



## AGENDA

### CALGARY PLANNING COMMISSION

TO BE HELD 2018 FEBRUARY 22 AT 1:00 PM  
IN CALGARY POWER RECEPTION HALL

1. Confirmation of Agenda
2. Confirmation of Minutes of Previous Meeting
3. Consent Agenda
4. Consideration of Tabled/Referred Items
5. Consideration of Planning Items
6. Consideration of Miscellaneous Items
7. Adjournment

NOTE:  
PLANS SUBMITTED TO THE COMMISSION, AS PART OF THE DEPARTMENTAL REPORTS,  
ARE INCLUDED FOR THE SOLE PURPOSE OF ASSISTING THE COMMISSION IN MAKING  
A DECISION AND ARE NOT TO BE USED FOR ANY OTHER PURPOSE.

ISC: Unrestricted



CALGARY  
**PLANNING  
COMMISSION**  
CENTENNIAL

A CENTURY OF VISION 1911 - 2011

## CONSENT AGENDA

**ITEM NO.: 3.01** Jihad Bitar

**COMMUNITY:** ROYAL OAK (WARD 1)

**FILE NUMBER:** LOC2017-0377

**PROPOSED REDESIGNATION:** From: Residential – Contextual One Dwelling (R-C1) District  
To: Residential – Contextual One Dwelling (R-C1s) District

**MUNICIPAL ADDRESS:** 101 Royal Birch Crescent NW

**APPLICANT:** Pao Truong

**OWNER:** Pao Truong  
Lei Zhang

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

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**ITEM NO.: 3.02** Jeff Quigley

**COMMUNITY:** CITADEL (WARD 2)

**FILE NUMBER:** LOC2017-0364

**PROPOSED REDESIGNATION:** From: Residential – Contextual One Dwelling (R-C1) District  
To: Residential – Contextual One Dwelling (R-C1s) District

**MUNICIPAL ADDRESS:** 252 Citadel Way NW

**APPLICANT:** Khirad Zaidi

**OWNER:** Khirad Zaidi  
Kashif Shave

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

**ITEM NO.: 3.03**

Fraser McLeod

**COMMUNITY:**

HAWKWOOD (WARD 2)

**FILE NUMBER:**

LOC2017-0363

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

522 Hawkford Way NW

**APPLICANT:**

Wenqi Shi

**OWNER:**

Lihua Chen  
Wenqi Shi

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

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**ITEM NO.: 3.04**

Jennifer Cardiff

**COMMUNITY:**

HUNTINGTON HILLS (WARD 4)

**FILE NUMBER:**

LOC2018-0001

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

604 - 68 Avenue NW

**APPLICANT:**

Philip Cory Fraser

**OWNER:**

Philip Cory Fraser  
Ronna Fraser

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

**ITEM NO.: 3.05**

Joseph Yun

**COMMUNITY:**

SIGNAL HILL (WARD 6)

**FILE NUMBER:**

LOC2017-0366

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

113 Sierra Vista Terrace SW

**APPLICANT:**

Domenic Di Salvo

**OWNER:**

Domenic Di Salvo

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

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**ITEM NO.: 3.06**

Sabrina Brar

**COMMUNITY:**

MONTGOMERY (WARD 7)

**FILE NUMBER:**

LOC2017-0336

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

2222 - 44 Street NW

**APPLICANT:**

Maximus General Contracting

**OWNER:**

Maximus General Contracting

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

**ITEM NO.: 3.07**

Jennifer Cardiff

**COMMUNITY:**

MAYLAND HEIGHTS (WARD 10)

**FILE NUMBER:**

LOC2018-0003

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

1203 Motherwell Road NE

**APPLICANT:**

Molly K. Hamilton

**OWNER:**

Molly K. Hamilton  
Shawn Hamilton

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

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**ITEM NO.: 3.08**

Melanie Horkan

**COMMUNITY:**

ACADIA (WARD 11)

**FILE NUMBER:**

LOC2017-0407

**PROPOSED REDESIGNATION:**

From: Residential – Contextual One Dwelling (R-C1)  
District

To: Residential – Contextual One Dwelling (R-C1s)  
District

**MUNICIPAL ADDRESS:**

407 Acadia Drive SE

**APPLICANT:**

Kimberly Murfin

**OWNER:**

Kimberly Murfin  
Turc Harmesynn

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

## INDEX OF PLANNING ITEMS

**ITEM NO.: 5.01** Brad Bevill

**COMMUNITY:** MARLBOROUGH (WARD 10)

**FILE NUMBER:** LOC2017-0288

**PROPOSED REDESIGNATION:** From: Multi-Residential – Contextual Low Profile (M-C1d100) District  
To: Multi-Residential – High Density Low Rise (M-H1) District

**MUNICIPAL ADDRESS:** 3805 Marlborough Drive NE

**APPLICANT:** O2 Designs

**OWNER:** Minto Multi-Residential Income Partners I GP Inc

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

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**ITEM NO.: 5.02** Evan Fer

**COMMUNITY:** THORNCLIFFE (WARD 4)

**FILE NUMBER:** M-2018-002

**PROPOSED:** Laycock Park Pedestrian Bridge Report

**ADMINISTRATION RECOMMENDATION:** **FOR INFORMATION ONLY**

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**ITEM NO.: 5.03** Angelique Dean

**COMMUNITY:** SOUTHWOOD (WARD 11)

**FILE NUMBER:** LOC2017-0261

**PROPOSED REDESIGNATION:** From: Commercial – Neighbourhood 2 (C-C2) District  
To: Commercial – Corridor 1 f3.5h24 (C-COR1f3.5h24) District

**MUNICIPAL ADDRESS:** 9939 Elbow Drive SW

**APPLICANT:** O2 Planning and Design

**OWNER:** 1953075 Alberta Ltd. c/o Royop Development Corporation

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

**ITEM NO.: 5.04**

Angelique Dean  
(related to Item 5.05)

**COMMUNITY:**

SOUTHWOOD (WARD 11)

**FILE NUMBER:**

LOC2017-0273

**PROPOSED REDESIGNATION:**

From: Special Purpose – City and Regional  
Infrastructure (S-CRI) District

To: Special Purpose – School, Park and Community  
Reserve (S-SPR) District and DC Direct Control  
District to accommodate mixed use development

**MUNICIPAL ADDRESS:**

11015 Anderson Station Way SW

**APPLICANT:**

NORR Architects Engineers Planners

**OWNER:**

The City of Calgary

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

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**ITEM NO.: 5.05**

Angelique Dean  
(related to Item 5.04)

**COMMUNITY:**

SOUTHWOOD (WARD 11)

**FILE NUMBER:**

LOC2017-0273(OP)

**PROPOSED OUTLINE PLAN:**

Subdivision of 8.06 hectares ± (19.91 acres ±)

**MUNICIPAL ADDRESS:**

11015 Anderson Station Way SW

**APPLICANT:**

NORR Architects Engineers Planners

**OWNER:**

The City of Calgary

**ADMINISTRATION RECOMMENDATION:**

**APPROVAL**

**ITEM NO.: 5.06** Desmond Blik

**COMMUNITY:** BELTLINE (WARD 11)

**FILE NUMBER:** DP2017-2886

**PROPOSED DEVELOPMENT:** New: Retail and Consumer Service, Dwelling Unit  
(2 Buildings, 548 Units), Addition: Retail and Consumer  
Service

**MUNICIPAL ADDRESS:** 1520 - 4 Street SW and 319 - 15 Avenue SW

**APPLICANT:** Stantec Architecture

**OWNER:** Grosvenor Canada Limited

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

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**ITEM NO.: 5.07** Joseph Yun

**COMMUNITY:** BOWNESS (WARD 1)

**FILE NUMBER:** LOC2017-0281

**PROPOSED POLICY AMENDMENTS:** Amendment to the Bowness Area Redevelopment Plan

**PROPOSED REDESIGNATION:** From: Residential – Contextual One/Two Dwelling  
(R-C2) District

To: Residential – Grade-Oriented Infill (R-CG)  
District

**MUNICIPAL ADDRESS:** 4655 – 80 Street NW

**APPLICANT:** Lasting Legacies

**OWNER:** 1620719 Alberta Ltd (Cory Fell)

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

**ITEM NO.: 5.08** Fazeel Elahi

**COMMUNITY:** RICHMOND (WARD 8)

**FILE NUMBER:** LOC2017-0325

**PROPOSED POLICY AMENDMENTS:** Amendment to the Richmond Area Redevelopment Plan

**PROPOSED REDESIGNATION:** From: Residential-Contextual Two Dwelling (R-C2) District  
To: Residential – Grade-Oriented Infill (R-CG) District

**MUNICIPAL ADDRESS:** 2403 - 28 Avenue SW

**APPLICANT:** Civicworks Planning + Design

**OWNER:** South Calgary 24 Ltd

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**

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**ITEM NO.: 5.09** Adam Sheahan

**COMMUNITY:** SHAGANAPPI (WARD 8)

**FILE NUMBER:** LOC2017-0121

**PROPOSED REDESIGNATION:** From: Residential – Contextual One / Two Dwelling (R-C2) District  
To: Multi-Residential – Contextual Medium Profile (M-C2) District to accommodate Multi-Residential Development

**MUNICIPAL ADDRESS:** 1403, 1407, 1411 and 1415 – 26A Street SW

**APPLICANT:** Casola Koppe

**OWNER:** Fatima Borhot  
Mohamed Borhot  
Said Borhot  
David Brown  
Jennifer Brown  
Dimsdale Ranching Co Ltd  
Jason Johnston  
Laura Pasacreta

**ADMINISTRATION RECOMMENDATION:** **APPROVAL**



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.01**



LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

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**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge, there is no existing suite located on the parcel, however the application was submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

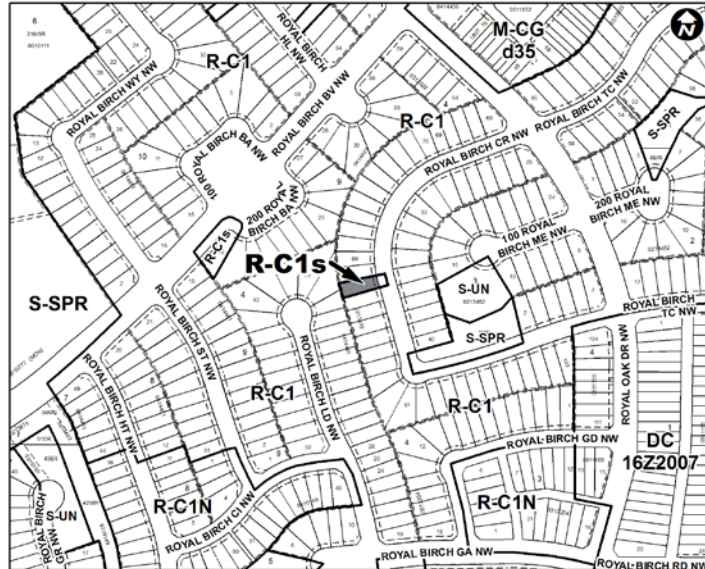
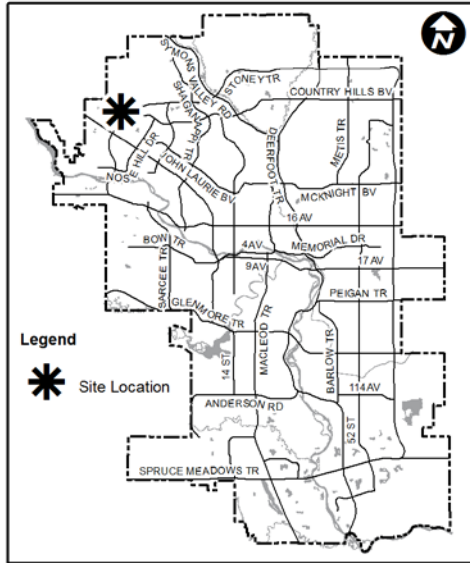
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

LOCATION MAPS



LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.04 hectares  $\pm$  (0.09 acres  $\pm$ ) located at 101 Royal Birch Crescent NW (Plan 0213482, Block 4, Lot 82) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

**Applicant:**

Pao Truong

**Landowner:**

Pao Truong  
Lei Zhang

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Royal Oak, the site is approximately 12.0 metres by 33.0 metres in size and is developed with a two-storey single detached dwelling and double-car garage that is accessed from Royal Birch Crescent NW. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 census, the following table identifies Royal Oak's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations.

<b>Royal Oak</b>	
Peak Population Year	2015
Peak Population	11,749
2017 Current Population	11,682
Difference in Population (Number)	-67
Difference in Population (Percent)	-1%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

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## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

### Rocky View/Calgary Intermunicipal Development Plan (IDP)

The subject site is located within the Rocky View/Calgary Intermunicipal Development Plan (IDP) Policy Area. The Policy Area contains lands immediately adjacent to the shared border. The subject site is not located within any special policy area or key focus area. As per the inter-municipal circulation and referral process, the application was circulated to Rocky View County for comments and no concerns were identified.

### Municipal Development Plan (2009)

The site is located within a “Residential Developing, Planned Greenfield with Area Structure Plan (ASP)” area as identified on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and Housing Diversity and Choice policies (subsection 2.3.1).

### Rocky Ridge Area Structure Plan (ASP) (Statutory – 1992)

There is no specific reference to the subject parcel or Secondary Suites in the policy.

## TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Royal Birch Crescent NW and there is no rear lane. The area is served by Calgary Transit via the transit bus service which is located approximately 250 metres from the site on Royal Oak Drive NW. The Tuscany light rail transit station is located approximately 2.8 kilometres from the site on Crowchild Trail NW. On-street parking adjacent to the site is unregulated.

## UTILITIES & SERVICING

Water, sanitary and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

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## ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

## GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

The Rocky Ridge/Royal Oak Community Association were circulated; administration received a 'no position' response.

### Citizen Comments

Two letters of objection were received by Administration. The following concerns were expressed in response to the proposal:

- Possibility of increased traffic and further lack of on-street parking in the area; and
- Tenants of secondary suites may lack care and consideration for the homes and the neighbourhood.

### Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission  
Secondary Suites  
PL 1264 (R2017-09)

0377  
↙

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?  
The benefits for home owner: It can bring additional revenue.  
The benefits for the surrounding community: 1/ It provides affordable and safe housing without affecting neighborhood character.  
2/ It increases the independent accommodation options for students, seniors and persons with disabilities.  
The benefits for the City of Calgary: 1/ The City can monitor the health and safety condition in the suite.  
2/ It provides affordable housing without government assistance.  
3/ It helps attract and retain people to live in Calgary.
  
- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?  
What was the response?  
We attempted to contact the neighbours, however, no one answered the door.  
We emailed the community association on Nov 31/2017, and was informed the email was forwarded to appropriate chair, HOA, and the Casades in Royal Oak. We have not heard back from them. Please see the attached printed email.
  
- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?  
1/ Our double garage can be shared by the tenants living in the primary dwelling and the secondary suite.  
2/ Two extra cars can be parked on the drive way.  
3/ In front of our house, there is room to park one car.  
4/ There is lots parking space available by the nearby playground with less than one minute walk.
  
- 4) Are there any potential negative impacts of this development that you are aware of?  
No, we are not aware.

**NOTE:** Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

ISC: Protected

J. Bitar

LAND USE AMENDMENT  
ROYAL OAK (WARD 1)  
WEST OF ROYAL BIRCH WAY NW ON ROYAL BIRCH  
CRESCENT NW

MAP 21NW

APPENDIX II

IMPORTANT TERMS

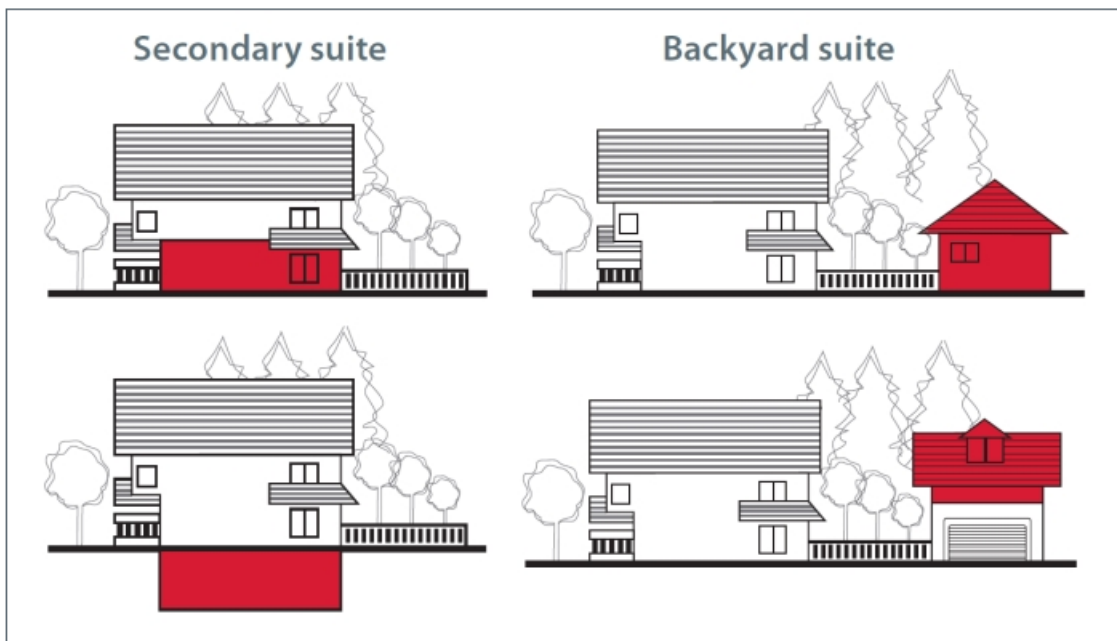
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.02**



LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration's knowledge, there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

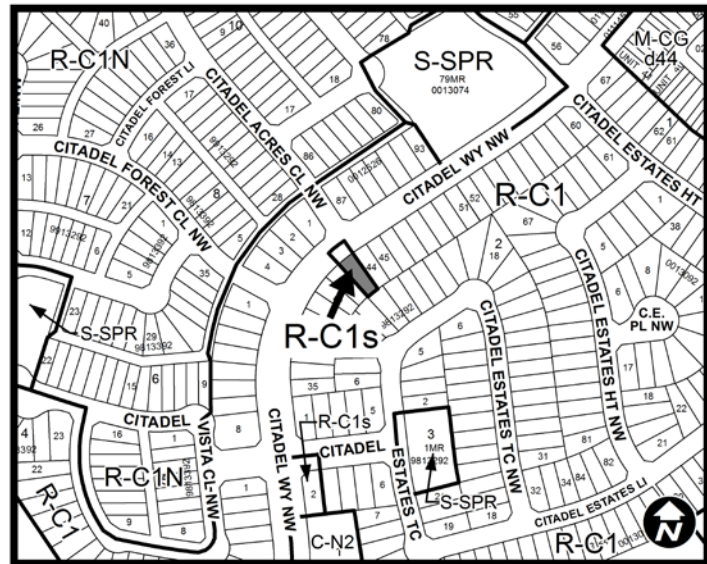
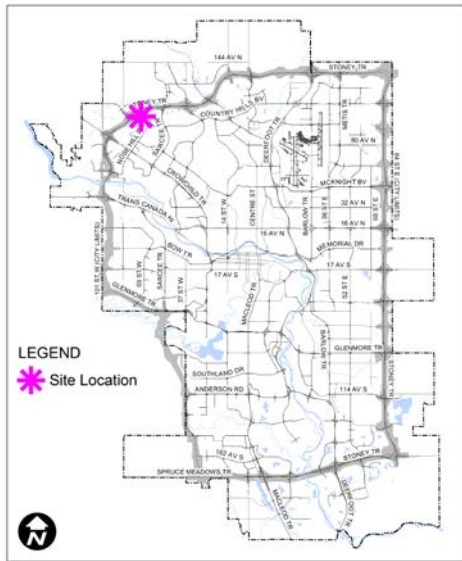
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

LOCATION MAPS



LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.04 hectares  $\pm$  (0.09 acres  $\pm$ ) located at 252 Citadel Way NW (Plan 9913292, Block 2, Lot 43) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

**Applicant:**

Khirad Zaidi

**Landowner:**

Khirad Zaidi  
Kashif Shavez

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Citadel, the site is approximately 11.6 metres by 33.0 metres in size and is developed with a single storey single detached dwelling with an attached double car garage that is accessed from Citadel Way NW. Single detached dwellings exist in all directions from the subject site.

According to data from The City of Calgary 2017 Census, the following table identifies Citadel's peak population and year, current population and the population amount and percentage difference between the peak and current populations.

<b>Citadel</b>	
Peak Population Year	2015
Peak Population	10,517
2017 Current Population	10,293
Difference in Population (Number)	-224
Difference in Population (Percent)	-2%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit and building permit processes. The parcel does not conform to all *Land Use Bylaw 1P2007* rules. As per the table below, the subject site does not meet the minimum parcel width requirements for the applicable land use district. Therefore, a development permit is required for the development of a Secondary Suite or a Backyard Suite. Administration is in support of the land use amendment application and the associated minor variance as there is sufficient parking and amenity space located on the subject lands for the proposed use.

LAND USE AMENDMENT  
 CITADEL (WARD 2)  
 CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

Bylaw Requirement		Provided	Variance (±)	
390 Parcel Width	12.0 m	11.6 m	-0.4 m	-3%

**LEGISLATION & POLICY**

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site. This land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

Crowchild Phase 4 Area Structure Plan (1983)

The subject lands are located within the Crowchild Phase 4 Area Structure Plan. Although the Crowchild Phase 4 Area Structure Plan does not specifically address the subject site, the proposal is in keeping with the residential land use policies of the ASP which encourage a wide variety of housing types.

**TRANSPORTATION NETWORKS**

Pedestrian and vehicular access to the site is available from Citadel Way NW and there is no rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 29 metres of the site on Citadel Way NW. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system.

**UTILITIES & SERVICING**

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

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**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required.

**GROWTH MANAGEMENT**

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

**PUBLIC ENGAGEMENT**

**Community Association Comments**

Administration received a response of 'No Objection' from the Citadel Community Association.

**Citizen Comments**

Administration received one (1) letter in opposition to the application. Reasons stated for opposition are related to increased noise from the subject site.

**Public Meetings**

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
CITADEL (WARD 2)  
CITADEL WAY NW AND CITADEL ACRES CLOSE NW

MAP 22NW

APPENDIX I

APPLICANT'S SUBMISSION



**Land Use Redesignation Applicant's Submission**

Secondary Suites

PL 1264 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

We would like to make legal secondary suite to provide safe accommodation to our parents. Secondary suite will provide extra space and help us to live together as a family. Our parents will get assistance they may need from family. This will allow them privacy and independence. As a result, they will be able to live independently in the community. Legalization of secondary suites can promote community economic development. for example, it can provide quality of life for all individuals it can also provide safe environment and safe place to live.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

Neighbors and community association support legal basement suites for family / parents to live together and for care of the elderly provided the suite meets the requirements as laid out by the City of Calgary (i.e. Heating, electrical, water, fire protection, parking etc.)

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

We have double attached garage and 2 parking available on drive way

- 4) Are there any potential negative impacts of this development that you are aware of?

No

**NOTE:** Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

ISC: Protected

J. Quigley

APPENDIX II

IMPORTANT TERMS

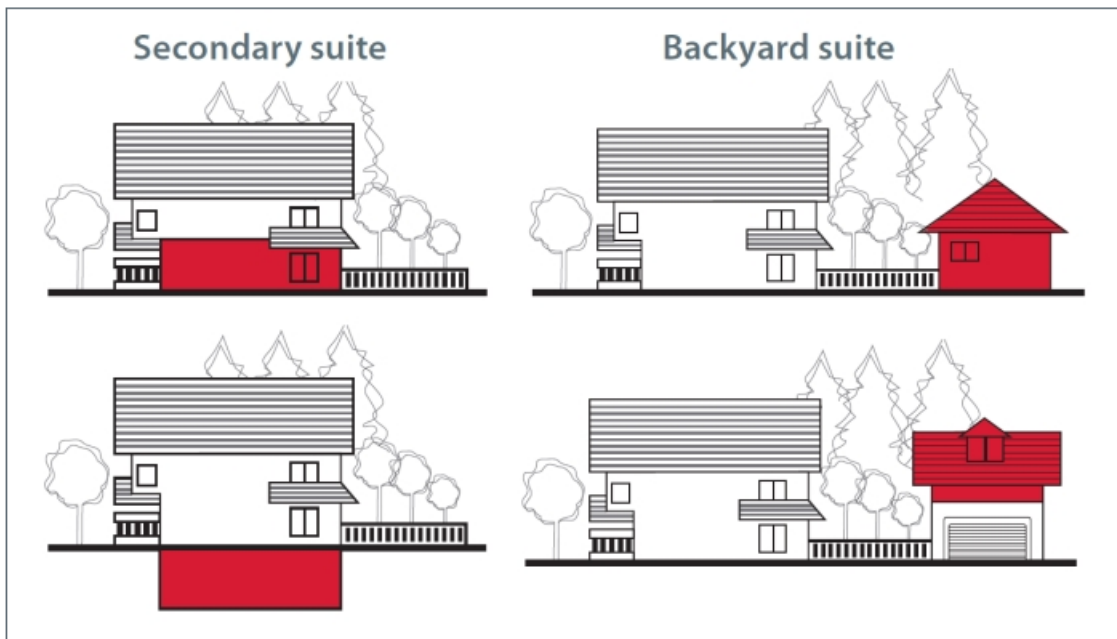
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a staff vertically. The background is white.

**Calgary Planning Commission 2018 February 22**

**Item 3.03**



LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

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**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is an existing suite located on the parcel and the application was submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

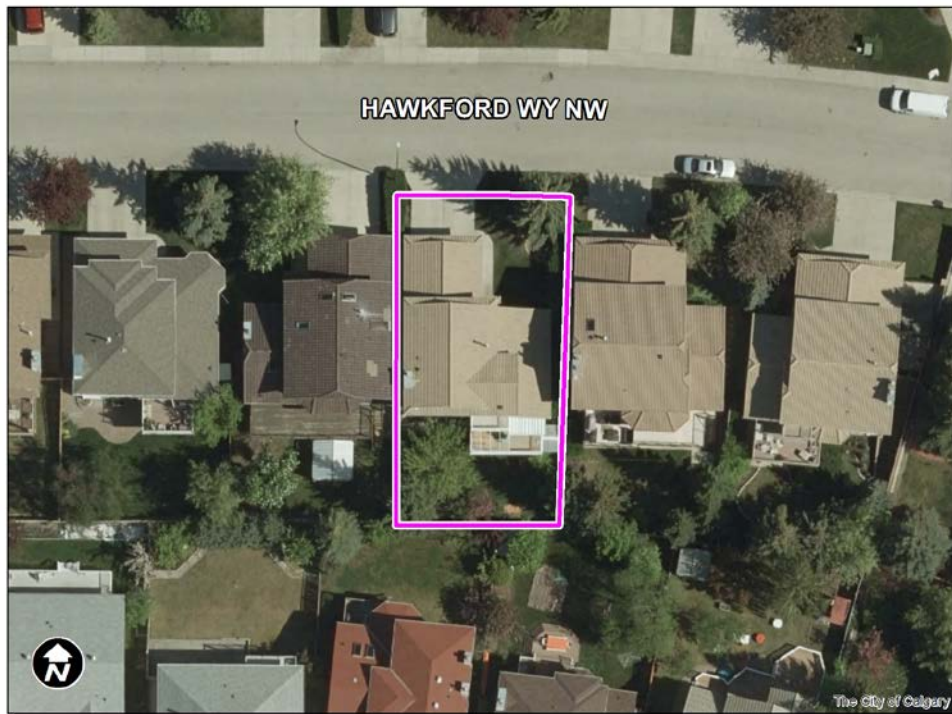
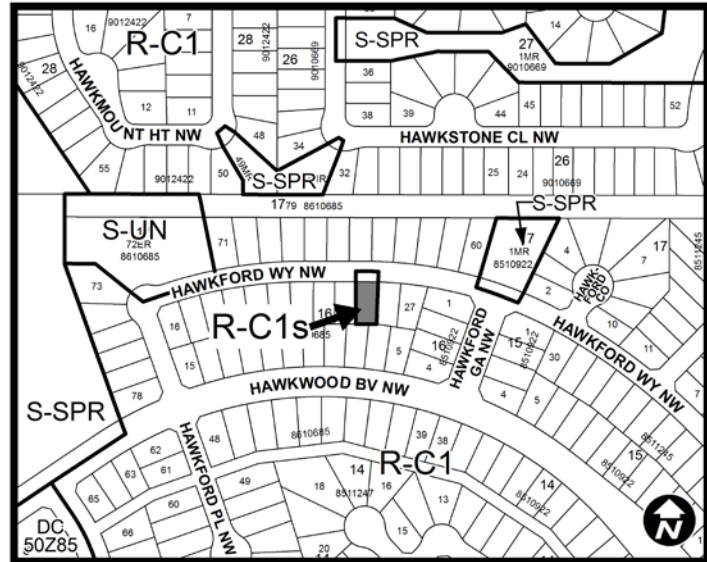
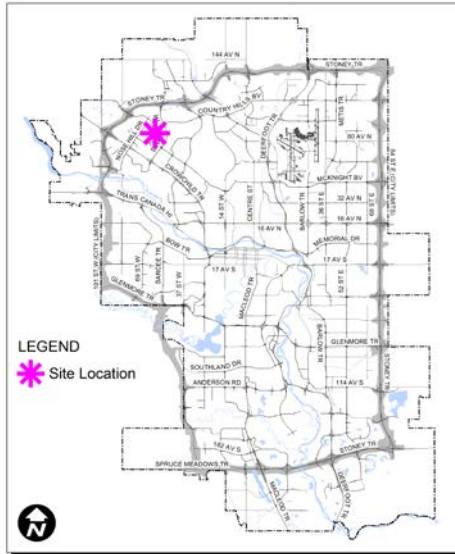
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and the Crowchild Phase 3 Area Structure Plan, and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

LOCATION MAPS



LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares  $\pm$  (0.11 acres  $\pm$ ) located at 522 Hawkford Way NW (Plan 8610685, Block 16, Lot 25) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

**Applicant:**

Wenqi Shi

**Landowner:**

Lihua Chen  
Wenqi Shi

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Hawkwood, the site is approximately 15 metres by 31 metres in size and is developed with a two-storey single detached dwelling with an attached two-car garage that is accessed from Hawkford Way NW. Surrounding development consists of low-density residential R-C1 development to the north, east, south and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Hawkwood's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

<b>Hawkwood</b>	
Peak Population Year	1998
Peak Population	11,165
2017 Current Population	9,512
Difference in Population (Number)	-1,653
Difference in Population (Percent)	-15%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

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## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

### Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

### Crowchild Phase 3 Area Structure Plan (1979)

The site is within the Residential Cell “C” area of the Crowchild Phase 3 Area Structure Plan (ASP). Although the ASP does not mention secondary suites, the document outlines that residential development should allow for the opportunity for unique design and planning innovation.

## TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from Hawkford Way NW and there is no rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 450 metre walking distance of the site on Hawkstone Drive NW. On-street parking adjacent to the site is unregulated.

## UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

## ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

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## GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

Administration received a letter of general opposition to the application from the Hawkwood Community Association (APPENDIX II).

Reasons stated for general opposition are summarized as follows:

- Homeowners may not reside on the property, which may result in two rental dwellings on one lot, and can lead to poor maintenance;
- Parking could become an issue, if it is not adequately accounted for on-site; and
- Hawkwood already has a number of illegal secondary suites.

### Citizen Comments

Administration received nineteen (19) letters in opposition to the application.

Reasons stated for opposition are summarized as follows:

- Increase in activity and transient population in the neighbourhood;
- Negative impact on available parking;
- Increase in traffic and congestion in close proximity to a school and playground zone;
- Decrease in property values;
- Opposition to renters;
- Implications of multiple garbage bins due to additional dwelling unit;
- Concerns regarding the regulation of secondary suites;
- Lack of communication between the applicant and adjacent homeowners;
- Potential that the property will become a double rental property in the future;
- The property has no back alley;
- Impacts of increased density on residents who bought into an R-C1 community;
- Previous experiences with renters at this property and in the neighbourhood;
- The community has a large number of rental homes with absentee homeowners;
- Desire to keep the single detached dwelling character of the neighbourhood;
- Impacts to safety of adjacent properties based on past experiences;
- Property has been rented out in the past; and
- Property may set a precedent for other land use redesignations in the community.

**LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL**

**MAP 14NW**

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**Public Meetings**

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

APPENDIX I

APPLICANT'S SUBMISSION



Land Use Redesignation Applicant's Submission

Secondary Suites

PL 1264 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

For us: Provide additional household income to pass through current economic recession.  
Provide living space for our son's young family in the near future.  
For city: Provide safe and affordable housing for tenant. Make use of existing infrastructure  
For community: promote community's reputation by sharing our wonderful living experience to new people we bring in.

2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?  
What was the response?

3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

our family only have one car. we would rent our suite to a single person or a couple who only have one car. we have double garage to share.

4) Are there any potential negative impacts of this development that you are aware of?

we don't think that there will be any negative impact.

**NOTE:** Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

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APPENDIX II

LETTERS SUBMITTED

Good morning Fraser,

My apologies ... I totally missed this one so here are our comments. These comments are standard for us regarding secondary suites:

We are generally opposed to rezoning RC1 to RC1S for the following reasons:

1. While there may only be one renter at this time, in future, there may end up being two renters if the house is sold and the homeowner does not reside in the house. This can lead to the property not being properly cared for and maintained.
2. Parking could become an issue if there is not adequate off-street parking, and
3. Hawkwood already has a number of secondary suites most of which are illegal which is a concern to us as the City cannot inspect nor enforce the rules in these cases.

Donna Chapman  
Planning and Development  
Hawkwood Community Association

LAND USE AMENDMENT  
HAWKWOOD (WARD 2)  
NORTH OF HAWKWOOD BOULEVARD NW AND EAST OF  
HAWKWOOD SCHOOL

MAP 14NW

APPENDIX III

IMPORTANT TERMS

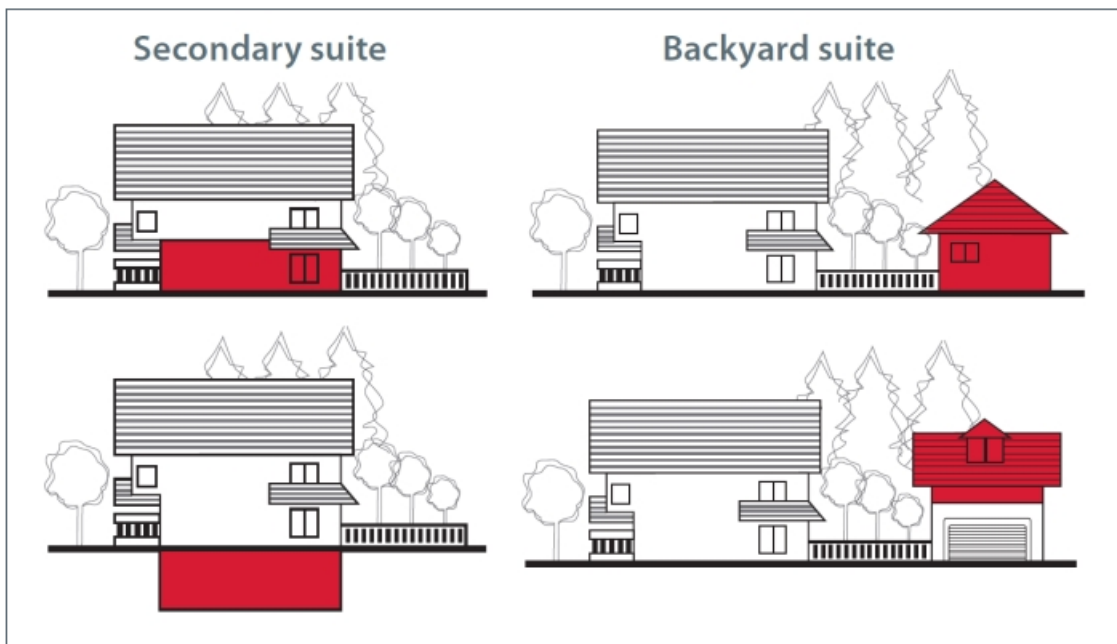
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a staff vertically. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.04**



LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

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**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

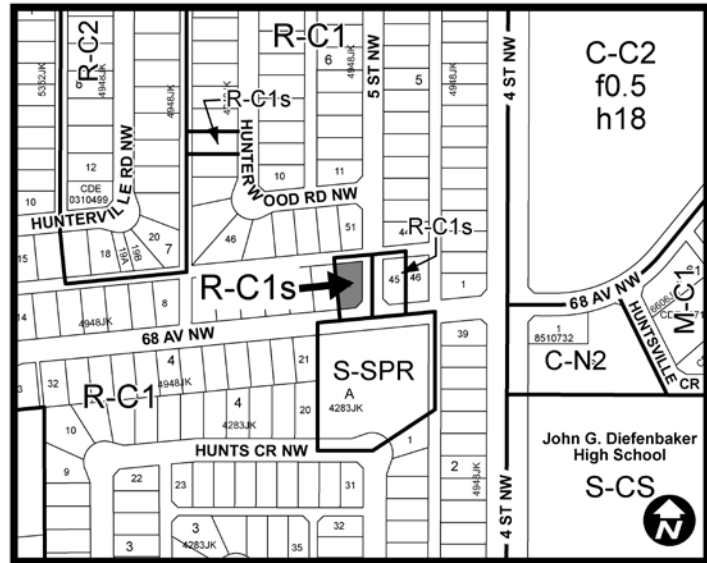
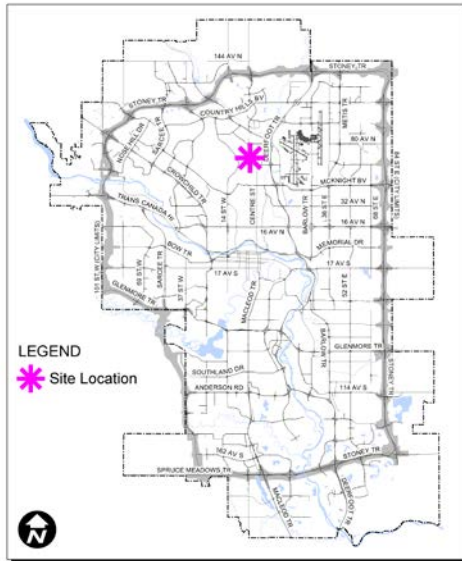
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

LOCATION MAPS



LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares  $\pm$  (0.15 acres  $\pm$ ) located at 604 - 68 Avenue NW (Plan 4948JK, Block 7, Lot 1) from Residential – Contextual One Dwelling (R-C1) District **to** a Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

**Applicant:**

Philip Cory Fraser

**Landowner:**

Philip Cory Fraser  
Ronna Fraser

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Huntington Hills, the site is approximately 13 metres by 33 metres in size and is developed with a one-storey single detached dwelling and a detached two-car garage that is accessed from the rear lane. Single detached dwellings exist to the north, east, and west of the site. A City park with playground exists to the south of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Huntington Hills peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

<b>Huntington Hills</b>	
Peak Population Year	1983
Peak Population	15,904
2017 Current Population	13,154
Difference in Population (Number)	-2,750
Difference in Population (Percent)	-17%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

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## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

### Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Huntington Hills.

## TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from both 68 Avenue NW and 5 Street NW, as well as from the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 150 metre walking distance of the site on 4 Street NW. BRT Route 300 is located approximately 500 metres walking distance of the site on Centre Street. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system.

## UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

## ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

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**GROWTH MANAGEMENT**

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

**PUBLIC ENGAGEMENT**

**Community Association Comments**

Administration did not receive a response from the Huntington Hills Community Association.

**Citizen Comments**

Administration did not receive any responses from citizens.

**Public Meetings**

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

APPENDIX I

APPLICANT'S SUBMISSION

December 20, 2017

To whom it may concern,

My wife and I own and live at 604 – 68 Ave, NW. We are applying to change the land use designation from RC-1 to RC-1s. The property is our personal residence where we live with our four children. I was raised in our home and we have owned it since we purchased it from my mother in 2001.

With several of our children becoming adults and starting post secondary education, we feel that a self contained suite would be ideal for one or both of them to venture into independent living in a safe place with affordable rent. After that it would be an excellent rental accommodation for someone desiring to make their home in a fully established, family friendly neighborhood.

Our home is located in an ideal location close to many great schools and services, on a corner lot with three off street parking spots, and directly across from a good sized park. It's only 75 meters from the bus stop for routes to down town, SAIT, U of C, Foothills Hospital, MRU, James Fowler and Saint Francis High Schools. Many amenities and schools are a short walk, including Superstore, Murray T. Copot Arena and Thornhill Aquatic & Rec. Center, Judith Umbach Library, Diefenbaker High School, Sir John A. Macdonald Jr. High, St. Helena Jr. High, and Huntington Hills Elementary Schools, as well as Huntington Hills Skate Park, and Huntington Hills Community Center.

The park across the street is great for throwing a frisbee, kicking a soccer ball, or playing with young children on the playground equipment. Nose Hill Park is close by and there are great bike paths in the area for cycling enthusiasts. Many sports programs are offered through the community center including soccer, curling, and hockey to name a few. This is a great community for active people to live in.

Thank you for considering our application.

Sincerely



Cory Fraser

LAND USE AMENDMENT  
HUNTINGTON HILLS (WARD 4)  
68 AVENUE NW WEST OF 4 STREET NW

MAP 9N

APPENDIX II

IMPORTANT TERMS

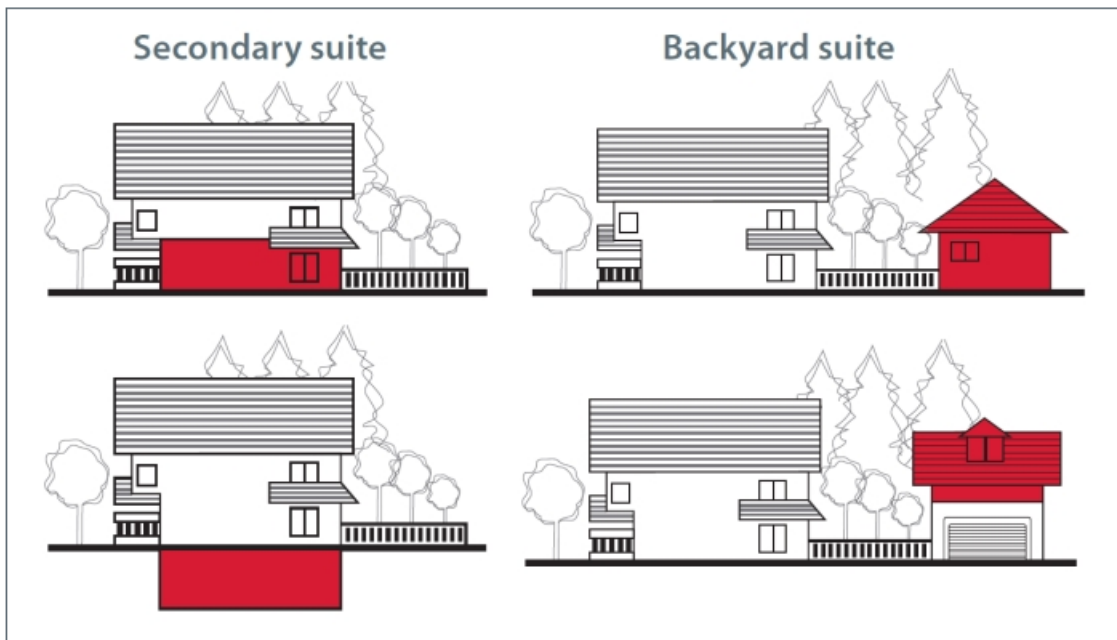
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms

**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.



**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.05**



LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

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**EXECUTIVE SUMMARY**

This Land Use Amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. There is an existing basement secondary suite located on the parcel. This application was submitted as a result of a complaint. The City’s records does not indicate if the illegal use has been discontinued.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with Land Use Amendment and Development Permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

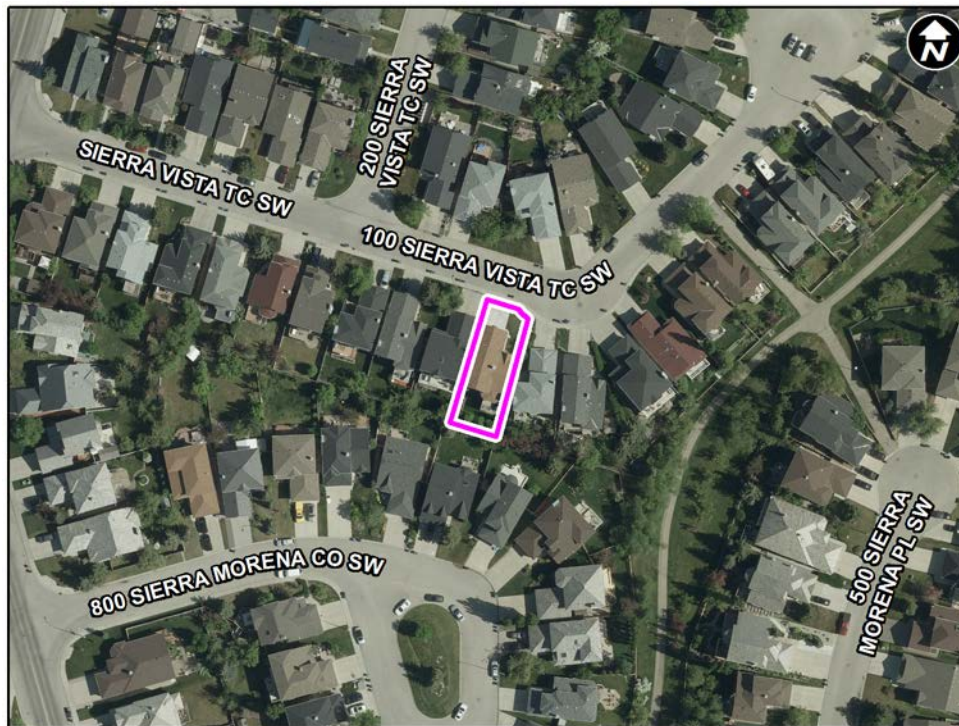
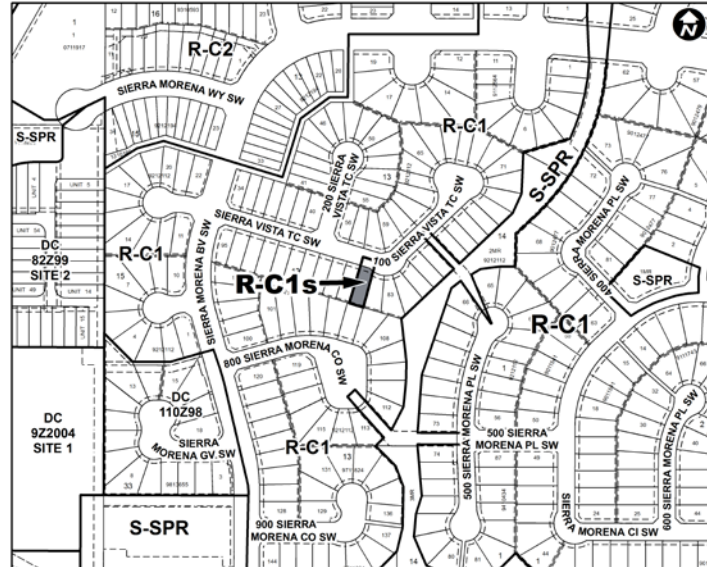
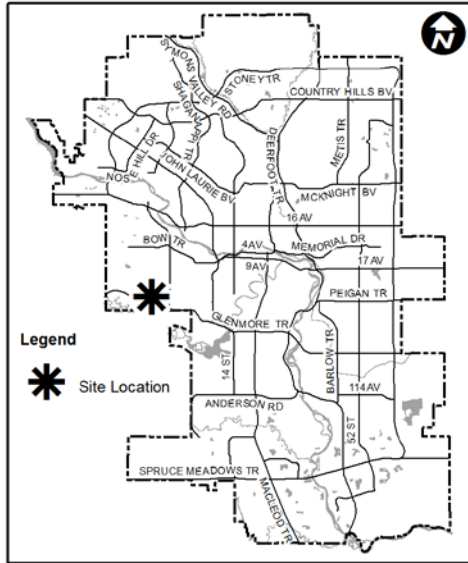
**REASONS FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

LOCATION MAPS



LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.04 hectares  $\pm$  (0.10 acres  $\pm$ ) located at 113 Sierra Vista Terrace SW (Plan 9212112, Block 13, Lot 85) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One (R-C1s) District.

LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

**Applicant:**

Domenic Di Salvo

**Landowner:**

Domenic Di Salvo

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Signal Hill, the site is approximately 12.0 metres by 34.0 metres. An existing two-storey single detached dwelling occupies the parcel with a two-car garage that is accessed from Sierra Vista Terrace SW. Two-storey single-detached dwelling units exist to the north, east, south, and west of the site. There is an existing basement secondary suite located on the parcel. This application was submitted as a result of a complaint. The City's records does not indicate if the illegal use has been discontinued.

According to data from The City of Calgary 2017 Census, the following table identifies Signal Hill's peak population of 14,177 in 2005, current 2017 population of 13,533 and -4 percent difference between the peak and current populations.

<b>Signal Hill</b>	
Peak Population Year	1973
Peak Population	14,177
2017 Current Population	13,533
Difference in Population (Number)	-644
Difference in Population (Percent)	-4%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required. In this instance, the secondary suite is proposed in the basement.

LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

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## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

### Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

Signal Hill is not subject to a Local Area Plan.

## TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from the front driveway off Sierra Vista Terrace SW, and there is no rear lane. The area is served by Calgary Transit bus route 39 (Coach Hill). The bus stop location is within 200 metres walking distance of the site on Sierra Morena Boulevard SW. On-street parking adjacent to the site is unregulated.

## UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time.

## ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

## GROWTH MANAGEMENT

This Land Use Amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

Administration did not receive a response from the Signal Hill Community Association.

### Citizen Comments

Administration received one (1) letter in opposition to the application.

Reasons stated for opposition are summarized as follows:

- allegations of unsavoury activity occurring within the premise; and
- Various alleged nuisances associated with unruly tenants on premise.

### Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
SIGNAL HILL (WARD 6)  
SIERRA VISTA TERRACE SW

MAP 2W

APPENDIX I

APPLICANT'S SUBMISSION

0366



Land Use Redesignation Applicant's Submission  
Secondary Suites  
HL 1264 (4/2017.03)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments must be limited to the area designated on this page to ensure it will fit the space requirements of the report.

1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

DOWNSIZE SECONDARY SUITE

2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

~~XXXXXXXXXX~~ FOR THE BETTER PART I GET ALONG WITH MY NEIGHBORS BUT NOT SURE WHAT THEY THINK ABOUT A IN LOW SUITE.

3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

CARAGE & DRIVEWAY

4) Are there any potential negative impacts of this development that you are aware of?

NO

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Freedom of Access to Information Act. If you consider this information to be personal, do not put it on the plans.

ISC Protected

**APPENDIX II**

**IMPORTANT TERMS**

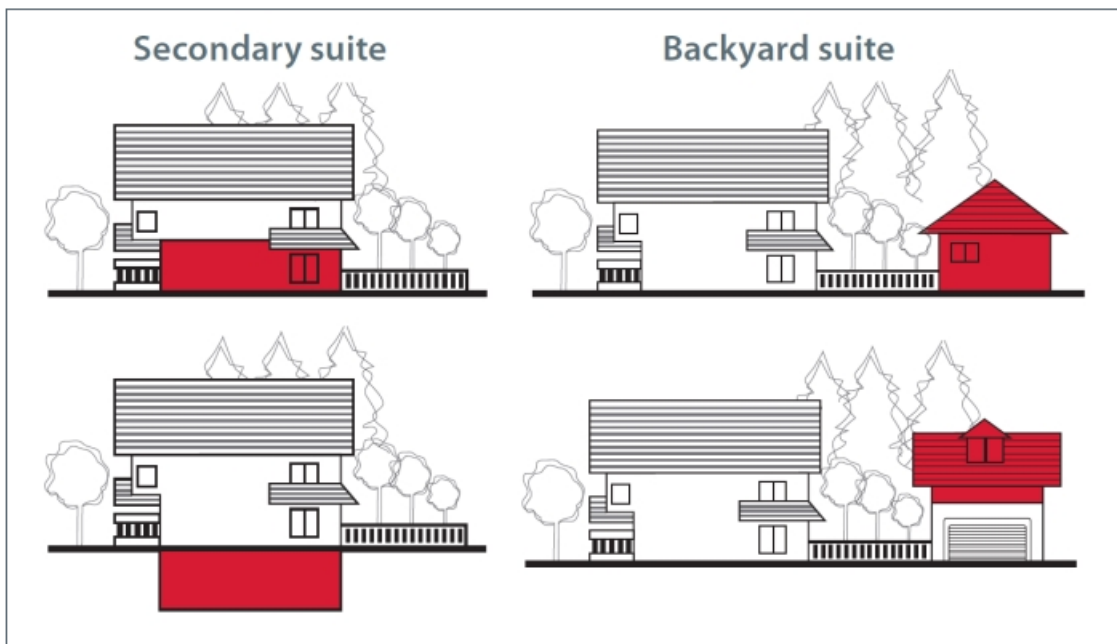
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

**Important terms**



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through the eyepiece of a theodolite. The theodolite is mounted on a tripod. The surveyor's right hand is on the instrument, and a pencil is visible in their left hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.06**



LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration’s knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

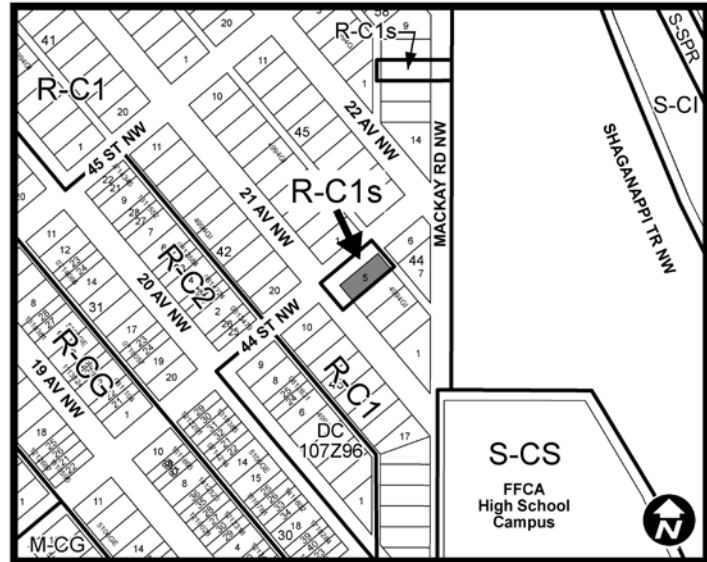
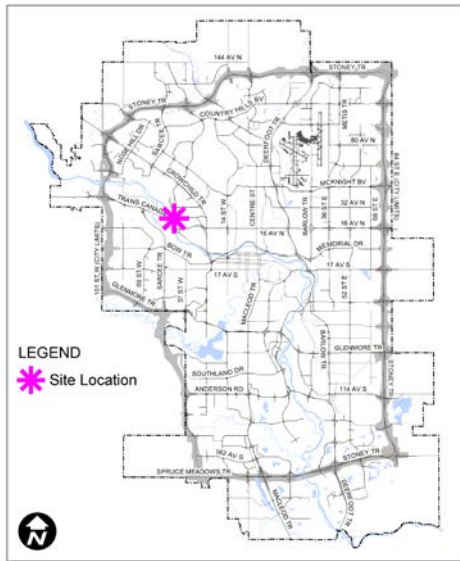
**REASONS FOR RECOMMENDATION:**

The proposed R-C1 district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and the Montgomery Area Redevelopment Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

LOCATION MAPS



LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares  $\pm$  (0.14 acres  $\pm$ ) located at 2222 - 44 Street NW (Plan 4994GI, Block 44, Lot 5) from Residential – Contextual One Dwelling (R-C1) District **to** Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

**Applicant:**

Maximus General Contracting

**Landowner:**

Maximus General Contracting

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Montgomery, the site is approximately 15 metres by 36 metres in size and is developed with a one-storey single detached dwelling with a detached two-car garage that is accessed from 44 Street NW. Single detached dwellings exist to the north, east, south, and west of the site. The Foundations for the Future Charter Academy High School Campus is located south east of the subject parcel.

According to data from The City of Calgary 2017 Census the following table identifies Montgomery's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

<b>Montgomery</b>	
Peak Population Year	1969
Peak Population	5,287
2017 Current Population	4,353
Difference in Population (Number)	-934
Difference in Population (Percent)	-18%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

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## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

### Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

### Montgomery Area Redevelopment Plan (2008)

The site is located within the “Low Density Residential” area on the Future Land Use Plan (Figure 1.3) in the *Montgomery Area Redevelopment Plan (ARP)*. Low-density residential development is encouraged in this area. In these locations, all R-1, R-2 and Direct Control Districts with low-density residential guidelines shall continue. Secondary suites are compatible and consistent with the low-density residential character of this area.

## TRANSPORTATION NETWORKS

Pedestrian and vehicular access to the site is available from 44 Street NW and there is a rear lane. The area is served by Calgary Transit Primary Transit Network bus service with a bus stop location within approximately 350 metres walking distance of the site on Bowness Road NW. On-street parking adjacent to the site is unregulated (through the Calgary Parking Authority’s residential parking permit system).

## UTILITIES & SERVICING

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

## ENVIRONMENTAL ISSUES

An Environmental Site Assessment was not required.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

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**GROWTH MANAGEMENT**

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

**PUBLIC ENGAGEMENT**

**Community Association Comments**

Administration received a letter in support to the application from the Montgomery Community Association (APPENDIX II).

**Citizen Comments**

Administration did not receive any responses from citizens.

**Public Meetings**

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

APPENDIX I

APPLICANT'S SUBMISSION



**Land Use Redesignation Applicant's Submission**

**Secondary Suites**  
(R2017-07)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

The redesignation will help to meet the need in the City for affordable housing. The proposed development on the site will meet all the requirements of a secondary suite. Initially it will be used to provide housing for my 87 year old mother whom is ready to move out of her principal residence but not ready to move into a seniors home. The long term use will be to provide affordable housing for students or staff at the University of Calgary, Foothills Hospital or Children's Hospital. My wife and I will benefit from additional retirement income.

- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

I have known the two adjacent neighbors since 2007 when I purchased the property. They have been aware of my plans for years and welcome the redevelopment plans for the property.

- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

The new double car garage will provide parking for the primary dwelling with access from the alley on the north side. The existing driveway access off of 44 Street NW will be maintained and provide off street parking for the secondary suite.

- 4) Are there any potential negative impacts of this development that you are aware of?

No negative impact. This proposal is consistent with the ongoing redevelopment in the inner city community of Montgomery.

**NOTE:** Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans.

LAND USE AMENDMENT  
MONTGOMERY (WARD 7)  
SOUTH OF 44 STREET NW AND EAST OF 21 AVENUE NW

MAP 25W

APPENDIX II

LETTERS SUBMITTED



03 December, 2017

Giyán Brenkman  
Planning Development and Assessment  
City of Calgary  
[Giyán.Brenkman@calgary.ca](mailto:Giyán.Brenkman@calgary.ca)  
403.268.2678

Dear Giyan:

**RE: LOC2017-0336 4440 21 AV NW**  
Land Use Amendment – R-C1 to R-C1s

The Planning Committee has reviewed the above-noted proposal for a Land Use Amendment.

The Committee has no issues with this application.

Regards,

Janice Mackett,  
Planning Committee Chair  
Montgomery Community Association  
cc: Councillor Druh Farrell  
Community Liaison – Ward 7  
Ms. Marilyn Wannamaker, President, MCA  
Members of the MCA Planning Committee

APPENDIX III

IMPORTANT TERMS

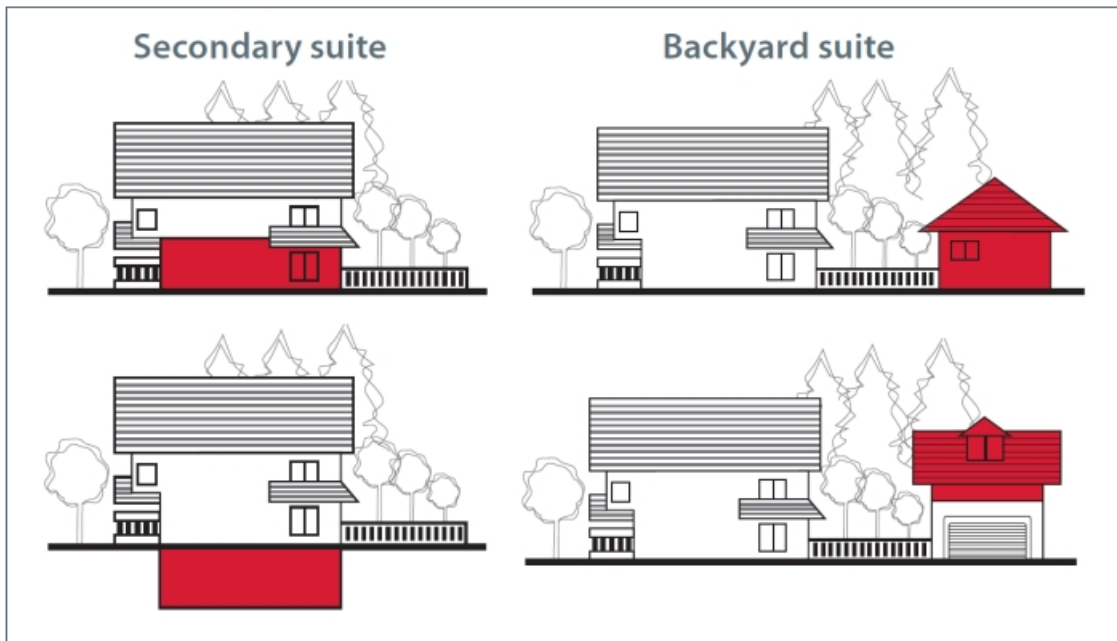
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

Important terms



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

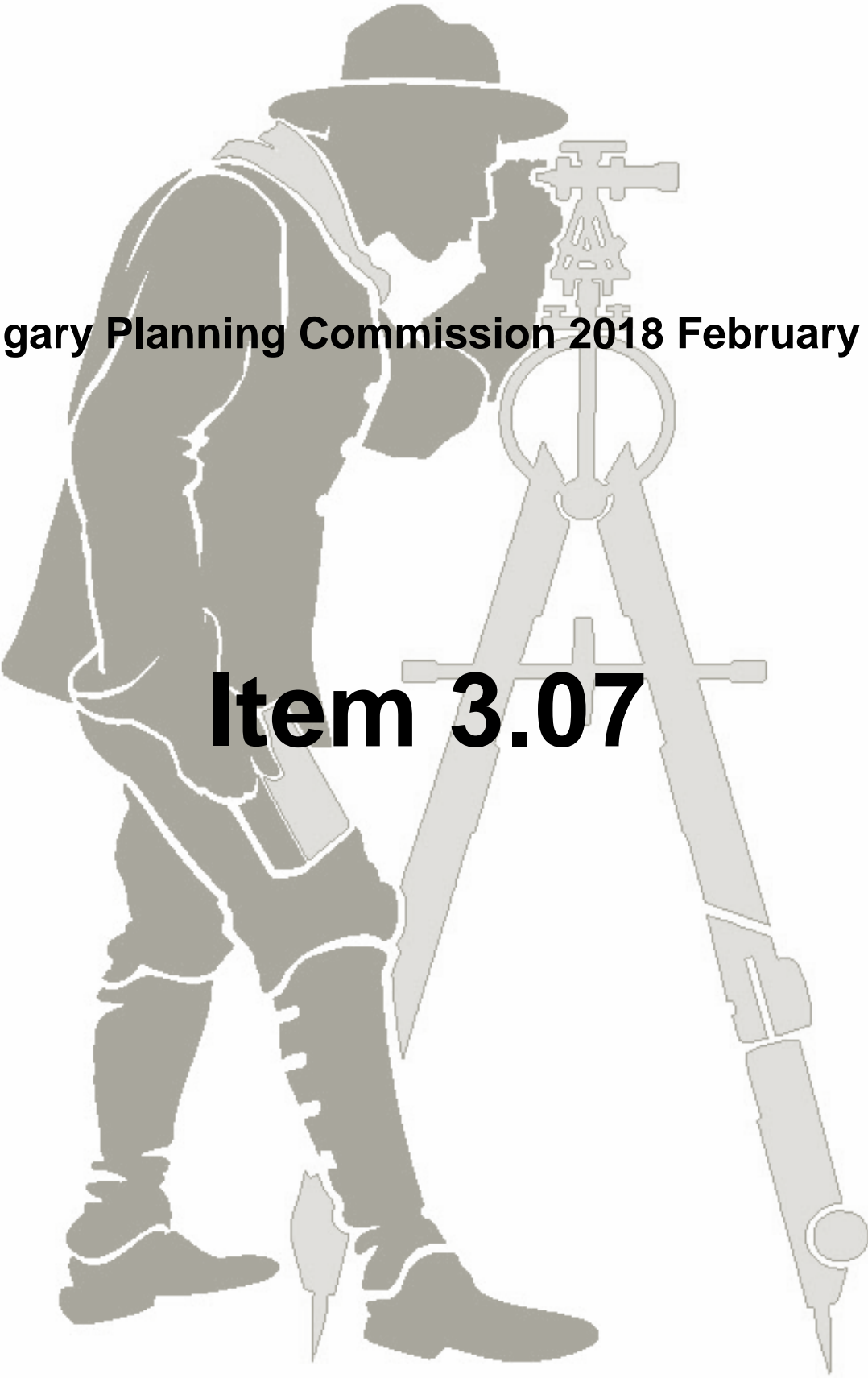
**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.





