. These will be defined by landscaping and furniture such as benches and tables under trellis structures. | | |
| | Views and Vistas | Designed to enhance views to natural areas and urban landmarks. | Uncertain (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | More information required. | | |
| | Applicant's Response | | |
| 15 | Although the site is flat, the varying heights of the different building typologies will allow for views of immediate areas within the development. Units along the boulevard will have views onto the landscaped boulevard as well as on to the rear landscaped pathways and private gardens. Units along the neighbouring Mountain View Park will enjoy views onto the park and the baseball diamond, as will many of the units in the multi-residential buildings. The Multi-residential buildings will enjoy views onto the internal park and may even be able to glimpse Ellistone Lake. The impact of the surface parking for these buildings will be minimized by the use of landscaping and trees. | | |

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| | UDRP Presentation #2 Commentary | | |
| | Although the site is flat, the varying heights of the different building typologies will allow for views of immediate areas within the development. Units along the boulevard will have views onto the landscaped boulevard as well as on to the rear landscaped pathways and private gardens. Units along the neighbouring Mountain View Park will enjoy views onto the park and the baseball diamond, as will many of the units in the multi-residential buildings. The Multi-residential buildings will enjoy views onto the internal park and may even be able to glimpse Ellistone Lake. The impact of the surface parking for these buildings will be minimized by the use of landscaping and trees. | | |
| | Vehicular Interface | | Support (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| 16 | The applicant is proposing a housing product on 68 Street with garages and entry ways on the ground floor with living space on the second floor and above. This may preclude the need for sound attenuation along 68 Street. The Panel supports concepts that eliminate the need for sound attenuation and creates more attractive streetscapes. | | |
| | Applicant's Response | | |
| | None. | | |
| | UDRP Presentation #2 Commentary | | |
| | No further comment necessary. | | |
| 17 | Other | unused | |
| Contextual Response <i>Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities</i> | | | |
| | Topic | Best Practice | Ranking |
| | Massing relationship to context | Relationship to adjacent properties is sympathetic | Do not support (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | The applicant is asked to focus the massing such that it reinforces the streets within the development. Front face to primary street, minimize gaps in the street face. | | |
| 18 | Applicant's Response | | |
| | Buildings on the West side of the boulevard have been turned to face the primary street to address this, minimizing gaps in the street face. Space between the buildings will be determined at DP stage when limiting distance calculations have been done. Areas which remain open or present a wider gap in the street face are designed for the landscaped pedestrian pathways. | | |
| | UDRP Presentation #2 Commentary | | |
| | Buildings on the West side of the boulevard have been turned to face the primary street to address this, minimizing gaps in the street face. Space between the buildings will be determined at DP stage when limiting distance calculations have been done. Areas which remain open or present a wider gap in the street face are designed for the landscaped pedestrian pathways. | | |

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| 19 | Massing impacts on Sun Shade | Sun Shade impacts minimized on public realm and adjacent sites | Uncertain (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | More information to be provided as project develops. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| No further comment. | | | |
| 20 | Massing orientation to street edges | Building form relates / is oriented to the streets on which it fronts. | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | The Panel recommends that the applicant review the orientation of housing along Ellisborough Boulevard to provide more frontage as noted in this review. | | |
| | Applicant's Response | | |
| | Buildings on the West side of the boulevard have been turned to face the street. The units along the boulevard have front pedestrian entrances facing the street. Buildings that address the landscaped pedestrian pathways that come off the boulevard have end units with front entrances on to the boulevard. | | |
| | UDRP Presentation #2 Commentary | | |
| Buildings on the West side of the boulevard have been turned to face the street. The units along the boulevard have front pedestrian entrances facing the street. Buildings that address the landscaped pedestrian pathways that come off the boulevard have end units with front entrances on to the boulevard. | | | |
| 21 | Massing Distribution on Site | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | Generally this is a low scale development with intensified density at the entry and retail areas. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 22 | Massing contribution to public realm at grade | Building form contributes to a comfortable pedestrian realm at grade | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| The Panel believes that more work is needed on Ellisborough Boulevard through the development to create a more walkable environment with more landscaping and a variety of housing types. | | | |

