

# BOW TO BLUFF URBAN DESIGN FRAMEWORK

29 FEBRUARY 2012





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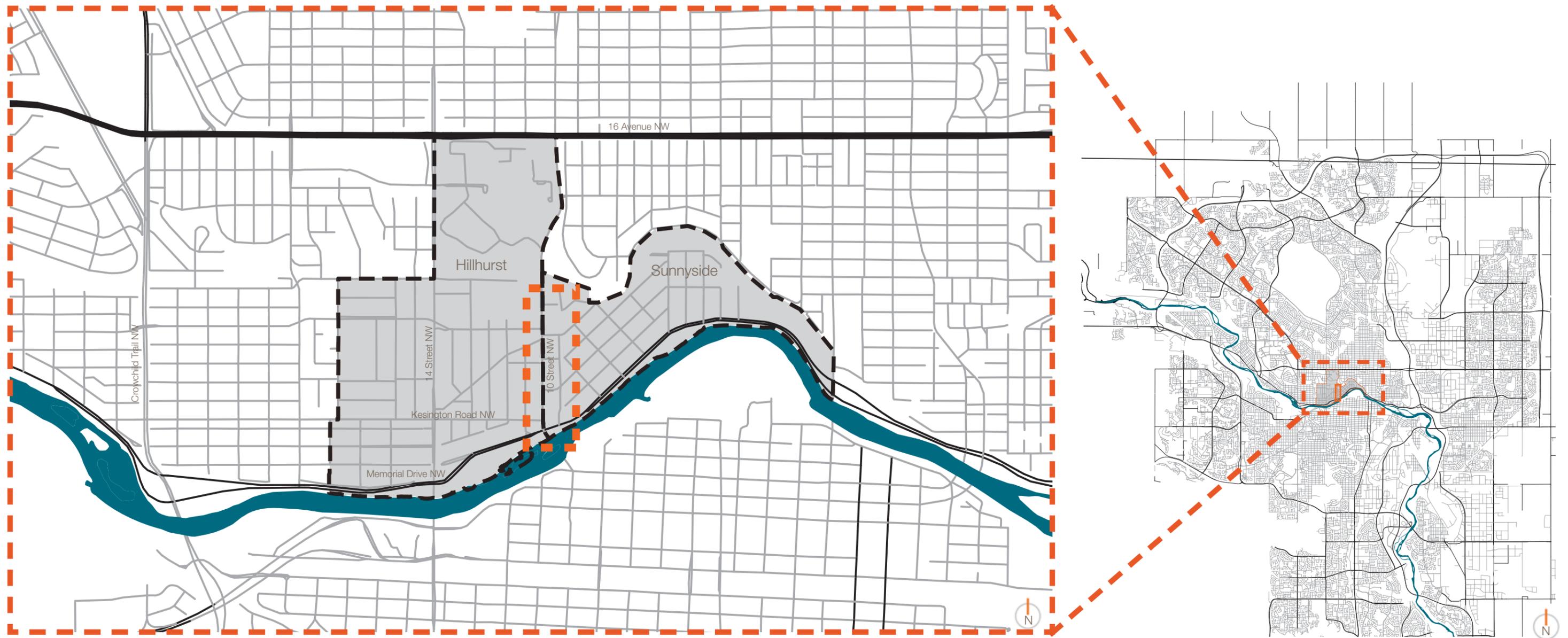


Figure 1.1 Location Map

*“Bow to Bluff is an independent, citizen-led initiative to transform the public corridor along the Sunnyside LRT line between the Bow River and McHugh Bluff into a great public space. Bow to Bluff proposes an innovative model of citizen-based engagement in Calgary that is positive, collaborative, and inclusive of all stakeholders. This project is a partnership of volunteer citizens from the Great Public Spaces (GPS) group of Civic Camp and the Hillhurst-Sunnyside Community Association (HSCA).”*

Bow to Bluff RFP, July 2011

*“According to the 2006 Census, Hillhurst-Sunnyside is made up of 4,630 households. The approximately 467 potential new dwellings planned or proposed within the TOD area represents an 8% increase in total number of households in the community. As the average number of persons per household in Hillhurst-Sunnyside is 1.8, this new TOD growth could represent 841 new community residents. This assessment would not include intensification outside of the ARP TOD policy area.”*

Bow to Bluff RFP, July 2011

## 1.0 INTRODUCTION

### 1.1 Project Purpose

‘The Bow to Bluff (B2B) – Hillhurst-Sunnyside LRT Corridor Revitalization Initiative’ provides an overarching, long-term citizen centered vision and functional framework to guide future development throughout the Bow to Bluff corridor.

As part of the Urban Design Framework for Revitalization of the Bow to Bluff corridor, the Design Team has created a set of ‘priority projects’ necessary for the corridor to be experienced as a functional and cohesive space. The implementation of these projects will not only respond to the primary community aspirations expressed during the consultation process but will contribute to the development of a livable, thriving and community-oriented corridor.

### 1.2 Planning Context

The current Area Redevelopment Plan (ARP) for the Hillhurst-Sunnyside Community details the land use and built form regulations for new development in the neighbourhood. Within the ARP document higher level specificity is present to define the context within the Bow to Bluff corridor; outlining any unique qualities or differences that are present within the Bow to Bluff corridor specifically. Within the Corridor, many unique relationships and desires have arisen during the Bow to Bluff consultation process, and this has largely directed most of the recommendations in this document. Creating a unique and cohesive corridor language (through park planning, pedestrian streets, artwork and furnishings), vegetation strategy, and the boundary along the LRT line will help unify the corridor as a distinctive public space within the Hillhurst-Sunnyside community, and as an important connector within the inner city.

### 1.3 Site Context

#### Location

The Bow to Bluff corridor consists of a series of public spaces that run along the Sunnyside LRT line from Memorial Drive to McHugh Bluff in the Hillhurst-Sunnyside neighbourhood of Calgary, Alberta. Figure 1.1 illustrates the site location within the Hillhurst-Sunnyside community and the broader Calgary context.

#### Project History

Bow to Bluff is a citizen-based initiative that will create great public spaces through innovative stakeholder engagement processes and placemaking activities.

In 2011, Bow to Bluff was awarded a grant from The City of Calgary’s Council Innovation Fund for its proposal to develop an inclusive and creative citizen-centered engagement process to serve as a blueprint for other placemaking projects in Calgary. Part of the funding has been used to prepare this Design Framework. This public funding has created the opportunity for establishing innovative and creative design solutions for implementing the goals of all citizen users and stakeholders of the Bow to Bluff corridor.

Over the next few years, the Bow to Bluff corridor will undergo major changes: Calgary Transit will extend the LRT platform to accommodate four-car CTrains and install a power substation; Battistella Developments is currently developing a mid-rise medium-density (101 unit) condominium building on 2nd Avenue and 9A Street; the City-owned warehouse on 2 Avenue and 9 Street has been demolished, and the site may be redeveloped, and; The City will implement public realm improvements, such as road narrowing and sidewalk widening along 2nd Avenue between 9A and 10th Streets.

Due to its location and connectivity, the Bow to Bluff corridor has enormous potential to transform into a major pedestrian and cycling connector, interspersed with attractive and active public spaces. As redevelopment in this area is already underway, it is critical that reinvestment in the public realm is coordinated and cohesive. This Urban Design framework sets that strategy.

#### Community Involvement

The Urban Design Framework for revitalization of the Bow to Bluff corridor focuses on the desires and concerns communicated to the Design Team and the Resources and Planning Committee (RPC) during the community engagement process. This process extended throughout the months of October and November of 2011. This included an open storefront where individuals were welcome to enter and provide their comments and concerns about the corridor, Sounding Boards; which were strategically located throughout the corridor and allowed for feedback, a mobile phone line for feedback via text messages, on-line surveys, and two interactive community design charrettes. Members of the Design Team, the RPC, and volunteers were represented at all of these events.

Meeting the needs of the community members individually and as a whole required that all stakeholders needed to understand the concerns and potential solutions in the same terms and at multiple scales. This was the purpose for the variety and length of the engagement process.

The variety of opportunities for individuals to participate in the engagement process allowed for over 2,500 members of the public to contribute over 2,000 different ideas of what they felt would allow for a great Bow to Bluff corridor.

## 1.4 How to Use This Document

The Urban Design Framework provides an overall framework and broad design guidelines for future development within the corridor. This Framework is intended to be used by developers (when preparing land use and development permit applications within the corridor), The City of Calgary (when addressing future public realm improvements and when reviewing developer applications), and, the Hillhurst-Sunnyside Community Association (when commenting on land use rezoning and development permit applications). Local residents and citizens at-large should also use the framework to understand the overall vision and champion implementation, particularly as it relates to flexible or temporary programmatic uses that activate and improve the quality of experience for all users of the corridor.

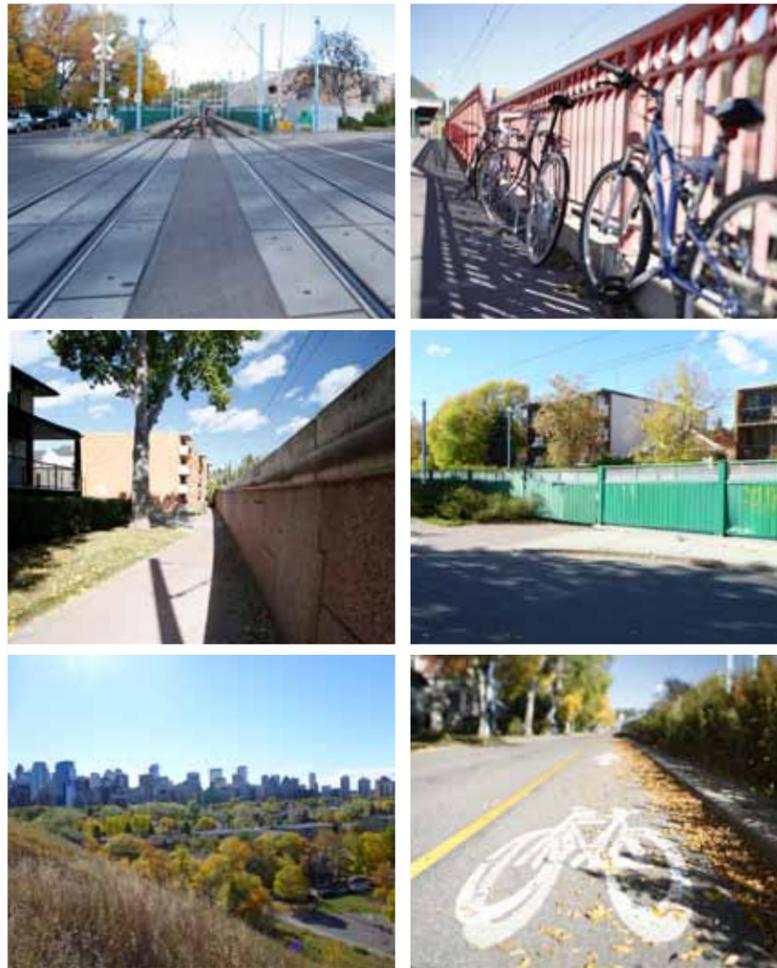
This Urban Design Framework does not seek to replace the council approved Hillhurst-Sunnyside Area Redevelopment Plan (ARP). Instead it is the next step in advancing the ARP's successful implementation.

Developing a set of common strategies or guidelines will help municipal elected officials, City of Calgary staff, developers and the public to understand, communicate and implement the shared goals and aspirations of the community. Further detailed design is required to reach this goal, and this is identified in Section 5.0 -Implementation.

This document should be used in conjunction with its companion document, the Bow to Bluff Process Guide. The Process Guide, produced by the Bow to Bluff Resources and Planning Committee (RPC), documents the entire citizen-led engagement process and history of the Bow to Bluff project. It is intended to be used by the City and future citizen groups as a new model for positive, collaborative and inclusive public engagement.



Images from the community engagement process



Site Constraint Observations

## 2.0 SITE ANALYSIS

The site analysis section covers the current conditions that exist within the corridor.

### 2.1 The Current Situation

An analysis of the Bow to Bluff corridor was undertaken through a number of site visits and community walking tours. This provided an informed view of the present day situation, and was verified through community comments throughout the engagement process.

The pocket parks and tenuous pathways comprising the Bow to Bluff corridor are isolated, shabby, neglected and unloved. As a result, they are havens for illicit activities such as drug trafficking, theft and antisocial behavior, instead of safe, usable places for legitimate community activities.

Despite the lack of defined uses within the corridor, the spaces are still well used by the following user groups:

- Pedestrians travelling to 10th Street NW to use the existing community facilities.
- Cyclists using the corridor as a key route to the river and its bike pathways.
- People using the Sunnyside transit station.
- Existing residents.

### 2.2 Population Data

According to the 2006 Census, Hillhurst-Sunnyside is made up of 4,630 households. The approximately 467 potential new dwellings planned or proposed within the Transit Oriented Development (TOD) area represents an 8% increase in total number of households in the community. As the average number of persons per household in Hillhurst-Sunnyside is 1.8, this new TOD growth could represent 841 new community residents. This assessment would not include intensification outside of the ARP TOD policy area.

### 2.3 Constraints Summary

A number of site constraints have been identified as potential hurdles for future development. The challenges include:

- The existing LRT line as a visual barrier to East-West movement.
- Memorial Drive as an obstacle for North-South pedestrian and cycle movement.
- The steep change in topography at McHugh Bluff.
- A lack of defined uses for the triangular pocket parks.
- Poor quality pedestrian connections throughout the area.
- Insufficient bike routes through the area.
- Poor quality and undefined public realm.
- A lack of 'active edges' along key routes such as the Safeway building and the LRT line.