**Calgary Planning Commission 2018 February 22**

**Item 3.07**





LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration's knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

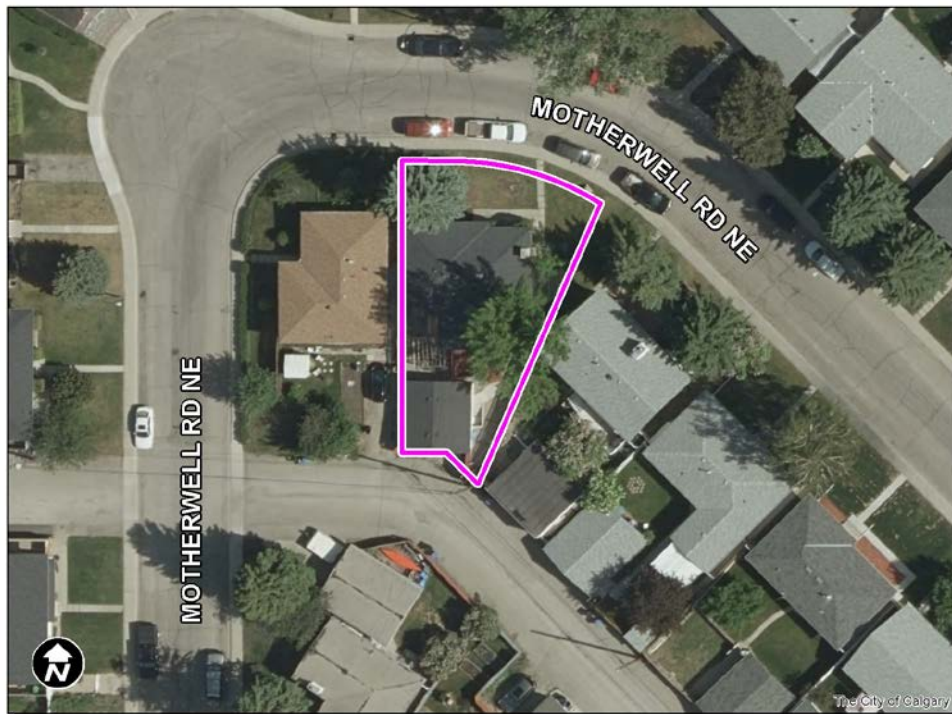
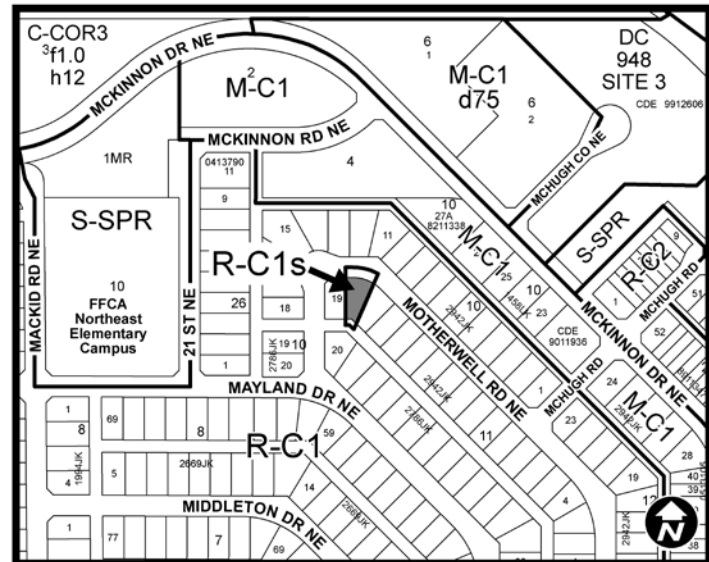
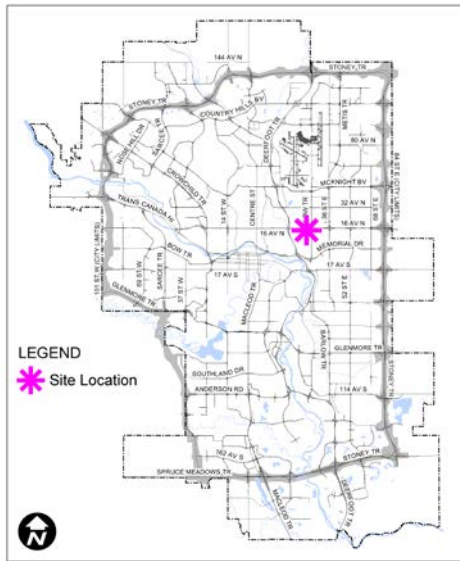
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

LOCATION MAPS



LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.05 hectares  $\pm$  (0.12 acres  $\pm$ ) located at 1203 Motherwell Road NE (Plan 2942JK, Block 11, Lot 18) from Residential – Contextual One Dwelling (R-C1) District **to** a Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

**Applicant:**

Molly K. Hamilton

**Landowner:**

Molly K. Hamilton  
Shawn Hamilton

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Mayland Heights, this pie-shaped site is approximately 16 metres by 31 metres in size and is developed with a one-storey single detached dwelling and detached two-car garage that is accessed from the rear lane. Single detached dwellings exist to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Mayland Heights' peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations if any.

<b>Mayland Heights</b>	
Peak Population Year	1986
Peak Population	6,375
2017 Current Population	5,808
Difference in Population (Number)	-567
Difference in Population (Percent)	-9%

A land use amendment to allow for a Secondary Suite was processed in the spring of 2017 under LOC2017-0084. While Calgary Planning Commission (CPC) had recommended approval of the application, City Council refused the application. A number of residents expressed concern over the application at the public hearing.

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

**LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE**

**MAP 24C**

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**LEGISLATION & POLICY**

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Mayland Heights.

**TRANSPORTATION NETWORKS**

Pedestrian and vehicular access to the site is available from Motherwell Road NE and the rear lane. The area is served by Calgary Transit bus service with a bus stop location within approximately 300 metre walking distance of the site on McKinnon Drive NE. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system).

**UTILITIES & SERVICING**

Water, sanitary, and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required.

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

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## GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

Administration received a letter of objection to the application from the Crossroads Community Association (APPENDIX II).

Comments provided are summarized as follows:

- Applicant claims to have contacted the Community Association regarding this application. The Community Association has had no contact with this applicant.
- Applicant has not provided information on parking.
- Concerns with the applicant not residing at the property.
- Community Association would like to review site plans and floor plans for the proposed development.

### Citizen Comments

Administration received one (1) letter in support of the application.

Reasons stated for support are summarized as follows:

- Supports the legalization of basement suites for the following reasons: (1) legal investment for homeowners; (2) safe and affordable rental accommodations for the public; (3) a regulated rental suite for the community at large; and (4) code compliant dwelling with risk of fire controlled, and increased tax revenue for the City of Calgary.
- Other municipalities have allowed legalized suites in detached residences of a certain size.

Administration received letters in opposition to the application from six (6) citizens, and a community petition signed by 40 people representing 26 dwellings.

Reasons stated for opposition are summarized as follows:

- There are plenty of legal and illegal apartments and basement suites in the area.
- Parking congestion on the street.
- Subject property is located on a corner; street parking impacts visibility.
- Negative experience with illegal suite at address directly across the street from this application.

**LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE**

**MAP 24C**

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- Desire to have area remain a one-dwelling district.
- Suspicion the garage is rented out separate to the house.
- Applicant does not reside at the property.
- Applicant made similar application in the summer of 2017, which was denied by City Council. The application has not changed from the one that was denied.
- Concerns that property values will be reduced for neighbouring properties.

**Public Meetings**

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

APPENDIX I

APPLICANT'S SUBMISSION



**Land Use Redesignation Applicant's Submission**  
**Secondary Suites**  
PL 1264 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

- 1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?  
Our suite will increase safe affordable rental housing options, allow renters to have access to a yard, provide rental housing close to schools, parks, recreation centres, and other amenities. It will help maintain the community's population with modest growth, maximize the use of existing infrastructure in the neighbourhood, keep the community population stabilized without significantly changing the character of the existing neighbourhood and provide more housing options for families, helping bring children back into areas where school enrolment has declined. It will provide better population stability resulting in better use of land and City services, like transit and create a better choice in Calgary's housing market which will help to attract and retain employees in Calgary.
- 2) Provide information on how you engaged with the neighboring land owners and/or the Community Association?  
What was the response?  
I contacted nearby residents, the community association and Councillor Chabots office to ask for support of our application, via personal telephone calls, letters and emails. Some people were concerned about increased densification and the associated problems that may go along with that such as parking issues and increased crime. Some preferred homeowners versus renters living in the community because they felt they took better care of their property and some were concerned about out of province landlords who may not be aware of what is going on in there property. Others felt the real issue for them was whether they had Good neighbors. It didnt matter whether they were renters or homeowners. They wanted people to be respectful and part of the community, to take care of their property and to be their friends.
- 3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?  
We have a double car garage in the rear of the house which is accessable via the alley. In addition to this, there is enough room for three cars to park directly in front of the house.
- 4) Are there any potential negative impacts of this development that you are aware of?  
This house has had a basement suite for many many years and to date, there has never been one complaint from the neighbors regarding the care of the property or the actions of the landlord or the tenants.

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

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**APPENDIX II**

**LETTERS SUBMITTED**

Tuesday January 16, 2018

Good Morning,

After reviewing the above noted Land Use Amendment Application, Crossroads Community Association has some concerns:

- The applicant claims to have contacted the Community Association to discuss this application – we have not had any contact with this applicant pertaining to this property, which makes us question their other claims of contacting nearby residents and Councillor Chabot's office (also, Andre Chabot has not been the councillor for this community for nearly 3 months, which raises another red flag)
- The applicant hasn't answered the question about the where the parking will exist for both the secondary suite and primary dwelling, they merely indicate the stock of parking around the property and don't indicate who will park where
- The applicant seems to suggest that they will not be living in the property (or even the city) while this property is being rented. We view this as a negative as they will not be able to maintain a high level of oversight on the activities that occur on the property, which could provide problems for adjacent property owners
- We would like to review site plans for the property and floor plans for the residence

In summary, we are against this application and would encourage the applicant to speak with us to properly explain their intent for this property.

Jason Pare  
Planning Director, Crossroads Community Association

LAND USE AMENDMENT  
MAYLAND HEIGHTS (WARD 10)  
MCKINNON DRIVE AND 19 STREET NE

MAP 24C

APPENDIX III

IMPORTANT TERMS

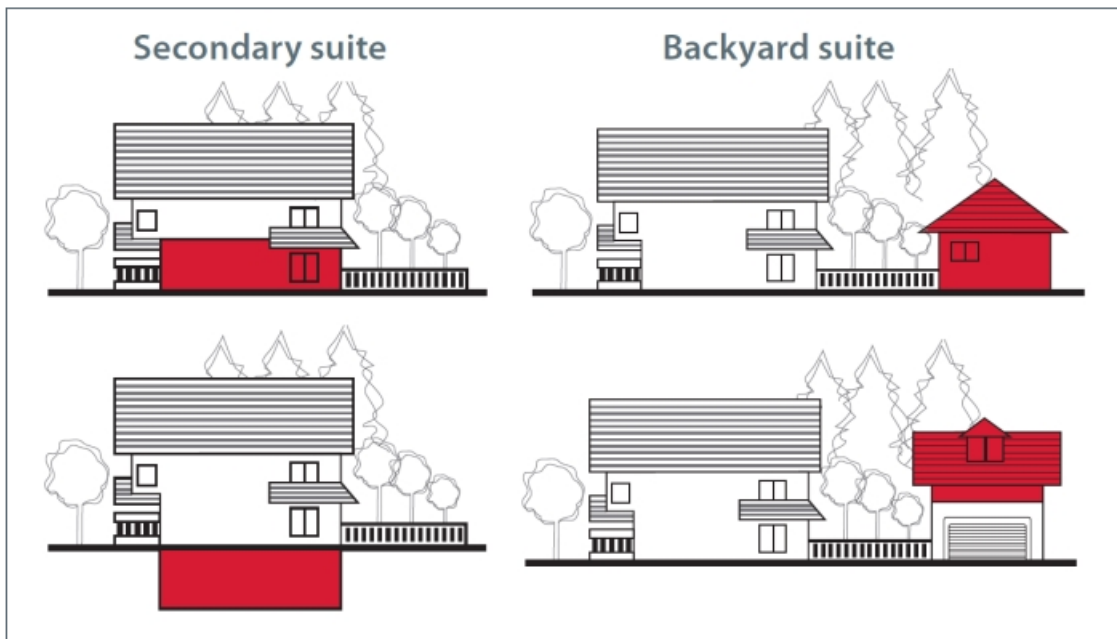
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simply and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms



**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.

**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 3.08**



LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate a single residential parcel from a Residential – Contextual One Dwelling (R-C1) District to a Residential – Contextual One Dwelling (R-C1s) District to allow for either a Secondary Suite or a Backyard Suite as an additional use. The site contains an existing single detached dwelling. To Administration's knowledge there is not an existing suite located on the parcel and the application was not submitted as a result of a complaint.

**PREVIOUS COUNCIL DIRECTION**

On 2013 September 16, Council directed Administration to remove fees associated with land use amendment and development permit applications for secondary suites to encourage the development of legal and safe secondary suites throughout the city.

**ADMINISTRATION RECOMMENDATION**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

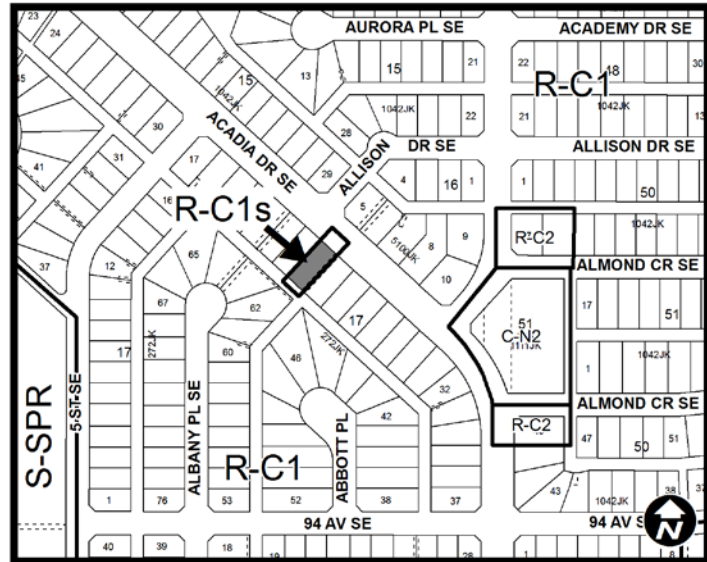
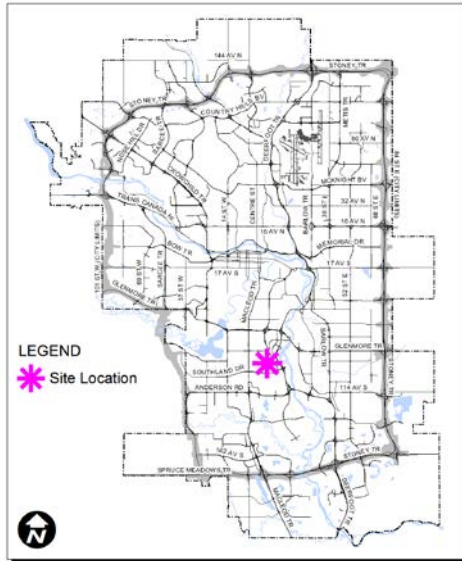
**REASON(S) FOR RECOMMENDATION:**

The proposed R-C1s district, which allows for one of two forms of secondary suite uses (Secondary Suite or Backyard Suite), is compatible with and complementary to the established character of the community. The proposal conforms to relevant policies of the Municipal Development Plan and will allow for development that has the ability to meet the intent of Land Use Bylaw 1P2007.

LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

LOCATION MAPS



LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares  $\pm$  (0.15 acres  $\pm$ ) located at 407 Acadia Drive SE (Plan 272JK, Block 17, Lot 24) from Residential – Contextual One Dwelling (R-C1) District to Residential – Contextual One Dwelling (R-C1s) District.

LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

**Applicant:**

Kimberly Murfin

**Landowner:**

Turc Harmesynn  
Kimberly Murfin

**PLANNING EVALUATION**

**SITE CONTEXT**

Located in a low density residential R-C1 setting in the community of Acadia, the site is approximately 16 metres by 36 metres in size and is developed with a one-storey, single detached dwelling and a single car garage that is accessed from the rear lane. Residential uses are immediately adjacent to the north, east, south, and west of the site.

According to data from The City of Calgary 2017 Census, the following table identifies Acadia's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations.

<b>Acadia</b>	
Peak Population Year	1972
Peak Population	13,589
2017 Current Population	10,660
Difference in Population (Number)	- 2,929
Difference in Population (Percent)	-22%

**LAND USE DISTRICTS**

The proposed R-C1s district allows for an additional dwelling unit (either a permitted use Secondary Suite or a discretionary use Backyard Suite) on parcels that contain a single detached dwelling.

Approval of this land use application allows for an additional dwelling unit (either a Secondary Suite or Backyard Suite) to be considered via the development permit process. A development permit is not required if a Secondary Suite conforms to all Land Use Bylaw 1P2007 rules – only a building permit would be required.

**LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE**

**MAP 22S**

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**LEGISLATION & POLICY**

South Saskatchewan Regional Plan (2014)

The site is located within the “City, Town” area as identified on Schedule C: South Saskatchewan Regional Plan Map in the *South Saskatchewan Regional Plan (SSRP)*. The SSRP makes no specific reference to this site. The land use proposal is consistent with the SSRP policies including the Land Use Patterns policies (subsection 8.14).

Municipal Development Plan (2009)

The site is located within a “Residential Developed – Established Area” on the Urban Structure Map (Map 1) in the *Municipal Development Plan (MDP)*. While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

There is no local area plan for Acadia.

**TRANSPORTATION NETWORKS**

Pedestrian and vehicular access to the site is available from Acadia Drive SE and the rear lane. The area is served by Calgary Transit bus service with bus stops located within approximately 70 to 150 metres walking distance of the site on Acadia Drive SE and 6 Street SE. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority’s residential parking permit system.

**UTILITIES & SERVICING**

Water, sanitary and sewer services are available and can accommodate the potential addition of a Secondary Suite without the need for off-site improvements at this time. Adjustments to on-site servicing may be required if a Backyard Suite is proposed at the development permit stage.

**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required.

LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

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## GROWTH MANAGEMENT

This land use amendment proposal does not require additional capital infrastructure investment, and therefore no growth management concerns have been identified at this time. The proposal is in alignment with MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

Administration received a letter with hesitant support of the application from the Acadia Community Association (APPENDIX II).

The Acadia Community Association stated that “We view secondary suites as a mechanism by which light density can be added to our community while preserving the character of our neighbourhood”.

### Citizen Comments

Administration received three (3) letters in opposition to the application.

Reasons stated for opposition are summarized as follows:

- increase traffic and parking issues;
- will set a precedent for other secondary suite applications;
- undesirable tenants;
- already enough rentals available;
- not enough parking on the parcel for use;
- decline in quality of neighbourhood will result;
- property values will decrease;
- property and garage already being listed for rent; and
- sufficient off street parking will not be available if the garage is separately rented out.

### Public Meetings

No public meetings were held by the Applicant or Administration.

LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

APPENDIX I

APPLICANT'S SUBMISSION



**Land Use Redesignation Applicant's Submission**

Secondary Suites

PL 1264 (R2017-09)

This form is to be filled out by the applicant and provided to The City of Calgary at the time of submission. These comments are included in a report which is presented to the Calgary Planning Commission and a Public Hearing of City Council. Your comments **must** be limited to the area designated on this page to ensure it will fit the space requirements of the report.

1) What are the benefits of the redesignation, for you, the surrounding community and the City of Calgary?

This is a rental property for the owner and would like to reach a broader market. The redesignation will provide housing to individuals/groups that may not require a large amount of space but would like their own space within their budget. Allows for greater diversity of people in the community.

2) Provide information on how you engaged with the neighboring land owners and/or the Community Association? What was the response?

We reached out to the neighbors by writing a letter of our intentions and have a conversation regarding any concerns. The letter states our plan is to rezone of our property to accommodate a secondary suites. There is a need for rentals, affordable rentals. The house is a fair size and can accommodate many people allowing affordable place for a diverse group of people.

3) Identify how you will provide the required parking for both the primary dwelling and the secondary suite on your parcel?

There is ample street parking along with up to 4 spaces in the back, with garage.

4) Are there any potential negative impacts of this development that you are aware of?

No.

NOTE: Applications must be submitted without personal information on any plans. Omitting this information will protect builders and tenants by reducing the risk of any personal information being wrongfully displayed, while also following the Province of Alberta's FOIP Act. If you consider the information to be personal, do not put it on the plans

LAND USE AMENDMENT  
ACADIA (WARD 11)  
ACADIA DRIVE SE SOUTH EAST OF 5 STREET SE

MAP 22S

APPENDIX II

ACADIA COMMUNITY ASSOCIATION LETTER



Acadia Community Association  
HOME OF THE ACADIA RECREATION COMPLEX

Planning and Development  
City of Calgary  
% Melanie Horkan

Jan. 28, 2018

Re: LOC2017-0407

The Acadia Community Association would like to offer its hesitant support to the application for re-designation from R-C1 to R-C1s at 407 Acadia Drive SE.

As Calgary continues to add to its population, we view Secondary Suites as a mechanism by which light density can be added to our community while preserving the character of our neighbourhood.

Our major hesitation with this application stems from the claim the applicant has made to have discussed this application with their neighbours. It has been brought to our attention that the neighbours have not been contacted, nor has the applicant reached out to the Community Association. We strongly support conversations amongst neighbours in order to clarify intent and impact. Taken in conjunction with existing Community Standards Bylaw concerns regarding snow clearing history, we submit that communication amongst neighbours should be reinforced. One of Acadia's goals with supporting Suites across our community is to encourage young families and seniors to be a vibrant part of our community. While snow clearing is considered to fall into the Community Standards realm, we are acutely aware that if Calgary is to encourage active modes of transportation in conjunction with its densification policies in established communities that the ability to navigate pedestrian corridors becomes a planning issue. Snow removal in this case is incredibly important, as the roadway directly adjacent is a snow route, often has large windrows, and is not an option for pedestrian access during the winter months. The rental of the property in and of itself is not a concern, however with multiple tenants the responsibility for snow clearing is not always clear and we would therefore use this opportunity to bring the importance of ownership duties to the attention of the applicant.

The parcel in question is on a major community corridor, and has transit access directly adjacent to it. In general, proximity to the primary transit network makes Acadia an ideal area for the addition of suites from a transportation perspective. As mentioned above, snow windrows along Acadia Drive can be problematic. This is especially true for street parking, and while the addition of multiple parking stalls on the property is not mandated by law, we do have some concerns that the separate rental of the garage and parking pad in the rear, as indicated on a current Rent Faster ad for this property, will substantially decrease parking access to the future tenants. Suites create a stepping-stone for potential long-term residents, as we have a number of medium-density parcels and a number of single-family dwellings, which tend to present a large financial jump for tenants. We have used this application to have a discussion with long term Acadian homeowners about the financial realities of renting in Calgary - the cost of rental often exceeds the cost of a mortgage on a monthly basis, and we therefore, in general, would like to see more units become available to help drive down price.

Kim Warnke,  
Planning Committee Chair  
On behalf of the Acadia Community Association  
planning@acadiaca.com

M. Horkan

APPENDIX III

IMPORTANT TERMS

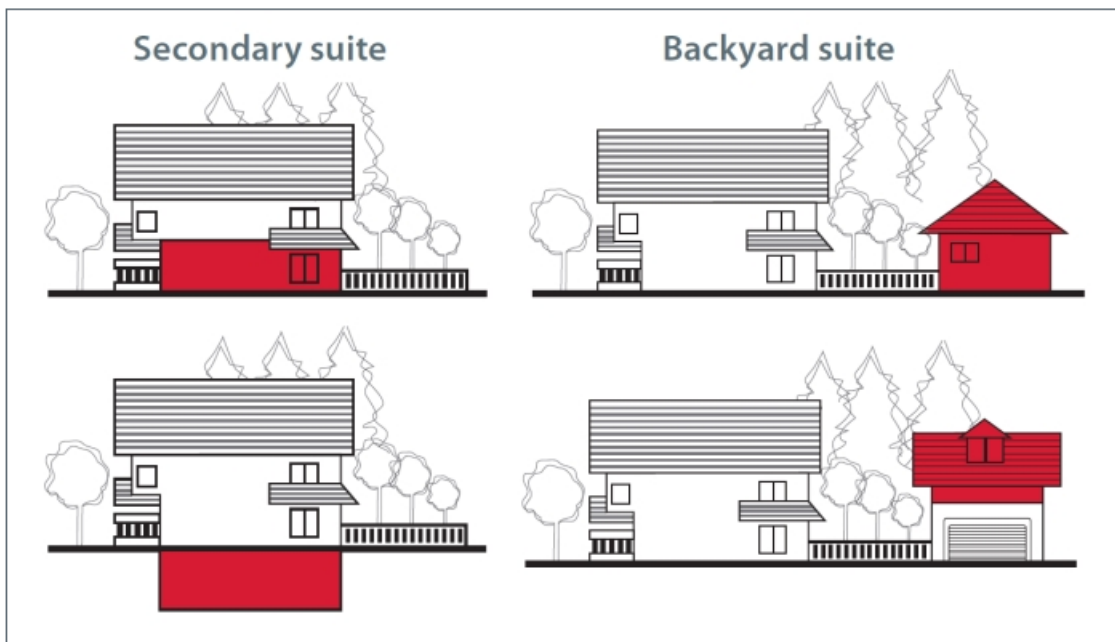
While there are specific Land Use Bylaw 1P2007 definitions and development rules for Secondary Suite and Backyard Suite uses, the following information is provided to simplify and enhance general understanding of these two different uses (Secondary Suite or Backyard Suite).

## Important terms

**Secondary suite:** A self-contained dwelling unit within the main residence that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as basement suites or in-law suites.



**Backyard suite:** A self-contained dwelling unit in a detached building that has separate living, cooking, sleeping and bathroom facilities. These are commonly referred to as garage suites, garden suites, or laneway homes.





A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a theodolite mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 5.01**



LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

**EXECUTIVE SUMMARY**

This is a land use amendment in the community of Marlborough. The site is located at 3805 Marlborough Drive NE, internal to and apart of the Applewood Village multi-residential development. The application seeks to redesignate a portion of the subject parcel from the Multi-Residential – Contextual Low Profile (M-C1d100) District to the Multi-Residential – High Density Low Rise (M-H1) District.

There is currently a development permit under review on this site for a six-storey, 146-unit multi-residential development.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

**REASON(S) FOR RECOMMENDATION:**

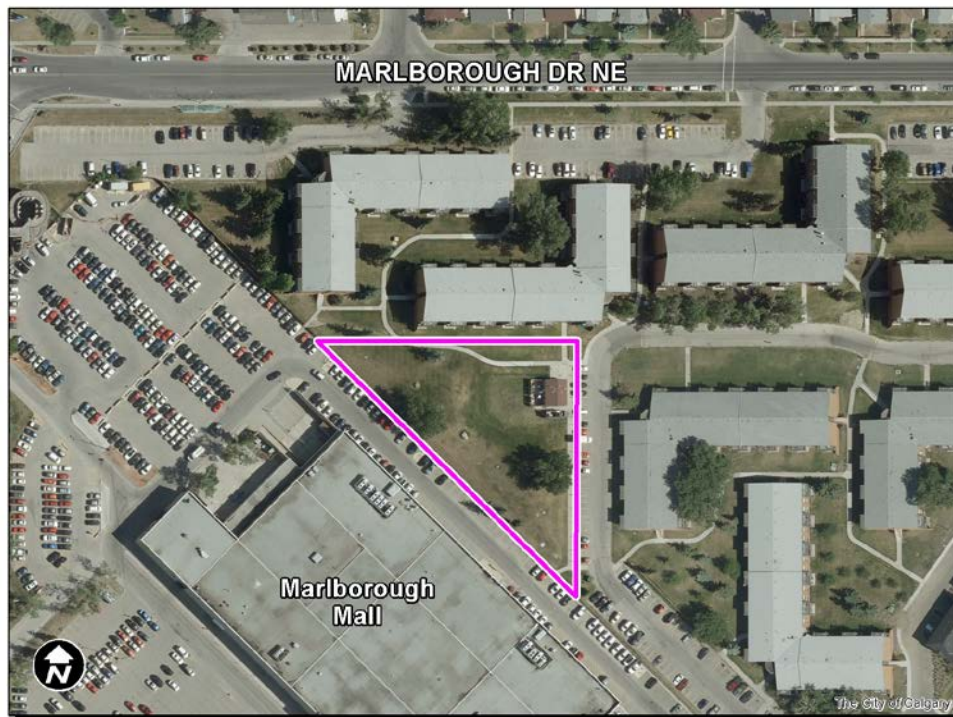
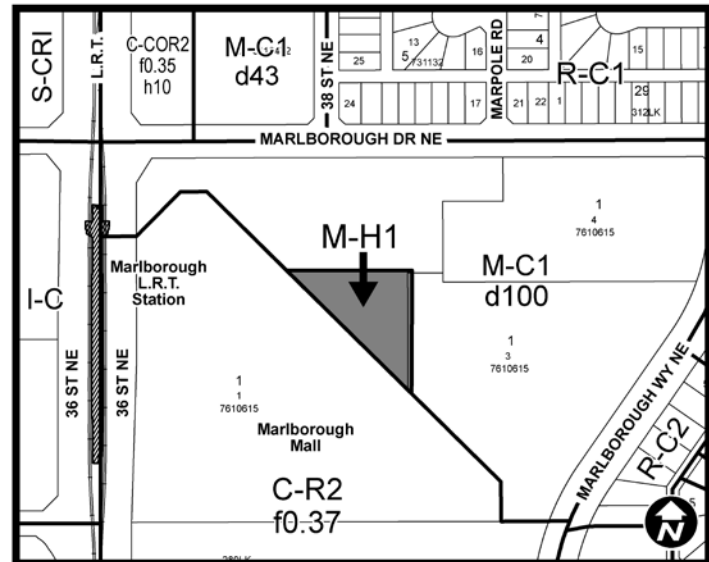
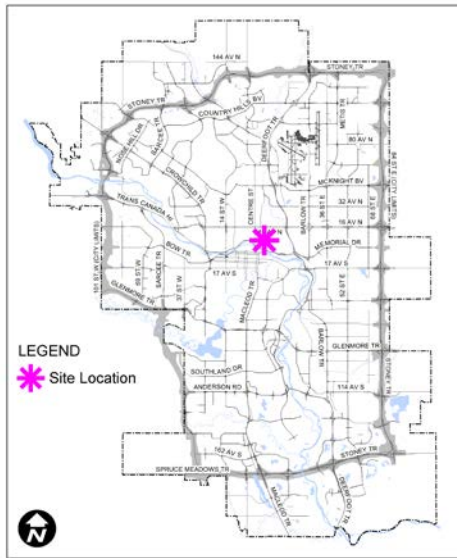
This redesignation application is intended to accommodate a future multi-residential development on a site located within walking distance from the Marlborough LRT Station and the Marlborough Shopping Centre. The proposed Multi-Residential – High Density Low Rise (M-H1) District is designed to provide multi-residential development in a variety of forms located at community nodes and transit and transportation corridors.

The proposal represents an appropriate increase in residential density for this parcel located within a transit oriented development area, the 36 Street NE urban main street and directly adjacent to the Marlborough Shopping Centre, that is identified as a Community Activity Centre. The proposed redesignation is in keeping with applicable policies of the Municipal Development Plan (MDP) and generally aligned with the principles of the Transit Oriented Development (TOD) Policy Guidelines.

LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

LOCATION MAPS



LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.42 hectares  $\pm$  (10.38 acres  $\pm$ ) located at 3805 Marlborough Drive NE (Portion of Plan 7610615, Block 1, Lots 2 and 3) from Multi-Residential – Contextual Low Profile (M-C1d100) District to Multi-Residential – High Density Low Rise (M-H1) District.

LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

**Applicant:**

O2 Designs

**Landowner:**

Minto Multi-Residential Income  
Partners I GP Inc

**PLANNING EVALUATION**

**SITE CONTEXT**

The subject site is located in the northeast community of Marlborough directly adjacent to the northeast corner of Marlborough Mall (Community Activity Centre) and approximately 150 metres to the east of the Marlborough LRT platform. The immediate area is predominantly developed with multi-residential uses located along 36 Street NE and low density residential development located east of Marlborough Way NE and 38 Street NE. The subject site is located within the Applewood Village – a multi residential rental housing development owned and operated by Minto. The site was originally developed in the 1970's and currently contains seven three-storey multi-residential buildings.

The triangular-shaped site, approximately 0.42 hectares in size, is currently being used as a grassy/landscaped area backing onto the Marlborough Mall. There is a small outbuilding located on the site that will be removed at the time of development.

According to the 2017 City of Calgary Census data the population of Marlborough peaked in 1982 with a decline of eight percent since that time. The following table summarizes population trends in the community of Marlborough.

<b>Marlborough</b>	
Peak Population Year	1982
Peak Population	10,025
2017 Current Population	9,226
Difference in Population (Number)	-799
Difference in Population (Percent)	-8%

**LAND USE DISTRICTS**

The existing Multi-Residential - Contextual Low Profile (M-C1d100) District allows for multi-residential development up to 14 metres in height and up to a maximum of 42 dwelling units on this site.

**LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE**

**MAP 22E**

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The proposed land use district is the Multi-Residential – High Density Low Rise (M-H1) District. This district does not limit a maximum unit density, but rather requires a minimum density of 150 units per hectare. Given the area of this site, this would require a minimum of 63 dwelling units with a maximum height of 26 metres. The maximum floor area ratio (FAR) is 4.0 which would allow for approximately 16,500 square metres of developable floor area on this site. The M-H1 District also allows for ancillary commercial uses within the building to serve the needs of the local residents, although this is not required.

Other land use districts were explored for this site through the review process such as the Multi-Residential – Contextual Medium Profile (M-C2) District, although it was determined that the location and context of this site lent itself well to the proposed district.

## **LEGISLATION & POLICY**

### South Saskatchewan Regional Plan (Statutory – 2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

### Municipal Development Plan (Statutory – 2009)

The subject site is located at the confluence of three Municipal Development Plan (MDP) typologies. These typologies include the Urban Main Street, the Residential Developed Established, and the Community Activity Centre.

The Urban Main Street and the Community Activity Centre encourage higher intensity redevelopment of sites where appropriate. Both of these typologies aim for intensities of jobs and population of 200 and 150 per hectare, respectively.

The Residential Developed Established areas encourage modest intensification of sites with the most intense redevelopments focused on activity centres. New developments should incorporate densities to support the transit network as well as provide opportunities to increase pedestrian, and cycling connectivity.

Considering the site's location relative to the three MDP land use typologies, the building envelope and uses allowable in the proposed M-H1 District are deemed to represent an appropriate transition between the Community Activity Centre and Urban Main Street along 36 Street NE on one end, and the Residential Developed Established area on the other.

### Transit Oriented Development Policy Guidelines (Non-Statutory - 2005)

The Transit Oriented Development Policy Guidelines (the guidelines) is a non-statutory document outlining guidelines for development within 600 metres of a transit station. These guidelines have six themes:

**LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE**

**MAP 22E**

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1. Ensure transit-supportive land uses;
2. Increase density around transit stations;
3. Create pedestrian-oriented design;
4. Make each station area a “place”;
5. Manage parking, bus and vehicular traffic; and
6. Plan in context with the local communities.

The guidelines encourage land uses with medium to high residential densities such as walk up apartments and high-rise apartments. This application is proposing a mid-rise predominantly residential land use district.

The guidelines also encourage short, direct and barrier free pedestrian connections between destinations in the area. The location of the site provides for the opportunity for short, direct and barrier free connections to the community activity centre as well as the LRT platform.

There is no local area plan for Marlborough.

### **TRANSPORTATION NETWORKS**

The site is accessed from Marlborough Drive NE and Marlborough Way NE, with no rear lane serving the site. Calgary Transit operates both bus and light rail within 150 metres of this site. Marlborough Drive NE has parking restrictions on the north side of the street, with Marlborough Way NE having no parking restrictions.

The number and location of required on-site bicycle and motor vehicle parking stalls as well as pedestrian connections to the LRT platform that will be required to accommodate the anticipated development will be reviewed and determined as part of the development permit.

### **UTILITIES & SERVICING**

All services are available to the site. Detailed design of the onsite services is being reviewed as part of the development permit. A storm water management report as well as a sanitary servicing study have been requested as part of the review for the current development permit.

### **ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required.

### **GROWTH MANAGEMENT**

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

**LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE**

**MAP 22E**

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**PUBLIC ENGAGEMENT**

**Community Association Comments**

The Marlborough Community Association (CA) submitted a formal response on 2017 November 16. The CA's letter did not explicitly address support nor opposition to the proposed land use redesignation. However, the Community Association did highlight issues with regards to motor vehicle parking in the general area and the Applewood Village residential complex. See APPENDIX II for additional information.

**Citizen Comments**

No comments received by CPC Report submission date.

**Public Meetings**

A meeting was held with the community association on 2018 February 04, as well as a public open house held by the developer on 2018 February 13.

LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

APPENDIX I

APPLICANT'S SUBMISSION

APPLICANT'S SUBMISSION

On behalf of Minto Multi-Residential Income Partners, O2 Planning + Design is submitting this application to re-designate the a portion of the parcel located at 3805 Marlborough Drive NE from Multi-Residential – Contextual Low Profile District (M-C1) to Multi-Residential – High Density Low Rise District (M-H1) with a FAR modifier of 4.0 and height modifier of 26 metres (MH-1 f4.0 h26). Minto Developers intends to build a new six-storey mid-rise wood-frame multi-family residential building on the site.

The site is 4,152 square metres and is currently located on an M-C1 parcel of 59,500 square meters (excluding the site area) called Applewood Village. Applewood Village is located in the community of Marlborough, between Marlborough mall, Marlborough Drive and Marlborough Way NE. The site itself is currently vacant, but enclosing the site are seven, four-storey multi-residential buildings. Located directly south-west of the site, is Marlborough Mall.

The development of this project will partake in the evolution of 36<sup>th</sup> Street Main Street. The development of this project will enhance the retail catchment of the Marlborough Mall and the ridership of the Marlborough CT station, located less than 200 meters of the site. As established in the public engagement sessions for the 36<sup>th</sup> Street Main Streets, the building will contribute to a portion of the 214 residential units that are expected to be built over the next 25 years, from 2016 – 2020. This re-zoning will help and enable the community to reach its desired population levels and offer more housing options in the area. The building will be integrated into the neighbourhood and offer its residents the opportunity to easily walk to shops and services for their daily needs while also providing residential uses.

LAND USE AMENDMENT  
MARLBOROUGH (WARD 10)  
36 STREET NE AND MARLBOROUGH DRIVE NE

MAP 22E

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APPENDIX II

COMMUNITY ASSOCIATION LETTER

**From:** Glen&Laura Greenwood  
**Sent:** Thursday, November 16, 2017 10:38 AM  
**To:** CPAG Circ <CPAGCirc@calgary.ca>  
**Cc:** Bevill, Brad C. <Brad.Bevill@calgary.ca>  
**Subject:** [EXT] FW: LOC2017-0288 Circulation Package

Good Morning,

I am attaching the Request for Comment on above noted property. Please see email below from Calgary Marlborough Community Association.

The parking situation surrounding Applewood Village is appalling. There are vehicles lined up on all sides of the streets and as mentioned below, some are parking in the Community Association parking lot that is directly across the street from this development. This is totally unacceptable as the CMCA parking lot is meant for users of the hall, **NOT** apartment/townhouse tenants.

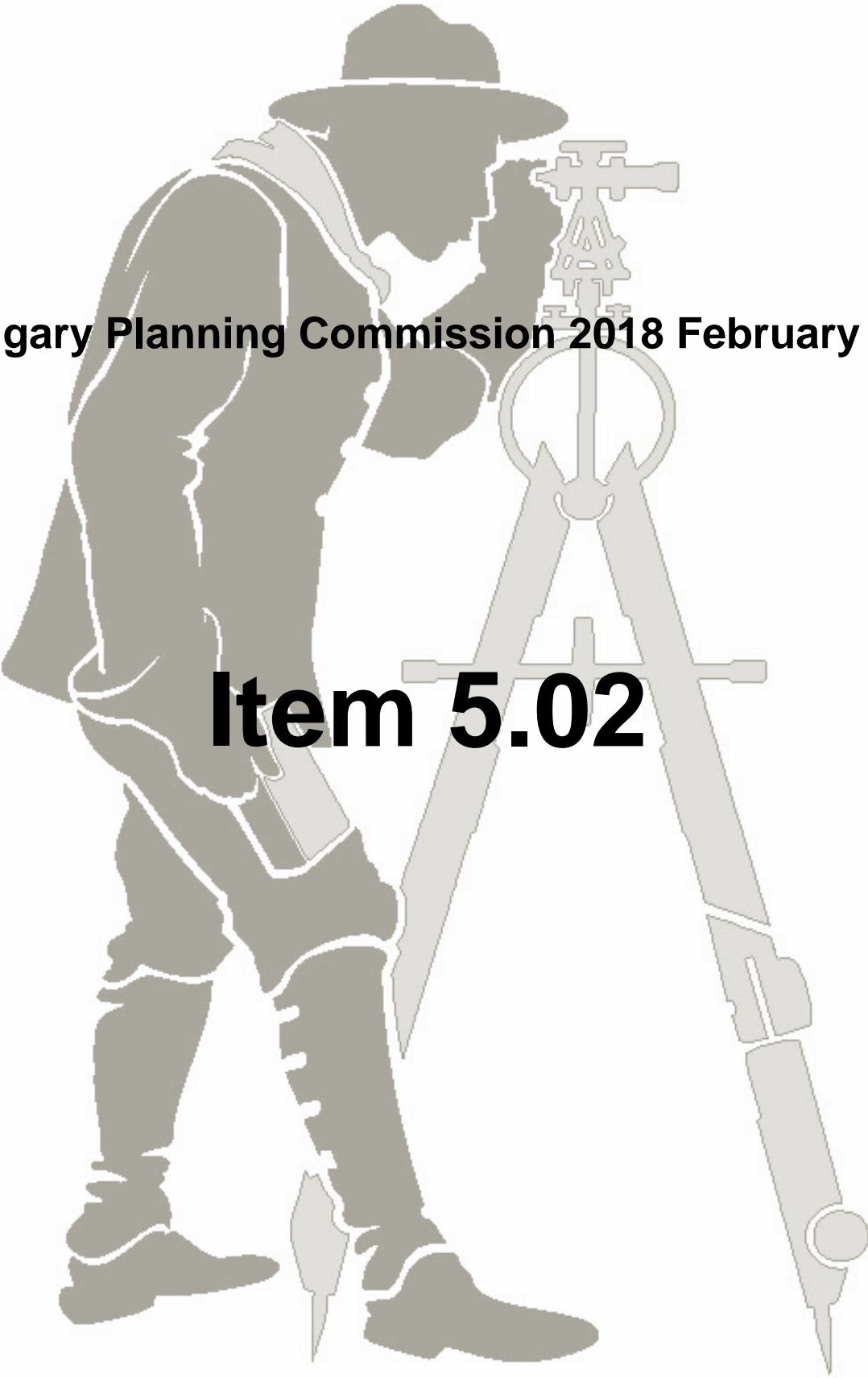
Thanks,

Laura Greenwood  
CMCA



**Calgary Planning Commission 2018 February 22**

**Item 5.02**





LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

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**EXECUTIVE SUMMARY**

This report presents information on the pedestrian bridge replacement project in Laycock Park over Nose Creek. The existing pedestrian bridge has been closed for several years due to its poor condition and as a result of damage from the 2013 flood. A feasibility study was undertaken in 2015 and determined that reconstructing the pedestrian bridge over Nose Creek was critical.

The scope of the project includes the design and construction of a new pedestrian bridge south of the current alignment and removal of the existing pedestrian bridge. Landscaping improvements throughout the park are currently in the design phase and will be completed by the City.

Construction is anticipated to take place from Q2 – Q4 of 2018.

**PREVIOUS COUNCIL DIRECTION**

Council previously approved the plan for Investing in Mobility 2015-2024 in May 2014. This project is funded through Program 223 within Investing in Mobility for Pedestrian Bridge Replacement and Upgrading.

**ADMINISTRATION RECOMMENDATION**

2018 February 22

That Calgary Planning Commission **RECEIVE AND ACCEPT** this report for information.

**REASONS FOR RECOMMENDATION:**

In 2015, the City undertook a feasibility study on the existing Laycock pedestrian bridge over Nose Creek and determined that due to its overall deteriorating condition, the structure had reached the end of its service life. To maintain connectivity in the area and integrate with several design plans proposed in the Park, the study concluded that the structure required replacement.

The proposed pedestrian bridge will provide the only access for pedestrians, cyclists and users with reduced mobility in Laycock Park across Nose Creek. The analysis conducted at the project outset confirmed that a replacement of the existing bridge crossing would provide best value to the City.

LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

LOCATION MAPS – AREA PLAN



LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

DETAIL PLAN



LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Calgary Planning Commission **RECEIVE AND ACCEPT** the Laycock Park Pedestrian Bridge Report for information.

**LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK**

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**PLANNING ANALYSIS**

Laycock Park is located in the community of Thorncliffe, immediately southwest of Deerfoot Trail and 64 Avenue NE. The park is bordered by residential communities to the west, industrial areas to the south, a major arterial (64 Avenue NE) to the north, and a CP Rail line to the east. Three out of the four directions are faced with significant barriers to active modes of travel. Given that, the park is connected to the north and south by a regional pathway which extends from downtown to Nose Creek Parkway in the north and serves as an important walking and cycling connection within The City.

The site is generally well served by walking and cycling infrastructure within the park and to the east. To the north, the regional pathway crosses underneath 64 Avenue NE and continues to Nose Creek Parkway. However, access to 64 Avenue NE is challenged by a steep gradient which exceeds accessibility standards, and limited to a single pathway connection at the northeast side of the park.

The park currently includes greenspace with an off-leash dog area, baseball diamonds, and playgrounds. Nose Creek runs through the Park in a north-south direction, and an existing pedestrian bridge has been the only means of crossing the Creek within the Park. The existing pedestrian bridge is a 3 span timber bridge built in 1975, originally designed to accommodate legal truck traffic. The structure has since been reverted to pedestrian and cyclist use along with occasional use by Parks maintenance vehicles. The bridge experienced significant scour as a result of the 2013 flood and due to the poor overall structural condition, it has since been closed. The existing pedestrian bridge will be removed and disposed of as part of the project.

A feasibility study was undertaken in 2015 and determined that reinstating the connectivity over Nose Creek for pedestrians and cyclists was critical. Following a site inspection which included a structural assessment of the existing structure, and after evaluating the costs of repairs, it was recommended that rebuilding a new bridge close to the existing bridge was warranted.

City Parks is currently redesigning Laycock Park to include more environmental, functional and aesthetic features and activities to the site. Future improvements include realigning and reconstructing the regional pathway, adding a new playground and basketball court, and improving the landscaping and natural vegetation in the park. The parking lot on the east side on Nose Creek is also planned to be reconstructed, which will be connected to Laycock Park with the new pedestrian bridge. The location and alignment of the new pedestrian bridge provides for maximum opportunity to enhance the current park and integrate with the future parks upgrading plans and provide universal accessibility.

**LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK**

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**Design and Aesthetics**

Context sensitivity and design of the structure from an aesthetic perspective are important elements of the design. The existing site consists of an interesting mix of park, creek, residential, industrial, and rail elements.

The design of the bridge aims to tie these diverse urban elements together while providing the functional requirements of crossing from one side of the creek to the other.

A timber bridge was developed as a means to provide a design solution that introduces simple, yet innovative concepts into the Park. The use of exposed wood structural elements influenced a concept that highlights the material and uses a form that will be easily interpreted by the park users.

The selected bridge type is a timber glulam beam bridge that includes fiberglass reinforced polymer wrapped timber deck panels. A bicycle railing will be provided at a height of 1400 millimetre above the deck in accordance with the City's Access Design Standards. To minimize the environmental impact at the site, the bridge is being designed with no intermediate piers within Nose Creek.

The location and alignment of the new pedestrian bridge provides for maximum opportunity to enhance the current park and integrate with the future parks upgrading plans. Key design criteria includes:

- Safety;
- Accessibility for all users;
- Connectivity with existing and future pathways;
- Design and construction to current engineering standards;
- Sustainability;
- Cost and schedule; and
- Aesthetics.

The pedestrian bridge is a key component to the design and functionality of the park. The design of the bridge was deemed to be of a modern aesthetic, representing the contemporary nature of the park design and user experience while highlighting sustainable materials and current technologies of the structure type.

Architectural support to the engineering team in development of bridge aesthetics and context-sensitive design have resulted in design alternatives that are respectful of the Park including bridge form, railing design, and choice of materials.

While the new Laycock Park Pedestrian Bridge is to remain respectful of the natural environment of this site and be functional for pedestrians, cyclists and maintenance vehicles, the bridge will serve to further define the character of the site and will be a landmark in Laycock Park.

**LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK**

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A critical design requirement on all pedestrian bridges is pedestrian safety and accessibility; this is best achieved by maintaining clear sightlines on and around the bridge to avoid undesirable behavior. These safety features will be incorporated into the design of the Laycock Park Pedestrian Bridge structure, along with the following site-specific features:

- Pathway grades under 5 percent;
- Maximum 3:1 pathway side slopes;
- Keeping pedestrians away from the CP Rail Line;
- Coordinating with City Parks' landscaping project to keep densely treed areas away from travel lines;
- Way finding

The proposed pedestrian bridge will incorporate CPTED guidelines in relation to the overall park design and performance requirements.

### **Sustainability**

The new pedestrian bridge aligns with the City's sustainability policies and will result in long term social, environmental, and economic benefits. The pedestrian bridge promotes healthy lifestyles by maintaining a critical connection to the Park and regional pathway system and encourages greater interaction between residents in the communities. The design of the pedestrian bridge also incorporates innovative and sustainable materials which will reduce initial and ongoing maintenance costs and provide long term value to the City.

The use of timber for a pedestrian bridge represents a particularly sustainable option for the project, given Canadian leadership in innovation and sustainable forestry practices. The material is fully renewable, fully recyclable upon the end of its service life, and is generated without depleting the environments resources. The material provides an important service to the environment through the capture of carbon into the material. The manufactured material can be repaired and provided with lifecycle services with low impact means and methods. Timber protection will include water-based treatment that is environmentally neutral and safe for the bridge users.

### **Landscaping**

The proposed bridge has been designed to consider the user connections and visual sightline opportunities to other proposed key park features such as the vanishing pond, new playground and basketball court. In addition, the design for the structure looks to provide a unique experience for the user with entry features that tie to the adjacent landscape in an exciting and pleasing way.

**LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK**

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The proposed pedestrian bridge will be integrated into the landscaping improvement redesign currently underway for Laycock Park. This integration will focus on the bridge landings and pathway alignment in order to provide a comfortable and enjoyable user experience.

The planting strategy is intended to improve the existing landscape through low-impact design which includes no maintenance native plant species, as well as grasses and rock riprap to minimize upkeep.

**Stakeholder Consultation and Public Engagement**

In 2015 during the feasibility study phase of this project, an online survey targeted at local communities and businesses was conducted in regards to the existing pedestrian bridge over Nose Creek to receive feedback on the proposed location of a replacement structure, usage, desired connections, and other general information. The responses indicated a strong desire to improve the connection between bike routes and the bridge and to move the bridge closer to the playground. This feedback was used to finalize the alignment of the new bridge location immediately downstream of the existing pedestrian bridge.

A public information session was held at the Thorncliffe Community Association during the detailed design phase of the project. The information session included other projects currently underway in Laycock Park including the Parks and Landscaping upgrades project being delivered by the Parks Department.

Information on the project is currently available on the City of Calgary's website. Project updates are posted regularly and the site will continue to be updated during construction with information for the public.

LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

APPENDIX I



Figure 1: Laycock Park Proposed Landscape Improvements

LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

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Figure 2: New Pedestrian Bridge - Profile

LAYCOCK PARK PEDESTRIAN BRIDGE  
THORNCLIFFE (WARD 4)  
LAYCOCK PARK OVER NOSE CREEK

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Figure 3: New Pedestrian Bridge – User View looking West



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a pencil in their right hand. The transit instrument is a complex mechanical device used for measuring angles in geodesy and engineering.

**Calgary Planning Commission 2018 February 22**

**Item 5.03**



LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S

**EXECUTIVE SUMMARY**

This application proposes to redesignate the northwestern corner parcel at the intersection of Southland Drive SW and Elbow Drive SW from Commercial – Neighbourhood 2 (C-C2) District to Commercial – Corridor 1 f3.5 h24 (C-COR1 f3.5 h24) District in order to accommodate a future higher intensity, mixed-use development within close proximity to the Southland LRT Station.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

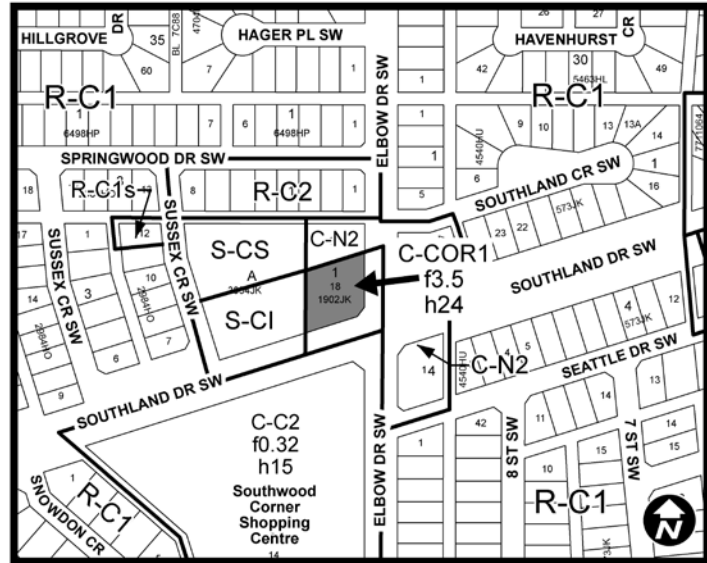
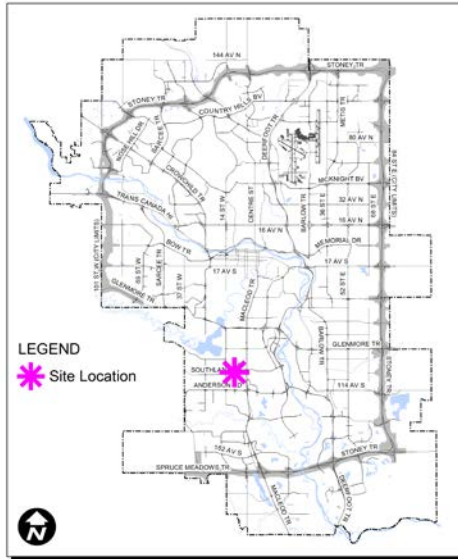
**REASON(S) FOR RECOMMENDATION:**

The redesignation of this parcel to C-COR1 would enable the sensitive intensification of the site along the Primary Transit Network, an in close proximity to the Southland LRT Station. This application meets the goals of the Municipal Development Plan and the Calgary Transportation Plan by enabling pedestrian-friendly, mixed-use development that supports the objectives and policies of a neighbourhood activity centre.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S

LOCATION MAPS



LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S

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**ADMINISTRATION'S RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.20 hectares ± (0.49 acres ±) located at 9939 Elbow Drive SW (Plan 1902JK, Block 1, Lot 18) from Commercial – Neighbourhood 2 (C-N2) District **to** Commercial – Corridor 1 f3.5h24 (C-COR1f3.5h24) District.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S

**Applicant:**

O2 Planning and Design

**Landowner:**

1953075 Alberta Ltd. c/o Royop  
Development Corporation

**PLANNING EVALUATION**

**SITE CONTEXT**

The subject site is located on the northwestern corner of the intersection of Elbow Drive SW and Southland Drive SW, and is approximately a 600 metre walking distance west of the Southland LRT Station. The Calgary Public Library Southwood Branch exists west of the subject site. North of the library is a passive recreational area that gets flooded for outdoor skating, as well as, a playground located in the northwest corner of the site. Neighbourhood commercial development exists directly north of the site with low density residential development located further north. Across Elbow Drive SW and Southland Drive SW, the other three corners of the intersection all contain commercial retail uses, with a newly constructed neighbourhood commercial development to the east, a gas station to the southeast, and the large "Southwood Corner" commercial development to the south.

The subject parcel was historically used as a gas station, which closed in 1998 and has since been remediated. A development permit for a restaurant with drive through for this site was approved in 2002 but later lapsed, and the site remains vacant to date.

<b>Community Name</b>	
Peak Population Year	1978
Peak Population	8,101
2017 Current Population	6,214
Difference in Population (Number)	-1,887
Difference in Population (Percent)	-23%

**LAND USE DISTRICTS**

This application proposes to redesignate this parcel from its current Commercial – Neighbourhood 2 (C-C2) District designation to the Commercial – Corridor 1 (C-COR1) District with a floor area ratio modifier of 3.5 and a maximum height of 24 metres, an increase from the current maximum under C-N2 of 10 metres.

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW**

**MAP 16S**

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Shadow studies were requested in order to determine the impact of the height increase on the surrounding areas, and those details have been included in Appendix III of this report. The shadow studies exhibit the maximum height and building envelope, though the ultimate built form has yet to be determined. The images show that the 24 metre maximum height increases shadowing on the existing park and residences only during the shadowing extremes at 8:00 am on the Spring and Autumn solstices (March 21 and September 21 respectively), and on select areas at different times on the Winter solstice (December 21). In order to further minimize the potential shadow impacts on adjacent properties, building placement will be carefully considered at the development permit stage. These strategies may include bringing the building closer to the intersection and therefore further away from the park and affected residential properties, potentially further reducing the shadow impacts.

## **LEGISLATION & POLICY**

### Municipal Development Plan (MDP)

The MDP identifies this parcel as part of the Established Area. Policies in the MDP for Established areas call for redevelopment opportunities to be focused on the Neighbourhood Activity Centres (NAC), with appropriate densities, a mix of land uses, and a pedestrian-friendly environment to support an enhanced Primary Transit Network.

As there is no local area policy plan in place for this site, it is not designated as a Neighbourhood Activity Centre in any legislated documents. However, when the characteristics of a NAC from Section 3.3.4 of the MDP are considered, this commercial area, including all corners of the intersection of Southland Drive SW and Elbow Drive SW, can be considered consistent with the character of a NAC. This application proposes a moderate intensification over the current land use and enables redevelopment on this site that would meet the intent of these policies and the goals of the MDP.

### Calgary Transportation Plan (CTP)

The CTP identifies Southland Drive SW as an Arterial Street as well as part of the Primary Transit Network. Increasing intensity along the Primary Transit Network and in close proximity to the Southland LRT Station supports the key directions and goals of the CTP.

### South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW**

**MAP 16S**

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**TRANSPORTATION NETWORKS**

The existing commercial site is located in close proximity to transit stops on Elbow Drive and Southland Drive, with transit service to be provided adjacent to the subject lands. Transit service connections to the Heritage and Southland LRT stations are provided by bus service along Elbow Drive and Southland Drive. It is anticipated that upgrades to these transit stops will be required with redevelopment of the parcel and will be determined at the development permit stage.

Vehicular access to the site was previously accomplished through several curb cuts to the site on both Elbow Drive and Southland Drive. Future access will be limited to appropriate locations away from the intersection of Elbow Drive and Southland Drive. The site does not have direct access to a lane and therefore all access, parking, and service-oriented maneuvering will be determined at the development permit stage. A Transportation Impact Assessment and Parking Study may be required at the time of development to determine the appropriate location of access, parking and pedestrian connectivity to the public realm as well as transit stops.

**UTILITIES & SERVICING**

Sanitary sewers are not presently available to service this site. A sanitary sewer extension will be required as part of the development permit application process.

Water and storm sewer mains are available and can accommodate the potential redevelopment of the subject site without the need for off-site improvements.

**ENVIRONMENTAL ISSUES**

The subject site was formerly the location of the Southwood Esso gas station. Remediation associated with this previous use has been addressed. Some contamination exists off-site, within the City right of way lands, adjacent to the subject site. Further risk management maybe required at the development permit stage.

**GROWTH MANAGEMENT**

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW**

**MAP 16S**

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**PUBLIC ENGAGEMENT**

**Community Association Comments**

This application was circulated to the Southwood Community Association and the Federation of Calgary Communities (FCC). The FCC responded to the circulation with “No Objection”, and the Southwood Community Association responded with a letter, attached in APPENDIX II. The Southwood Community Association has chosen to remain neutral on the land use amendment application, but noted several key points for consideration. These issues have been considered in the review of this application and will also be considered during the development permit review.

Administration, the applicant, the developer, and the Southwood and Haysboro Community Associations also met on 2017 December 12 to further discuss the current application, as well as opportunities and constraints for development of the site in general. The Community Associations were both very receptive to the redevelopment vision for the site and are interested in continuing to work with the developer as they move toward development of the site.

**Citizen Comments**

The application was circulated to the owners of nearby properties and 5 letters of concern were received from the residents of 4 nearby properties along Sussex Crescent SW. All of the letters noted concerns with existing traffic congestion in the area, and worried that a large new development with inadequate parking may cause parking issues. Existing traffic concerns were shared with Transportation Planning, and have been noted for review at the development permit stage along, with the potential need for a parking study.

Some residents also raised concerns over possible shadowing impacts and pedestrian connectivity from the future development. These concerns have been noted and shadow impacts have been assessed as part of this land use amendment review, but will be further studied at the development permit stage in order to clearly assess what the impacts of the proposed built form would be.

**Public Meetings**

The applicant held a Public Engagement session on 2017 November 30 at the Southwood Community Association Building. The session was well attended by members of the community as well as Councillor Farkas and File Manager. The session presented information on the land use amendment application, and provided attendees with the opportunity to participate in a visioning exercise for the future development of the site. The event was well received by the community members and feedback was generally positive.

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW**

**MAP 16S**

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**APPENDIX I**

**APPLICANT'S SUBMISSION**

On behalf of Royop Development Corporation, O2 Planning + Design is submitting this application to redesignate the parcel located at 9939 Elbow Drive SW from Commercial Neighbourhood 2 (C -N2) to Commercial Corridor 1 with a FAR modifier of 3.5 and height modifier of 24 metres (C-COR 1 f3.5h24). The proposed C-COR1 f3.5h24 land use district will enable the development of a mixed-use building on the site.

The site is 1,975 square metres and sits on the corner of Elbow Drive and Southland Drive in the community of Southland. Currently vacant, the site was previously used for a gas station and has since been remediated in preparation for redevelopment. Adjacent uses to the site include a single storey commercial development to the north that includes with a liquor store and beauty salon, the Southwood Public Library to the west and a city park that includes a small playground to the northwest. Single storey commercial developments are located on each of the adjacent corners of the intersection.

Royop intends to build a mix-use building with retail uses such as shops and restaurants at grade with the potential for office and/or residential uses on the upper floors. The development of this project will contribute to the evolution of Southland Drive and Elbow into more urban mixed-use and pedestrian friendly streets by providing highly permeable and pedestrian focused ground level that positively contributes to the improvement of the public realm. The building will be integrated into the neighbourhood and offer local residents the opportunity to walk to shops and services for their daily needs while also providing additional employment or residential uses.

The local community will be consulted during the application process to ensure that they are informed of the proposed development and have the opportunity to contribute their thoughts and ideas regarding the proposed project.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S

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**APPENDIX II**

**LETTERS SUBMITTED**

The Southwood Community Association will take a neutral position on this application. We have not heard from any community members regarding this application; therefore we are unable to oppose nor support the application.