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| Applicant's Response | | | |
| By removing the garages from the units that front on to the boulevard, more defined pedestrian pathways have been created. Apart from pedestrian crossings over the road, vehicles will remain separate to the pedestrians along the boulevard. The pathways have been widened and now give way to individual front entrances to each unit as well as more landscaping directly in front of each unit. Between the street parking and the pathways, landscaped planters have been provided to enhance the pedestrian experience. There will be at least four different building types facing the boulevard. The majority of the buildings along the East side of the boulevard will be 2 or 3-storey townhomes with varying colour palettes and materials. The retail/commercial buildings will be a maximum of 3-storeys in height and will provide an alternative frontage. Other two and three storey townhomes, different in design from the fore mentioned townhomes, will also front on to the boulevard. Five storey multi-residential buildings towards the centre of the boulevard will form another alternative frontage onto this street. | | | |
| UDRP Presentation #2 Commentary | | | |
| By removing the garages from the units that front on to the boulevard, more defined pedestrian pathways have been created. Apart from pedestrian crossings over the road, vehicles will remain separate to the pedestrians along the boulevard. The pathways have been widened and now give way to individual front entrances to each unit as well as more landscaping directly in front of each unit. Between the street parking and the pathways, landscaped planters have been provided to enhance the pedestrian experience. There will be at least 4 different building types facing the boulevard. The majority of the buildings along the East side of the boulevard will be 2 or 3storey townhomes with varying colour palettes and materials. The retail/commercial buildings will be a maximum of 3storeys in height and will provide an alternative frontage. Other 2 and 3storey townhomes, different in design from the fore mentioned townhomes, will also front on to the boulevard. 5Storey multiresidential buildings towards the centre of the boulevard will form another alternative frontage onto this street | | | |
| 23 | Other | unused | |
| Safety and Diversity <i>Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.</i> | | | |
| 24 | Topic | Best Practice | Ranking |
| | Safety and security | CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances. | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | Improving the overall open space hierarchy will assist in improving the overall CPTED principles. | | |
| | Applicant's Response | | |
| | None | | |
| UDRP Presentation #2 Commentary | | | |
| As noted in previous review. | | | |

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| 25 | Pedestrian Level Comfort - Wind | Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions. | No comments from UDRP |
| 26 | Pedestrian Level Comfort - Snow | Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions. | No comments from UDRP |
| 27 | Weather Protection | Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages. | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by applicant and reviewed by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 28 | Night Time Design | | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by applicant and reviewed by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 29 | Barrier Free Design | Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps. | No comments from UDRP |
| 30 | Winter City | Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity. | Potential (Presentation #1) Support with comments (Presentation #2) |
| | UDRP Presentation #1 Commentary | | |
| | To be provided with later building designs. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |

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| 31 | Other | unused | |
| Service / Utility Design <i>Promote design that accommodates service uses in functional and unobtrusive manner. Place service uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.</i> | | | |
| | Topic | Best Practice | Ranking |
| 32 | Waste / Recycling | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by the applicant and review by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 33 | Enmax (Power) / Atco (Gas) | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by the applicant and review by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 34 | Transformer / Switchgear | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by the applicant and review by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |
| 35 | Exhaust / Intake | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by the applicant and review by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| As noted in previous review. | | | |

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|----|--|--------|---|
| 36 | Electrical Vaults | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | No information provided at this stage. To be developed by the applicant and review by Urban Design. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| | As noted in previous review. | | |
| 37 | Loading | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | Better definition of where loading activities would occur for the CRU's is needed at the DP stage. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| | As noted in previous review. | | |
| 38 | Fire Truck Access | | Support with comments (Presentation #1 and #2) |
| | UDRP Presentation #1 Commentary | | |
| | Private roads are proposed throughout the development. There were some concerns from Administration regarding the proposed cul-de-sacs with no turn-arounds. The Panel does not view this as an issue as long as the proposed design meets the fire department's requirements. The reduced amount of asphalt respects some of the City's environmental/sustainability goals. | | |
| | Applicant's Response | | |
| | None | | |
| | UDRP Presentation #2 Commentary | | |
| | As noted in previous review. | | |
| 39 | Other | unused | |

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RED CARPET (WARD 9)
WEST OF 68 STREET SE AND NORTH OF 17 AVENUE SE

MAP 14E

APPENDIX VII

COMMUNITY ASSOCIATION LETTER

From: Steve [mailto:steve_ayrheart@gmail.com]
Sent: Sunday, August 14, 2016 9:00 PM
To: CPAG Circ
Cc: Andrew Sutherland; Janet Rivard; lhooper@shaw.ca; Ron Motteram; Sherry Desjardins; Bob Rivard; Deborah Loewen
Subject: Request for comment on application. File Number LOC2015-0085. Comments from Applewood Park Community Association.

To: Circulation Control
Planning, Development & Assessment # R201 Regarding File Number LOC2015-0085.

The Applewood Park Community Association board has the following comments and concerns regarding the development application with the above file number.

The board is concerned with how the increased population density will affect traffic on 68 Street and will there be traffic lights installed on 68 Street at the north entrance / exit into and out of Applewood Park. Will the lights be installed before construction on the project begins?