*"Create comfortable spaces for family gathering"*

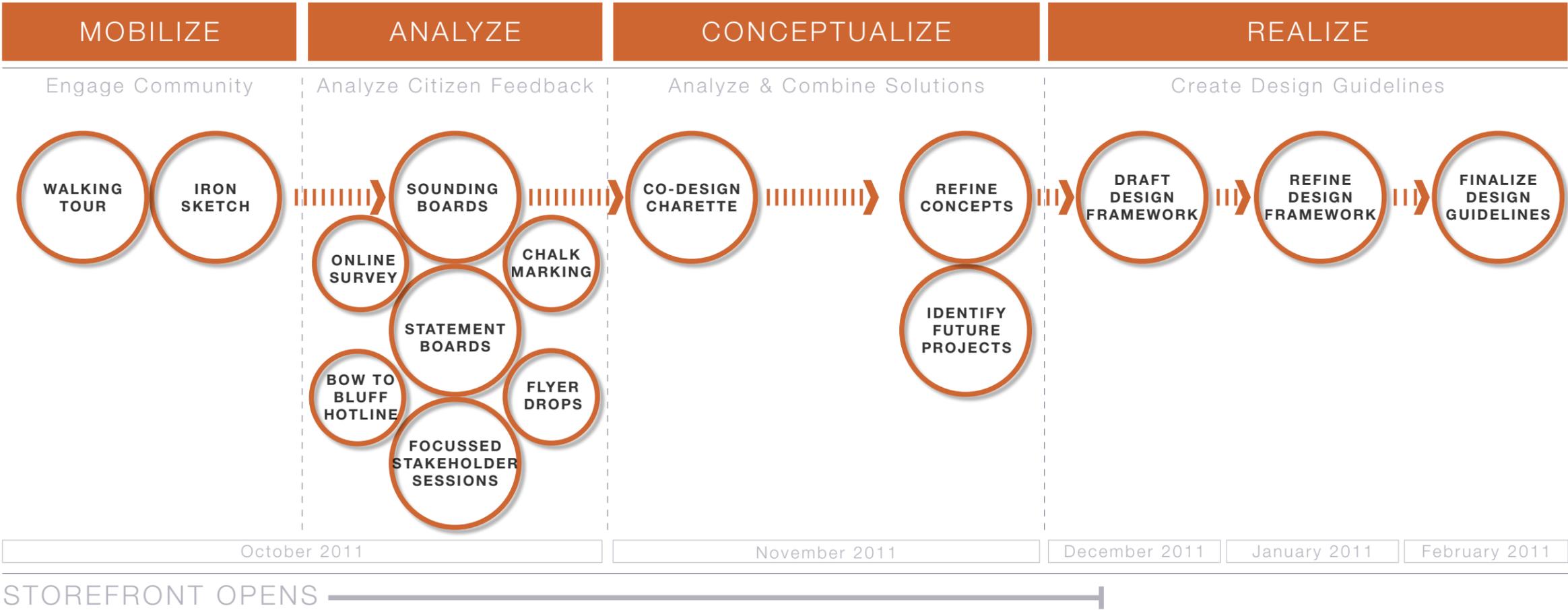
Bow to Bluff Community Participant, October 2011

*"Community spaces for people to gather and share with each other"*

Bow to Bluff Community Participant, October 2011

*"More places and spaces for kids!"*

Bow to Bluff Community Participant, October 2011



### 3.0 PUBLIC ENGAGEMENT

This section discusses the stages and elements that contributed to the public engagement component of the Bow to Bluff project. The comments from these engagements are the main contributors to the corridor design concept.

*“Perhaps a place to sit down? I want to sit and read a good book while I’m waiting but it is not possible”*

Bow to Bluff Community Participant, October 2011

### 3.1 Program Development

The Bow to Bluff process of engagement was designed to provide as many entry points into the conversation as possible. Using a variety of innovative approaches, citizens were asked their views on the corridor as it exists today and what they would like to see in the future. Full details of the public engagement process can be found in the Urban Design Framework’s companion document, the Bow to Bluff Process Guide.

### 3.2 Design Workshops

The initial round of engagement featured:

An engagement storefront - a dedicated physical space that was open several evenings and weekends, extending opportunities for the community to contribute their ideas face to face with an individual.

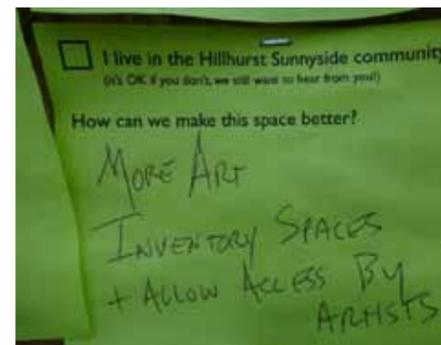
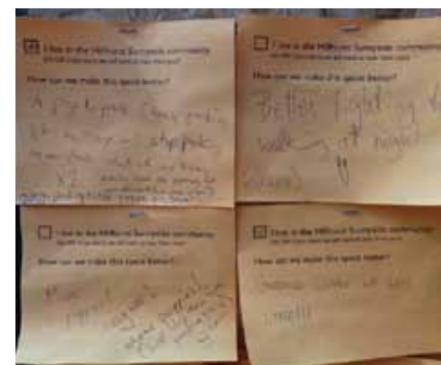
Community “sounding boards” - a series of boards within the corridor, allowing citizens to share their ideas in the space.

Opportunity for input through web surveys, texting, email, Twitter, and Facebook.

The feedback solicited in this initial 3-week period informed the development of a series of design problems. These problems highlighted the common themes and areas of tension that were heard during the initial engagement period and created the focus for a codesign workshop held in early November. At this workshop, citizens and stakeholders collaborated with the Design Team to develop initial design concepts for the future of the Bow to Bluff corridor. This initial work was then further developed and refined by the Design Team over the period of two weeks.

In late November, a second workshop was held to solicit feedback on the initial design concepts for the corridor. Using an interactive open house format, participants were able to review the concepts, provide feedback and sit down and sketch with O2 designers to refine the ideas presented.

The end result has been one of the most successful place-based engagement processes in Calgary’s history, with over 2,500 citizens being engaged in just 7 weeks, generating over 2,000 individual ideas.



Sticky notes on display in the storefront and in the corridor

### 3.3 What We Heard

Through the community engagement process eight core design elements emerged that should form the foundation of any public realm improvements in the Bow to Bluff corridor.

These elements include:

- Improving circulation and connectivity within and through the corridor with a strong emphasis on creating a pedestrian priority over the automobile.
- Creating a clear and cohesive streetscape design that blends the lines between people space and vehicular space.
- Creating year round recreational amenities in the park spaces throughout the corridor.
- Developing usable community gathering and adaptable spaces within the open and green spaces in the corridor.
- Updating the existing LRT fence to increase sight lines, safety and the aesthetic quality of the barrier.
- Encouraging, wherever commercial improvements are recommended, a local and non-chain business culture to retain the emotional connection to a small village.
- Respecting the local heritage and culture in any future site design.
- Integrating a range of permanent, temporary, professional and amateur public art into as many public improvements as reasonably possible in the corridor.

Grand Opening



Walking Tours and Sounding Boards



### 3.4 Community Vision

Based on the extensive engagement process, the community vision for the Bow to Bluff corridor has been defined as follows:

*“To promote high quality, well defined public spaces and routes that are attractive, functional and convenient for all. The Bow to Bluff corridor will be a vibrant four season space, filled with community gathering spaces and recreational opportunities available throughout the summer and winter months. A cohesive streetscape design and enhancement of the LRT fence will provide for a primarily pedestrian environment. There will be emphasis on local and non chain business, respect for local heritage and the addition of public art to establish the Bow to Bluff corridor as a lively year round space.”*

Iron Sketch Event



Design Workshop



Images from the engagement process

## 4.0 CORRIDOR CONCEPT PLAN

The following section outlines the design requirements and overall concept for the Bow to Bluff corridor concept plan. These concepts were synthesized from ideas communicated through the B2B consultation process.

### 4.1 Importance of the Public Realm

The public realm, including streets, squares and parks, play a major part in the character, attractiveness and success of any community. It also has an important role in encouraging sustainable modes of travel such as walking, cycling and public transit.

The creation and maintenance of a healthy public realm for the Bow to Bluff corridor is essential to the achievement of a safe, healthy, vibrant and attractive community. As well as being easy to move around in, it is important that people feel the corridor is a safe and welcoming place. The corridor's key public spaces (including triangle parks) need to be versatile and serve people from all ability and age groups, who may use the spaces in different ways. This will include integration of universal accessible design principles in all locations. In addition to everyday use, other activities need to be accommodated, such as pre-scheduled and spontaneous community events, cultural activities and festivals. The spaces in the corridor need to accommodate a broad range of uses throughout each day.

#### Cohesive Design Concept

To enhance the success of the Bow to Bluff Corridor, each of the individual projects must play a part in a cohesive design concept. This will enhance the effectiveness of initiatives by both the Community and the City and will help reduce development and operating costs.

### 4.2 Overarching Planning Principles and Design Objectives

The Bow to Bluff Design Framework identifies major improvements within the existing corridor as well as at major intersections such as 9A Street NW and 2 Avenue NW.

The following principles apply to the design of the existing public realm, and the creation of new spaces and routes:

- Ensure a high quality public realm by protecting and enhancing existing spaces and routes.
- Create new spaces and routes that respond to the functional requirements of the corridor.
- Create pedestrian routes that are clear, direct, convenient, safe and well-signed, and can be used comfortably for diverse modes of travel and user groups.
- Create cycle routes that are needed as extensions to the City of Calgary's comprehensive cycling strategy. Creating a wider area that is clear, connected, convenient and safe and can be used without creating significant conflicts between commuter cyclist and casual cyclist or pedestrians.
- Ensure that new public spaces and routes that arise as a result of new development are well defined by buildings. Frontages to the public realm should be designed as 'active' as possible and contain windows and well-defined main entrances, and have activity-generating uses on the main floor.
- Avoid large blank elevations, unrelieved enclosures and entire blocks of uninterrupted street level parking as they reduce the vitality and surveillance on the street.

- Provide new street furniture of a high quality, contemporary design to complement and enhance the urban context, rather than install commercially available reproductions of 'pastiche,' imitation or historical styles.
- Seek 'quality' before 'quantity' in the selection of street furniture.
- Encourage innovative and attractive multi-use street furniture and coordinate all items within the public realm, including traffic and other signage, to minimize street clutter.
- Coordinate street furnishings with City of Calgary to allow for continued and ongoing maintenance.
- Consider the need for maintenance, resistance to vandalism and access to underground services at the initial design stage in any streetscaping scheme and in the selection of street furniture, not as an after thought.
- Consider the needs of the weekly markets, the annual fair, temporary kiosks and other events and festivals in the design of public spaces.
- Strengthen specific connections from the corridor to adjacent communities and facilities (Bow River, Kennington, Hillhurst, Rosedale/Crescent Heights, Riley Park, 10th Street bike lanes, ACAD/SAIT campuses).
- Encourage the public space to evolve over time into a great public place by allowing citizen-generated placemaking activities and experimentation.

### 4.3 Circulation and Connectivity

Getting around the neighbourhood is an important part of everyday life. A cohesive and accessible network of interacting modes of transport needs to be enhanced to allow for easier movement throughout the Bow to Bluff corridor. Additionally, the open spaces need to be integrated together to communicate a natural flow from one place to another. This system must allow for easier movement of pedestrians and bicycles, while at the same time calming traffic to create safer transit and motor vehicle routes throughout the corridor.

The current flow through the Bow to Bluff Corridor operates functionally as a direct route from one location to another. While this is desirable for those passing through with no intentions of stopping within the corridor, it does little for those that want to interact with, or enjoy what the corridor has to offer.

Two strategies are used to enhance circulation within the corridor. First, the concepts of Woonerfs or pedestrian shared streets are strongly applied, in particular to 9A Street. This means creating street spaces that are pedestrian oriented in terms of scale, speed and right of way. Second, to increase connectivity into the corridor, key locations are highlighted that require improvements to increase accessibility from other places.

Figure 4.1 illustrates the hierarchy of connections through the corridor as well as the major intersections that require interventions to encourage people to use the space.



Images illustrating existing routes through the corridor

Legend

- New Pedestrian and Cycling Pathways
- - - Possible Mews Locations
- - - Existing Bike Lanes
- ■ ■ ■ Existing Vehicular Roads
- Existing Pedestrian Pathways to be enhanced
- Potential Locations for Woonerf Streets
- ⊙ Key intersections requiring design interventions

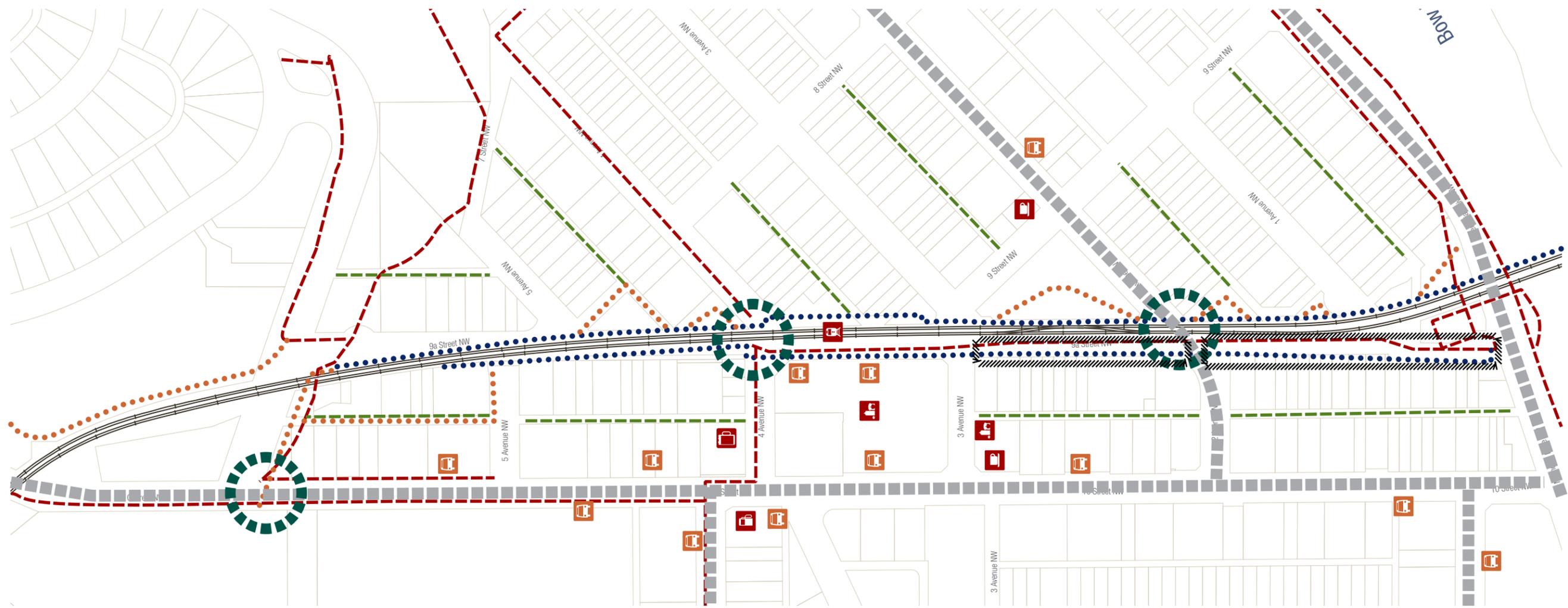


Figure 4.1 Corridor Connectivity

## 4.4 Open Space

Currently, the parks and open spaces scattered throughout the Bow to Bluff corridor are fragmented, disjointed and in many instances accentuate a feeling of discomfort for users. While individual pockets of open space present opportunities for a variety of community functions, this is not currently being harnessed due to improper placement of pathways and uncomfortable or non-existent green connections. Developing connections between these existing fragments and placing new green space in existing voids will improve the cohesive design of public realm, allowing for the entire Bow to Bluff corridor to read as a whole, not simply a series of individual nodes.