However, there are some issues that exist with this site that I would like to have noted to the application. I appreciate that these comments may not relate strictly to the land use application, but it is imperative that these points are taken into consideration.

- Access: access to the site will be difficult. The current access points to the site will create many issues to the traffic using both southbound Elbow Drive and westbound Southland Drive, as well as users of the site turning to northbound Elbow Drive.
- Connectivity: the site is located in an area where connectivity should not be an issue, however, we would like to see improvements made to the pedestrian realm (i.e. sidewalks and curb cuts). I believe that this is a City issue, and many improvements need to be made to better the pedestrian realm, not only from a connectivity perspective, but also from a safety perspective given decreased sight lines with a building located on the corner and sidewalks that are not wide enough to support heavier foot traffic.
- Height: the height of the building may prove to be an issue to the neighbours in Southwood living to the west of the site. In discussion with homeowners in that area, they believe that they will lose their morning sunlight.
- Location of building on site: given the C-COR1 designation, the building can be located on directly on the corner. This is imperative that this is achieved because I believe that this will decrease some of the access issues that will be present. If the building at the end of the day is set back to allow vehicular traffic in front, then it would render the development absolutely useless.

I have been in discussion with the applicant and landowner for this site. They are going to be hosting a design workshop with the community to go over what we, as a community, would like to see on the site. I believe they will be having a strong consultation program with this application.

Should you require any further information from myself, please contact me.

Regards,  
Becky Poschmann, BCD, BA  
Director of Development  
Southwood Community Association

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

APPENDIX III

SHADOW STUDIES



MARCH 21 8 am



MARCH 21 10 am



MARCH 21 12 pm



MARCH 21 2 pm



MARCH 21 4 pm



MARCH 21 6 pm



JUNE 21 8 am



JUNE 21 10 am



JUNE 21 12 pm



JUNE 21 2 pm



JUNE 21 4 pm



JUNE 21 6 pm

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S



SEPTEMBER 21 8 am



SEPTEMBER 21 10 am



SEPTEMBER 21 12 pm



SEPTEMBER 21 2 pm



SEPTEMBER 21 4 pm



SEPTEMBER 21 6 pm



DECEMBER 21 8 am



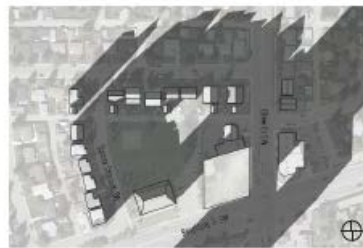
DECEMBER 21 10 am



DECEMBER 21 12 pm



DECEMBER 21 2 pm



DECEMBER 21 4 pm



DECEMBER 21 6 pm

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
SOUTHLAND DRIVE SW & ELBOW DRIVE SW

MAP 16S



MARCH 21 8 am



JUNE 21 8 am



SEPTEMBER 21 8 am



MARCH 21 4 pm



JUNE 21 4 pm



SEPTEMBER 21 4 pm



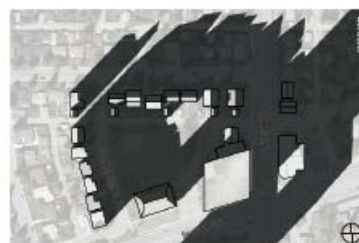
DECEMBER 21 10 am



DECEMBER 21 12 pm



DECEMBER 21 2 pm



DECEMBER 21 4 pm

A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through the eyepiece of a theodolite. The theodolite is mounted on a tripod. The surveyor's right hand is on the instrument, and a pencil is visible in their left hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 5.04**



LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**EXECUTIVE SUMMARY**

This land use amendment application is to redesignate the parcel currently containing the Anderson Light Rail Transit (LRT) Station's Park N Ride surface parking lots from Special Purpose – City and Regional Infrastructure (S-CRI) District to Direct Control Districts to accommodate mixed-use development on the site.

**PREVIOUS COUNCIL DIRECTION**

A previous Land Use Amendment application LOC2015-0143 for redevelopment of this site was referred back to Administration at the 2015 October 5 Combined Meeting of Council.

*AMENDMENT, Moved by Councillor Demong, Seconded by Councillor Keating, that the Agenda for today's meeting, as amended, be further amended by bringing forward and referring Report, Land Use Amendment, Southwood (Ward 11), Macleod Trail S and 109 Avenue SW, Bylaws 168D2015 and 169D2015, CPC2015-173, to the Administration for further refinement based upon other work streams including but not limited to the transit parking strategy, Transit Orientated Development funding work, the Calgary Parking Authority strategy, and the completion of the Anderson Station Area Redevelopment Plan, returning to Council, through the Calgary Planning Commission, when the work is complete.*

The above-referenced Land Use Amendment application LOC2015-0143 has since been cancelled. A new concept for the site has been submitted as part of this Land Use Amendment and Outline Plan Application LOC2017-0273.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

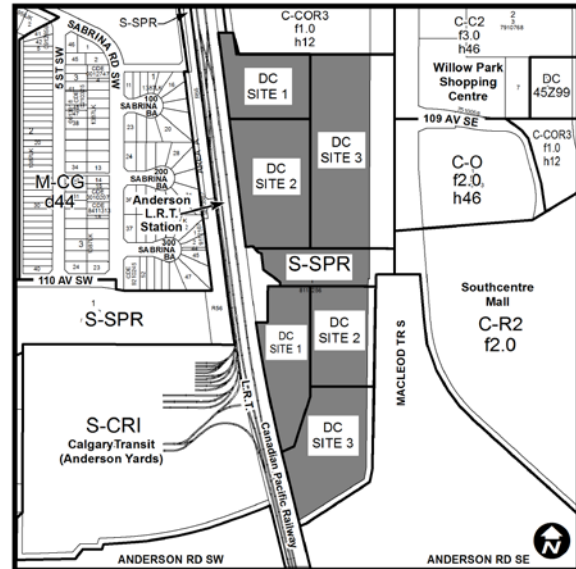
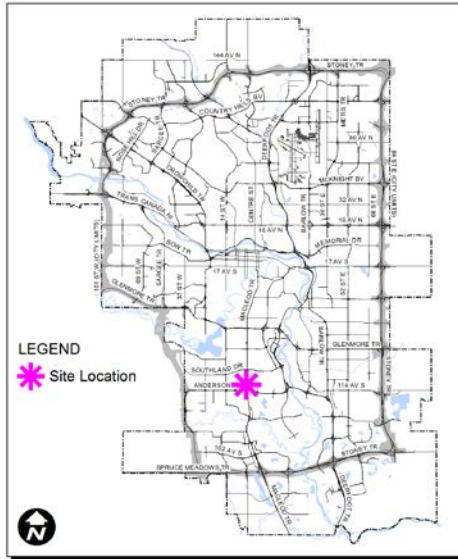
**REASON(S) FOR RECOMMENDATION:**

This application would enable the redevelopment of the Anderson LRT Station with mixed-use, transit oriented development that is in line with the intensification targets set out in both the Calgary Transportation Plan and the Municipal Development Plan, as well as the specific policies guiding the redevelopment of the station in the Anderson Station Area Redevelopment Plan. The proposal would see the elimination of surface parking lots on this site, replaced with a structured parking facility, and will introduce amenities for all modes of travel to the site.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

LOCATION MAPS



LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 7.49 hectares  $\pm$  (18.50 acres  $\pm$ ) located at 11015 Anderson Station Way SW (Plan 8111256, Block 1) from Special Purpose – City and Regional Infrastructure (S-CRI) District **to** Special Purpose – School, Park and Community Reserve (S-SPR) District and DC Direct Control District to accommodate mixed use development with guidelines (APPENDIX II).

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Applicant:**

NORR Architects Engineers Planners

**Landowner:**

The City of Calgary

**PLANNING EVALUATION**

**SITE CONTEXT**

This application is for the proposed redevelopment of the Anderson Station Light Rail Transit (LRT) Park n Ride site. The area is currently one titled parcel that contains the LRT station itself, the bus drop-off areas and approximately 1,665 parking stalls. This application is intended to enable redevelopment of the site to include mixed-use and office development and achieve a more activated, pedestrian-friendly urban village adjacent to a busy transit station.

This parcel lies within the primarily residential community of Southwood, with existing low density residential development located west of the parcel across the rail corridor, which has the potential to redevelop with densities up to 44 units per hectare. Adjacent to the southwestern boundary of the site is the Calgary Transit Anderson Yards, a large maintenance facility for buses and C-Trains. Future access to the maintenance facility will be accommodated via a new all-turns access onto Anderson Road SW, adjacent to the southern boundary of that site. An emergency access easement is proposed to be provided through this site for access to the maintenance facility across the rail corridor.

Directly east of the parcel, across Macleod Trail S, are major retail developments including Southcentre Shopping Centre and Willow Park Village, as well as the Southcentre Executive Tower office building.

A large auto dealership is located directly adjacent to the northern boundary of the site, with office towers and a restaurant further north of the dealership.

Site constraints include the proximity to the CP Railway and adjacency to both Macleod Trail S and Anderson Road S. Additionally, a 25 KV transmission line runs adjacent to the eastern property line along Macleod Trail S, but lies outside the property line.

<b>Southwood</b>	
Peak Population Year	1978
Peak Population	8,101
2017 Current Population	6,214
Difference in Population (Number)	-1,887
Difference in Population (Percent)	-23%

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

A previous Outline Plan application for this site was approved by Calgary Planning Commission at its 2015 August 27 meeting, and the associated Land Use amendment referred to Council with a recommendation of approval. However, at its 2015 October 5 Public Hearing, Council referred the item back to Administration;

*“for further refinement based upon other work streams including but not limited to the transit parking strategy, Transit Oriented Development funding work, the Calgary Parking Authority strategy, and the completion of the Anderson Station Area Redevelopment Plan, returning to Council, through the Calgary Planning Commission, when the work is complete.”*

The Anderson Station Park and Ride Plan was approved by Council at its 2016 October 03 meeting, following a recommendation of approval from the SPC on Transportation and Transit at its 2016 September 21 meeting. This plan provided details on how the loss of parking stalls on this site could be offset and replaced in alternative locations.

The Anderson Station Mobility Hub Funding Model Report was approved by Council at its 2017 February 13 meeting. The report details the funding plan for the Anderson Station Mobility Hub's transit related infrastructure (Park and Ride Stalls, Anderson Station Pedestrian Bridge, Station Infrastructure and Anderson Road All Turns Access) through their inclusion and evaluation as an investment candidate in the next update of Investing in Mobility, Transportation's 10-year capital plan.

The Anderson Station Area Redevelopment Plan was approved by City Council at its 2017 June 12 Public Hearing, containing specific policies for the redevelopment of this site as a major mixed-use redevelopment site.

## **LAND USE DISTRICTS**

The application proposes six redevelopment sites, within two Mixed-Use-based DC Direct Control Districts. The site itself is affected by a number of external factors that influence the development potential of the site. One such constraint is the site's adjacency to the rail corridor and the Canadian Pacific (CP) Railway. As Administration is currently developing specific policies for development in proximity to railways, conditions have been added to the associated Outline Plan in order to ensure that a further review be completed at the development permit stage, to ensure that the build-out of those sites is in compliance with policies in place at that time.

This Land Use Amendment application proposes that the central portion of the plan area be redesignated to S-SPR to accommodate the two future Municipal Reserve (MR) parcels that will become the central public plaza as required in the Anderson ARP. The LRT station building and surrounding lands will retain the existing S-CRI designation, and the remaining lands will be separated into two distinct DC Districts; one for the higher intensity mixed-use development north of the station and public plaza based on the Mixed Use – Active Frontage (MU-2) land use, and a Mixed Use - General (MU-1) based DC for the lands south of the public plaza,

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

intended to accommodate more flexible mixed-use development, with the allowance for larger use facades in this southern development area, while still ensuring a mix of uses that will complement the more intense development directly adjacent to the station itself.

Both DC Districts propose lower heights for the sites closest to the existing residential development in the Southwood community in order to minimize the visual and shadowing impacts on the existing residences. The sites adjacent to Macleod Trail S have all been given higher building height maximums, increasing in height from north (46 metres) to south (70 metres) as determined on Map 8 of the Anderson Station ARP.

Density targets in this application are proposed as Floor Area Ratio (FAR) maximums with both maximum and minimum building heights. All sites in this redevelopment area must have a minimum building height of 12 metres as per the policies contained in Section 5.1 of the ARP. The MU-2 based DC Sites 1 and 2, as well as the MU-1 based DC Site 1, all located along the western edge of the plan area, have the lowest maximum building heights at 26 metres due to their adjacency to the existing residential development, and in line with the building heights approved in the ARP. FAR maximums for these sites increase from 3.0 for the southernmost DC (MU-1) Site 1 parcel, to 3.5 for DC (MU-2) Site 2 directly across from the LRT platform, and further to 4.5 FAR for the northwestern parcel that is expected to contain the new Calgary Transit parkade, on DC (MU-2) Site 1.

The two MU-2 based DC development parcels along Macleod Trail S and identified as Site 3 have a proposed FAR of 4.5 and maximum building heights of 46 metres, in order to accommodate mixed use development, including a potential hotel.

The MU-1 based DC Sites 2 and 3, south of the new public plaza, have proposed building height maximums of 50 and 70 metres respectively, with FAR of 5.0 on both, given their adjacency to Macleod Trail S and their greater distance from the existing low density residential development. This is intended to allow flexibility for these sites to develop with larger towers without creating shadow impacts on adjacent sites.

The MU-1 based DC District proposed for the redevelopment sites south of the existing Transit station and public plaza is intended to accommodate more flexible mixed-use development, with the allowance for larger use facades in this southern development area, while still ensuring a mix of uses that will complement the more intense northern development sites directly adjacent to the station itself. This DC District also includes provisions for floor plate sizes, building separation distances, building setbacks from parks and streets, building stepback rules, and façade articulation requirements. These rules have been designed to ensure that development on these sites respects the transit-oriented development vision for the area.

The northern MU-2 based DC District establishes rules related to building orientation and uses that are intended to create a more pedestrian-friendly, activated street front within the site. The proposed DC contains the requirement for street-oriented entrances for all at-grade uses, regulates building setbacks to create a pedestrian-friendly, activated streetscape, and provides location flexibility for uses within the buildings. The rules in this district are intended to work in

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

conjunction with the Design Guidelines that are included in the associated Outline Plan application, which establish guidelines related to the design of the buildings, streetscapes, and set active frontage areas in order to achieve a vibrant, attractive transit-oriented development area that prioritizes active modes of travel.

The DC Districts also propose the inclusion of Signs – Class F and Signs – Class G with specific rules. These provisions are in order to permit the future installation of smaller, pedestrian-oriented Third Party Advertising Signs and Digital Third Party Advertising Signs with specific rules for the size, orientation and height of those signs. This section also includes a provision that allows the existing large Third Party Advertising sign (billboard sign) on site to remain operational until that site is redeveloped and the parcel is no longer under the City's ownership, at which point it would no longer meet the rules in the DC for that sign type and would be removed.

The DC Districts also provide a provision that vehicular and bicycle parking stalls may be provided on any parcel in the district. This rule is intended to work in tandem with the policies in Section 4.7 of the ARP that state that reduced parking requirements shall be applied in the plan area, encouraging relaxations down to and including zero parking. These policies will be applied at the development permit stage when the proposed mix of uses are known and the parking requirements can be determined. Any necessary parking stalls are intended to be provided on any combination of parcels in order to encourage parking efficiencies in this TOD area.

The MU-1 and MU-2 base districts allow for the provisions of a wide variety of residential and commercial uses in order to allow development to move with market demand as this site redevelops over a number of years. This plan anticipates an intensity at full build-out of 776 people and jobs per gross developable hectare, based on the development of 1,152 residential units mixed with commercial and office development.

## **LEGISLATION & POLICY**

### South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

### Municipal Development Plan (MDP)

The Municipal Development Plan identifies this area as a Major Activity Centre, setting out a minimum intensity target of 200 people and jobs per gross developable hectare. This plan anticipates providing an intensity of 776 people and jobs per gross developable hectare, thus far exceeding the minimum target set out in the MDP.

### Calgary Transportation Plan (CTP)

The Calgary Transportation Plan calls for “compact, mixed-use development and pedestrian-friendly designs... along the existing and future Primary Transit Network.” This plan includes many features intended to improve the pedestrian experience at this station, such as the parks plazas linking the station to the pedestrian bridge over Macleod Trail S, stepbacks on buildings

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

to create more human-scale buildings, and activated street frontages. The proposal also includes a broad range of uses to provide a varied, mixed-use development within close proximity to the station, in line with the direction in the CTP.

Anderson Station Area Redevelopment Plan (ARP)

The Anderson Station Area Redevelopment Plan was approved by Council at its 2017 June 12 Public Hearing, and identifies this site as the “Anderson Transit Village.” The policies in Section 2.2.1 Anderson Transit Village provide guidance for this redevelopment proposal and assign the Community – High Density building block (from the MDP’s Developed Areas Guidebook) to the eastern portion of the site adjacent to Macleod Trail S, calling for high density residential developments and a wide range and combination of uses. The proposed DC Districts would allow the site to develop in alignment with the intent of the Community – High Density building block.

The western portion of the plan area, adjacent to the rail corridor and the existing Southwood community, is assigned the Community – Centre building block, calling for high density residential uses, a concentration of non-residential uses including institutional, and a wide range and combination of uses. The proposed DC Districts would allow the site to develop in alignment with the intent of the Community – Centre building block, while maintaining respect for the existing community.

The ARP also contains policies in Section 3.3 guiding the development of the Anderson Transit Village Public Plaza, calling for the provision of the centrally-located park/plaza and providing direction on design elements for the park. This application proposes two centrally-located Municipal Reserve (MR) parcels that will form the Anderson Transit Village Public Plaza, thus satisfying the requirements, and meeting the intent of the ARP.

This Outline Plan and Land Use Amendment application was evaluated considering all of the policies in the ARP and is considered to be in alignment with the vision and intent of the document for this major redevelopment site.

## TRANSPORTATION NETWORKS

The Plan sets out a mobility network for the Plan area which provides for walking, cycling, public transportation as well as private vehicles. Although multiple modes of transportation are accommodated in the Plan area, creating a walkable community with a vibrant street life is a key principle of this Plan.

A Transportation Impact Assessment (TIA) was undertaken to assess the impact redevelopment of the Plan area would have on the transportation network. Upgrades will be required to the transportation network in accordance with the conclusions of the TIA completed in support of this application and the proposed Anderson ARP. Upgrades to area intersections to support Transit movements are planned in the short term.

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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A regional pathway is proposed along Macleod Trail S connecting the Plan area to the north and a proposed active modes bridge over the train tracks will provide connection to the west.

Parking policies and supply ratios have been established in the Anderson ARP to support the TOD intent of the site and to mitigate vehicular impacts on the network. The parking relaxations are supported with adequate walking and cycling facilities, as well as primary transit service.

**UTILITIES & SERVICING**

Services are currently available on the subject site and can be upgraded to accommodate development on the site. Utility placement within the proposed roads can accommodate all utilities as well as the inclusion of District Energy.

**ENVIRONMENTAL ISSUES**

Due to concerns related to the car dealership (north), and Anderson Transit Garage, west of the CPR tracks, an updated Phase II Environmental Site Assessment report will be required at the Development Permit stage.

**ENVIRONMENTAL SUSTAINABILITY**

Utility line assignments have been placed in order to include the potential for District Energy to serve this redevelopment site, providing more environmentally sustainable energy source for new buildings. Additionally, City of Calgary facilities are expected to achieve a LEED gold standard, and so the development of a new parking structure to serve the Park n Ride component of the project can be expected to include more environmentally sustainable features.

**GROWTH MANAGEMENT**

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

**PUBLIC ENGAGEMENT**

**Community Association Comments**

This application was circulated to the Federation of Calgary Communities, and the Southwood and Willow Ridge Community Associations. Only the Southwood Community Association submitted comments on this application, choosing to remain neutral on the application itself, but offering comments related to the future development of the site and its

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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potential impact on the community. The letter also notes the importance of good pedestrian connections and bridges into Southwood, which will be considered with the Anderson Station Mobility Hub Funding Model. The comments received are included in APPENDIX III.

**Citizen Comments**

Notification letters were sent to adjacent property owners and no objections were received. One phone inquiry from an area resident was received and no concerns were raised.

**Public Meetings**

Public meetings were held in conjunction with the recently-approved Area Redevelopment Plan, therefore no further public engagement events were hosted in conjunction with this outline plan application.

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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**APPENDIX I**

**APPLICANT'S SUBMISSION**

Real Estate and Development Services (REDS) has submitted a Land Use Redesignation and Outline Plan application to facilitate the redevelopment of City owned lands at Anderson Station into a Transit Oriented Development precinct. The proposed development comprises 8.0 ha (19.9 ac) in the community of Southwood and falls entirely within a 400 metre radius of the Anderson LRT Station. The site includes the LRT station, bus terminal and park and ride area which currently provides 1,665 surface parking stalls.

A Land Use and Outline Application was submitted in 2014 (LOC2014-0143). The Outline Plan was approved by Calgary Planning Commission in August 2015; however, Council referred the Land Use Amendment back to Administration in October 2015 for further refinement and alignment with other ongoing streams of work at the time including the Transit Parking Strategy, TOD Development Funding and the completion of the Anderson Station Area Redevelopment Plan (ARP). Since then, a significant amount of work has been conducted on a regional park and ride strategy and on finalizing the ARP. The Anderson Station Park and Ride Plan was approved in 2016, and the ARP was approved by Council in June 2017. With the completion of this work, a new Outline Plan and Land Use Amendment has been prepared by REDS that is consistent with these plans as well as the Municipal Development Plan which identifies the area as a Major Activity Centre.

The current application continues to facilitate a compact, pedestrian oriented development that contains a mix of uses and a high quality public realm while maintaining a minimum of 500 park and ride stalls within a new parkade. Redevelopment aims to transform an underutilized asset and optimize existing infrastructure by introducing higher densities and transit supportive land uses into an existing community adjacent to a major transit hub. The mix of uses, density, building heights and non-standard street sections are consistent with the previous application (LOC20014-0143). Changes to the site design have been made to improve bus circulation and pedestrian mobility throughout the site.

The proposed land use districts are Direct Control (DC) Districts based on the Mixed Use One (MU-1) and Mixed Use Two (MU-2) Districts, and Special Purpose – School, Park and Community Reserve (S-SPR) District. DC Districts are required for several reasons namely to enable maximum building heights in excess of 10 storeys, allow district energy in structures below grade, and to facilitate shared parking between parcels. The S-SPR district will accommodate a public plaza/park area situated at the entrance of the station which will serve as the main gateway to the site and central public gathering space. The park spaces will also accommodate an east-west regional pathway that will connect Southwood to the station area and to other commercial, retail and recreational opportunities across Macleod Trail.

To conclude, the application implements the vision of approved land use planning policy and provides a solid Park and Ride Plan that ensures minimal net loss of parking stalls corresponding to the phased redevelopment of the Anderson Station lands. The proposal will enable a greater range of housing choice and amenities in an established area at densities that support transit use. It also optimizes existing infrastructure, enhances pedestrian and cyclist connectivity and increases people's mobility choices while creating a new compact urban form with a distinct sense of place in south Calgary. For these reasons, we request the support of Calgary Planning Commission and City Council in this application.

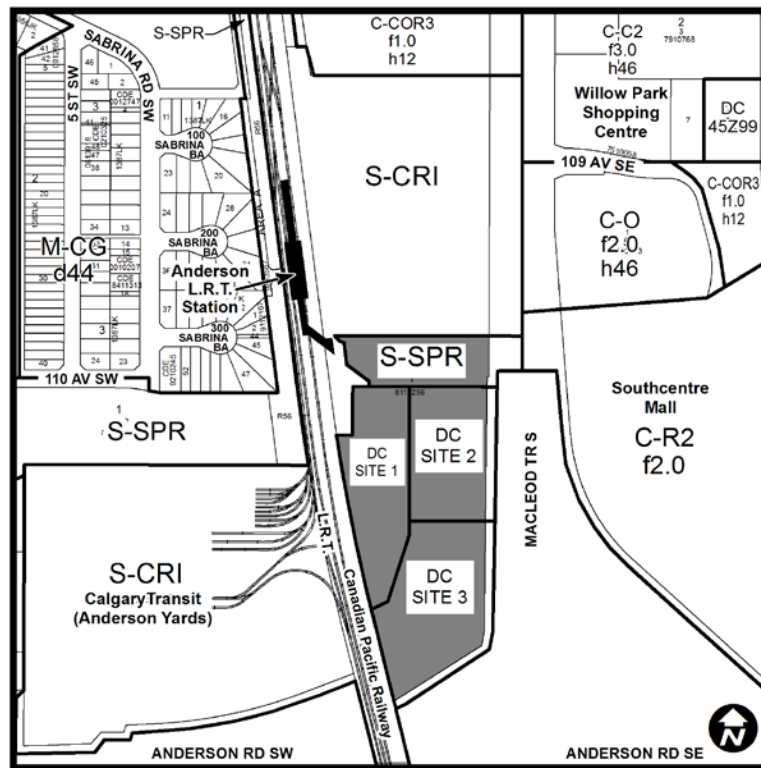
LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

APPENDIX II

PROPOSED DIRECT CONTROL GUIDELINES

Bylaw 1: based on Mixed Use – General (MU-1) District



**Purpose**

1 This Direct Control District is intended to accommodate:

- (a) a mix of residential and commercial **uses** in a compact urban form that are located in the same **building** or in multiple **buildings** throughout an area;
- (b) **developments** where residential and commercial **uses** are supported at **grade** facing a **street**; and
- (c) mid to high rise **development** with varying **building heights** up to 70.0 metres.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Compliance with Bylaw 1P2007**

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3, 4 and 14 of Bylaw 1P2007 apply to this Direct Control District.

**Reference to Bylaw 1P2007**

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

**Permitted Uses**

- 4 The *permitted uses* of the Mixed Use – General (MU-1) District of Bylaw 1P2007 are the *permitted uses* in this Direct Control District.

**Discretionary Uses**

- 5 (1) The *discretionary uses* of the Mixed Use – General (MU-1) District of Bylaw 1P2007 are the *discretionary uses* in this Direct Control District with the addition of:

- (a) **Market;**
- (b) **Restaurant: Food Service Only – Large;** and
- (c) **Restaurant: Licensed – Large.**

- (2) The following *uses* are *discretionary uses* only if they are owned or operated on by, or on behalf of, the City:

- (a) **Sign – Class F;** and
- (b) **Sign – Class G.**

**Bylaw 1P2007 District Rules**

- 6 Unless otherwise specified, the rules of the Mixed Use – General (MU-1) District of Bylaw 1P2007 apply to this Direct Control District.

**Additional Sign – Class F and Sign – Class G Rules**

- 7 (1) Notwithstanding Section 111 (6) (b) and Section 115.2 (5) (b) of Bylaw 1P2007, **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** may contain *copy* visible from a pathway.

- (2) Notwithstanding Section 115.2 (6) of Bylaw 1P2007, where a **Digital Third Party Advertising Sign** is visible from and located within 125.0 metres of a *building* containing a **Dwelling Unit**, the *sign* must not operate, or must only display a blank screen between 10 p.m. and 7 a.m.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

- (3) Notwithstanding Section 112 (1), (3) and (4) and Section 115.3 (1), (2) and (3) of Bylaw 1P2007, **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** must not be located within 20.0 metres of any **Freestanding Sign** facing the same oncoming traffic, nor any other **Third Party Advertising Sign** or **Digital Third Party Advertising Sign**.
- (4) **Copy** on **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** must be oriented towards pedestrians.
- (5) **Third Party Advertising Signs** may only be illuminated indirectly in a manner that prevents the trespass of light onto **adjacent parcels**.
- (6) Notwithstanding Section 113 and Section 115.4 of Bylaw 1P2007, the maximum height of a **Third Party Advertising Sign** or a **Digital Third Party Advertising Sign** is 3.0 metres and the maximum **sign area** is 4.5 square metres.
- (7) Sections 114 (11) (b) and (e) and Section 115.5 (10) (b) and (d) of Bylaw 1P2007 do not apply in this Direct Control District.
- (8) Subsections (1) through (5) of this Section do not apply to a **Third Party Advertising Sign** or a **Digital Third Party Advertising Sign** where:
  - (a) it existed on the date of approval of this Direct Control Bylaw;
  - (b) it is located on a **parcel** owned by **The City**; and
  - (c) it is approved by a **development permit** issued by **The City** on or before the date of approval of this Direct Control Bylaw.

**Façade Width for Uses Facing a Street**

- 8 There is no restriction on the length of the **building** façade that faces a **street** containing an individual **use** on the floor closest to **grade**.

**Additional Rules for Multi-Residential and Commercial Uses**

- 9 (1) All **units** in a **Multi-Residential Development** that are located on the floor closest to **grade adjacent** to a **Park** must have an individual, separate, direct access to **grade** along that **frontage**.
- (2) **Developments** which contain one or more commercial **uses** on the floor closest to **grade adjacent** to a **Park** must have direct access to **grade** along that **frontage**.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Setback Area**

10 There is no requirement for a minimum **setback area** except:

- (a) where a **parcel** shares a **property line** with a **special purpose district**, a minimum **setback area** of 6.0 metres is required.
- (b) for the floor closest to **grade**, the maximum **building setback** from a **property line** shared with a **street** is 3.0 metres for 60 per cent of the length of the **building** façade that faces the **street**.
- (c) for the floor closest to **grade**, the maximum **building setback** from a **property line** shared with Macleod Trail S is 6.0 metres for the entire length of the **building** façade.

**Building Separation**

- 11 (1) Where the widest dimension of a **balcony** faces a **property line** shared with another **parcel**, the minimum setback of a **balcony** from the shared **property line** is 4.0 metres
- (2) The façade of a **building** located above 26.0 metres from **grade** must provide a minimum horizontal separation of:
- (a) 24.0 metres from the façade of any other **building** on the same **parcel**; and
  - (b) 12.0 metres from a **property line** shared with another **parcel**, unless otherwise referenced in Sections 17, 20 and 23 of this Direct Control District.
- (3) The façade of a **building** referenced in subsection (2) does not include **balconies**.

**Ground Floor Height**

- 12 (1) Unless otherwise referenced in subsection (2), the minimum height of the floor closest to **grade** of a **building** is 4.5 metres as measured vertically from the floor to the ceiling.
- (2) There is no minimum height for a **Dwelling Unit** that is located on the floor closest to **grade** in a **building**.

**Floor Plate Restrictions**

- 13 (1) Each floor of a **building** located partially or wholly above 26.0 metres above **grade** has a maximum **floor plate area** of:

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

- (a) 750 square metres of **gross floor area** for “Residential Uses”; and
  - (b) 2,400 square metres of **gross floor area** for “Commercial Uses”.
- (2) Where this Section refers to “Commercial Uses,” it refers to the **uses** listed in Sections 4 and 5 of this Direct Control District other than “Residential Uses”.
- (3) Where this Section refers to “Residential Uses,” it refers to the **uses** listed in the Residential Group in Schedule A of Bylaw 1P2007.

**Additional Motor Vehicle Parking Requirements**

14 **Motor vehicle parking stalls** and **bicycle parking stalls** required for **uses** in this Direct Control District may be shared and supplied on any one **parcel** or combination of **parcels** in this Direct Control District.

**Development Authority – Power and Duties for Relaxations**

15 The **Development Authority** may relax the rules in Sections 10, 11, 13, 16, 19 and 22 in this Direct Control District provided the test for relaxations as set out in Bylaw 1P2007 is met.