We would anticipate that many of the residents of Ellstone Village will be young families and therefore we are concerned about the amount of green space available and how much of it will be dedicated to playground space with playground equipment for children as well as soccer fields and baseball diamonds. There is at this time a fairly heavy usage of the playground facilities in Applewood Park with many of the people using the facilities coming from out of the area.

What will be the duration of the construction period?

How will busing be affected, particularly the 305 express and the 45?

Thank you very much for your time.

Sincerely,
Steve Ayrheart
President Applewood Park Community Association

APPENDIX VIII

WHAT WE HEARD REPORT



Ellis Town Land-use and Outline Plan Application

Stakeholder Report Back: What We Heard
September 2017

Project overview

The City of Calgary received a proposal (file number LOC2015-0085) from Cosola Koppe Architects, representing the developer and landowners, to redesignate (rezone) the subject site.

The subject site is an 11.4 hectare parcel of residual undeveloped land in the community of Red Carpet. The property abuts the west side of 68 Street SE, in the area north of 17 Avenue SE.

The proposal is to change the allowable uses and development form for this property and close a portion of an adjacent road right-of-way. An Outline Plan showing the concept plan for the development is included as part of the application. The application allows for:

- Under the M-CG District (8.8 hectares), a maximum of 494 townhouses limited to 12 metre in height (about 2-3 storeys);
- Under the C-COR1f3.0h16 District (0.7 hectares), mixed-use and commercial buildings (e.g. commercial storefronts with apartments or offices above), up to 16 metres (about 4 storeys);
- Under the M-H1f3.0h18d215 District, apartment buildings (276 units) up to 18 metres (about 5 storeys); and
- the uses listed in the proposed C-COR1, M-H1 and M-CG designations.

Engagement overview

On August 29, 2017 the City held an information session at the Penbrooke Community Association, to share details of the application with community members and project members and the developer were on hand to answer any questions they had. There were 68 people in attendance.

What we heard

At this event, attendees were given the opportunity to share their thoughts on the application with the project team by filling out a comment form. We received 27 completed comment forms.

Citizens provided a diversity of comments and the main themes and areas for concern identified by community members were as follows:

- Citizens are concerned that the number of units being proposed is too much density and it will further impact traffic in the area. For example; citizens are concerned by the traffic volume on 68th Street and feel that this will worsen.
- Some citizens feel that the proposed heights of the buildings are too high for the area and do not fit in with the surrounding community



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- Some citizens would like to see better pedestrian connections through the site and in the surrounding areas
- Some citizens are supportive of future development and feel it will bring revitalization to the area
- ▶ For a verbatim listing of all the input that was provided, please see the [Verbatim Responses](#) section.

Next steps

Citizen feedback provides Administration and Council with valuable local knowledge of the community and the proposed development area. Administration will be sharing this report with the applicant and once Administration is ready to make their recommendation for the application, this input will also be used in reports provided to Calgary Planning Commission and City Council.

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Verbatim Comments

Content is captured as it was provided by citizens. No edits have been made unless there was personal information or offensive language which is removed with an indication that this has happened.

| |
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| Know City wants densitybut ? - this is too much. 2 to 4 stories only. Have no 5 storeys in the area. Senior population growing - stairs are hard to climb. Develop some unique places with 1 floor townhomes or with 2 suites, one for a caregiver (NOT a renter). Consider your commercial area through the perspective of a "gathering" place so programming can occur. Need some tot lots as I assume families are also the target. Make sure lots of parking underground & safe for children. Do not like the word "burrough" - too English, we are very cultural diverse. Please keep me informed [personal info removed]. |
| I hope this development goes ahead, so long as the density is not too high. |
| Signalize the intersection of 68th St and 14th Ave. Intersection is unsafe for everybody, including cars, transit, buses. Plus parking along 14 Ave - makes roadway too narrow, like a one lane roadway. |
| 68 St traffic is a serious problem now at such hours. If this is built, traffic will be a real serious problem. I don't see any plans to help (ie. Widening 68 St, extend Memorial drive then to Stoney or put under 17 Ave. No senior facilities ie. Housing. No special bus lanes from 17 ave into Ellistown. |
| Feel anything over 4 storeys high will not fit into the area & will be out of place. Traffic flow on 68th St can be heavy now anytime of day. Increased traffic will just add to the problem. If turning left from abbercove DR SE onto 68th is hard enough now. Feel that the number of proposed units is way too high for this area. Need to consider schooling for families in all those units. |
| There is no doubt that the field could use something put there however adding more people in a tight space is a little ridiculous. Why not a sportsplex or a library? Why not take care of the people in the existing communities? That piece of land next to a park could be used more wisely, again promote healthy living build an ice rink, skate park, rock wall. Somewhere people can go without having to travel far from home, send their kids and feel safe knowing where they are. As well a road on 14th is unwise with how many people park on that road nowadays why are you trying to make a situation worse. Money may make our world spin but isn't time to take care of what's existing instead? So putting low income housing is not what any of the local residents want. Put it out by East Hills. We do not want or need crime put into our neighbourhood. Another issue is the fact that we have Global Fest, not enough security this year. With tons of people having lack of respect for the people in their homes now add more? You all need to sit and rethink the use of this space! |
| I think it is a Great Idea & very overdue! The weed filled eye-sore does nothing to promote our neighbourhoods. A big park would have been great, but ideally condos, retail & green space makes sense. I do hope green space is visible more than buildings but still nice to see something being done! |
| Any buildings should not exceed the height of present buildings adjacent to or across from them. Other than that - have at er! |