The proposed restructuring of open spaces includes enhancement to existing green space as well the introduction of new types of parks to better serve the growing diversity of the resident and visitor experiences of the Hillhurst-Sunnyside community.

The new structure identified in the Overall Concept Plan (Figure 4.5) allows for increased comfort and a variety of public gathering and interactions within any of the small triangle parks in the corridor. The concepts for the new park designs are flexible in nature, to allow for the community to customize the spaces as desired at the time of construction, but also to allow for use of the area in the future for a wide variety of events and programs. Each space should adhere to a series of design requirements that when followed, will help resolve most of the issues related to the functionality and safety concerns that arose during the consultation process.

### Open Space Design Guidelines:

- Divert movement flows into the core of the parks away from the LRT fence to encourage individuals to engage with the public gathering spaces and not simply pass by them (Figure 4.2).
- Utilize species or drought adaptive species when planting the parks. These species will require fewer resources to remain healthy. Existing plants to be pruned appropriately to allow for proper sight lines through the parks so as to improve safety and sense of security in the parks.
- The connections between the parks should be treated as part of the park spaces. This will enhance the feeling of the corridor being one continuous space, and not a series of individual 'after-thoughts' adjacent the CTrain.
- In locations where the pathway crosses roads or back alleys, the adjacent park design should be used to calm traffic and increase pedestrian safety by creating a pedestrian focused environment as opposed to a vehicle focused one.



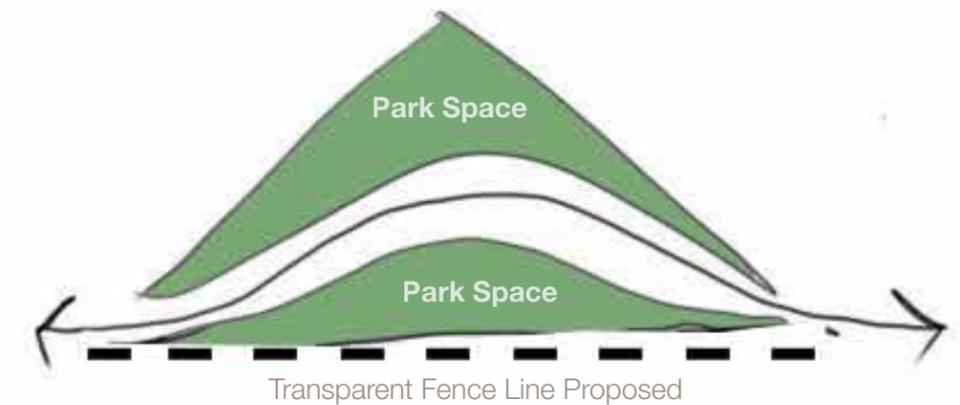
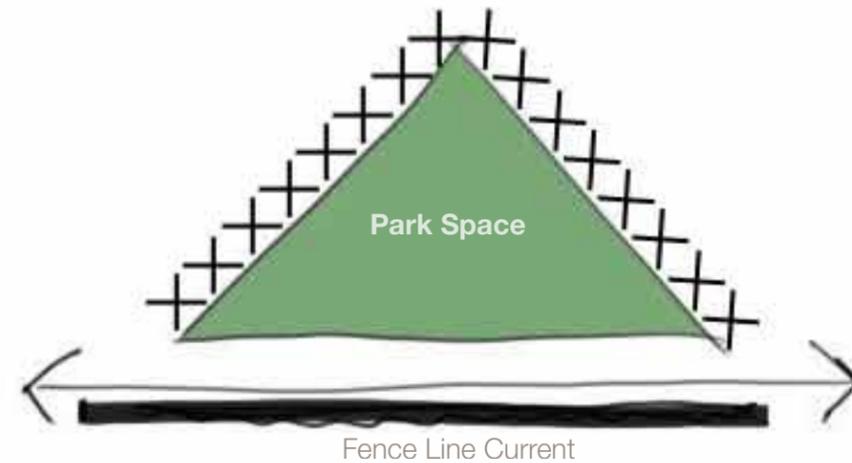
Images illustrating existing open space network

## 4.5 Park Approach

Through appropriate design, the corridor will change from a series of individual spaces to a single linear parkway that extends the green spaces along the Bow River and McHugh Bluff through the Hillhurst-Sunnyside community. The corridor will become a place that is comprised of multiple plazas, markets, outdoor classrooms, public art pieces, intersections, green walls and promenades; all linked by continuous and accessible public gathering spaces.

Potential uses for the public gathering spaces within each park could include the following:

- Public Art Space
- Mix of moveable and fixed seating
- Hookups for cafés and markets
- Fire pits
- Public ovens
- Ping pong tables
- Chess tables
- Water fountains
- Movie screens
- Climbing structure
- Play structure
- Opportunities for exercise
- Performance space, busker stations



Landscape buffer helps to screen the fence and diverts people into the park space

Figure 4.2 Parks Approach

*“sitting / meeting places for clubs and spontaneous community gathering”*

Bow to Bluff Community Participant, October 2011

*“places for public art and cultural events...”*

Bow to Bluff Community Participant, October 2011

*“traffic calming would be great, less cars and more pedestrian, more public spaces and benches, better lighting to increase walking at night...”*

Bow to Bluff Community Participant, October 2011

*“gathering spaces for socializing, watching in different sizes and locations along the corridor in connection with activity nodes for exercise, child’s play, exhibits, and buskers...”*

Bow to Bluff Community Participant, October 2011

#### 4.6 The Overall Concept Plan

1. Gradual ramp up McHugh Bluff with stairs to the half way point
2. Park development (new park extending from the McHugh Bluff access point to 10th Street NW)
3. Wifi internet in the corridor
4. Public art installation within the parks
5. Active living / fitness equipment
6. Improved connectivity between corridor and Riley Park over 10th Street NW
7. Vegetated Green wall along section of the LRT transit bridge
8. Temporary refurbishment to the LRT fence by installing rentable art panels and removing the top 0.5m of the fence
9. Improved recreational pathway on 9A Street NW north of 5 Avenue NW
10. Streetscape improvements through alley to extend pedestrian travel zone between 4 Avenue NW and 5 Avenue NW
11. Integrated planting / wire / corten along LRT transit fence
12. Surface treatment across key intersections to allow for safe crossings
13. LRT station platform improvements
14. Art installation on the large Safeway wall
15. Woonerf
16. Green infrastructure integrated into the new building and the surrounding streetscape
17. Open air market
18. Temporary pavillion for public art
19. mews
20. Kiosk at active living park
21. Park development (4 Avenue NW park)
22. Park development (3 Avenue NW park)
23. Central corridor civic space
24. Park development (2 Avenue NW park)
25. Park development (1 Avenue NW park)
26. Park development (Memorial Drive Active Living park)

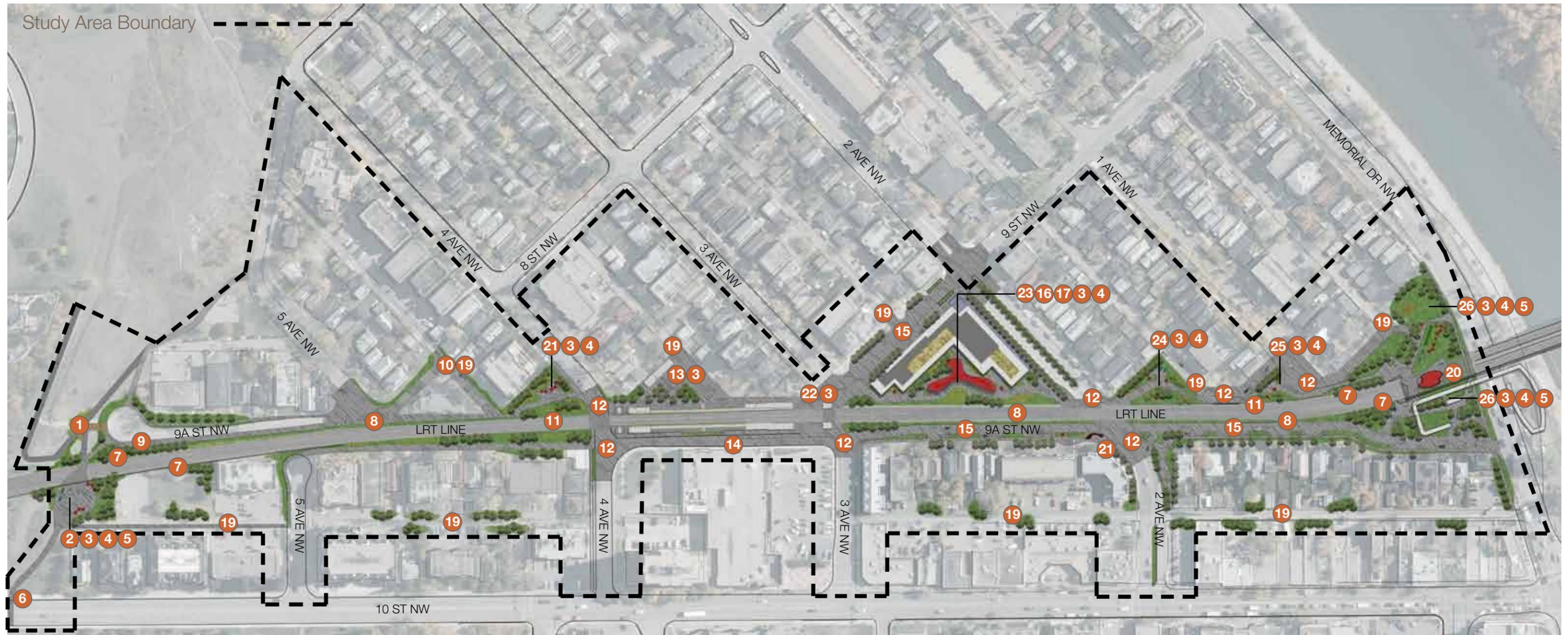


Figure 4.3 Overall Concept Plan

#### 4.7 Major Public Spaces and Associated Design Elements

Through extensive community engagement, the Design Team has identified a number of key spaces that should be enhanced to improve the aesthetics and functionality of the Bow to Bluff corridor. Key design elements for these spaces are described on the following pages. Visualizations help to illustrate the overall intent for each of the spaces. In summary, the major public spaces identified for redevelopment are as follows:

- 1 Park 1 - Active Living Park at Memorial Drive
- 2 Triangular Pocket Parks
- 3 Central Corridor Civic Space
- 4 9A Street as a Shared Street
- 5 LRT Fence
- 6 McHugh Bluff Access
- 7 Laneway Mews



Figure 4.4 Major Spaces Location Map

**1 Active Living Park at Memorial Drive**

This space occupies a prime location within the Bow to Bluff corridor. It sits at a major gateway on the southern end of the corridor at Memorial Drive. Any new interventions in this space will set the precedent for future redevelopment in the area. The design of the space must be complimentary to the river pathway enhancements currently underway north of the Bow River. Key design elements are identified on the plan below.