**Site 1 (0.99 ha/2.45 ac)**

**Application**

16 The provisions in sections 17 through 18 apply only to Site 1.

**Building Height and Street Wall Stepback**

- 17 (1) The minimum **building height** is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), the maximum **building height** is 26.0 metres.
- (3) Where a **parcel** shares a **property line** with a **street**, the maximum **building height** is:
- (a) 12.5 metres measured from **grade** within 5.0 metres of that shared **property line**; and
  - (b) 26.0 metres measured from **grade** at a distance greater than 5.0 metres of that shared **property line**.
- (4) A **building** façade **adjacent** to a **street** must have a minimum streetwall stepback of 2.0 metres at a **building height** of 12.5 metres or greater.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**Floor Area Ratio**

18 The maximum *floor area ratio* is 3.0.

**Site 2 (0.90 ha/2.22 ac)**

19 The provisions in sections 20 through 21 apply only to Site 2.

**Building Height and Street Wall Stepback**

- 20 (1) The minimum *building height* is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), (5) and (6), the maximum *building height* is 50.0 metres.
- (3) Where a *parcel* shares a *property line* with a *street* or a **Park**, the maximum *building height* is:
- (a) 12.5 metres measured from *grade* within 5.0 metres of that shared *property line*; and
  - (b) 50.0 metres measured from *grade* at a distance greater than 5.0 metres of that shared *property line*.
- (4) A *building* façade *adjacent* to a *street* must have a minimum streetwall stepback of 2.0 metres at a *building height* of 12.5 metres or greater.
- (5) Where a *parcel* shares a northern *property line* with a **Park**, the maximum *building height* is:
- (a) 12.5 metres measured from *grade* within 12.0 metres of that shared *property line*;
  - (b) 20.5 metres measured from *grade* between 12.0 metres and 30.0 metres from that shared *property line*; and
  - (c) 50.0 metres measured from *grade* at a distance greater than 30.0 metres from that shared *property line*.
- (6) Where a *parcel* shares a southern *property line* with a **Park**, the maximum *building height* is:
- (a) 12.5 metres measured from *grade* within 8.0 metres of that shared *property line*; and
  - (b) 50.0 metres measured from *grade* at a distance greater than 8.0 metres from that shared *property line*.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**Floor Area Ratio**

21 The maximum *floor area ratio* is 5.0.

**Site 3 (1.23 ha/3.04 ac)**

22 The provisions in sections 23 through 24 apply only to Site 3.

**Building Height and Street Wall Stepback**

- 23 (1) The minimum *building height* is 12.0 metres.
- (2) Unless otherwise referenced in subsections (3) and (5), the maximum *building height* is 70.0 metres.
- (3) Where a *parcel* shares a *property line* with a *street*, the maximum *building height* is:
- (a) 12.5 metres measured from *grade* within 5.0 metres of that shared *property line*; and
  - (b) 70.0 metres measured from *grade* at a distance greater than 5.0 metres from that shared *property line*.
- (4) A *building* façade *adjacent* to a *street* must have a minimum streetwall stepback of 2.0 metres at a *building height* of 12.5 metres or greater.
- (5) Where a *parcel* shares a *property line* with a *Park*, the maximum *building height* is:
- (a) 12.5 metres measured from *grade* within 9.0 metres of that shared *property line*;
  - (b) 16.5 metres measured from *grade* between 9.0 metres and 70.0 metres from that shared *property line*; and
  - (c) 70.0 metres measured from *grade* at a distance greater than 70.0 metres from that shared *property line*.

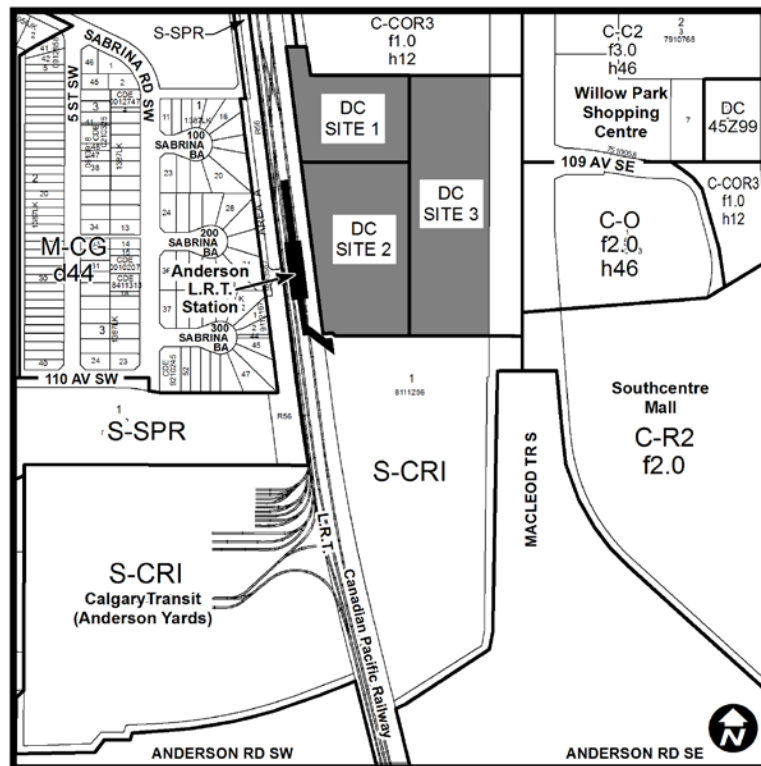
**Floor Area Ratio**

24 The maximum *floor area ratio* is 5.0.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

Bylaw 2: based on Mixed Use – Active Frontage (MU-2) District



**Purpose**

- 1 This Direct Control District is intended to accommodate:
- (a) a mix of commercial and residential **uses** in a compact urban form;
  - (b) **developments** where commercial **uses** are required at **grade** to promote activity at the **street** level;
  - (c) **developments** with storefronts along a continuous block face on the commercial **street**;
  - (d) mid to high rise **development** with varying **building heights** up to 46.0 metres;
  - (e) **district energy** operations within a **building** above or below **grade**.

**Compliance with Bylaw 1P2007**

- 2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3, 4 and 14 of Bylaw 1P2007 apply to this Direct Control District.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Reference to Bylaw 1P2007**

- 3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

**Defined Uses**

4 **‘Utility Building – With Functions Below Grade’**

- (a) means a *use*:
- (i) where a **building** is greater than 10.0 square metres in **gross floor area**;
  - (ii) where water steam, sewage treatment or disposal, irrigation, drainage, gas, electricity, heat, waste management, water heating and cooling for the purpose of **district energy** and telecommunications are located;
  - (iii) where the **use** can be located partially or wholly below or above **grade**; and
  - (iv) that does not include a **Sewage Treatment Plant** or a **Water Treatment Plant**.

**Permitted Uses**

- 5 The **permitted uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

**Discretionary Uses**

- 6 (1) The **discretionary uses** of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of the following **uses**:
- (a) **Utility Building – With Functions Below Grade**;
  - (b) **Market**;
  - (c) **Power Generation Facility – Medium**;
  - (d) **Restaurant: Food Service Only – Large**; and
  - (e) **Restaurant: Licensed – Large**.
- (2) The following **uses** are **discretionary uses** only if they are owned or operated on by, or on behalf of, the **City**:
- (a) **Sign – Class F**; and
  - (b) **Sign – Class G**.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Bylaw 1P2007 District Rules**

7 Unless otherwise specified, the rules of the Mixed Use – Active Frontage (MU-2) District of Bylaw 1P2007 apply to this Direct Control District.

**Additional Sign – Class F and Sign – Class G Rules**

- 8 (1) Notwithstanding Section 111 (6) (b) and Section 115.2 (5) (b) of Bylaw 1P2007, **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** may contain **copy** visible from a pathway.
- (3) Notwithstanding Section 115.2 (6) of Bylaw 1P2007, where a **Digital Third Party Advertising Sign** is visible from and located within 125.0 metres of a **building** containing a **Dwelling Unit**, the **sign** must not operate, or must only display a blank screen between 10 p.m. and 7 a.m.
- (3) Notwithstanding Section 112 (1), (3) and (4) and Section 115.3 (1), (2) and (3) of Bylaw 1P2007, **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** must not be located within 20.0 metres of any **Freestanding Sign** facing the same oncoming traffic, nor any other **Third Party Advertising Sign** or **Digital Third Party Advertising Sign**.
- (4) **Copy** on **Third Party Advertising Signs** and **Digital Third Party Advertising Signs** must be oriented towards pedestrians.
- (5) **Third Party Advertising Signs** may only be illuminated indirectly in a manner that prevents the trespass of light onto **adjacent parcels**.
- (6) Notwithstanding Section 113 and Section 115.4 of Bylaw 1P2007, the maximum height of a **Third Party Advertising Sign** or a **Digital Third Party Advertising Sign** is 3.0 metres and the maximum **sign area** is 4.5 square metres.
- (7) Sections 114 (11) (b) and (e) and Section 115.5 (10) (b) and (d) of Bylaw 1P2007 do not apply in this Direct Control District.

**Location of Uses Within Buildings**

9 The following **uses** must not be located on the floor closest to **grade** of a **building** facing 109 Avenue SW and the portion of Anderson Station Way south of 109 Avenue SW and north of Southport Way SW:

- (a) **Addiction Treatment;**
- (b) **Assisted Living;**
- (c) **Catering Service – Minor;**
- (d) **Counselling Service;**
- (e) **Custodial Care;**

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

- (f) Dwelling Unit;
- (g) Office;
- (h) Place of Worship – Medium;
- (i) Place of Worship – Small; and
- (j) Residential Care.

**Façade Width for Uses Facing a Street**

- 10 (1) Unless otherwise referenced in subsections (2) and (3), the length of the **building** façade that faces a **street** containing an individual **use** on the floor closest to **grade** is a maximum of 15.0 metres except where a **use** faces Macleod Trail and Southport Way SW.
- (2) For an individual **Drinking Establishment – Medium, Drinking Establishment – Small, Market, Restaurant: Food Service Only – Large, Restaurant: Food Service Only – Medium, Restaurant: Food Service Only – Small; Restaurant: Neighbourhood, Restaurant: Licensed – Large, Restaurant: Licensed – Medium, Restaurant: Licensed – Small, Retail and Consumer Service or Supermarket use** located on the floor closest to **grade**, the length of the **building** façade that faces a **street** may be increased to 30.0 metres where all of the other **uses** that share the same façade meet the requirements of subsection (1).
- (3) The length of the **building** façade that faces a **street** containing an individual **Financial Institution, Medical Clinic, Medical Marijuana Counselling, Office, Parking Lot – Structure or Payday Loan use** on the floor closest to **grade** is a maximum of 9.0 metres.
- (4) Subsection (3) does not apply when the **Financial Institution, Medical Clinic, Medical Marijuana Counselling, Office, Parking Lot – Structure or Payday Loan use** is contained within a **building** with a façade adjacent to Southport Way SW.

**Setback Area**

- 11 There is no requirement for a minimum **setback area** except:
- (a) where a **parcel** shares a **property line** with a **special purpose district**, a minimum **setback area** of 6.0 metres is required.
  - (b) for the floor closest to **grade**, the maximum **building setback** from a **property line** shared with a **street** is 3.0 metres for 60 per cent of the length of the **building** façade that faces the **street**.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

- (c) for the floor closest to **grade**, the maximum **building setback** from a **property line** shared with Macleod Trail S is 6.0 metres for the entire length of the **building** façade.

**Building Separation**

- 12 (1) Where the widest dimension of a **balcony** faces a **property line** shared with another **parcel**, the minimum setback of a **balcony** from the shared **property line** is 4.0 metres
- (2) The façade of a **building** located above 26.0 metres from **grade** must provide a minimum horizontal separation of:
- (a) 24.0 metres from the façade of any other **building** on the same **parcel**; and
- (b) 12.0 metres from a **property line** shared with another **parcel** unless otherwise referenced in Sections 18, 21, and 24.
- (3) The façade of a **building** referenced in subsection (2) does not include **balconies**.

**Ground Floor Height**

- 13 (1) Unless otherwise referenced in subsection (2), the minimum height of the floor closest to **grade** of a **building** is 4.5 metres as measured vertically from the floor to the ceiling.
- (2) There is no minimum height for a **Dwelling Unit** that is located on the floor closest to **grade** in a **building**.

**Floor Plate Restrictions**

- 14 (1) Each floor of a **building** located partially or wholly above 26.0 metres above **grade** has a maximum **floor plate area** of:
- (a) 750 square metres of **gross floor area** for "Residential Uses"; and
- (b) 2,400 square metres of **gross floor area** for "Commercial Uses".
- (2) Where this Section refers to "Commercial Uses," it refers to the **uses** listed in Sections 5 and 6 of this Direct Control District other than "Residential Uses".
- (3) Where this Section refers to "Residential Uses," it refers to the **uses** listed in the Residential Group in Schedule A of Bylaw 1P2007.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Additional Motor Vehicle Parking Stall Requirements**

- 15 (1) **Motor vehicle parking stalls** and **bicycle parking stalls** required for **uses** in this Direct Control District may be shared and supplied on any one **parcel** or combination of **parcels** in this Direct Control District.
- (2) **A Utility Building – With Functions Below Grade:**
- (a) requires a minimum of 2.0 **motor vehicle parking stalls** per 100 square metres of **gross useable floor area**; and
- (b) does not require **bicycle parking stalls – class 1** or **bicycle parking stalls – class 2**.

**Development Authority – Power and Duties for Relaxations**

- 16 The **Development Authority** may relax the rules in Sections 10, 11, 12, 14, 18, 21 and 24 in this Direct Control District provided the test for relaxations as set out in Bylaw 1P2007 is met.

**Site 1 (0.73 ha/1.80 ac)**

**Application**

- 17 The provisions in sections 18 through 19 apply only to Site 1.

**Building Height and Streetwall Stepback**

- 18 (1) The minimum **building height** is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), the maximum **building height** is 26.0 metres.
- (3) Where a **parcel** shares a **property line** with a **street**, the maximum **building height** is:
- (a) 12.5 metres measured from **grade** within 5.0 metres of that shared **property line**; and
- (b) 26.0 metres measured from **grade** at a distance greater than 5.0 metres of that shared **property line**.
- (4) A **building** façade **adjacent** to a **street** must have a minimum streetwall stepback of 2.0 metres at a **building height** of 12.5 metres or greater.

**Floor Area Ratio**

- 19 The maximum **floor area ratio** is 4.5.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**Site 2 (1.25 ha/3.09 ac)**

**Application**

20 The provisions in sections 21 through 22 apply only to Site 2.

**Building Height and Street Wall Stepback**

- 21 (1) The minimum **building height** is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), the maximum **building height** is 26.0 metres.
- (3) Where a **parcel** shares a **property line** with a **street**, the maximum **building height** is:
- (a) 12.5 metres measured from **grade** within 5.0 metres of that shared **property line**; and
- (b) 26.0 metres measured from **grade** at a distance greater than 5.0 metres of that shared **property line**.
- (4) A **building** façade **adjacent** to a **street** must have a minimum streetwall stepback of 2.0 metres at a **building height** of 12.5 metres or greater.

**Floor Area Ratio**

22 The maximum **floor area ratio** is 3.5.

**Site 3 (1.81 ha/4.47 ac)**

**Application**

23 The provisions in sections 24 through 25 apply only to Site 3.

**Building Height and Street Wall Stepback**

- 24 (1) The minimum **building height** is 12.0 metres.
- (2) Unless otherwise referenced in subsection (3), the maximum **building height** is 46.0 metres.
- (3) Where a **parcel** shares a **property line** with a **street**, the maximum **building height** is:
- (a) 12.5 metres measured from **grade** within 5.0 metres of that shared **property line**; and

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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- (b) 46.0 metres measured from **grade** at a distance greater than 5.0 metres of that shared **property line**.
- (4) A **building** façade **adjacent** to a **street** must have a minimum streetwall stepback of 2.0 metres at a **building height** of 12.5 metres or greater.

**Floor Area Ratio**

**25** The maximum **floor area ratio** is 4.5.

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**APPENDIX III**

**COMMUNITY ASSOCIATION COMMENTS**

January 8, 2018

Angie Dean - File Manager

This email is in response to the request for comments that is dated December 21, 2017 for LOC2017-0273. I am speaking on behalf of the Southwood Community Association, and we will remain neutral with this application.

Below are concerns that I believe are imperative to the outcome of the development as a whole when relating to Southwood. I appreciate that these comments are not related to the land use applications, but they relate to the final paragraph of the applicant's submission in the application package.

The outcome of this project will have a major impact on Southwood, and whether it is a positive or negative outcome will depend on the connectivity this site will have on the existing community. Given that all of the development associated with these lands are located to the east of the LRT and CP rail lines, it is imperative that the pedestrian connection the City is planning to construct allows for safe and smooth movement between the two sites. This pedestrian connection will be the only connection the west part of Southwood will have, rather than using the McLeod Trail access points.

In discussions with engineers at previous board meetings, we were informed that the build life of the pedestrian bridge will be 75 years; however, the design that is being proposed will only support the current volume; it is not taking into account the population influx that could be felt in the coming 75 years. The MDP clearly outlines that the pedestrian realm is the most important aspect of transportation, followed closely by transit. If we are not provided an adequate pedestrian crossing to access the transit services then most of the people living on the west side of the rail lines in Southwood will continue to use their vehicles.

What I am requesting, and I hope the City can accommodate this request, is designing and constructing a pedestrian bridge where the width can accommodate three (3) persons using wheelchairs. The average width of a wheelchair is 32 inches, a width that allows a user to access any door width (that is built to code). I would request that the City considers a minimum of 3m wide bridge.

If the build life of the bridge is 75 years, but the design life of the it is only 20 years; why does the City not consider designing a bridge that could withstand its 75 years build life? If we are to come back to the pedestrian bridge in 20 years and determine that it does not support the volume, then it would most likely be demolished and rebuilt to accommodate the volume. The City has spent a lot of attention ensuring pedestrian bridges built in the core are able to accommodate greater volumes, so why can't this bridge? The final build out of Anderson could double the population of Southwood, and we could see greater increases in our population with other developments that could occur along McLeod. We

A. Dean

**LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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have to remember that people living on the east side of the rail line will want to use the services that are located on the west side, they same as the west side using the east side services. If this bridge does not reflect these movements, then it has already failed before it has been constructed.

I urge the City to think hard and long on the design of the pedestrian connection into the Anderson area from Southwood. We are excited to see what comes of the developments on this site, and many people living on the west side of the rail lines would like to access the site from the bridge rather than be forced to drive all the time.

Regards,  
Becky Poschmann  
Director of Development  
Southwood Community Association

LAND USE AMENDMENT  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

APPENDIX IV

OUTLINE PLAN





A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The instrument has a circular level and a vertical telescope. The surveyor is holding a pencil in their right hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 5.05**



**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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**EXECUTIVE SUMMARY**

This Outline Plan application complements the associated Land Use Amendment application to redevelop the lands surrounding the Anderson Light Rail Transit (LRT) Station with design guidelines to be applied at the development stage. The 8.06 hectare (19.91 acre) site, located northwest of the intersection of Macleod Trail S and Anderson Road SW in the Anderson Station Area Redevelopment Plan (ARP) area, currently contains 1,665 surface parking stalls and this proposal would enable new transit oriented mixed-use development on the site, in place of the surface parking lots.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION**

2018 February 22

That Calgary Planning Commission **APPROVE** the proposed Outline Plan.

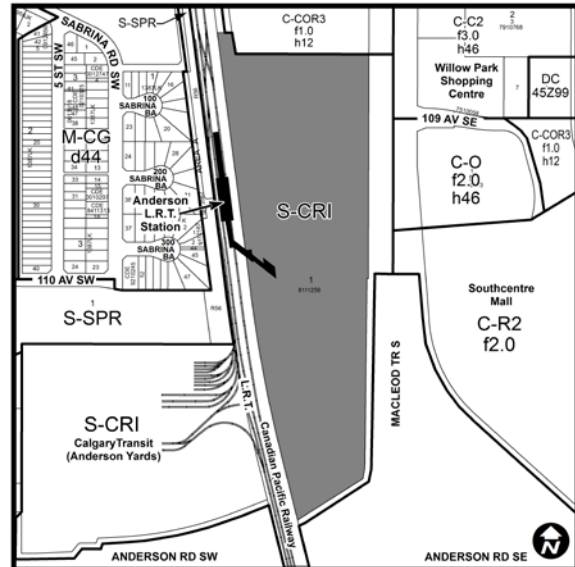
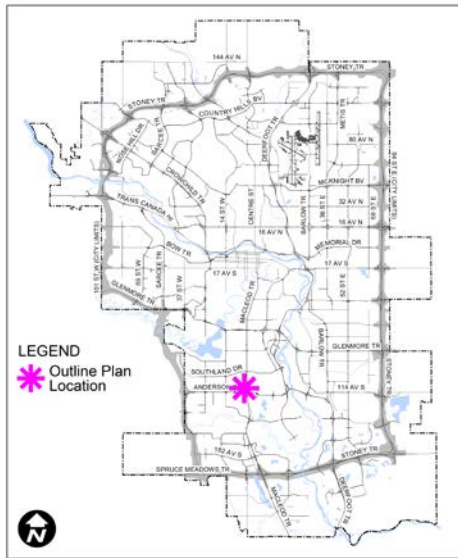
**REASON(S) FOR RECOMMENDATION:**

This Outline Plan applies the policies contained in the Anderson Station Area Redevelopment Plan (ARP) to enable the redevelopment of the Anderson LRT Station with a mixed-use, transit oriented redevelopment that is in line with the intensification targets and policies set out in the Calgary Transportation Plan (CTP) and the Municipal Development Plan (MDP). The proposal supports the replacement of surface parking lots with a structured parking facility, and will introduce amenities for all modes of travel to and from the site.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

LOCATION MAPS



OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Calgary Planning Commission **APPROVE** the proposed Outline Plan to subdivide 8.06 hectares  $\pm$  (19.91 acres  $\pm$ ), with conditions (APPENDIX I).

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**Applicant:**

NORR Architects Engineers Planners

**Landowner:**

The City of Calgary

**Address**

11015 Anderson Station WY SW

**Legal Description**

Plan 8111256, Block 1

## **PLANNING EVALUATION**

### **SITE CONTEXT**

This application is for the proposed redevelopment of the Anderson Station Light Rail Transit Park and Ride site. The area is currently one titled parcel that contains the LRT station itself, the bus drop-off areas and approximately 1,665 parking stalls. This application is intended to enable redevelopment of the site to a mixed-use, transit-oriented development area with a more activated, pedestrian-friendly urban village adjacent to a busy transit station.

This parcel lies within the primarily residential community of Southwood, with existing low density residential development located west of the parcel across the rail corridor, which has the potential to redevelop with densities up to 44 units per hectare. Adjacent to the south western boundary of the site is the Calgary Transit Anderson Yards, a large maintenance facility for buses and C-Trains. Access through this site to the maintenance facility will be eliminated in the future upon construction of a new all-turns intersection onto Anderson Road SW as part of the Anderson Station Mobility Hub Funding Model Report approved by Council 2017 February 13.

Directly east of the parcel, across Macleod Trail S, are major retail developments including Southcentre Shopping Centre and Willow Park Village, as well as the Southcentre Executive Tower office building. A large auto dealership is located directly adjacent to the northern boundary of the site, with office towers and a restaurant further north of the dealership.

### **LAND USE DISTRICTS**

The application envisions the creation of a vibrant, active, mixed-use redevelopment on the existing Park and Ride lot, and in order to achieve that vision, it proposes two central Special Purpose – School, Park and Recreation (S-SPR) parcels that are intended to become the central Public Plaza spaces, and six redevelopment sites under two DC Direct Control District land uses, based on the Mixed Use – General (MU-1) and Mixed Use – Active Frontage (MU-2) Districts.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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The proposed DC Districts are distinguished into two areas, with the Mixed Use – General (MU-1) based DC south of the public plaza, and the Mixed Use – Active Frontage (MU-2) based, more active development sites north of the public plaza. Both DC Districts set out specific rules related to setback widths, building height, separation and stepback provisions, façade rules, signage provisions and various site-specific rules. The DC Districts are intended to implement and complement the policies in the Anderson Station ARP and be used in conjunction with the Design Guidelines from the Outline Plan associated with this application.

**DENSITY**

This application proposes density targets measured as Floor Area Ratio (FAR) with both maximum and minimum building heights. The sites closest to the existing residential development in the Southwood community, the Mixed Use – Active Frontage (MU-2) based DC Sites 1 and 2, as well as Mixed Use – General (MU-1) based DC Site 1, have the lowest maximum building heights, with 26-metre maximums, in order to minimize the impact on the existing community. The Mixed Use – Active Frontage (MU-2) based DC sites propose an FAR of 4.5 on Site 1 which is intended to accommodate the future LRT parking structure, and 3.5 for Site 2 in order to accommodate a future potential mixed use building. The two Mixed Use – Active Frontage (MU-2) based DC development parcels along Macleod Trail S and identified as Site 3 have an FAR of 4.5 and maximum building heights of 46 metres, in order to accommodate mixed use development including a potential hotel.

In the southern portion of the plan area, the Mixed Use – General (MU-1) based DC Site 1, adjacent to the rail corridor and the Southwood community, proposes a maximum building height of 26 metres, which is consistent with the Mixed Use – Active Frontage (MU-2) based DC Sites 1 and 2 directly north, and a 3.0 FAR to accommodate a smaller development on this narrower parcel. The Mixed Use – General (MU-1) based DC Sites 2 and 3 have proposed building height maximums of 50 and 70 metres respectively, with FAR of 5.0 on both, given their adjacency to Macleod Trail S and their greater distance from low density residential. This is intended to allow flexibility for these sites to develop with larger towers without shadowing adjacent sites.

The Mixed Use – General (MU-1) and Mixed Use – Active Frontage (MU-2) base districts allow for a wide variety of residential and commercial uses in order to allow development to move with market demands as this site redevelops over a number of years. This plan anticipates an intensity at full build-out of 776 people and jobs per gross developable hectare, based on the development of 1,152 residential units mixed with commercial and office development.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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**LEGISLATION & POLICY**

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

The Municipal Development Plan identifies this area as a Major Activity Centre, setting out density targets of 200 people and jobs per gross developable hectare. This plan anticipates providing an intensity of 776 people and jobs per gross developable hectare, thus far exceeding the targets set out in the MDP.

Calgary Transportation Plan (CTP)

The Calgary Transportation Plan calls for “compact, mixed-use development and pedestrian-friendly designs... along the existing and future Primary Transit Network.” This plan includes many features intended to improve the pedestrian experience at this station, such as the parks plazas linking the station to the pedestrian bridge over Macleod Trail S, stepbacks on buildings to create more human-scale buildings, and activated street frontages. The proposal also includes a broad range of uses to provide a varied, mixed-use development within close proximity to the station, in line with the direction in the CTP.

Anderson Station Area Redevelopment Plan (ARP)

The Anderson Station Area Redevelopment Plan was approved by City Council at its 2017 June 12 Public Hearing, and identifies this site as the “Anderson Transit Village.” The policies in Section 2.2.1 Anderson Transit Village provide guidance for this redevelopment proposal and assign the Community – High Density building block (from the MDP’s Developed Areas Guidebook) to the eastern portion of the site adjacent to Macleod Trail S, calling for high density residential developments and a wide range and combination of uses. The proposed DC Districts would allow the site to develop in alignment with the intent of the Community – High Density building block.

The western portion of the plan area, adjacent to the rail corridor and the existing Southwood community, is assigned the Community – Centre building block, calling for high density residential uses, a concentration of non-residential uses including institutional, and a wide range and combination of uses. The proposed DC Districts would allow the site to develop in alignment with the intent of the Community – Centre building block, while maintaining respect for the existing community.

The ARP also contains policies in Section 3.3 guiding the development of the Anderson Transit Village Public Plaza, calling for the provision of the centrally located park/plaza and directing the design of such. This application proposes two centrally-located Municipal Reserve (MR) parcels that will form the Anderson Transit Village Public Plaza, thus satisfying this requirement, and meeting the intent of the ARP.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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This Outline Plan was evaluated considering all of the policies in the ARP and is considered to be in alignment with the vision and intent of the document for this major redevelopment site.

**SUBDIVISION DESIGN**

The Outline Plan proposes a simple street layout largely determined by pre-existing intersection connections to Macleod Trail S as well as access and movement patterns intended to optimize access for Calgary Transit and its patrons, and residents or users of the new developments. The intersection of Macleod Trail S and 109 Avenue SE will continue to serve as the main entry point into the development and therefore 109 Avenue SW has been extended straight into the site to connect with Southport Way SW, which will remain adjacent to the rail corridor along the western edge of the site. An additional east-west road connection is proposed adjacent to the future Public Plaza, and perpendicular to Southport Way, connecting to Macleod Trail with a right-in-only access into the site.

A new north-south connection is proposed as Anderson Station Way SW, which would be privately-owned, between the Mixed Use – Active Frontage (MU-2) based DC Sites 2 and 3 in order to provide the option to a future developer to construct a shared parkade beneath the road and serving both sites. This roadway is expected to be publicly accessible, and therefore this plan also contemplates the future extension of that alignment to the north of this site. A Public Access Easement will be registered over the parcels within this plan area and north of 109 Avenue SW, which will be retained under City ownership and will accommodate the future parking structure. This easement area would be a direct extension to the north of the proposed private road, in order to secure the potential for future access through this site to the north, in the event that the adjacent privately-owned car dealership should redevelop. This same alignment continues as public road dedication south of the public plaza in order to service the Mixed Use – General (MU-1) based DC parcels in the southern portion of the site, with the provision of an emergency access easement at the terminus of the road for access to the City of Calgary Transit Maintenance Facility in the future when the primary access to that facility is moved to the new intersection on Anderson Road SW.

**Urban Design**

Urban Design Guidelines have been included as part of this Outline Plan proposal in order to guide development of this important Transit Oriented Development site. The Design Guidelines are attached to this report in APPENDIX VII. These Design Guidelines establish a concept plan for the development of the site, and introduce built form and site design principles to be employed at the development permit stage for Active Frontages, Potential Active Frontages, Primary Frontages, and Supporting Frontages.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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The design guidelines focus particular attention on the active frontages proposed along proposed Anderson Station Way, which is intended to become a pedestrian-oriented “high street,” and DC (MU-2) Site 2, which will interface with both the internal (private) high street, and the existing Anderson LRT Station platform and Southport Way SW, where the potential for an urban plaza style pedestrian mews area would act as a central gathering space.

The Design Guidelines also include concept plans for the three potential open spaces in the plan area, which includes the two public plaza parcels that connect the LRT Station entrance with the pedestrian bridge over Macleod Trail S, as well as the potential third park space between DC (MU-1) Sites 2 and 3.

**Municipal Reserve**

The total MR owing for the Outline Plan area is 0.91 hectares (2.24 acres), which includes 10 percent of the 8.06 hectare (19.91 acre) Gross Developable area, as well as a 0.10 hectare (0.25 acre) Deferred Reserve Caveat that is currently on the land title, deferred onto this parcel from the Calgary Transit Anderson Yards. Two parks (MR-1 and MR-2), functioning as public plazas, are proposed at the center of the outline plan, which link the adjacent residential areas in Southwood with the C-Train Station and commercial uses on the east side of Macleod Trail S. The total area of MR1 and MR2 is 0.41 hectares (1.0 acres). The balance of reserve owing in the amount of 0.51 hectares (1.25 acres) will be deferred by caveat on the titles of the remnant parcel for the station head and development lands south of the proposed MR lands (S-CRI, DC (MU-1) Sites 2 and 3), pending future subdivision of these lands.

A decision rendered by the Joint Use Coordinating Committee (JUCC) states that the Municipal Reserve (MR) dedication for the Mixed Use – General (MU-1) based DC Sites 2 and 3 will be determined at a later stage when the proposed uses on those sites is known. If the proposed development on those sites creates sufficient demand for an additional public park, then the 0.25 hectare (0.62 acre) area shown on the Outline Plan crosshatched with ‘+’ will be dedicated as an MR parcel. If there is not sufficient demand for park land, then the JUCC has determined that cash-in-lieu of dedication would be collected at that time.

**TRANSPORTATION NETWORKS**

The Plan sets out a mobility network for the Plan area which provides for walking, cycling, public transportation as well as private vehicles. Although multiple modes of transportation are accommodated in the Plan area, creating a walkable community with a vibrant street life is a key principle of this Plan.

A Transportation Impact Assessment (TIA) was undertaken to assess the impact redevelopment of the Plan area would have on the transportation network. Upgrades will be required to the transportation network in accordance with the conclusions of the TIA completed in support of this application and the proposed Anderson ARP. Upgrades to area intersections to support Transit movements are planned in the short term.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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A regional pathway is proposed along Macleod Trail connecting the Plan area to the north and a proposed active modes bridge over the train tracks will provide connection to the west.

Parking policies and supply ratios have been established in the Anderson ARP to support the TOD intent of the site and to mitigate vehicular impacts on the network. The parking relaxations are supported with adequate walking and cycling facilities, as well as primary transit service.

**UTILITIES & SERVICING**

Services are currently available on the subject site and can be upgraded to accommodate development on the site. Utility placement within the proposed roads can accommodate all utilities as well as the inclusion of District Energy.

**ENVIRONMENTAL ISSUES**

Due to concerns related to the car dealership (north), and Anderson Transit Garage, west of the CPR tracks, an updated Phase II Environmental Site Assessment report will be required at the Development Permit stage.

**ENVIRONMENTAL SUSTAINABILITY**

Utility line assignments have been placed in order to include the potential for District Energy to serve this redevelopment site, providing more environmentally sustainable energy source for new buildings. Additionally, City of Calgary facilities are expected to achieve a Leadership in Energy and Environmental Design (LEED) gold standard, and so the development of a new parking structure to serve the Park n Ride component of the project can be expected to include more environmentally sustainable features.