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| <p>This is a much needed development - beautiful actually and it would make living in the NE/SE feel less like we are "the poor end of the city". But, would someone <u>please</u> extend the sidewalk on the north side of 17 Ave SE so a person can walk, bikes, use their motorized scooter etc. to go from Applewood, Ellisborough, the trailers parks to Easthills Mall without walking in mud or being bogged down where one sidewalk ends and another one begins.</p> |
| <p>It seems really nice. My only concern is how accurate. The pictures are showing. Flat roofed condos, In our winter climate a sloped roof is preferred.</p> |
| <p>Looks okay so far.</p> |
| <p>Would like to see some single family stand alone houses. Not liking the idea of 5 story units, would prefer nothing over 3 stories.</p> |
| <p>Traffic on 68 St is congested now. Adding all these units will create even reater problem. I say no to the project. Resolve the 68 St first and then go for this proposal.</p> |
| <p>Can see a lot of traffic conjection already on 68 E to 17 to Eastport shopping - Costco, Walmart, banks, Theater. Only 1 left turn at light on 68 & 17 so at rush hour lots of traffic now. Will be much more conjected with another 900 or so people.</p> |
| <p>We live in Penbrooke trailer park. Some of the concerns we have is there are only one entrance intot he park on 17 Ave SE at 68 St SE. If there was ever an emergency we would have major concerns. We also need lights at the corner of 17 Ave SE at 62 St. It would be nice if they would open up 14 Ave SE into our park or off 68 St SE.</p> |
| <p>We don't need more people in the area it will be crowded we thought it is going to be comercial (like Superstore or No Frills) & would suggest a senior community type (Bungalow style villas)</p> |
| <p>Worn idea. No no no building. I need commercial or build something for old people like bungalow. Not 2000 pepole in the area to crowded for this area.</p> |
| <p>Very worried about increased traffic on 14 Ave & 65 St. 14 Ave - is a major bus route and already there are many people parking on both sides during the day. Makes road a little narrow, even worse in winters when there are snowbanks. During both rush hours there is an increase of traffic cutting through on both 65 St & 14 Ave.</p> |
| <p>Live at [personal info removed] limited access to property is restricted to one way off of 17 Ave next 7-11. I have had three neighbors in car accidents this year alone! What can be done mayor's office and area councillor don't car.</p> |
| <p>Looking forward to new neighbours!</p> |
| <p>I, as a resident of Applewood, I strongly oppose the rezoning of land. The fact that existing structures (ie trailer park) will be an eyesore to future development I believe is what is pushing this review ahead. That, in itself, is a shameful situation. The fact of the matter is there are many, many units already located w in a small radius. As it is, 17th Avenue is cluttered with condominiums, & I am mindful of the further condo development that will be undertaken shortly east of both street. How many condos are too many?? I believe the space should be left zoned as is. We do need another 1,300 units w in such a small area. I do not mind stating that any decision in favour of this rezoning will impact my vote @ election time.</p> |

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| Not happy - need less population - more green space. Amount of traffic on 14 Ave is very concern. ? Needs to everyone - more studies are needed! No access to 14 ave / traffic lights on 14 flow of traffic needs to improve. |
| Really needs more planting & is lacking in looks - at least the complexes on 17 Ave across from the dump have better appeal. This looks like I expect - ghetto. Enough of cementing everything. |
| I am a resident of Heritage Estates. I have one concern, about foot traffic or automobile traffic, increasing due to new project. I do not wish to see extra folks in my neighbourhood that do not live there. I feel there would be a lack of respect for the homes and property of the residents. Please consider separating the two developments. Thank you. |
| I think Mountview Mobile Home park will make a nice neighbour. |