**Key map**

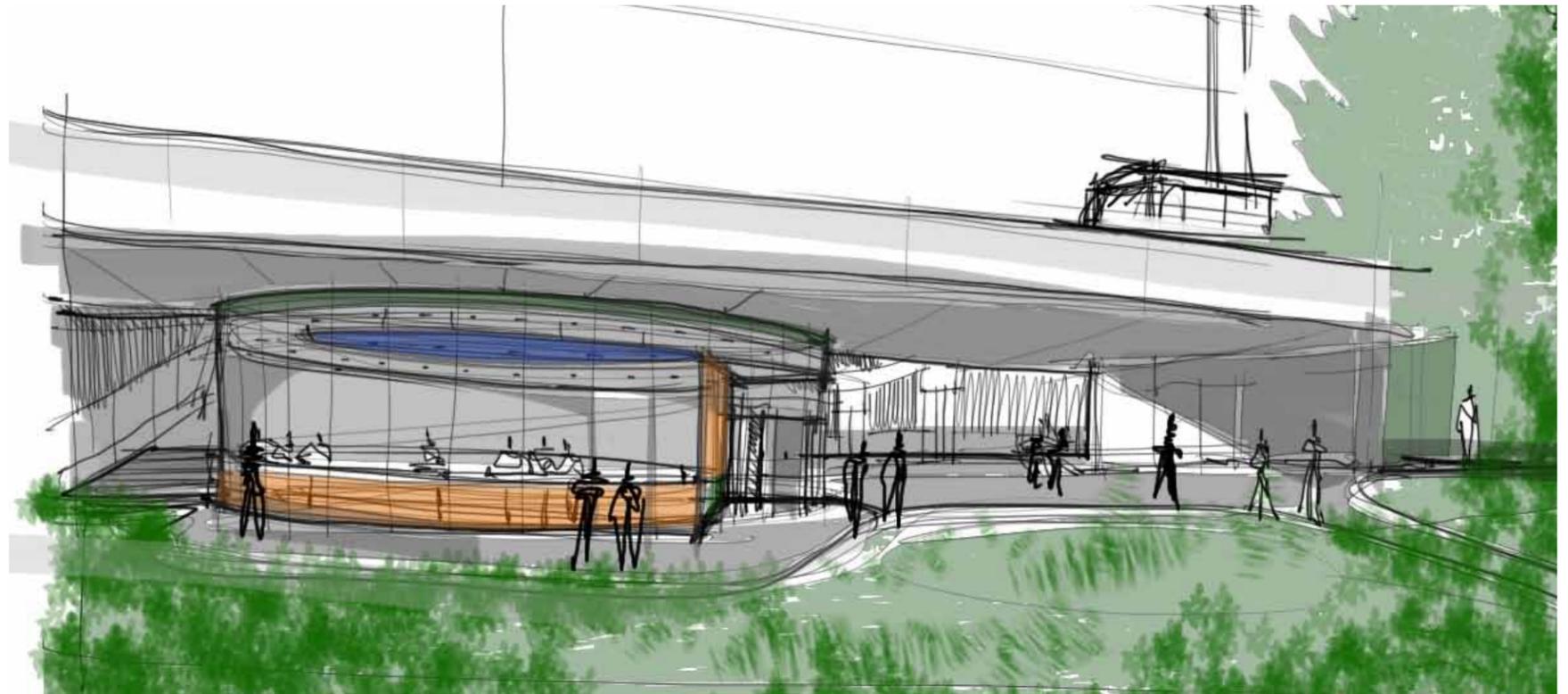


Figure 4.5 Kiosk Concept Sketch

Potential kiosk/market/café space integrated into bridge structure

Extended sidewalk space to accommodate moveable furniture and pedestrian traffic

Opportunity to integrate art installations into the kiosk/bridge

Open space designated for community focussed and active living (fitness) equipment

Flexible / moveable seating and 'active living' equipment allows for spaces to be customized depending on the season and desired use

Pathway and open space configurations allow for increased accessibility and appropriate green space



Figure 4.6 Active Living Park Design Elements



Credit: NHL University Park by Okra



Credit: Salford Central from: Wikipedia commons

Precedent Images, park space under structure and café kiosk



Credit: Place Au Changement by Collectifc



Credit: Place Au Changement by Collectifc



Credit: Place Au Changement by Collectifc

Active Living Park  
Renderings and Precedent  
Images

## 2 Triangular Pocket Parks

At the core of the Bow to Bluff corridor are a series of small triangular parks that run the length of the Hillhurst-Sunnyside LRT line. These individual pocket parks are located along the East side of the LRT line running from Memorial Dr. NW to McHugh Bluff. The current park structure is isolating and lacking a cohesive flow from one public green space to the next. Further to this, the formation of the parks allows for the majority of traffic to bypass the park, and not engage with the public green space that exists along the corridor. While the physical size and shape of the parks are large enough to allow for a variety of community desires to be achieved, the current relationship between the parks, pathways and community is problematic. The lack of defined uses for these spaces was a key issue that was raised as part of the community engagement process.

The proposed reconfiguration of these spaces is based on the parks concept identified in Figure 4.2 and allows flexibility for a variety of community functions to occur within them.

### Key map



Park boundaries blend with the surrounding context to allow for a natural flow in and out of the spaces

Gathering spaces are removed from the central pathway but still accessible and open to those passing by

Flexible seating options accommodate all potential users of the spaces

The flow for users in and out of the parks brings users into the spaces, increasing traffic and thus increasing safety

The transit fence blends into the green space of the park eliminating the discomfort it created previously



Figure 4.7 Triangular Pocket Park Design Elements (A)



Precedent Images (bottom and opposite), sculptural play spaces used to create visual interest, increase play quality and define space.



Credit: Van Beuningenplein Playground by Carve Landscape Architecture



Credit: Van Campenvaart Playground by Carve Landscape Architecture



Credit: Sculptural Playground in Schulberg  
by Annabau



Credit: Stone's Throw Playground  
by Archiculture



Credit: Lalaport Toyosu  
by Earthscape



Active living nodes are pulled through the entire corridor to allow for continuous usage of the parks year round

Flexible programmable zones are left available for a variety of uses dependent upon budget, community desires and time

Primary pathways have widened areas to accommodate safe and spontaneous gathering and community iteration

Occasional pathway pinch points exist to force cyclists to slow down and create interest while looking down the corridor

*"child friendly spaces for play and laughter..."*

Figure 4.8 Triangular Pocket Park Design Elements (B) — z

Bow to Bluff Community Participant, October 2011

## 2 Triangular Pocket Parks (continued)

Integrating the open spaces together through a continual design language and open accessibility allows for the entire corridor to read as one space, not as a series of isolated nodes as currently exists. To achieve this, additional 'forgotten' spaces need to be considered in the redevelopment of the corridor. Whether immediately adjacent to the transit corridor or slightly removed, any space that introduces people to the corridor, or improves their experience is important.

Additional spaces are created throughout the Bow to Bluff corridor and increase the quality of the public realm of the corridor.

### Key map



By bringing users into the laneways, these become part of the corridor and allow for increased community interaction

Flexible site furnishings and gathering spaces allow for improved use of the parks throughout the corridor

Paving can be staggered and used as bands to signify important sections and areas of higher visual interest

The road, laneway and pathways begin to blend into each other, creating more pedestrian oriented spaces

A lower and planted transit fence creates a visual connection across the tracks and along the whole corridor



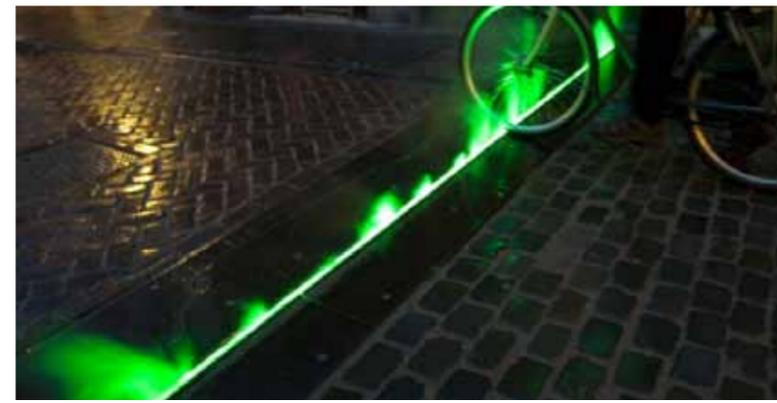
Credit: Mathildeplein by Buro Lubbers

Credit: LØrenskog Central Square by Østengen & Bergo AS

Figure 4.9 Triangular Pocket Park Design Elements (C)



Credit: Sub-Face of the Railroad Bridge  
by Exit Paysagistes Associés

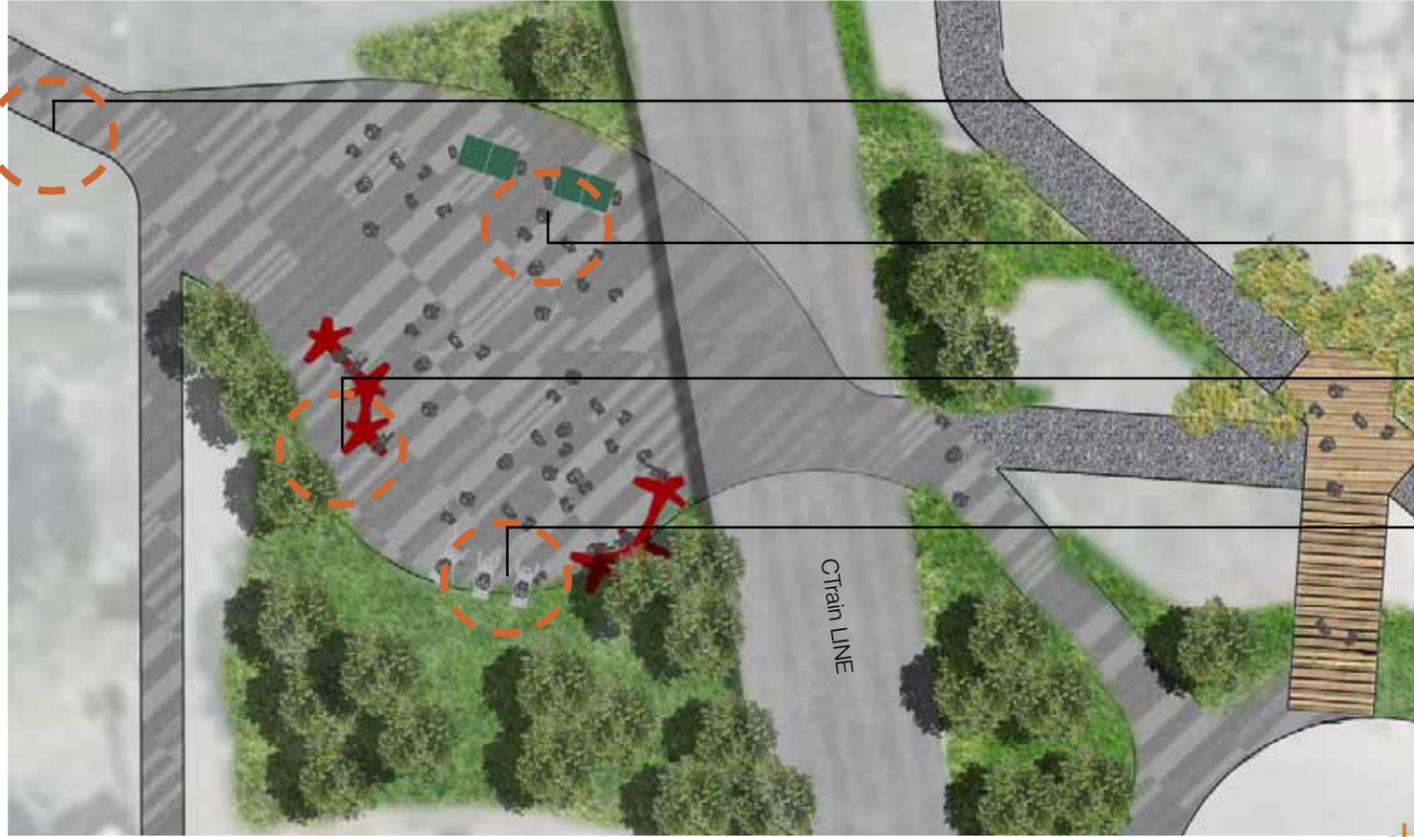


Credit: Domplein  
by Okra

*“small spaces for community music performances and children plays”*

Bow to Bluff Community Participant, October 2011

More and safer connections to the corridor allows for a greater use of the corridor on a daily basis



Active living nodes are placed through out the corridor to encourage use of the parks year round

Flexible seating options accommodate a range of potential uses of the spaces

Flexible open space allows for a variety of activites to occur year round

*“outdoor workout / exercise spaces”*

Bow to Bluff Community Participant, October 2011

Credit: Winnipeg Skating Shelters  
by Patkau Architects

Figure 4.10 Triangular Pocket Park Design Elements (D)



### 3 Central Corridor Civic Space

The Central Corridor Civic Space occupies a key location within the Bow to Bluff corridor. Located at 1020 2nd Ave NW (old triangle building site, near the Sunnyside LRT Station), the site is currently owned by the City of Calgary and is vacant. The previous triangle warehouse has been demolished with no confirmed status for the future of the site.

The ARP identified future land use for the site as 'Medium Density Mid Rise,' primarily allowing for multi-family residential type uses, and a building to a maximum of 26 metres in height and a 4.0 floor area ratio.

#### Key map



Regardless of the chosen function for this space, design recommendations emerged during the public consultation process that outline what potential the site could have. The City has identified the site as an opportunity for future affordable housing, but the timing and type of redevelopment model has yet to be established. A majority of citizens engaged identified the site as a unique and significant opportunity for revitalization, and as a potential catalyst for setting the tone for wholesale improvement to the corridor. Several key land use and programming ideas emerged. The potential for realizing the majority of the identified opportunities will be dependent on economic feasibility. As such, any future detailed redevelopment vision and stakeholder consultation exercise should respect the need for design trade-offs and flexibility in policy to allow for all interested stakeholders to realize their goals.

Changes in paving indicate entry into a pedestrian space

Scale changes in the road further emphasize the pedestrian scale of the roads

Following the ARP, west side of the site should be setback to allow for an enhanced landscaped area, establishing a well conceived and integrated transition between the public and private realm.

Consider green roofs as a supplementary means to achieved enhanced landscaping ARP policy goals.