**PUBLIC ENGAGEMENT**

**Community Association Comments**

This application was circulated to the Federation of Calgary Communities, and the Southwood and Willow Ridge Community Associations. Only the Southwood Community Association submitted comments on this application, choosing to remain neutral on the application itself, but offering comments related to the future development of the site and its potential impact on the community. The letter also notes the importance of good pedestrian connections and bridges into Southwood, which will be considered with the Anderson Station Mobility Hub Funding Model. The comments received are included in APPENDIX VI.

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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**Citizen Comments**

Notification letters were sent to adjacent property owners and no objections were received. One phone inquiry from an area resident was received and no concerns were raised.

**Public Meetings**

Public meetings were held in conjunction with the recently-approved Area Redevelopment Plan, therefore no further public engagement events were hosted in conjunction with this Outline Plan application.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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APPENDIX I

CONDITIONS OF APPROVAL

**Planning:**

1. **Prior to affected Tentative Plan approval** (and submission of construction drawings), any new proposed community and/or street names must be submitted to the Subdivision Authority for processing and Calgary Planning Commission/Council approval.
2. At the Development Permit stage, all development must conform to all requirements of the City with respect to proximity to the freight rail corridor.
3. At the Development Permit stage, a risk assessment by a qualified professional may be required to support proposed development adjacent to the freight rail corridor.
4. At the Development Permit stage, a noise study by a qualified professional is required if residential, hotel and/or sensitive uses are proposed within 30 metres of the freight rail corridor property line.
5. At the time of construction, the developer must install and maintain a fence with a minimum height of 1.8 metres along the mutual property line with the freight rail corridor.
6. At the development stage, buildings should be designed to strategically locate servicing and loading functions, access to parkades, and building mechanical systems (such as ventilation screens) in order to minimize the impact on the streetscape.
7. At the development stage, buildings should be designed and constructed with attention to detail and a similar quality of finishing materials on all street frontages.
8. **Concurrent with registration of the applicable tentative plan**, the developer shall register a Deferred Reserve Caveat in the amount of 1.25 acres on the titles of all the parcels within Phase 2, including the remnant parcel for the station head and development lands south of the proposed MR2 lands (S-CRI, DC (MU-1) Sites 2 and 3). The deferred reserve must be collected through the first subdivision application for phase 2. Note: The station head parcel (S-CRI) must be included in Phase 2 as required by JUCC.

**Development Engineering:**

9. **Prior to endorsement of any Tentative Plan**, an update to the Sanitary Servicing Study for Anderson Station TOD that was submitted by ISL in 2014 is required if the proposed density has increased significantly.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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10. **Prior to endorsement of any Tentative Plan**, an update to the Staged Master Drainage Plan for Anderson Station TOD that was submitted by ISL in 2014 is required if the drainage concept has changed.
  11. **Prior to endorsement of any Tentative Plan**, provide the onsite water network design and offsite tie-ins for review by Water Resources. If you wish to discuss further with Water Resources please contact 403-268-5697 to arrange a meeting.
  12. **Prior to approval of stripping and grading and/or the affected tentative plan**, whichever comes first, the applicant must provide confirmation that the recommendations outlined in the report titled "Phase I Environmental Site Assessment, 11015 Anderson Station Way SW Plan 8111256; Block 1 Calgary, AB (dated March 2015) by Envirotech engineering have been undertaken. All reports will be reviewed to the satisfaction of The City of Calgary (Environmental and Safety Management).
  13. **Prior to Stripping and Rough Grading approval**, an erosion and sediment control plan for the site is to be submitted to Water Resources for review and approval.
  14. **Prior to endorsement of any Tentative Plan/prior to release of a Development Permit**, execute a Development Agreement. Contact the Public Infrastructure Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email [urban@calgary.ca](mailto:urban@calgary.ca).
  15. Make repayment arrangements with the City of Calgary for part cost of the existing surface improvements constructed on the west side of Macleod Trail S, along the east boundary of the development area
  16. Development charges and fees are applicable. Contact the Public Infrastructure Coordinator, Calgary Approvals Coordination for further information at 403-268-6739 or email [urban@calgary.ca](mailto:urban@calgary.ca).
  17. The developer, at its expense, but subject to normal oversize, endeavours to assist and boundary cost recoveries shall be required to enter into an agreement to:
    - a. Install the offsite sanitary sewers, storm sewers and water mains and construct the offsite temporary and permanent roads required to service the plan area. The developer will be required to obtain all rights, permissions, easements or rights-of-way that may be required to facilitate these offsite improvements.
    - b. Construct the underground utilities and surface improvements within the boundaries of the plan area.
    - c. Construct the MSR/MR within the plan area.
    - d. Construct the regional pathway within and along the boundaries of the plan area, to the satisfaction of the Director of Parks Development.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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**Transportation:**

18. **Prior to endorsement of a Tentative Plan**, finalize all modified street cross section to the satisfaction of the Director, Transportation. The proposed modified street cross sections has been approved in principle by Transportation Planning. However, width (Right-of-way), form and function may vary slightly as a result of changes that may be needed to accommodate deep/shallow utilities. Transportation Planning will work with the applicant during tentative and construction drawings stage to finalize the roadway cross section.
19. **Prior to the Endorsement of the applicable Tentative Plan**, the developer shall provide a Letter of credit for pedestrian-activated crossing signals at the corner of Southport Way near the station head, location to be confirmed at construction drawings. This is due to anticipated desire line for pedestrians to and from the LRT station head and the proposed development. Note that the developer shall also provide a letter, under Corporate Seal, indicating that they are responsible for any additional costs of signalization that could be in excess of the amount identified in the Letter of Credit.
20. All proposed upgrades to Macleod Trail intersections and roadways within the outline plan area/boundary are the responsibility of developer. Intersection improvements shall be determined at each subdivision or development permit.
21. Development will be subject to the terms and conditions of a Standard Development Agreement. Contact the Subdivision Development Coordinator, Planning and Development for further information.
22. Access to Anderson Station Way and the proposed Park n' Ride parkade shall be controlled via an operable gate in lieu of a median at the intersection. The gate shall be designed at construction drawing stage and included with the applicable tentative plan. The gates shall be operated by Roads Traffic department.
23. The Outline Plan is subject to the proposed phasing as approved by Council in TT2016-0719. At the effected Tentative Plan and/or Development Permit application, the applicant must demonstrate completion of Park and Ride facilities to replace the displaced parking in the Plan area.
24. **Prior to endorsement of the first Tentative Plan**, the design of the transit queue jumps at Willow Park Drive / Macleod Trail shall be approved by the director of Roads and the applicant shall enter into a cost sharing agreement with Roads for the design and construction of the improvement.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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25. **In conjunction with each Tentative Plan**, functional-level plans shall be submitted as a component of the Tentative Plan submission package to the satisfaction of Transportation Planning and Roads, for the staged development arterial and collector standard roadways, inclusive of the staged development of the at-grade intersections, and to the satisfaction of the Director, Transportation Planning and Roads. Additional road Right-of-way may be required to accomodate transitions and local widenings at intersections.
  26. **Prior to endorsement of a Tentative Plan**, two access points (defined as intersections or roundabouts that provide direct access into or out of an area for vehicular traffic) are to be provided to an “available” regional transportation network infrastructure for any new residential, commercial or industrial areas. “Available” is defined as follows:
    - The ability to construct or contribute towards construction of a regional transportation network infrastructure required to provide connection to the Tentative Plan; and
    - The ability to construct or contribute towards construction of a pedestrian/active modes system to service the Tentative Plan.
  27. **In conjunction with each Tentative Plan**, Transit service shall be provided to the satisfaction of the Director, Transit and the Director, Transportation Planning
  28. In conjunction with the construction of the road network, transit stops shall be provided to the satisfaction of the Director, Transportation Planning.
  29. The developer shall provide, at their cost, a standard/architecturally compatible transit patron waiting amenity at the bus zone, as identified by Calgary Transit and Transportation Development Services.
  30. Accesses for all multi-family sites shall be designed and located to the satisfaction of the Director, Transportation Planning.
  31. All crosswalks where Regional Pathways or Multi-use pathways intersect with the street shall be designed to the satisfaction of the Director, Transportation Planning. In general terms, at-grade mid-block crossings are not permitted, but may be considered for site specific conditions, at the discretion of Transportation Development Services and Roads.
  32. Prior to approval of the related Development Permit, a noise analysis is to be submitted to and approved by Transportation Planning for the residential development adjacent to Macleod Trail and Anderson Road.

Note: All noise attenuation features (noise walls, berms, visual screening etc.) and ancillary facilities required in support of the development shall be constructed entirely within the development boundary (location of noise walls, berms, etc and associated ancillary works shall not infringe onto the road right-of-ways).

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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33. **Prior to approval of any development**, the developer shall provide a statement on the condition of those roads and their ability to handle the construction and development traffic that will be generated by the development. Confirm maintenance requirements with the Calgary Roads Maintenance Section. Where development will cause excess wear and tear on adjacent rural or existing roads directly attributable from the subject site for construction traffic, the developer will be responsible for the additional maintenance and/or upgrade of the roads, or pay to the City the costs caused by excess wear and tear. The developer shall enter into a Construction Access Roads Agreement with Roads Maintenance provided that the proposed access roads are either unimproved or subject to a load ban. Contact the Planning and Infrastructure Engineer, Roads at 403-268-1033.
  34. The developer, at its expense, shall be required to relocate any affected utilities.
  35. All roads and intersections shall be designed to Calgary Complete Street Guide standards and constructed at the expense of the developer and to the satisfaction of Directors, Roads and Transportation Planning.
  36. Any proposed community entrance features shall be located on private sites, not within public land, rights-of-way, or roadways.
  37. No direct vehicular access or front driveways shall be permitted to or Macleod Trail and Anderson Road, and a restrictive covenant shall be registered **concurrent with the registration of the final instrument** to that effect at the Tentative Plan stage.

**Parks:**

38. The developer shall submit for review and execution a Landscape Maintenance Agreement or Optional Amenities Agreement for landscaping and park design elements that are beyond the minimum requirements of the Parks Landscape Construction Specifications. Contact the Parks Landscape Construction Coordinator at 403-268-1334 for details.
39. **Prior to the approval of the affected tentative plan**, finalized concept plans for all Municipal Reserve sites and Regional Pathway shall be submitted for Parks' review and approval. Parks may require changes to the concept plans that are approved on the outline plan, based on further detailed information provided at the tentative plan stage or when the landscape construction drawings are under review.
40. Construct all regional pathway routes within and along the boundaries of the plan area according to Parks' **Development Guidelines and Standard Specifications – Landscape Construction** (current version), including setback requirements, to the satisfaction of the Director, Parks.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

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41. Plant all public trees in compliance with the approved Landscape Construction Drawing for Boulevard and Median Tree Line Assignment.
42. The developer is responsible for constructing all Municipal Reserve parcels within the boundaries of the plan area according to the approved concept plans and built in accordance with the Parks' ***Development Guidelines and Standard Specifications – Landscape Construction*** (current version).
43. Rehabilitate all portions of the MR lands and the regional pathways along and/or within the boundaries of the plan area that are damaged as a result of this development, all to the satisfaction of the Director, Parks.

OUTLINE PLAN  
 SOUTHWOOD (WARD 11)  
 MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

**APPENDIX II**  
**SUBDIVISION DATA SHEET**

	HECTARES	ACRES
GROSS AREA OF PLAN	8.06	19.91

LAND USE (Mixed-Use)	HECTARES	ACRES	# OF UNITS (FOR MULTI RESIDENTIAL)
DC (MU-1)	3.12	7.71	
DC (MU-2)	3.79	9.36	
Total Mixed-Use	6.91	17.07	1,152

	HECTARES	ACRES	% OF NET AREA
ROADS (CREDIT)	1.87	4.62	23.2
ROADS (PRIVATE)	0.28	0.70	3.5

RESERVES	HECTARES	ACRES	% OF NET AREA
MR	0.41	1.00	5.0
DEFERRED RESERVE CAVEAT	0.51	1.25	6.3

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**APPENDIX III**

**APPLICANT'S SUBMISSION**

Real Estate and Development Services (REDS) has submitted a Land Use Redesignation and Outline Plan application to facilitate the redevelopment of City owned lands at Anderson Station into a Transit Oriented Development precinct. The proposed development comprises 8.0 ha (19.9 ac) in the community of Southwood and falls entirely within a 400 metre radius of the Anderson LRT Station. The site includes the LRT station, bus terminal and park and ride area which currently provides 1,665 surface parking stalls.

A Land Use and Outline Application was submitted in 2014 (LOC2014-0143). The Outline Plan was approved by Calgary Planning Commission in August 2015; however, Council referred the Land Use Amendment back to Administration in October 2015 for further refinement and alignment with other ongoing streams of work at the time including the Transit Parking Strategy, TOD Development Funding and the completion of the Anderson Station Area Redevelopment Plan (ARP). Since then, a significant amount of work has been conducted on a regional park and ride strategy and on finalizing the ARP. The Anderson Station Park and Ride Plan was approved in 2016, and the ARP was approved by Council in June 2017. With the completion of this work, a new Outline Plan and Land Use Amendment has been prepared by REDS that is consistent with these plans as well as the Municipal Development Plan which identifies the area as a Major Activity Centre.

The current application continues to facilitate a compact, pedestrian oriented development that contains a mix of uses and a high quality public realm while maintaining a minimum of 500 park and ride stalls within a new parkade. Redevelopment aims to transform an underutilized asset and optimize existing infrastructure by introducing higher densities and transit supportive land uses into an existing community adjacent to a major transit hub. The mix of uses, density, building heights and non-standard street sections are consistent with the previous application (LOC20014-0143). Changes to the site design have been made to improve bus circulation and pedestrian mobility throughout the site.

The proposed land use districts are Direct Control (DC) Districts based on the Mixed Use One (MU-1) and Mixed Use Two (MU-2) Districts, and Special Purpose – School, Park and Community Reserve (S-SPR) District. DC Districts are required for several reasons namely to enable maximum building heights in excess of 10 storeys, allow district energy in structures below grade, and to facilitate shared parking between parcels. The S-SPR district will accommodate a public plaza/park area situated at the entrance of the station which will serve as the main gateway to the site and central public gathering space. The park spaces will also accommodate an east-west regional pathway that will connect Southwood to the station area and to other commercial, retail and recreational opportunities across Macleod Trail.

To conclude, the application implements the vision of approved land use planning policy and provides a solid Park and Ride Plan that ensures minimal net loss of parking stalls corresponding to the phased redevelopment of the Anderson Station lands. The proposal will enable a greater range of housing choice and amenities in an established area at densities that support transit use. It also optimizes existing infrastructure, enhances pedestrian and cyclist connectivity and increases people's mobility choices while creating a new compact urban form with a distinct sense of place in south Calgary. For these reasons, we request the support of Calgary Planning Commission and City Council in this application.

OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

APPENDIX IV

OUTLINE PLAN

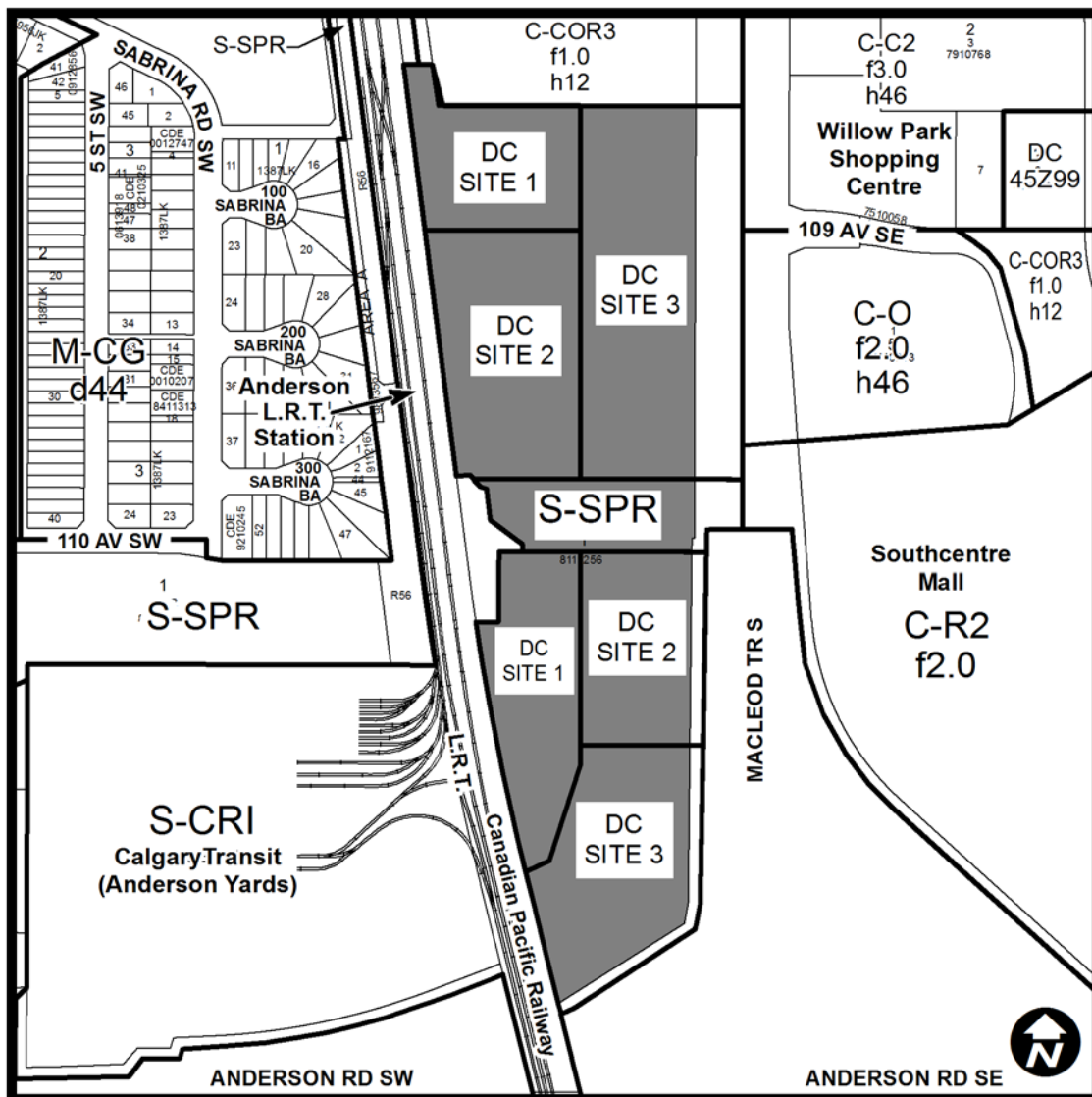


OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW

MAP 16S

APPENDIX V

PROPOSED LAND USE DISTRICT MAP



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**APPENDIX VI**

**COMMUNITY ASSOCIATION COMMENTS**

January 8, 2018

Angie Dean - File Manager

This email is in response to the request for comments that is dated December 21, 2017 for LOC2017-0273. I am speaking on behalf of the Southwood Community Association, and we will remain neutral with this application.

Below are concerns that I believe are imperative to the outcome of the development as a whole when relating to Southwood. I appreciate that these comments are not related to the land use applications, but they relate to the final paragraph of the applicant's submission in the application package.

The outcome of this project will have a major impact on Southwood, and whether it is a positive or negative outcome will depend on the connectivity this site will have on the existing community. Given that all of the development associated with these lands are located to the east of the LRT and CP rail lines, it is imperative that the pedestrian connection the City is planning to construct allows for safe and smooth movement between the two sites. This pedestrian connection will be the only connection the west part of Southwood will have, rather than using the McLeod Trail access points.

In discussions with engineers at previous board meetings, we were informed that the build life of the pedestrian bridge will be 75 years; however, the design that is being proposed will only support the current volume; it is not taking into account the population influx that could be felt in the coming 75 years. The MDP clearly outlines that the pedestrian realm is the most important aspect of transportation, followed closely by transit. If we are not provided an adequate pedestrian crossing to access the transit services then most of the people living on the west side of the rail lines in Southwood will continue to use their vehicles.

What I am requesting, and I hope the City can accommodate this request, is designing and constructing a pedestrian bridge where the width can accommodate three (3) persons using wheelchairs. The average width of a wheelchair is 32 inches, a width that allows a user to access any door width (that is built to code). I would request that the City considers a minimum of 3m wide bridge.

If the build life of the bridge is 75 years, but the design life of the it is only 20 years; why does the City not consider designing a bridge that could withstand its 75 years build life? If we are to come back to the pedestrian bridge in 20 years and determine that it does not support the volume, then it would most likely be demolished and rebuilt to accommodate the volume. The City has spent a lot of attention ensuring pedestrian bridges built in the core are able to accommodate greater volumes, so why can't this bridge? The final build out of Anderson could double the population of Southwood, and we could see greater increases in our population with other developments that could occur along McLeod. We

A. Dean

**OUTLINE PLAN  
SOUTHWOOD (WARD 11)  
MACLEOD TRAIL S AND ANDERSON ROAD SW**

**MAP 16S**

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have to remember that people living on the east side of the rail line will want to use the services that are located on the west side, they same as the west side using the east side services. If this bridge does not reflect these movements, then it has already failed before it has been constructed.

I urge the City to think hard and long on the design of the pedestrian connection into the Anderson area from Southwood. We are excited to see what comes of the developments on this site, and many people living on the west side of the rail lines would like to access the site from the bridge rather than be forced to drive all the time.

Regards,  
Becky Poschmann  
Director of Development  
Southwood Community Association

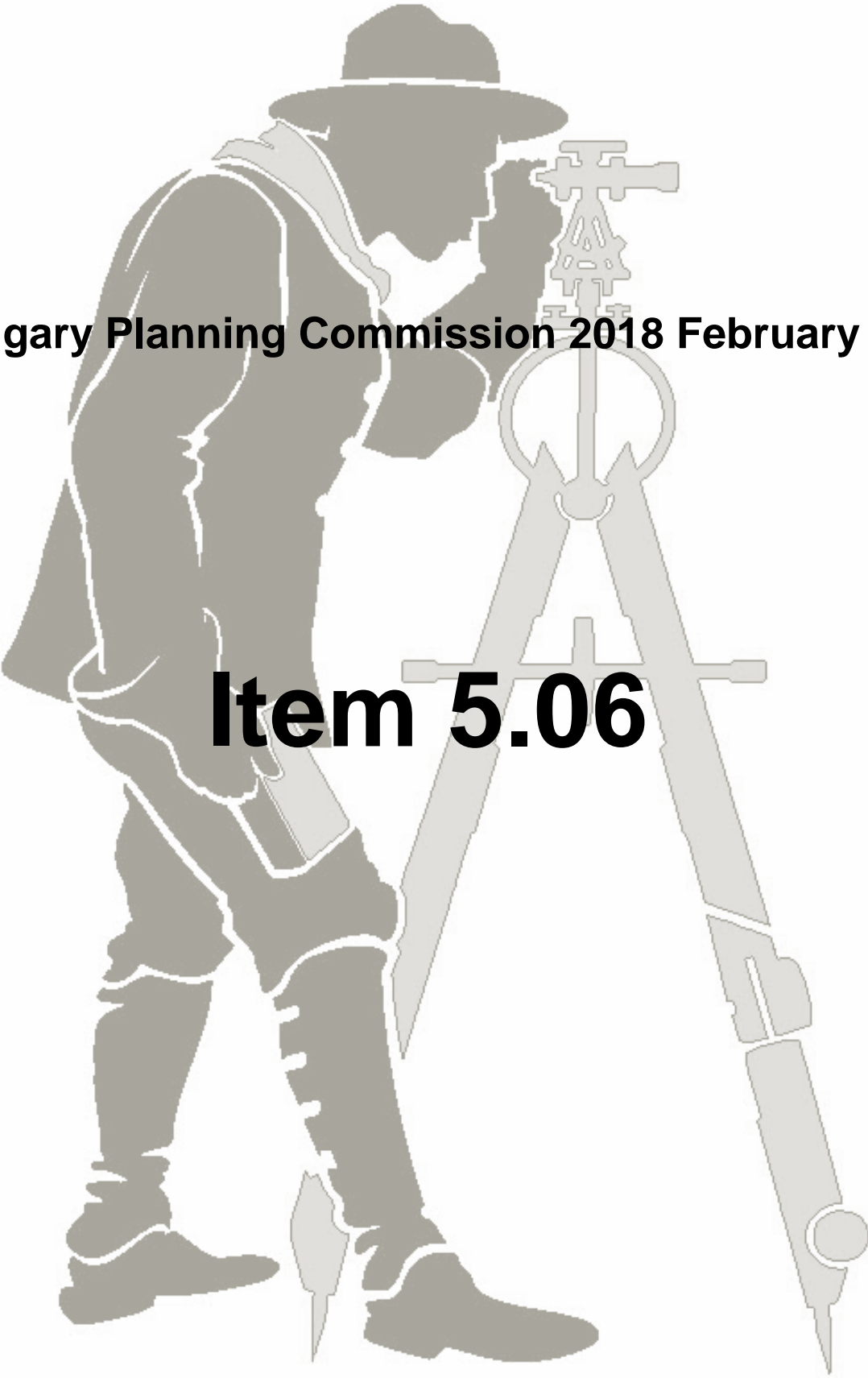
APPENDIX VII

ANDERSON STATION DESIGN GUIDELINES



**Calgary Planning Commission 2018 February 22**

**Item 5.06**





**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

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**EXECUTIVE SUMMARY**

This development permit application proposes mixed use development in the community of the Beltline. The proposal includes the restoration of an existing office building, demolition and reconstruction of a three-storey retail annex, and the construction of two new residential towers providing 548 dwelling units and approximately 1,800 square metres (20,000 square feet) of new retail and commercial space along the 4 Street SW corridor.

The application has been considered under the provisions of the Land Use Bylaw 1P2007 (LUB) and the Municipal Development Plan (MDP). A land use amendment and policy amendment to accommodate mixed use development with heritage density transfer was presented to Calgary Planning Commission on 2017 December 14 and is scheduled to go before Council on 2018 February 20.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission **APPROVE** the Development Permit application.

**REASON(S) FOR RECOMMENDATION:**

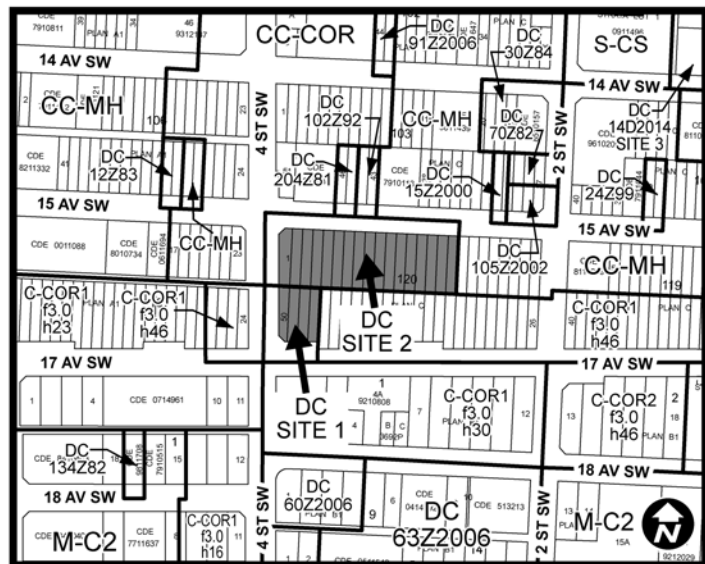
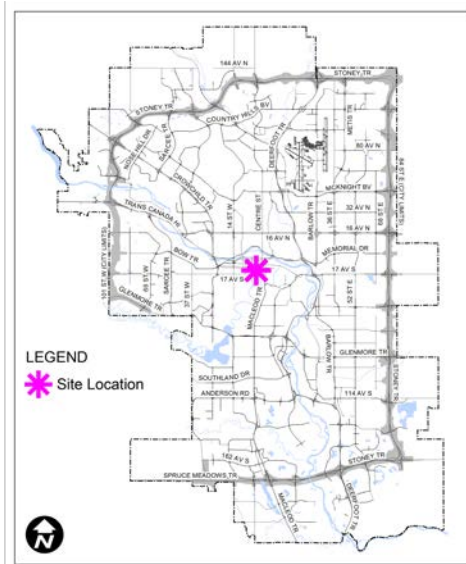
Administration recommends approval of this application due to its positive impact on the continued retention of two municipally-designated heritage resources and the benefits of high density, mixed-use redevelopment at a strategic gateway to Calgary's Centre City. The subject site is at the junction of two of the city's most dynamic Main Streets, and the proposal has significant urban design merit in terms of both built form and public realm. Furthermore, the proposed development aligns with relevant policy from the Centre City Guidebook, the Centre City Plan, and the Beltline Area Redevelopment Plan (ARP).

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

MAP 15C

**LOCATION MAPS**

Note: reference to DC sites 1 and 2 relates to proposed guidelines in LOC2017-0175 (APPENDIX VI).



DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
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BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

1. The Calgary Planning Commission **RECEIVE AND ACCEPT** this report and appendices for information.
2. Recommend the Development Authority, without having to return to Calgary Planning Commission, **APPROVE** the Development Permit with conditions (APPENDIX III), subject to the approval by Council of the land use bylaw amendment associated with LOC2017-0175.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
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BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

**Applicant:**

Stantec Architecture

**Landowner:**

Grosvenor Canada Limited

**Address**

1520 - 4 Street SW and  
319 - 15 Avenue SW

**Legal Description**

Plan C, Block 120, Lots 1 to 17 and  
47 to 50

**PLANNING EVALUATION**

**SITE CONTEXT**

**Surroundings**

The proposed development site includes an existing office building at the northeast corner of 17 Avenue at 4 Street SW and a low-rise commercial building and surface parking lot along 15 Avenue east of 4 Street SW. These sites are currently addressed as 1520 - 4 Street SW and 319 - 15 Avenue SW. Density will be transferred to the latter site to accommodate new commercial and residential (townhouse and apartment) development.

To the east is an eight-storey apartment building (Randal House) dating to 1973, designated Centre City Multi-Residential High Rise (CC-MH) District.

To the west, across 4 Street SW, are a single storey commercial building with multiple retail and restaurant spaces, and another single storey commercial building containing the Rose and Crown pub, designated Commercial – Corridor 1 (C-COR1f3h46) District and Centre City Commercial Corridor (CC-COR) District, respectively.

To the north, across 15 Avenue SW, are a mix of apartment and single-detached homes, designated Centre City Commercial Corridor (CC-COR) District, DC Direct Control (to allow for commercial uses in the homes), and Centre City Multi-Residential High Rise (CC-MH) District.

To the south are three single detached dwellings converted to retail/restaurant use and a low-rise office building, designated Commercial – Corridor 1 (C-COR1f3h46) District.

**Density**

The density of the proposed development comes to 9.0 FAR, including the transfer of 2.48 FAR (13,642.5 square metres) of heritage density, 0.43 FAR (2,349.9 square metres) bonus for the provision of publicly-accessible private open space, and 1.09 FAR (6,000.0 square metres) bonus for a contribution to the Beltline Community Investment Fund.

The proposed development includes a total of 59,100 square metres of floor area (residential and commercial) and 548 dwelling units.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

Site Activity and Recent History

Both 17 Avenue and 4 Street SW are identified as Main Streets in the Municipal Development Plan (MDP) and as key corridors in the Centre City Plan and Beltline Area Redevelopment Plan (ARP). The Beltline has recently seen high-density mixed-use development near to this site, focused primarily along 1 Street SW. A single-storey commercial building was constructed within the last three years at the northwest corner of 4 Street and 17 Avenue SW.

**LAND USE DISTRICTS**

The proposed site is subject to a DC Direct Control District (LOC2017-0175, guidelines in APPENDIX VI) which enables the transfer of heritage density from two source sites. The two source sites, which are municipally designated heritage resources, include the Barnhart Apartments at 1121 - 6 Street SW (proposed DC Direct Control site 3) and the Findlay Apartments at 1324 - 1 Street SW (proposed DC Direct Control site 4). Density will be decreased from 5.0 FAR to 1.4 and 1.5 FAR respectively.

In addition to the density transfer, the Direct Control District guidelines also allow for a number of additional restaurant and entertainment uses on portions of the site fronting 4 Street and 17 Avenue SW. The southern portion of the site (the existing office tower) is covered by DC site 1, which is based on the Centre City Mixed-Use District (CC-X), while the northern portion of the site is covered by DC site 2, which is based on the Centre City Multi-Residential High Rise Support Commercial District (CC-MHX).

The Direct Control District guidelines are included in APPENDIX VI.