Flexible community space, either as a standalone structure or integrated at-grade within the primary building to accommodate gathering and markets in all seasons

Flexible park furnishings and gathering spaces allow for the flow and safety of the parks to continue throughout the corridor

The road, laneway and pathways should blend into each other, creating pedestrian oriented spaces

*"more people friendly development..."*

Bow to Bluff Community Participant, October 2011

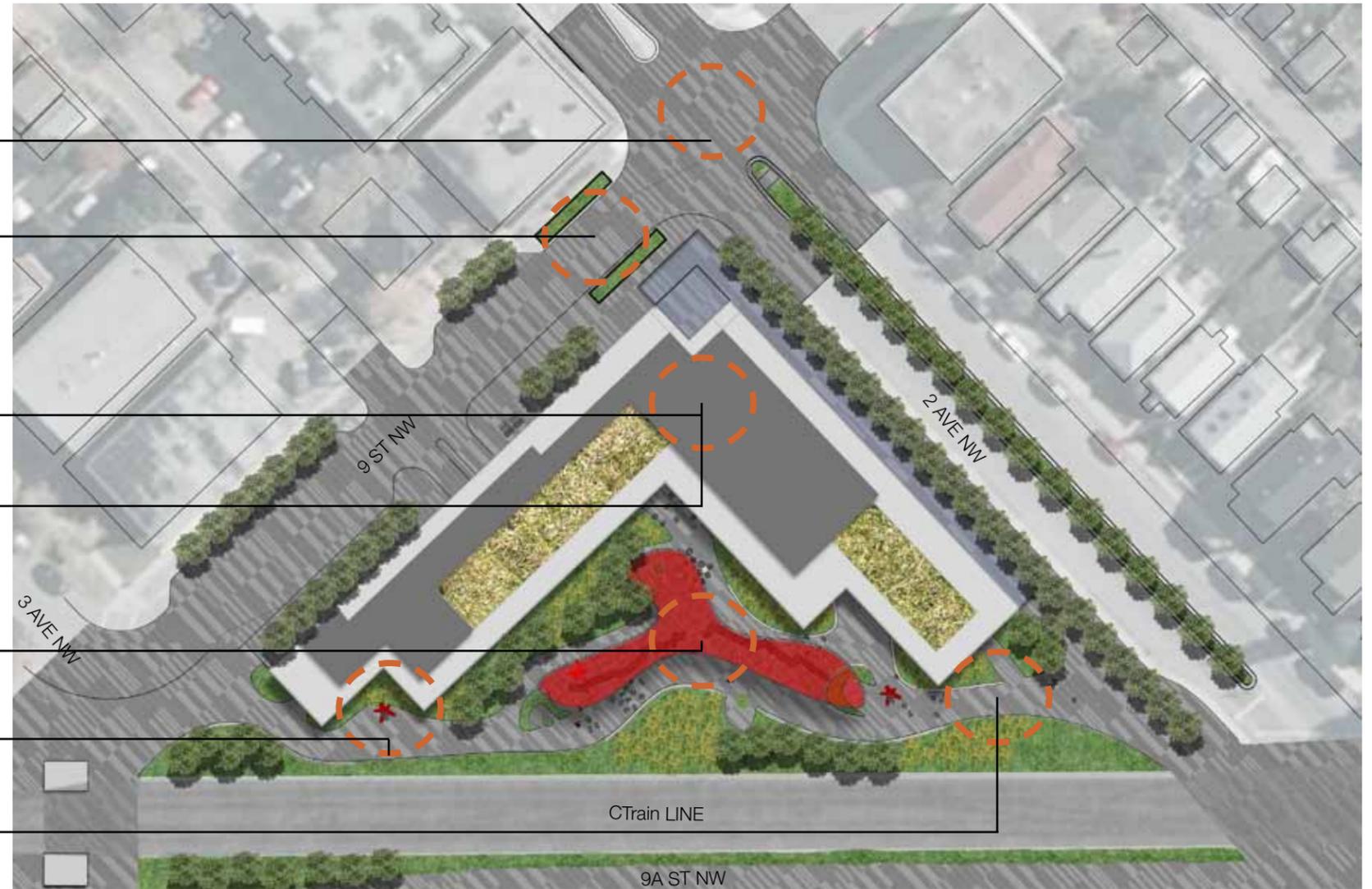
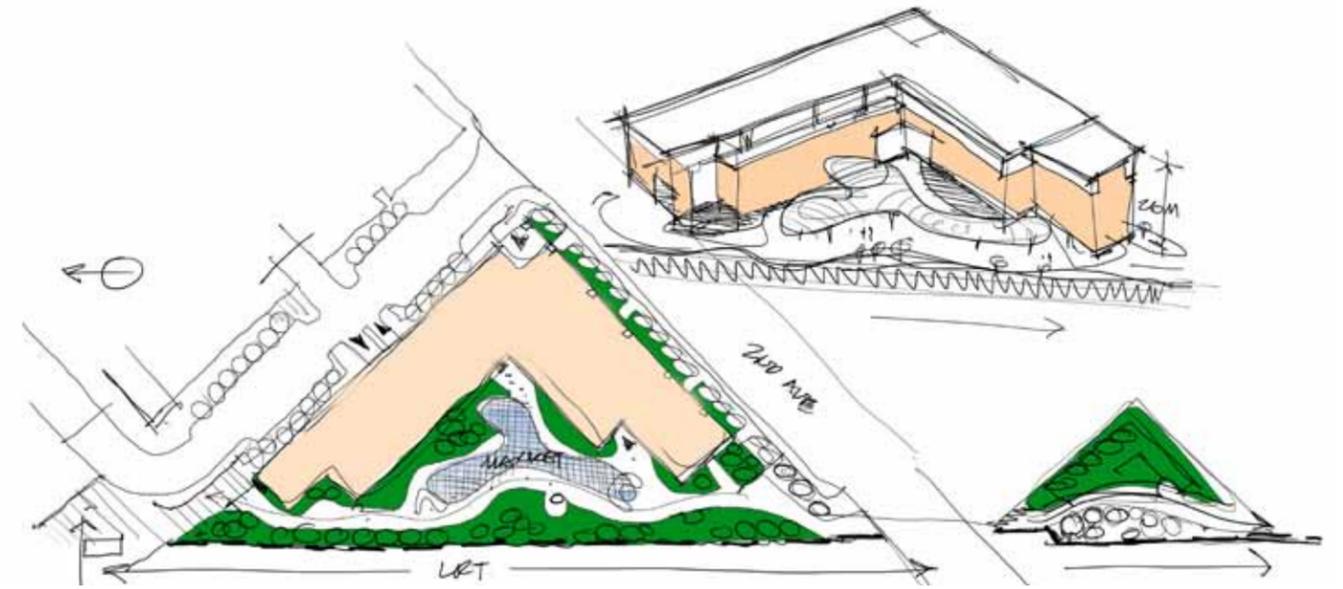


Figure 4.11 Central Corridor Civic Space Design Elements



Credit: Besiktas Fish Market by GAD



Credit: Merida Factory by Selgascano



Credit: Garscube Landscape Link by Rankinfraser Landscape Architecture



Credit: Centro Abierto de Actividades Ciudadana by ParedesPino Architects



Credit: Rooke Reserve by CGIP Australia



Credit: Eras de Cristo by Federico Wulff Barreiro

**4 9A Street as a “Woonerf”**

A woonerf is the Dutch term for a “living street” in which the needs of vehicles are secondary to the needs of users and the street as a whole. 9A Street NW is a ‘shared space’ and its design should acknowledge this. Primarily it should include surfacing that is level, with minor height differences between separate spaces along the street (between the sidewalk and road), and be designed to be used by pedestrians, playing children, bicyclists and very low-speed vehicles.

**Key map**



The woonerf design along the length of 9A Street NW should create a comfortable, low-speed, pedestrian oriented street suitable for residential use with some interspersed commercial uses as per the previously developed local Area Redevelopment Plan. The public street space should be defined with physical barriers that restrict vehicle speeds and access.

Pedestrians should have access to the entire street, while vehicle access to some areas should be restricted by physical barriers.

Physical barriers should consist of landscaping, varied paving material, bollards, tactile/texture changes and intermittent parking spaces as opposed to curbs, gutters, and painted lines. The barriers should be integral parts of the streetscape design rather than devices traditionally used for traffic control.

Paving materials should direct drainage of storm water to the

landscaped swales to allow for the water to be used by vegetation. Other low impact development (LID) stormwater management techniques should be considered.

Where crossings occur at the intersections of 2nd, 3rd, and 4th Avenue NW, varied paving, elevational changes (e.g. speed tables) and limited signage are used to warn drivers that they are entering a low-speed zone. Physical barriers should require drivers to slow and change direction to negotiate the intersections.

The paving materials along the woonerf should interrupt the conventional street, ensuring an uninterrupted path for pedestrian movement. The paving material should flow around either side of the central traffic area.

Physical barriers such as landscaping, planters, and bollards are used to prevent vehicle access to delineate pedestrian only areas. The barriers are always used as an integral piece of the streetscape design.

Temporary kiosks can be placed throughout the woonerf and used for markets, art displays or shade

Physical barriers such as landscaping and planters are used to protect temporary pedestrian spaces

Extended green space into the road right of way can be utilized for gardens, yards or barbeques

Gated emergency access is provided at the end of 9A Street NW

Bollards are used to control entry to pedestrian only zones

Parking stalls are still located throughout the woonerf, and primarily serve the local residents

Paving and elevational changes to warn drivers they are entering a pedestrian focussed environment



Figure 4.12 9A Street Design Elements



Figure 4.13 9A Street Visualization



Figure 4.14 9A Street Concept Sketch



Credit: New Road  
by Gehl Architects



Credit: Arena Boulevard  
by Karres en Brands Landscape  
Architecture



Credit: Grounds of Hargreaves  
Mall by Rush Wright Landscape  
Architecture



Credit: St-Flour  
by Insitu Architects Paysagistes

Examples of Woonerf streetscape design

The woonerf or shared surface concept means creating a functional right of way that:

- Accommodates all modes of transport with non-motorized users taking precedent.
- Promotes low vehicle speeds and discourages through traffic.
- Is safe for pedestrians, bicycles and vehicles.
- Is accessible for all people, including those with disabilities.
- Are narrow and pedestrian in nature.
- Identifies vehicle, parking, pedestrian and bicycle lanes through changes in surface treatment (usually pavers) and not curbs or line painting.
- Is durable and maintainable.

This woonerf design is an important design component for the function of the entire Bow to Bluff corridor. The woonerf is a proven, viable and exciting solution to many of the problems created by the automobile in residential neighborhoods. By using 9A Street NW as an experimental zone for woonerf design within Calgary, it allows for a unique solution that juxtaposes many current City of Calgary design assumptions with a solution that puts pedestrians and automobiles on the same level. Creating woonerfs within the residential context reduces traffic problems, noise and safety concerns created by traditional traffic speeds and volumes. Allowing for the flexibility of experimental 'complete streets,' to occur, the opportunity for staged transformations to be integrated over time is emphasized through the woonerf mentality that currently exists along the length of 9A street.

A significant component of the woonerf design is the creation of public spaces - in the street - to be used by the community, creating additional public outdoor space. Additional benefits could include higher property values and community networking.

These woonerf design elements can occur over time as regular maintenance and street improvements occur. The ultimate goal for the length of 9A street NW during the process of isolated improvements should be for a functional woonerf.

*"...less vehicular traffic equal engagement from people...it also makes business focus on people walking and not driving by..."*

Bow to Bluff Community Participant, October 2011

## 5 LRT Fence

The fence was installed along the transit corridor as a result of multiple community consultations. At the time of these consultations, the transit line was viewed as an eyesore and was thought to create unnecessary noise pollution in the neighbourhood. As a result, the current green transit fence was constructed. Today, the fence provides (to some degree) safety and sound attenuation functions. Throughout the Bow to Bluff community consultation process, the green transit fence was often identified as an issue within the community. The majority of the concerns related to the aesthetic quality, height (blocking views) and the uncomfortable aesthetic condition when one is moving adjacent to it.

Today, the fence is in need of reconditioning and functional reconsideration. A more appropriate solution that addresses a variety of requirements including safety, sound attenuation, aesthetics and visual connection is needed.

During the consultation process, many comments were received regarding the fence. These ranged from 'removing the fence completely' to 'repainting' and 'better maintenance of transit infrastructure.' The communities concerns with the fence relate to the visual connection from one side of tracks to the other and the general aesthetic of the fence itself. A single solution applied over the entire length of the corridor to replace or remove the fence will not work, as the fence needs to work both the overall design as well as the specifics of individual locations.

The fence as a boundary marker is an essential component of the corridor design. As design must be flexible to allow for options that adapt to different locations, a range of design options is needed including varying heights, forms and materials. Material possibilities for the fence include; planted green wall sections (as visual breaks or vertical community gardens), art features (sculptural, paintings, murals etc), cable sections (to increase sight lines) and corten steel (as heritage components to the corridors industrial past).

Fence design options include:

- Use the fence to help define safe movement on either side of the corridor
- Allow the fence to merge with the aesthetic and functional design of the surrounding context. This may include creating a green wall with additional planting adjacent the fence.
- Lower the overall height of the fence in key locations to allow for views and site lines from one side of the LRT tracks to the other.
- Allow for fence materials to change and alternate throughout the corridor.

### Key map



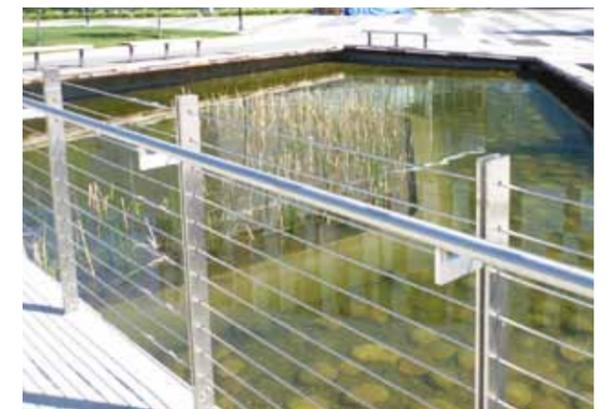
*"greenery along the train line...graffiti mural rather than grey paint"*

Bow to Bluff Community Participant, October 2011

*"Replace the ugly green fence with a more transparent design to create better line of sight to the surrounding neighborhoods"*

Bow to Bluff Community Participant, October 2011

Examples of the existing LRT fence (left) Green Walls, Corten and Cable Fencing (below and right)



Credit: Westminster Presbyterian Church  
by Coen and Partners



Credit: Green Building Components  
by Tournesol Siteworks



The proposed concept for the fence and walls uses a kit of options with components that can be used to create a variable but related barrier that provides both utility, visual interest, visual permeability, personal comfort, and a sense of security throughout the corridor.

The components of the fence are:

- A green wall section, which can be planted with vines or utilized as public garden space (vertical community gardens) dependent on the location through the corridor.
- A solid section with the major component material being corten steel to add color and textural interest in all seasons. Using the corten steel at the base of the wall will also allow for some sound attenuation from the CTrain.
- Blank steel sections that can be filled with public art. These pieces of public art along the wall can either be permanent or temporary in nature.
- Cable fence sections are placed in areas where site lines are necessary for safety or for desired views from one area to another. Views might be possible across the tracks, at major intersections or within the parks.
- Fence segments around intersections should be lowered to a minimum.

Individual components for the refurbishment of the transit fence could begin to occur with changes to maintenance occurrences to the transit fence. This might include (with the proper sound, structural, and safety studies) a removal of select corrugated metal panels and replacement with new elements from the 'kit of options.'

After the appropriate acoustic, safety and detail design studies, the option for complete removal of the fence in isolated areas should also be considered.



Figure 4.15 LRT Fence Treatment

## 6 McHugh Bluff Access

Safe and easy access in and out of the Bow to Bluff Corridor is important. During 2011 an existing flight of stairs at the end of 9A Street NW up McHugh Bluff were prematurely removed while pathway improvements were being conducted at the top of the Bluff. Since this error occurred, it has been repeatedly communicated to the Bow to Bluff initiative that a form of access up McHugh Bluff needs to be installed.

### Key map



*"Better pathways through McHugh Bluff so we can still walk but still control erosion and prevent the slope from being too steep."*

Bow to Bluff Community Participant, October 2011

The previous stairs (left), temporary slope stabilization in location where stairs were removed (center) and desire lines forming along the bluff since the removal of the stairs (right)



Existing asphalt pathway with new granular top layer for slip resistance

Planting to stabilize slope and mitigate climbing up slope

Landings with integrated seating as resting points and viewing platforms

Stairs to midpoint of McHugh Bluff to integrate into streetscape

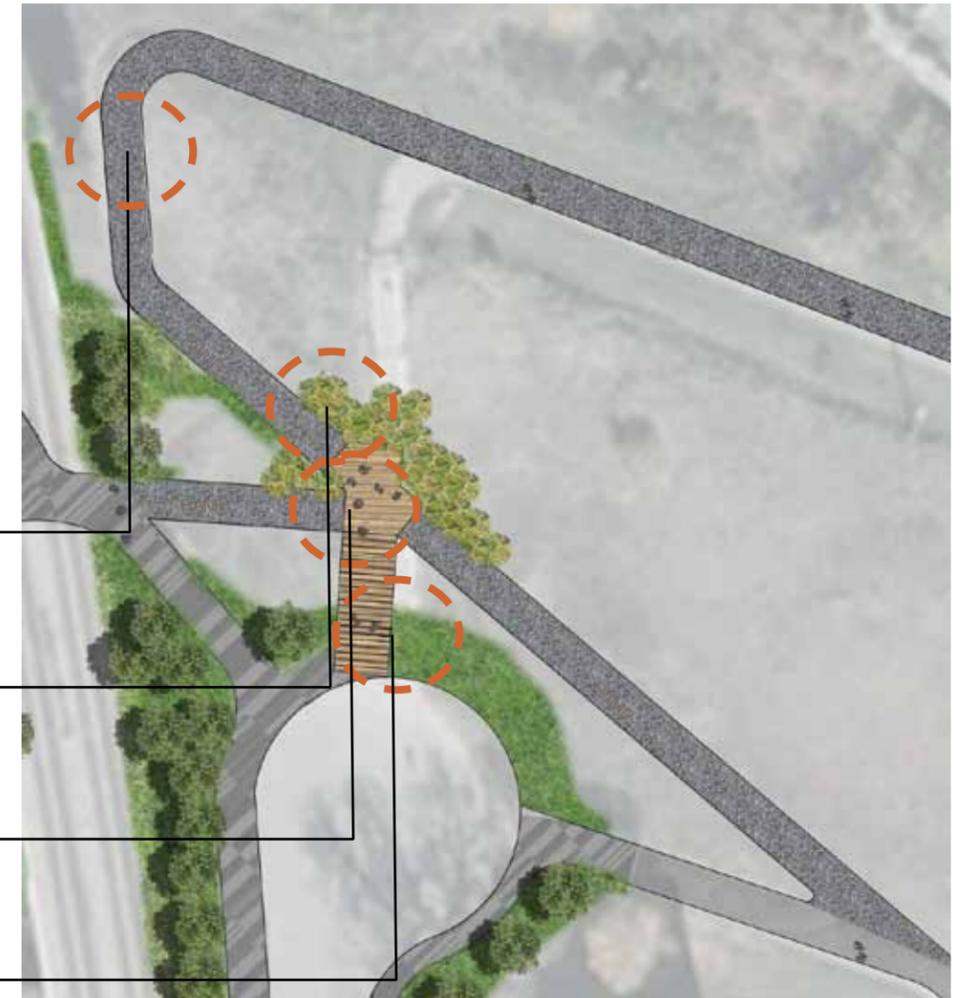
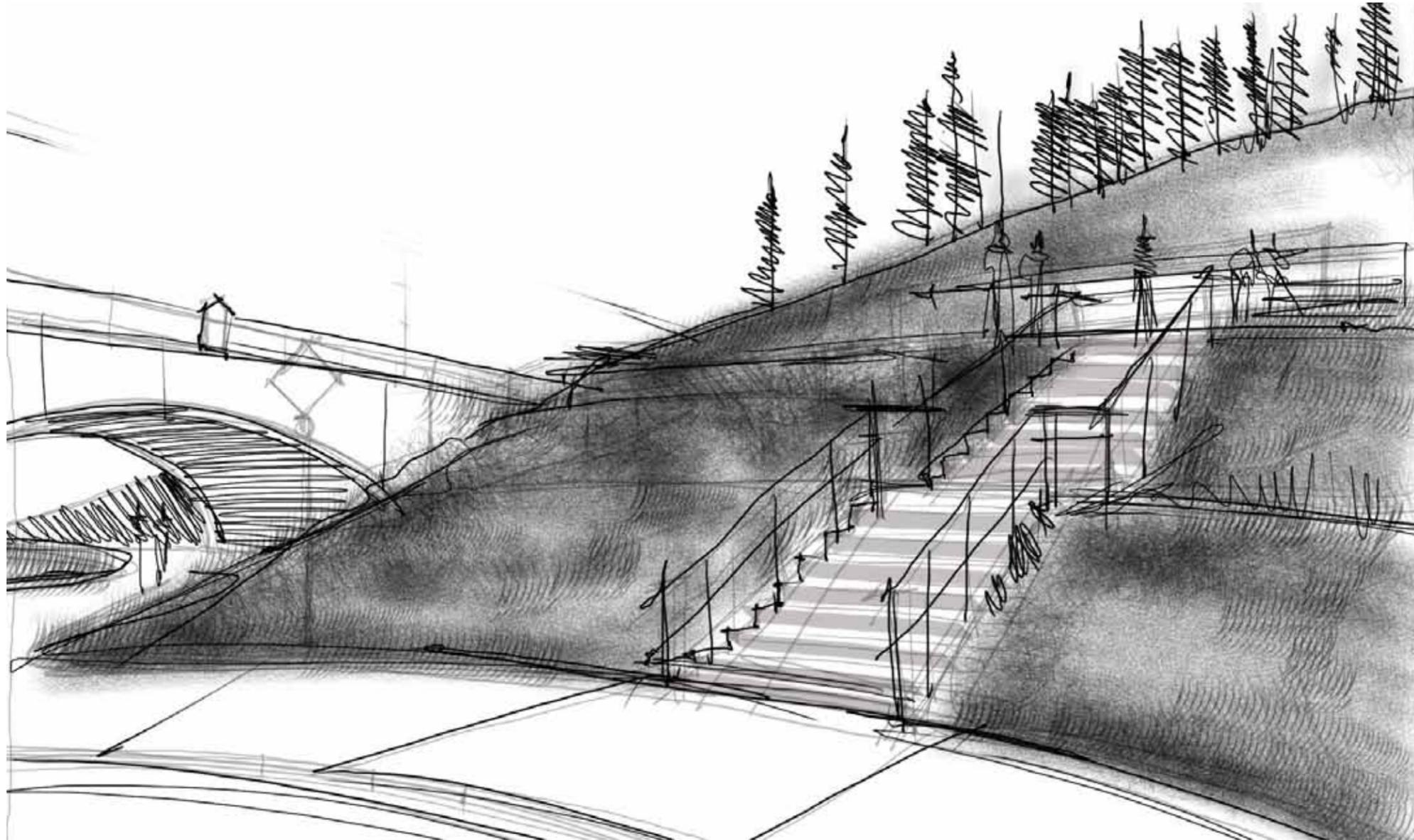


Figure 4.16 McHugh Bluff Design Elements



From the community consultation that was conducted, a series of design requirements were determined for proposed access to the Bluff. They are as follows:

- Minimum impact to the surrounding landscape should occur.
- Native plant species should be used to stabilize the slope.
- A gradual ramp is desired in addition to a single flight of stairs to accommodate all potential users of the trail system.
- Viewing nodes along the bluff should be integrated along the Bluff.
- Multiple access points at the landing are desired to allow for access to the ramp from each direction.
- The turning circle for 9A Street NW at the base of the slope should be integrated into the new connection.
- The surfacing materials should consider the usability of the structure in all season and weather conditions.
- The structure should require a minimum level of maintenance.
- The pathway at the top of the bluff needs a layer of gravel over the asphalt to increase slip resistance and user safety.



Credit: Vedahaugane  
by LJB



Credit: Parque de la Ereta  
by Marc Bigarnet & Frédéric Bonnet



Credit: Belvederes Drestische AA  
by Strootman



Credit: Promenade de  
Champlain, Quebec City,  
Quebec

Examples of Viewing Planforms

## 7 Laneway Mews

### Key map



Laneways provide locations for important utilities, services, parking and loading zones. However, they also expand the usable public realm and active frontage within the corridor.

A mews is a British term describing laneways or alleys that have been converted overtime from being used for rows of horse stable buildings servicing larger dwellings to that of back-street secondary residential dwellings, as well as small businesses. These laneways are often transformed into pleasant and desirable areas that create the qualities of a modest front street.

The Bow to Bluff Corridor's laneways should be seen as more than functional elements. Properly designed they can provide pleasant pedestrian circulation corridors and also serve private businesses as entries and patios. Improvements to back lanes should include permeable paving, planting, burying utilities, and allowing for housing and small business to exist. In some residential areas, there are opportunities for secondary suites.

The evaluation of the Laneway Mews arose from both, an expressed community desire during the engagement process and from the 'Livable Laneways' project, which was a project by Sustainable Calgary in collaboration with the Hillhurst-Sunnyside Community Association.

The design direction for the Laneway Mews is as follows:

- Lane access may be limited or reconfigured to complete streetscapes and provide opportunities for community gathering, open space, landscaping, mid-block pathways, courtyards and plazas.
- Lanes will only be closed where adequate street circulation can be maintained and where access to utilities, parking, deliveries and garbage collection will not be impeded.
- Development along lanes should be designed to provide adequate sight lines among buildings, open spaces, streets, and lanes for the benefit of both visual interest and public safety.
- Lane frontages should provide clearly visible pedestrian routes among features such as courtyard entries, pathways, landscaping, common building entranceways, and individual entrances to dwelling units.
- Lane access to buildings should be clearly indicated and well lit to promote not only safety, but also the perception of safety, especially in corners and other secluded areas.
- Lighting, landscaping, paving and burying of utilities along lanes should be coordinated to prevent unnecessary or inefficient duplication, reinstallation, or reconstruction.



Credit: West Vancouver Thurlow Laneway. (Hiscock, Keough, & Zoidak (2011))



Credit: West Vancouver Thurlow Laneway. (Hiscock, Keough, & Zoidak (2011))

The key design elements for Laneway Mews improvements have been communicated to the Bow to Bluff team through the Livable Laneways Project conducted in 2011 (Hiscock, Keough, & Zoldak (2011) Livable Laneways Project. Sustainable Calgary Society, Calgary, Alberta) are as follows:

- **Laneway Signs.** Use a distinctive sign design with names placed at the entrance to the lane and name the lanes thematically or use a local historical figure or event. Many of the inventoried lanes have heritage items on ‘display’ (e.g. vintage cars) or original turn of the century carriage houses or barns (there are likely other items of historical significance within these structures that could be featured in a Heritage Lane).
- **Feature Lighting.** Install lighting throughout the laneway and coordinate this with other fixtures in the corridor to save on purchase and maintenance costs.
- **Communal Gardens.** Install linear or small plot laneway gardens - vegetables, flowers, fruit trees or berry bushes.
- **Mini-Plazas.** Where extra wide parking spaces are located, these can be transformed into small community plazas with multi-layered purposes.
- **Local Business Establishments.** Laneways and unused garages could be used as spaces for small local business. This could be supported by an innovative ‘local business license’. Such a license would be available to local businesses defined as those designed to serve the local community. With

such a license parking requirements could be waved as the locals can all easily access the business by bike or walking. Types of businesses may include: bakery, laundromat, Re-use/Recycling store, Small tool rental and repair business, hair salon, Yoga or Artist studios.

- **Laneway Housing.** Several laneway houses exist in Sunnyside (e.g. two carriage house heritage style houses currently at the corner of 1st Avenue and 2nd Avenue, lanes on 6th Street, and a new townhouse construction at 7th Street on the 3rd Avenue and 4th Avenue lane). Appropriately-scaled secondary residential dwellings along the laneway should be encouraged to activate the laneway back-yard frontage with doors and ‘eyes-on-the-lane.’
- **Cluster Parking.** Vehicle parking in lanes could be redeployed to the outer edge of the lane leaving the interior of the lane a pedestrian zone except for utility vehicles and periodic resident drop off and pickup.
- **Car-Share Parking.** Provide parking for Car-Share vehicle(s)
- **Permeable Surface.** Install hard permeable surfaces (e.g. bricks) to aid water filtration and pedestrianization.
- **Elevated Linear Gardens.** The New York Highline inspired elevated linear gardens could be incorporated into laneways with public stairway access and direct elevated access from condominium and apartment buildings. Alternatively Green roof structures can be added to existing or new buildings and would create additional public space.



Credit: Crack Garden  
by CMG Landscape Architecture



Credit: Making Space in Dalston  
by J & L Gibbons Landscape  
Architects / MUF Architecture /  
Art

## 5.0 COMMON DESIGN ELEMENTS

The common design elements will help provide a consistent character within the corridor. A series of elements including paving, site furnishings, lighting, public art, amenities and planting will support the enhancement of the corridor. In selecting available options for these components, it is important to work with the City of Calgary in the selection process to ensure the ongoing and long term maintenance of them. These elements are described in the following pages.

### 5.1 Unit Paving / Concrete / Asphalt

Unit paving provides a change to the tactile expression of a place. For many people, expressive paving indicates a pedestrian-oriented space, where people are considered first and hold a higher priority than the automobile. Using unit paving to indicate changes throughout the Bow to Bluff Corridor will enhance its perception as a continuous pedestrian environment. Proven paving materials from other applications are suggested for the corridor.

#### Requirements for the corridor paving are as follows:

- Paving should be a combination of unit pavers, concrete and asphalt depending on the location and potential use. This could include; complete unit paver streets, paving bands, patterns, etc.
- Utilizing a standard colour palette, a wide range of paving patterns can be created, each showing the individual character of that place, but still highlighting the cohesive language of the corridor.
- In places where shared pedestrian and vehicular spaces exist, common paving treatment should be used for both vehicular and pedestrian zones.
- Permeable paving options for all zones is desirable.
- Paving should allow for greater slip resistance for pedestrians.
- Paving should allow for greater skid resistance for vehicles.
- Paving should allow for ease of access to underground utilities.



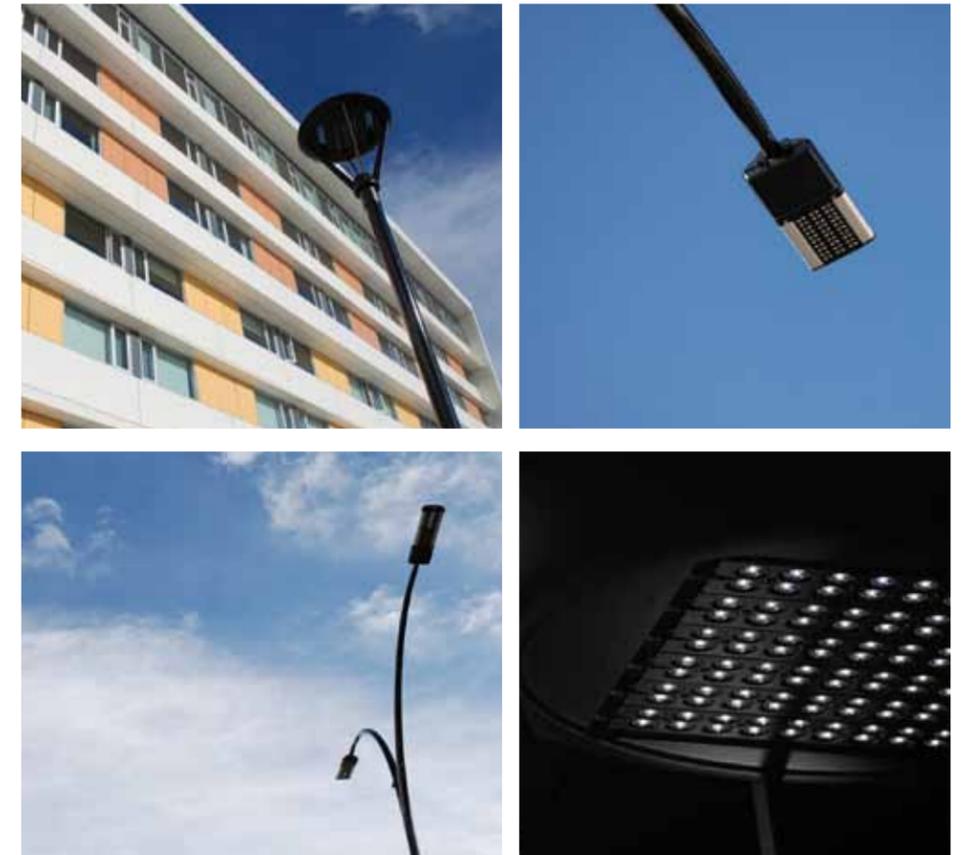
Examples of unit paving for both pedestrian and vehicular use

## 5.2 Lighting

Lighting within the corridor is necessary to improve safety, and enhance the usability and aesthetics of the spaces. Appropriate lighting types and levels will encourage people to use the Bow to Bluff Corridor in the evenings.

### Requirements for corridor lighting are as follows:

- Incorporate high quality lighting design including architectural lighting of buildings and features.
- Ensure spaces that are well lit, avoiding dark corners, with lighting designed to be in context and also to minimize 'light pollution'.
- Lighting will be full cut-off / dark sky compliant.
- High-efficiency luminaires should be used to reduce energy consumption.
- Long life LED's (50,000 to 100,000 hours) are encouraged.
- A single pole style will be chosen to create consistency in appearance, yet allow for a wide variety of applications. The pedestrian scale should be the focus. (For pedestrians, cyclists and vehicular).
- Fixtures will have a modern and timeless appearance.
- Where appropriate, luminaires can be mounted on existing bases for cost savings.
- Lights should be spaced evenly throughout the corridor and allow for a safe level of sight during all times of day.
- Lights should be oriented to illuminate the public realm and not interfere with private properties.
- Fixtures and poles should coordinate with the City of Calgary to allow for regular maintenance routines and affordable replacement when required.



Examples of light fixtures



Potential bench options illustrating both fixed and moveable options

### 5.3 Site Furnishings

A cohesive collection of site furnishings will help unify the unique aesthetic character of the corridor. New development should ensure an integrated and coordinated approach to site furnishing elements such as seating, bollards, bicycle racks and trash receptacles.

#### Requirements for the corridor seating:

- A combination of fixed and moveable seating should be provided. Any moveable seating should be placed in areas where gathering is likely, such as around kiosks, art installations, markets, etc.
- A single bench system will be chosen to allow for long-term economic viability and consistency. A standard bench will allow for ease of maintenance and replacement.
- The selected system will allow for different configurations and groupings.
- Seating should allow for optional backrests, depending on location.
- Wood is recommended as the primary seating material due to its added comfort in cold weather climates (lower heat conduction versus metal and concrete). Hard woods such as Ipe are recommended to provide lower maintenance requirements.
- Benches should be placed in groupings within designated community gathering places to allow for flexibility of use and interaction between visitors.
- Benches should coordinate with the City of Calgary to allow for regular maintenance routines and affordable replacement when required.

#### Requirements for corridor bollards:

- A single bollard will be selected that can be easily fabricated or sourced.
- Bollards will have a modern and timeless appearance.
- Bollards will have a duplex coating finish for lasting corrosion resistance – hot-dip galvanized and powder coated.
- Bollard design will facilitate a variety of mounting options depending on location – cast in place, bolted mounting plate, or hinged collapsible for emergency access.
- Bollards should coordinate with the City of Calgary to allow for regular maintenance routines and affordable replacement when required.

#### Requirements for corridor bicycle racks:

- A single bike rack will be selected that can be easily fabricated or sourced.
- Bicycle racks will have a modern and timeless appearance.
- Bicycle racks must provide a high-level of theft protection, similar to a ‘U’ or ‘D’ bicycle rack.
- Bicycle racks should be placed in groupings of no less than three within sight lines of potential destination spaces.
- Bicycle racks should coordinate with the City of Calgary to allow for regular maintenance routines and affordable replacement when required.

#### Requirements for corridor waste receptacles:

- A single waste receptacle and recycling station design will be selected that can be easily fabricated or sourced.
- Waste receptacles will have a modern and timeless appearance.
- Waste receptacles should allow for easy maintenance by the City and access by users.
- Waste receptacles should be placed within viewing range of seating areas, but not too close as to cause discomfort from odors, animals or insects.
- Waste receptacles should coordinate with the City of Calgary to allow for regular maintenance routines and affordable replacement when required.

A modern and timeless bollard that integrates lighting



Simple bike racks that allow for bikes to remain upright and secure



Accessible waste and recycling station



## 5.4 Public Wireless Internet

With the numbers of individuals having access to devices with wireless internet it is highly desirable for wireless hotspots to be provided in outdoor public spaces. This will allow for individuals to remain engaged with their online lifestyle, but enjoy great public spaces. It also allows for parents to stay connected while engaging with their children outdoors, or alternatively, allow for young adults to stay connected while escaping the indoors.

### Requirements for corridor public WiFi:

- A reliable connection that is accessible from all locations within the Bow to Bluff Corridor.
- Security features to limit use and prevent illegal use of the service.
- Integrate the service with the City of Calgary libraries to provide access to services such as periodicals and electronic book downloads.



Public WiFi allows individuals to complete online tasks outdoors

## 5.5 Public Art

Public art within the corridor can take many different forms. Elements of public art can often have an educational component to them, or alternatively, change one's perspective on the world.

### Requirements for public art within the corridor:

- Promote public art and bespoke features to enhance spaces, buildings and blank facades and to add to the legibility of the public realm and the distinctiveness of the corridor.
- Public art should include a combination of rentable and temporary spaces and permanent pieces that are interspersed throughout the corridor.
- Certain public art pieces should incorporate an educational component.
- A public self-monitoring and respect program should be created along with each public art piece.



The *Limelight: Saturday night* art project by Sans Façon allows for individuals to be part of the art piece

## 5.6 Vegetation

Planting within the corridor should be used to create visual interest, follow CPTED principles and ensure replacement of trees over time.

### Key planting requirements along the corridor:

- Ensure that plantings provide interest on streets, soften buildings, delineate spaces.
- Promote tree planting and landscaping appropriate to the particular context. Require agreements (enforceable through appropriate planning conditions) to secure long-term maintenance of new soft landscape.
- Ensure existing biodiversity along the corridor is protected and enhanced (with particular emphasis on the escarpment).



The current planting on McHugh Bluff

## 5.7 Planting Palette

The planting palette is designed to transition from ornamental species in the central parts of the corridor to more native species as the corridor reaches the escarpment and the Bow River.



\*Native Species

## 6.0 PRIORITY PROJECTS

In response to the public consultation process, recommendations have been made for improvements to the corridor. These strategic improvements improve the public realm, and influence private developers and residents to make improvements to their spaces.

The list of priority projects is not an exhaustive list of every idea expressed for the corridor during the consultation process, but rather, a strategic identification of opportunities for future developments that will help achieve the vision for the future of the Bow to Bluff Corridor.

In identifying the priority projects for the Bow to Bluff Corridor, consideration was given to the following factors:

- Green pockets and linear strips
- Area street modification and programming
- Street furniture-lighting, treatment, landscaping
- Pedestrian sidewalks, walkways, and crossings
- Bikeways and infrastructure
- Regional pedestrian-bike trail connections
- Stair and slope to McHugh Bluff
- Connection to 10th Street NW and Riley Park
- Connection to Memorial Drive NW and Downtown
- Public art strategy
- Heritage elements, celebration-preservation
- Signage, sense of district, wayfinding, gateway
- LRT Station
- LRT pedestrian and vehicle crossings
- LRT right-of-way
- City-owned future redevelopment site
- Interface with existing-future abutting land uses
- Intersecting laneways
- Urban agriculture / community gardens
- Stormwater management (LID)

## 6.1 Implementation Methodology

The following project matrix identifies the location, brief project description, priority, contribution to the community, implementation responsibility, and very high level estimates of costs.

“Early gains” or projects that can be achieved in the short term over the next 3 years are identified. These are key projects that should be progressed by the Bow to Bluff RPC now in order to maintain progress towards the overall vision.

The criteria for the identification of priority projects are listed below:

### A. Site Availability

The availability of land is the most basic requirement. Much of the proposed public realm within the Bow to Bluff study area is owned by the City of Calgary.

### B. Feasibility

Projects which have unresolved feasibility issues are less attractive. Projects that are attractive for other reasons will demand to have feasibility questions answered quickly.

### C. Beneficial Impact

Schemes which have a major public and community impact are more likely to attract support and be prioritized.

### D. Existing Negative Conditions

Projects which overcome existing high profile negative conditions warrant more immediate attention.

### E. Associated Projects

Sometimes a public realm scheme will need to be implemented in order for another project to succeed. Conversely, a public realm scheme often relies on associated development in order to be worthwhile.

### F. Cost and Value

Public realm projects must be affordable and good value for money.

### G. Funding

Funding must be available at the right time.

### H. Political Will

Most projects, especially high profile and costly ones, need to be driven by a political will in order to succeed.

Using the above criteria, the projects outlined on following pages are identified as being vital to the success of the Bow to Bluff Corridor. Some of these are attractive because design work can begin now and could be implemented without delay. Other more complex projects are of interest because implementation will be a longer needs to be started as soon as possible. Others demand attention in response to the feedback received during the community consultation period.

### J. ‘Bite-Sized’ Change and Removing Barriers

A number of the small-scale or ‘bite-sized’ ideas and opportunities outlined in this Framework do not require significant funding, or any funding. Instead, their implementation is dependent on the removing policy and/or procedural barriers that make flexible, innovative, and/or temporary uses difficult to achieve for engaged citizens and corporate or third-party groups. To overcome these barriers, a next step should be to create a simple programming protocol that outlines the types of uses and operating responsibilities associated, thereby establishing increased clarity and accessibility, for both the City of Calgary and proposed users.

## 6.2 Priority Projects

Map Ref. (p.12)	Project	Priority	Key Steps Involved	Relative Impact on the Community	Key Personnel Involved	Method of Delivery	Estimated Timeframe	Opinion of Probable Cost*
1	Gradual ramp up McHugh Bluff with stairs to the half way point	High	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates.</li> <li>2. Liaise with City of Calgary Parks department as a key stakeholder.</li> </ol>	High - the community has expressed concern over the lack of connection in this location. Reinstating the steps and incorporating additional viewing features would have a huge impact on the community.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> </ul>	Public funding	~ 9 months	~\$150,000.00
2	Park Development (new park extending from the McHugh bluff Access to 10 Street NW)	Medium	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>2. Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>3. Liaise with adjoining landowners for community input.</li> </ol>	Intermediate - this improvement project would enhance a key gateway into the northern portion of the corridor. Although the connections already exist, improvements to the public realm would encourage more people to use this route into the corridor.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> </ul>	Public funding	~ 9 months	~\$75,000.00
3	Install WiFi internet within public parks	High	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Parks department to assess the feasibility of providing WiFi within parks and funding implications associated with the installation and maintenance of the units.</li> <li>2. Appoint contractor to prepare detailed cost estimates for installing and maintaining the WiFi devices within the parks.</li> </ol>	Intermediate - while the installation of WiFi internet would be considered an asset to the corridor, it is not necessarily integral to the enjoyment of the spaces. It could however be used as a pilot project for other parks within Calgary and could raise the profile of Hillhurst/ Sunnyside community as a leader in innovation.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> </ul>	Public funding	~ 6 months	~
4	Public art installation within the public parks	Medium	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Parks department to determine any potential sources of funding.</li> <li>2. Engage in discussions with local artists or public art specialists to prepare detailed designs for appropriate public art.</li> <li>3. Engage the local community for input to the proposed works and to encourage a sense of ownership to the final piece.</li> </ol>	Intermediate - public art is an important component to any public space. However, it needs to be carefully thought out and appropriate for the setting. It is preferable for park redevelopment (planting and streetscape improvements) to occur at the same to ensure a cohesive approach to design.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 9 months (pending park redevelopment)	~
5	Active living (fitness equipment) within the parks	High	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects for detailed design of play space and fitness equipment.</li> <li>2. Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>3. Liaise with City of Calgary Planning department (policy and approvals) to determine any available sources of funding from private developers.</li> </ol>	High - the community need to see physical improvements happening along the corridor and playspace/fitness equipment is considered to be a important part of enhancing how people use the space.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> <li>• City of Calgary - Planning</li> </ul>	Public and/or private funding	~ 6 months	~\$10,000.00- \$20,000 per zone
6	Improved connectivity between corridor and Riley Park over 10 Street NW	High	<ol style="list-style-type: none"> <li>1. Liaise with the City of Calgary Parks and Transportation departments to determine the alignment of the bike path.</li> <li>2. Conduct necessary community engagement for directly affected neighbours.</li> </ol>	High - the bike paths are already well used throughout the corridor and any extension to these would only bring further benefits to cyclists in the community.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Parks</li> <li>• City of Calgary - Transportation</li> </ul>	Public funding	~ 4 months	~\$175,000.00
7	Vegetated green wall at key sections of the LRT bridge	High	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Transit department to determine appropriate locations for green wall.</li> <li>2. Appoint specialist Landscape Architects to prepare detailed designs and associated cost estimates for green wall.</li> </ol>	High - the visual aesthetic of the LRT fences and walls has been a common theme throughout the engagement process. Improving this with a green wall should be seen as high priority.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Transit</li> </ul>	Public funding	~ 9 months	~
8	Temporary refurbishment to LRT fence by installing rentable art panels within the LRT transit fence and removing the top 0.5m (mesh section)	Medium	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Transit to determine the scope and feasibility of including art panels along the fence as well as determining appropriate locations.</li> <li>2. Engage in discussions with local artists and students (SAIT ACAD is considered a primary asset) to discuss possible installations.</li> </ol>	High - this would provide an innovative means of improving the aesthetic appearance of the fence and could be a pilot project for future transit areas within Calgary. The project would need to run parallel with any planting improvements.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• Calgary Transit</li> </ul>	Public funding	~ 6 months	~
9	Improved recreational pathway along west section of 9A Street NW	Medium	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed streetscape enhancement design.</li> <li>2. Liaise with City of Calgary Transportation and Line Assignment departments to ensure improvements are acceptable.</li> </ol>	Medium - this portion of the corridor provides an important link to McHugh Bluff. Improving the aesthetic quality of the space would provide an appropriate environment for the wider connections along the pathways.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Transportation</li> <li>• City of Calgary - Line Assignment</li> </ul>	Public and/or private funding	~ 9 months	~\$90,000.00

\*Anticipated Cost is an estimate only and will need to be verified by a qualified cost consultant. The costs noted above do not include professional fees.

Map Ref. (p.12)	Project	Priority	Key Steps Involved	Relative Impact on the Community	Key Personnel Involved	Method of Delivery	Estimated Timeframe	Opinion of Probable Cost*
10	Streetscape improvements to alley between 4 and 5 Avenue NW	Medium	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed streetscape enhancement design.</li> <li>2. Liaise with adjoining residents to ensure the proposed plans are supported.</li> <li>3. Liaise with City of Calgary Transportation and Waste and Recycling departments to ensure improvements are acceptable.</li> </ol>	Medium - the existing pathway connection adjacent to the LRT line is narrow. Diverting the pathway around the lane would provide an active use of this space and reduce the perception of crime due to the increased level of pedestrians and cyclists.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• Adjoining residents</li> <li>• City of Calgary - Transportation</li> <li>• City of Calgary - Waste and Recycling</li> </ul>	Public and/or private funding	~ 9 months	\$70,000.00
11	Integrated planting / green wall along LRT fence	High	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Transit department to determine appropriate locations for green wall.</li> <li>2. Appoint specialist Landscape Architects to prepare detailed designs and associated cost estimates for green wall.</li> </ol>	High - the LRT fence has been identified as a major eyesore along the corridor, and any planting improvements will have an immediate aesthetic benefit to the area.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• Calgary Transit</li> <li>• City of Calgary - Parks</li> </ul>	Public funding	~ 6 months	~
12	Improve surface treatment across key intersections to allow for safer pedestrian crossing	High	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed streetscape enhancement design.</li> <li>2. Liaise with City of Calgary Transportation department to ensure improvements are acceptable.</li> </ol>	High - a number of intersections exist that do not allow for easy pedestrian movement. Incorporating well designed streetscape elements will improve safety at these locations.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Transportation</li> <li>• City of Calgary - Transit</li> </ul>	Public and/or private funding	~ 9 months	~\$280,000.00
13	LRT station platform improvements	High	<ol style="list-style-type: none"> <li>1. Appoint consultant team (consisting of architects and engineers) to prepare detailed station enhancement designs.</li> <li>2. Liaise with City of Calgary Planning and Transportation departments for detailed design review.</li> </ol>	High - plans are in place to improve the LRT station to allow for four-car trains. A number of comments from community members focussed on the dated appearance of the station area. Improvements to this space would be integral to raising the standards within the corridor.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Planning</li> <li>• City of Calgary - Transportation</li> <li>• Calgary Transit</li> </ul>	Public funding (secured)	~ 12 months	~
14	Art installation on the wall at Safeway	High	<ol style="list-style-type: none"> <li>1. Liaise with the landowner to determine project scope, feasibility and funding implications.</li> <li>2. Engage in discussions with local artists and students (SAIT ACAD is considered a primary asset) to discuss possible installations such as a mural or light installation.</li> </ol>	High - the blank facade is highly visible from the LRT station and is considered a major blight along the corridor. Any improvement to this elevation would be immediately noticed.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• Landowner (Safeway)</li> <li>• Local artists and/or students</li> </ul>	Public and/or private funding	~ 4 months	~
15	Woonerf concept on 9A Street NW	High	<ol style="list-style-type: none"> <li>1. Appoint Landscape Architects to prepare detailed streetscape enhancement design.</li> <li>2. Liaise with City of Calgary Transportation and Line Assignment departments to ensure improvements are acceptable.</li> </ol>	High - this portion of the corridor is already well used by cyclists and pedestrians, therefore incorporating a shared surface approach to the streetscape would be noticed by many. The site also occupies a prominent location at the southern gateway to the corridor.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Transportation</li> </ul>	Public and/or private funding	~ 24 months	\$1,000,000.00
16	Green infrastructure integrated into new building and surrounding streetscape	High	<ol style="list-style-type: none"> <li>3. Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>4. Liaise with City of Calgary Corporate Properties as landowner.</li> <li>5. Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>6. Liaise with adjoining landowners for community input.</li> </ol>	High - this site is currently identified for affordable housing within the Hillhurst-Sunnyside ARP therefore redevelopment of this site could occur in the near future and. This would provide the corridor with a major focus and public space. Any improvements to the public space could be funded through new development initiatives.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary Corporate Properties</li> <li>• City of Calgary - Planning</li> </ul>	Public and/or private funding	~ 12 months	~
17	Open air market	High	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Corporate Properties to determine the timeframe for redevelopment of the warehouse site.</li> <li>2. Liaise with City of Calgary Parks and Development Approvals department to determine the feasibility for including a public market within the park.</li> </ol>	High - the central location of this space within the corridor means that it has the potential to be a major attraction for the community. However, the public space would be dependent on the redevelopment of the warehouse building.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary Corporate Properties</li> <li>• City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 12 months pending site redevelopment	~\$500,000
18	Temporary pavillion for public art	Medium	<ol style="list-style-type: none"> <li>1. Liaise with City of Calgary Planning and Development and Building Approvals departments to determine the feasibility for including a temporary pavillion for public art.</li> </ol>	Intermediate - this would provide a welcome focus for people using the intersection on 2 Avenue NW. It would need to be carefully managed to ensure that the space is well maintained and providing appropriate art pieces.	<ul style="list-style-type: none"> <li>• Bow to Bluff RPC</li> <li>• City of Calgary - Development and Building Approvals</li> </ul>	Public funding	~ 4 months	~\$15,000.00

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Map Ref. (p.12)	Project	Priority	Key Steps Involved	Relative Impact on the Community	Key Personnel Involved	Method of Delivery	Estimated Timeframe	Opinion of Probable Cost*
19	Mews (apartments above garages and laneway revitalisation efforts)	Medium	1. Liaise with City of Calgary Planning and Development and Building Approvals departments to discuss the potential of including statutory policies to encourage laneway development.	Intermediate - this would not have an immediate impact on the corridor but would enhance the way people use the rear lanes. Ultimately this would be developer driven.	<ul style="list-style-type: none"> <li>City of Calgary - Land Use and Planning Policy</li> <li>City of Calgary - Development and Building Approval Departments</li> </ul>	Private funding	Developer driven	~
20	Temporary kiosk/market/art gallery in Park 1 (near Memorial Drive)	Medium	1. Liaise with City of Calgary Parks and Development Approvals department to determine to determine the feasibility for including a temporary public market or art space within the park.	High - due to its prominent location at the intersection of Memorial Drive, the redevelopment of this space would provide a huge impact for this prime gateway site and the community that uses it.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary - Parks</li> <li>City of Calgary - Development and Building Approvals</li> </ul>	Public funding	~ 4 months	~\$
21	Park Development (4 Avenue NW)	Medium	<ol style="list-style-type: none"> <li>Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>Liaise with adjoining landowners for community input.</li> </ol>	High - 4 Avenue NW park is directly north of the LRT station and currently one of the most undesired park locations within the corridor. The development of this space would allow for a safe location for community gathering and build out from the transit platform.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary - Parks</li> <li>Adjacent residents</li> </ul>	Public funding	~ 9 months	~\$55,000.00
22	Park development (3 Avenue NW)	Low	<ol style="list-style-type: none"> <li>Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>Liaise with adjoining landowners for community input.</li> </ol>	Low - Because the 3 Avenue plaza space is very small and the space will be highly influenced by the transit platform improvements and the future warehouse development it should not occur till these are complete.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary - Parks</li> <li>Adjacent landowners</li> </ul>	Public funding	~ 4 months	~\$55,000.00
23	Central corridor civic space	High	<ol style="list-style-type: none"> <li>Liaise with City of Calgary Corporate Properties to determine the timeframe for redevelopment of the warehouse site.</li> <li>Liaise with City of Calgary Parks and Development Approvals department to determine to determine the feasibility for including a public market within the park.</li> </ol>	High - the central location of this space within the corridor means that it has the potential to be a major attraction for the community. However, the public space would be dependent on the redevelopment of the warehouse building.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary Corporate Properties</li> <li>City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 12 months pending site redevelopment	~\$350,000
24	Park development (2 Avenue NW)	High	<ol style="list-style-type: none"> <li>Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>Liaise with adjoining landowners for community input.</li> </ol>	High - 2 Avenue NW park is directly South of the warehouse site and one of the largest of the triangle parks. The development of this space would allow for a safe location for community gathering before the warehouse site is developed and act as an extension of this space when it is complete.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary Corporate Properties</li> <li>City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 9 months	\$90,000.00
25	Park development (1 Avenue NW)	Medium	<ol style="list-style-type: none"> <li>Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>Liaise with adjoining landowners for community input.v</li> </ol>	Medium - this is one of the smallest parks within the corridor therefore the potential uses of the space will always be limited. However, the addition of common paving elements within the cul-de-sac bulb will provide significant improvements to the public realm.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary Corporate Properties</li> <li>City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 6 months	~\$45,000.00
26	Park development (Memorial Active Living Park)	High	<ol style="list-style-type: none"> <li>Appoint Landscape Architects to prepare detailed design drawings and associated cost estimates for park redevelopment.</li> <li>Liaise with City of Calgary Parks department as a key stakeholder.</li> <li>Liaise with adjoining landowners for community input.</li> </ol>	High - due to its prominent location at the intersection of Memorial Drive, the redevelopment of this space would provide a huge impact for this prime gateway site and the community that uses it.	<ul style="list-style-type: none"> <li>Bow to Bluff RPC</li> <li>City of Calgary Corporate Properties</li> <li>City of Calgary - Parks</li> </ul>	Public and/or private funding	~ 18 months	~\$275,000.00

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- O2 Planning + Design: Mark Crisp, Doug Olson, Andrew Palmiere, Micheal Williamson
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### Citizen Resources and Planning Committee:

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- David White

### The City of Calgary

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- Second Cup
- Sunnyside Market
- Sunnyside School
- Sustainable Calgary
- Tamarack Institute for Community Engagement
- Unique Blends
- Vendome Cafe

# BOW TO BLUFF URBAN DESIGN FRAMEWORK

29 FEBRUARY 2012

Bow to Bluff Initiative, Resources and Planning Committee

c/o Hillhurst-Sunnyside Community Association

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