**LEGISLATION & POLICY**

South Saskatchewan Regional Plan (SSRP)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

Municipal Development Plan (MDP)

This application aligns with the Municipal Development Plan with respect to development in the Centre City, site and building design (section 2.4.2), and Centre City intensification (3.2.1). In particular, the proposal includes a memorable pedestrian passage linking two Main Streets, complies with policy around the location, siting, and design of tall buildings, and provides an enhanced public realm through both building design and streetscape improvements. Through its location, intensity, and mix of uses, the proposal contributes to further reinforcement of the Centre City as the focus of business, employment, cultural, recreation, retail, and high-density housing within Calgary.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

Centre City Guidebook

This application aligns with the policies set out within the Mixed-Use High Density building block in the Centre City Guidebook, as well as the policies relating to urban design, open space, mobility, and infrastructure and environment. In particular, the proposal meets the policies set out in s3.1.1 which seek to ensure that high-density mixed-use development has a positive impact on its immediate surroundings and the Centre City more broadly. The proposal aligns with the urban design policies in s4.1 relating to building setbacks and grade separation, site access and loading, building massing, street wall, and active frontages.

Centre City Plan

This application aligns with the expectations and concept set out in the Urban Mixed-Use typology of the Centre City Plan and the policies for the Victoria Crossing Centre neighbourhood set out in section 4.2.6. In particular, the proposal supports and improved public realm at the junction of 17 Avenue and 4 Street SW. The proposal also aligns with policies relating to built form and massing (s7.7.3), as well as architectural guidance (s7.8) relating to large sites and tall buildings.

Beltline Area Redevelopment Plan

With respect to the Beltline Area Redevelopment Plan, the application supports the plan's heritage objectives and aligns with the Urban Mixed-Use district. The proposal includes boulevard trees along all three street and avenue frontages, adding to the city's tree canopy and contributing to the streetscape of two key Main Streets. Shadow studies indicate that the proposal complies with the 20.0 metre shadow setback area for Haultain Park as set out in the Beltline ARP.

Amendments to the Beltline Area Redevelopment Plan (through LOC2017-0175) were required to:

- adjust the boundaries of the Urban Mixed-Use district to align with the site context (Map 4: Density Areas) in order to enable comprehensive redevelopment of a larger site with a form that better supports the vision of a comfortable, dense, mixed-use Main Street as set out in the ARP and the Centre City Plan; and
- to encourage adjacent development to connect to a proposed pedestrian connector (a physical link between and within urban blocks that is called for in the Beltline Area Redevelopment Plan) linking 17 Avenue with 4 Street SW through the receiver sites (Map 3: Land Use Concept).

These amendments are included in APPENDIX VII.

Land Use Bylaw 1P2007

The proposed development generally complies with the Land Use Bylaw. Although there are a number of relaxations, they are minor in nature, reflect constraints stemming from inclusion of the existing office tower, and, in administration's view, reflect a high quality outcome in urban design terms. These Bylaw discrepancies are identified in the table below and are supported by Administration as the development still meets the intent of the rules, and relaxation of the rules has no adverse impact on adjacent developments.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

MAP 15C

Bylaw Relaxations		
Regulation	Standard	Provided
247 Outdoor Café	(f) has a maximum area of 25.0 square metres in the C-N1, C-N2, C-C1, C-C2, C-COR1, C-COR2, C-O, C-R1, I-B, CC-MHX, CC-X, CC-COR, CC-ER, CC-ERR, CC-EMU, CC-EIR, CC-EPR, CC-ET, MU-1, MU-2, M-H1, M-H2, M-H3, M-X1 and M-X2 districts.	Plans indicate two outdoor patios greater than 25.0m <sup>2</sup> : Office Tower –Unit 1: 53.85m <sup>2</sup> (+28.85m <sup>2</sup> ); and Office Tower – Rooftop: 207.24m <sup>2</sup> (+182.24m <sup>2</sup> ).  <b>Relaxations granted: Administration is supportive of increased patio sizes in relation to the specific location of the project and the context of 17 Avenue and 4 Street.</b>
1138 Building Setbacks	(1) The building setback from a property line shared with all other streets is a minimum of 3.0 metres and a maximum of 6.0 metres	Plans indicate the following non-compliant building setbacks:  North (min) – 0.28m (-2.72m); North (max) – 9.03m (+3.03m); West (min) – 0.50m <b>beyond</b> the property line; South (min) – 0.63m (-2.38m).  <b>Relaxations granted (north and south): Administration is supportive of variation and increases to the setback areas which contribute to a positive public realm and acknowledges the constraints of an existing building.</b>  NOTE: An encroachment agreement will be required at time of construction.
1099 Projections Into Setback Areas (max.)	(5) Eaves may project a maximum of 0.6m, and window wells may project a maximum of 0.8m, into any setback area.	Plans indicate the following non-compliant eaves/awnings:  - West (rooftop eave) – 1.51m beyond the property line(at the furthest point); - West (main entry canopy) – 3.36m beyond the property line (at the furthest point); - West (unit canopies) – 1.84m beyond the property line (at the furthest point); and - South (seasonal awning) – 2.37m beyond the property line (at the furthest point).  <b>Relaxations granted: Administration considers the encroachment of canopies beyond the bylaw requirement to be beneficial to sidewalk users.</b>

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

1139 Floor Plate Restrictions	Each floor located partially or wholly above 25.0 metres above grade has a maximum: (a) floor plate area of 650.0 square metres; and	Plans indicate the following non-compliant floor plates: Office Tower – 951.24m <sup>2</sup> (+301.24m <sup>2</sup> ) and West Tower – 655.91m <sup>2</sup> (+5.91m <sup>2</sup> ).  <b>Relaxations granted: Administration is aware that the office tower deficiency reflects an existing building while the west tower deficiency is minor in nature and continues to meet the intent of the floor plate area maximum.</b>
	Each floor located partially or wholly above 25.0 metres above grade has a maximum: (b) horizontal dimension of 37.0 metres.	Plans indicate the following non-compliant floor plates: Office Tower – 46.57m (+9.57m); West Tower – 40.34m (+3.34m); and East Tower – 39.47m (+2.47m).  <b>Relaxations granted: Administration is aware that the Office Tower deficiency reflects an existing building while the east and west tower deficiencies are minor in nature.</b>
1142 Use Area	(1) Unless otherwise referenced in subsections (3) and (4), the maximum use area for uses on the ground floor of buildings in the Centre City Multi-Residential High Rise Support Commercial District is 300m square metres.	Plans indicate two ground floor units that exceed 300.0m <sup>2</sup> : Office Tower (unit 1) – 340.99m <sup>2</sup> (+40.99m <sup>2</sup> ); and West Tower (Unit 8) – 412.01m <sup>2</sup> (+112.01m <sup>2</sup> ).  <b>Relaxation granted: Administration is comfortable with a mix of CRU sizes given that 6 of 8 proposed CRUs meet the requirement and that the alley and L02 CRU locations provide for additional small business space.</b>
1143 Location of Uses Within Buildings	(2) "Commercial Uses" and Live Work Units: (a) must be located on the first 2 floors only, with the exception of Hotel uses;	Plans indicate Commercial Uses located above the second floor in the Office Tower and Commercial Annex.  <b>Relaxations granted: Administration is aware that the office tower deficiency reflects an existing building and that the proposed uses on the third floor of the Commercial Annex enable a better framing and animation of the 4 Street SW streetscape in addition to allowing for greater flexibility and synergy between the Office Tower and the Commercial Annex.</b>

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**MAP 15C**

	(4) A minimum of 80 percent of the gross floor area of buildings in the CC-MHX district must contain Addiction Treatment, Assisted Living, Custodial Care, Dwelling Units, Hotel, Live Work Units, Place of Worship - Small, Place of Worship - Medium, Protective and Emergency Service, Residential Care or Utility Building uses.	Plans indicate a building (Office Tower) with no Dwelling Units.  <b>Relaxations granted: Administration is aware that the office tower deficiency reflects an existing building.</b>
1106 Amenity Space	(7) Private amenity space (b) have no min. dimensions of less than 2.0 m..	Plans indicate multiple balconies less than 2.0m in depth.  <b>Administration is satisfied that the deficiency in private amenity space in the development is offset by communal amenity spaces in each of the towers. Furthermore Haultain Park, Central Memorial Park, Beaulieu Gardens, and Rouleauville Square are located within 3 blocks of the subject site.</b>
1101 Specific Rules for Landscaped Areas	(2) The maximum hard surfaced landscaped area is 50.0 percent of the required landscaped area.	Plans indicate a hard surfaced landscaped area of 2507.99m <sup>2</sup> (+1494.94m <sup>2</sup> ) or 123.78% (+73.78%) of the required landscaped area.  It should be noted that 24.22m <sup>2</sup> of the surface material is either not indicated or of an unacceptable material.  <b>Relaxations granted: Administration is satisfied with the diversity and durability of materials proposed in the amenity spaces in light of anticipated use and climatic conditions.</b>
1101 Specific Rules for Landscaped Areas	(4) Where a landscaped area above grade is fragmented into isolated spaces, a minimum of 30.0 percent of each space must be covered with soft surfaced landscaping.	Plans do not indicate a soft surfaced material on a minimum of 30.0% of each isolated above grade landscaped area.  <b>Relaxations granted: Administration notes that the landscape is satisfied that the proposed development provides appropriate soft surface landscaping, including trees and shrubs in areas destined for public use (including commercial and residential common areas), which meet the 30% requirement, while providing a flexible hard surface for areas more akin to private residential balcony type spaces.</b>

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

1102 Planting Requirements	(2) A minimum of 1.0 trees and 2.0 shrubs must be planted for every 45.0 m <sup>2</sup> of landscaped area provided.	Plans indicate 25 (-40) trees.  <b>Relaxation granted: Administration is satisfied that the applicant has exhausted all available shrub and tree planting opportunities. See additional comments below relating to relocation, reconfiguration, and addition of street trees along the site perimeter.</b>
99 Rules for canopy Signs	(3) A structure used to display canopy signs must: (c) not extend further than 2.4m from the wall of the building to which it is attached; and	Plans indicate a canopy projecting 3.12m (+0.72m) from the wall of the building to which it is attached.  <b>Relaxations granted: Administration considers the encroachment of canopies beyond the bylaw requirement to be beneficial to sidewalk users.</b>
	44 visitor stalls required.	Plans indicate 20 (-35) resident visitor stalls.  <b>Relaxations granted: Supported through shared parking analysis with retail parking supply.</b>
	209 Commercial stalls required.	Plans indicate 158 (-51) commercial stalls.  <b>Relaxations granted: Supported through rationale provided in Parking Study / Transportation Impact Assessment.</b>
	4 loading stalls required.	Plans indicate 3 (-1) loading stalls  <b>Relaxations granted: Supported through rationale provided in Parking Study / Transportation Impact Assessment.</b>
	60 bicycle stalls – class 2 required.	Plans indicate 36 (-24) class 2 bicycle stalls.  <b>Relaxation granted as amended plans resolve the issue (through inclusion of Class 2 bicycle stalls placed in right-of-way).</b>

**URBAN DESIGN REVIEW**

The design was reviewed by the City Wide Urban Design team throughout the CPAG review process. A number of revisions were requested with regard to enhancing the street interface and improving pedestrian connectivity.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

Reviews were conducted by the Urban Design Review Panel (UDRP), who provided additional comments and suggestions for improvements. The UDRP comments, together with the applicant's responses to these suggestions are attached in APPENDIX IV. The applicant's rationale and the resulting additional revisions were deemed appropriate and sufficient.

## **TRANSPORTATION NETWORKS**

A Transportation Impact Assessment was provided by the applicant to support assessment of a Development Permit application for the site, key findings are articulated in summary form below.

### Walking and Cycling

The site's Walk Score (a quantitative measure of proximity to services and amenities) is 97, with schools, commercial corridors, and parks within walking distance. The nearest full-service grocery store is within 950 metres (a 12 minute walk). The proposed development reduces the number of curb cuts along the 15 Avenue SW and 17 Avenue SW frontages.

### Transit

The site is within 850 metres (a 10 minute walk) of the Victoria Park-Stampede LRT station and within 800 metres of the downtown core. Stops for bus routes 3, 6, 7, 17, and 449 are within a block.

### Vehicle Access

17 Avenue SW and 4 Street SW are both classified as Neighbourhood Boulevards in the Calgary Transportation Plan, and direct vehicle access from these streets is discouraged. The site's frontage along 15 Avenue SW provides multiple access options for vehicles including loading and servicing.

### Parking and Vehicle Trips

There is limited on-street parking along 4 Street SW in order to accommodate a bus stop, and parking along 17 Avenue SW is prohibited during the afternoon peak. Parking along 15 Avenue SW is managed through time limits and pricing in order to balance supply and demand, including provision for loading zones.

Proposed on-site parking and loading including 20 residential visitor stalls and 158 commercial stalls has been evaluated through a comprehensive Transportation Impact Assessment which found the proposal to be adequately served from a transportation perspective and without substantial impacts on its surroundings.

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SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

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**UTILITIES & SERVICING**

Public water, sanitary and storm deep utilities exist within the adjacent public right-of-way. Development servicing will be determined at the development site servicing plan (DSSP) circulation stage, to the satisfaction of Water Resources. The DSSP circulation was deemed a prior to release requirement, given the proposed scope and existing site / building conditions.

A removable portion of the proposed development (canopy) is proposed to project within / over an existing Enmax utility and right-of-way. The applicant is required to produce formal correspondence from Enmax confirming that they have no objections to the said removable projection, prior to the release of the DP.

The applicant is also required to produce correspondence from Enmax confirming that they have no objections to the proposed tree alignment and locations in close proximity to existing Enmax facilities within the adjacent public right-of-way.

**ENVIRONMENTAL ISSUES**

The subject lands are not located within a current flood risk regulated zone, as per both the Land Use Bylaw and associated Council approved flood maps.

Administration reviewed Environmental Site Assessment documents that were submitted for the site, dated August 2017. This material had been submitted as there were sensitive historical land uses known within close proximity (such as gas stations, dry cleaners, auto repair shops, etc.). Said review outcome suggested the need for the applicant to produce a “Letter Report”, which was necessary as to compare the analytic results of the environmental conditions for the site. Said report was submitted and accepted in October of 2017, confirming (then) that there would be little risk to potential occupants.

**ENVIRONMENTAL SUSTAINABILITY**

The proposed development will allow more Calgarians to choose to live in a location well served by existing infrastructure and in close proximity to services, employment, and transit. Increased development of the subject site has the potential to allow for population growth with comparatively lower vehicle use relative to other sites.

**GROWTH MANAGEMENT**

The proposed application does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

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**PUBLIC ENGAGEMENT**

**Community Association Comments**

The Beltline Neighbourhoods Association (BNA) indicated through email and the standard comment form that they have no objections to the Development Permit application. The applicant team met with the BNA in October of 2016 and the 4 Street Business Improvement Association (BIA) in November 2016.

**Citizen Comments**

Multiple inquiries were received by Administration regarding the future of the two heritage-designated source sites. All those who inquired were pleased to hear that the proposed land use redesignation would help contribute to the ongoing retention of the Barnhart and Findlay apartments. Continued bar and restaurant use at the site was also supported.

**Public Meetings**

An applicant-led open house event was held in a vacant retail space on 4 Street at 13 Avenue SW on 2017 February 15. This allowed the applicant to refine their proposal with particular emphasis on enhancements to the design of proposed public amenities and streetscape.

A further City-led public meeting was held by Administration on 2017 September 12 at the McHugh House on 17 Avenue at Centre Street SW. Multiple comments were received regarding the proposal, including:

- satisfaction about ongoing retention of heritage resources;
- concern about provisions for transit shelters at the 4 Street/17 Avenue SW intersection;
- desire for improved public realm and building façade design that contributes to an active and lively streetscape;
- impacts on existing commercial tenants of the office building, and
- a desire to delay approving any new residential development until such time that the market value of dwelling units purchased in 2010 regain their original value plus inflation.

Comments regarding building design and public realm have been taken into consideration to inform review of the Development Permit application and have helped inform Administration's review, particularly with respect to the design of the public realm along 17 Avenue SW and 4 Street SW.

Issues relating to accommodations for existing commercial tenants as well as market conditions for residential development were referred to the applicant for their information.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

APPENDIX I

APPLICANT'S SUBMISSION



June 20, 2017  
File: 116500397.202.200

**Attention: Mr. Desmond Bliet**  
City of Calgary – Mail code 8075  
5th floor, Municipal [Building 600](#) MacLeod Trail SE  
P.O. Box 2100, [Stn. M](#), Calgary, AB, Canada T2P

Dear Mr. Bliet,

**Reference: Development Permit Rationale – 1520 Grosvenor Project  
Grosvenor Canada Ltd.  
Application Submission**

On behalf of Grosvenor Canada Ltd. ("Grosvenor"), Stantec Consulting Ltd. ("Stantec") is pleased to submit a Development Permit Application for review and approval by the City of Calgary for a multi-phase master planned mixed use development on 0.68-hectare (1.68 acres) site in the Beltline community of southwest Calgary. This development includes:

- the restoration of an existing office building, giving new life to a landmark location at the iconic 4<sup>th</sup> Street SW and 17 Avenue SW intersection; and
- the construction of two (2) residential towers along 15 Avenue SW providing 547 residential units and approximately 20,000 square feet of new retail/commercial space along the 4<sup>th</sup> Street SW corridor.

There are numerous unique design elements to this project we wish to highlight, from the urban design strategy to the architectural design of the buildings themselves.

**DEVELOPMENT CONCEPT**

Grosvenor 1520 located at 1520 4 Street SW, is proposed to be a three-phase development. The development proposes two residential towers providing 548 units that offer a range of housing types from studio apartments to three ~~storey~~ townhomes along 15 Avenue SW. In addition, a new retail podium along 4 Street SW provides a quality retail street presence substantially lacking in this area of the Beltline. This new podium will also house on the upper floors amenity spaces that would be available to office users and residents in the new towers.

The office tower at 4 Street/17 Avenue SW is proposed to be renovated to modern standards, with a traditional design aesthetic – offering refreshed office spaces for smaller tenants that wish to be part of the Beltline business community. A new rooftop restaurant space is also included, affording views to the mountains and downtown Calgary unlike anywhere else in this part of Calgary. This dynamic renovation includes the creation of a new public space currently called "The Alley". The Alley will include full scale retail storefronts and offer a new way to experience retail in Calgary, with a sense of discovery for those entering this unique area. Overall the site is compliant with parking regulations. A Parking Study has been submitted in support of this Development Permit Applications mix of parking on site as well as consideration for minor parking relaxations required.

Design with community in mind

D. Bliet

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**



June 13, 2017  
Mr. Desmond Bliet  
Page 2 of 6

Development Permit Rationale – 1520 Grosvenor Project  
Grosvenor Canada Ltd.  
Application Submission

**DESIGN CONSIDERATIONS**

**An iconic place along 17 Avenue**

17th Avenue is an iconic destination within Calgary. Calgarians eager to show visitors to our city consistently place this corridor on one of their top places to visit. With its diverse mix of restaurants, specialty stores and retail offerings, 17 Avenue is a unique urban area in our city. The redevelopment of one of the largest structures on this corridor required special consideration. A top-to-bottom renovation is proposed – adding a timeless façade that prioritizes the retail presence and establishes this corner of 17 Avenue and 4 Street as a prominent and central meeting place within the corridor. The addition of the Alley as a retail offering, as well as the rooftop restaurant, will establish this corner as a more meaningful contributor to the 17th Avenue experience, and more memorable place in the future.

**An iconic place in the skyline**

These two new towers represent the first significant skyline addition east of 4 Street in the Beltline. As such, considerable thought was given to the positioning of the towers to maximize the vibrancy of the 4 Street pedestrian realm. Although the site currently permits 9 FAR adjacent to 4th Street, by locating the residential towers further into the site, away from 4 Street, the new commercial component of the development better interfaces with the pedestrian-scaled nature of the corridor while minimizing negative micro-climate effects to pedestrians. Positioning the residential towers in this way also mitigates sightline issues with the existing office building and allows for more flexible, larger and more intensive retail uses on the street.

The design of the towers themselves is restrained, simple and elegant. They will be very visible landmarks along two of Calgary's most significant Beltline arteries for many years to come, and it is therefore imperative that they evoke a timeless quality.

**Amenity spaces**

Grosvenor 1520 integrates amenity spaces to maximize the offerings to future users of the site. A new retail annex is proposed at the intersection of 4 Street and 15 Avenue SW that will provide a blended amenity space for residents of the towers and office users. This amenity space will offer gym facilities and outdoor amenities unlike anywhere else in the Beltline. In addition, the residential towers will feature dedicated amenities such as a range of outdoor amenity spaces including two rooftop lounges, fully-equipped event spaces in each building, and well-appointed sitting areas with connections to the courtyard and the Alley.

**Visitor parking**

With this development being a true mixed use project with residential, office and commercial spaces, we propose scramble visitor parking to accommodate the overall project parking demands throughout the day. Please refer to the Parking Study submitted with this application for further information.

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SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**



June 13, 2017  
Mr. Desmond Bliiek  
Page 3 of 6

Development Permit Rationale – 1520 Grosvenor Project  
Grosvenor Canada Ltd.  
Application Submission

**Urban Design Considerations**

**Timeless design with contemporary flair**

The renovation of the existing office building and construction of a new retail annex offer an opportunity to build a retail presence not realized on this corner of 17 Avenue and 4 Street yet. In addition, Grosvenor recognizes a significant opportunity to bring some of the best qualities of 4<sup>th</sup> Street north across 17<sup>th</sup> Avenue for the first time. This new retail annex features a double-storey façade providing a scale that is consistent with the existing building configuration but far more pedestrian friendly. Wide window bays and an enhanced streetscape design provided an improved blend of the public and private realms along this corridor. The approach to the urban realm is at the service of the project becoming a living, dynamic, responsive part of the community, generating energy and activity in and around it, and improving the lives of those who encounter it.

**A critical mass.**

A mixed-use site of this scale requires careful thought to building placement and the interactions of the uses themselves. Thus, a strategy to locate the residential towers along 15 Avenue instead of directly on 4 Street affords space and progression of the buildings that is more amiable to the pedestrian experience along 4 Street and 17 Avenue. This configuration with three level townhomes and city flats fronting 15<sup>th</sup> Avenue continues the existing pedestrian realm priority in a residential context and allows the residential towers to be recessed from 15<sup>th</sup> Avenue itself. The residential towers also step down in height from the east to west to help transition to the contextual form along 17 Avenue and 4 Street.

**Pedestrian connections**

We have provided several pedestrian connections from our development to the existing urban framework. We have also added a new pedestrian route through the site in the form of The Alley. This new path elongates both the 17 Avenue retail street and 4 Street retail presence by wrapping a path around the existing office building and opening new retail spaces in this unique urban realm. Further information about this space is provided in the bonus section of this letter.

**Landscape design**

We have provided a thoughtful landscape design that is complementary to the existing city of Calgary streetscape as well as defining private landscaping features on rooftops and interior spaces to Grosvenor 1520. Care and consideration to street trees resulted in a strategy that improves upon existing conditions. Due to the infrastructure limitations of implementation of street trees along 4 Street, additional landscaping in the form of planters have been added. The intent is to use edge planting along 4 Street to make the sidewalk feel more pedestrian friendly and to activate the spaces in front of the retail units to accommodate sidewalk café-style seating. Along 17 Avenue, new street trees will be provided and a small plaza space will be developed at the corner of 17 Avenue and 4 Street. The project will also incorporate a public art component. The 15<sup>th</sup> Avenue streetscape will continue to offer a residential look and feel. New trees will include planted areas at the base instead of tree grates to visually extend the perception of a “front yard” for the townhomes. The project includes a courtyard between the two residential towers that will accommodate new landscaping throughout.

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SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**



Development Permit Rationale – 1520 Grosvenor Project  
Grosvenor Canada Ltd.  
Application Submission

June 13, 2017  
Mr. Desmond Bliiek  
Page 4 of 6

**Approach to the Architectural Design**

The site, at one of Calgary's most important Beltline intersections, sits in a uniquely vibrant position in the city. With three key interfaces – one along the city's busiest retail and entertainment street, another facing one of the most heavily trafficked pedestrian arteries in the Centre City, and a third along quieter primarily-residential Beltline avenue – the site offers a singular opportunity to bring to life the goals of Grosvenor's "Living Cities" philosophy.

The design is purposefully restrained to emphasize classical elegance. While not formally elaborate, the design is rooted in an ambition to achieve a modern day take on character space, and set a new standard for mixed-use projects in Calgary's inner-city. A complimentary design approach was applied to both the new structures and the redevelopment of the existing office tower so that the project feels cohesive and integrated.

The architectural language is meant to show deference to the project's place in the City. The materiality and scale of the commercial edges reference the context of fourth street's fabric. Grosvenor's history inspired the architectural language, but also draws from the urban character of the site at the interface between the Beltline and Cliff Bungalow-Mission. The result is abstracted into a timeless modern aesthetic that is deeply contextual and not defined by trends or styles. The design philosophy isn't about looking for something Avant Garde, nor is it about dreaming of days' past. This approach suits the eclectic mix of both old and new buildings the community, bridging eras and striking a balance.

**Public Bonusing Provisions and Rationale**

Concurrent to this application is a proposed Direct Control District that utilizes the CC-MHX zone as a base district but allows the bonusing provisions to achieve a 9.0 FAR on the sites along 15 Avenue SW. The bonusing provisions as proposed include:

**Heritage Density Transfer**

2.48 FAR is proposed to be achieved through the heritage density transfer as outlined in the concurrent Land Use Redesignation Application. As noted in that application the two donor sites are within the Beltline neighbourhood and have been reviewed by Heritage Planning for their applicability.

**Publicly Accessible Open Space**

As noted in our design rationale – a new publicly accessible open space is proposed in this development. This open space is unique in that normally a space provided is a plaza or a park in a prominent location. This space is proposed to create a new retail destination and connection points from 4 Street to 17 Avenue through an active pedestrian only Alley. This Alley will include active retail fronts and a unique public realm that will offer pedestrians a truly unique retail experience in Calgary. Patios, opportunities for public art and hand chosen destination retailers will be a part of this overall experience that not only adds to the uniqueness of the Grosvenor 1520 project, but also to the corridors in which it connects to. A high-quality paving finish and unique lighting elements also add to the overall character and form this space. Submitted with this application is a preliminary cost estimate of the construction of this open space system for review and acceptance. This space as costed is equal to 0.43 FAR required for bonusing.

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SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**



June 13, 2017  
Mr. Desmond Bliik  
Page 5 of 6

Development Permit Rationale – 1520 Grosvenor Project  
Grosvenor Canada Ltd.  
Application Submission

**BCIF Contributions**

The remaining 1.09 FAR required will be achieved with a Contribution to the Beltline Community Improvement Fund (BCIF). Final confirmation of this FAR and required contribution will be determined after acceptance of the open space cost estimation.

**DOCUMENTS ENCLOSED**

To fulfill the requirements of the Development Permit application, please find enclosed the following items:

**ADMINISTRATIVE DOCUMENTS**

- Completed Development Permit Application Form and Complete Application Requirement List;
- An application submission summary;
- A letter of authorization from the registered owner;
- Responses to Pre-application (PE2016-00841);
- The required fees for the Development Permit application;
- A current copy of the relevant Certificate of Titles; and
- Current copies of all Restrictive Covenants, Utility Rights-of-Way, Easements, and Caveats.

**SUPPORTING DOCUMENTS**

- ~~Colour~~ photographs of the site;
- Off-site Levy Calculation;
- Completed site contamination statement;
- Completed public tree disclosure statement;
- Completed abandoned well declaration form, including a map demonstrating the absence of any wells in the area;
- Parking Study; and
- Publicly Accessible Open Space Cost Estimation.

In conclusion, we believe that our development approach to this project meets the City of Calgary's expectations for mixed use development in unique spaces like the Beltline neighbourhood. We look forward to working with your team on this Development Permit application. If there are any questions or comments, please do not hesitate to contact.

Sincerely,

**STANTEC CONSULTING LTD**

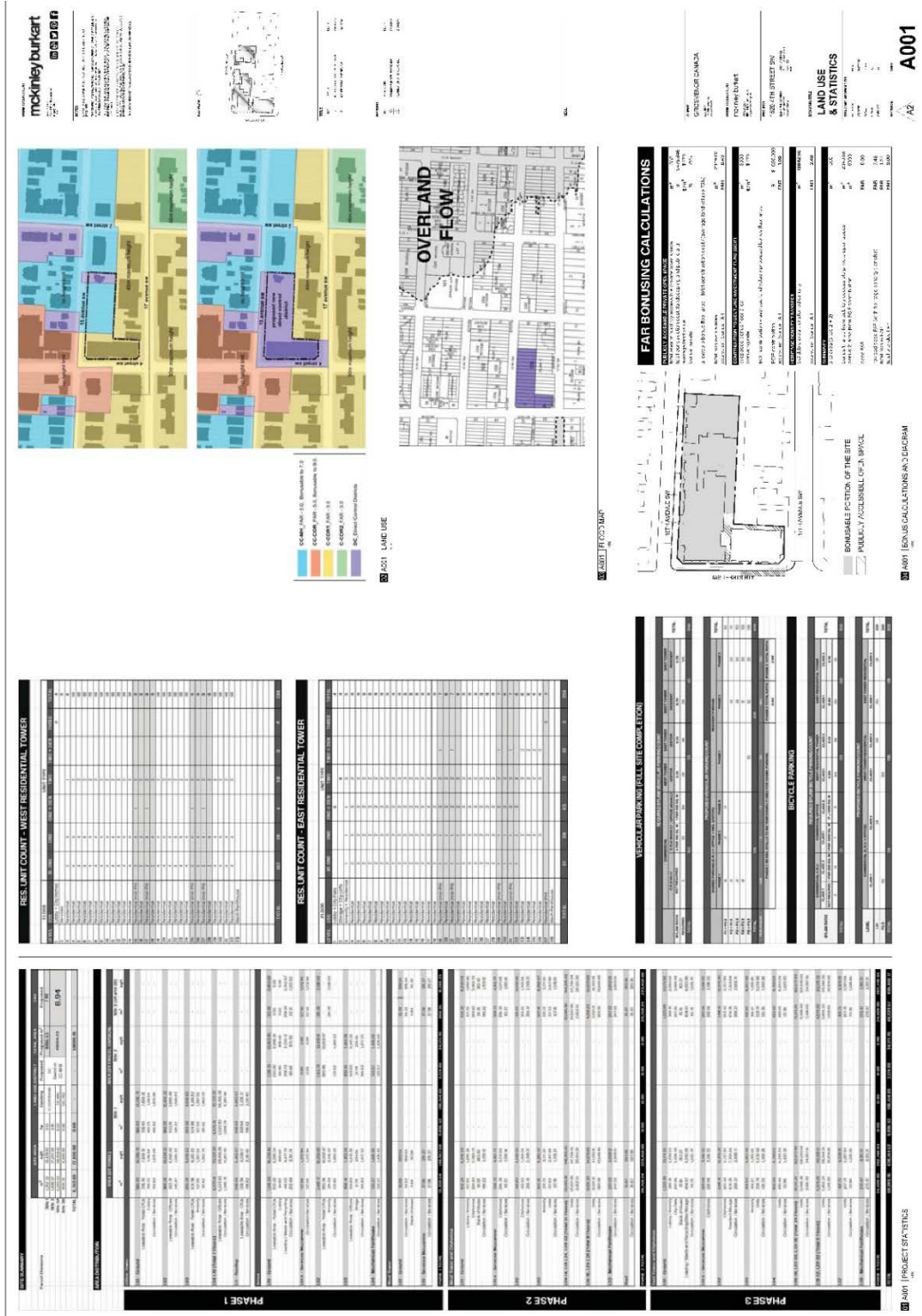
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 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C







DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

**VEHICULAR PARKING - PHASE 1**

PHASE 1 (refer to Phase 1 table on sheet A101)

- demolish existing annex building on the corner of 15th Avenue and 4th Street SW
- partially demolish existing Office Tower at 17th and 4th (existing structure to remain)
- demolish existing surface parking lot up to Phase 2 line, reconfigure parking to meet current bylaws standards on the rest of the surface lot to remain functional in Phase 1
- excavate, store, and construct Phase 1 parking entries below grade
- install underground stormwater system with full capacity for all future phases
- upgrade existing transformer vault to service Phase 1, install water metering and utility connections for Phase 1
- re-land existing Office Tower and install all new building systems
- construct Commercial Annex up to Phase 2 line, temporary exterior walls to be finished and clad with composite metal panels
- construct full scope of Pedestrian Alley, including installation of public art and masonry walls along property line
- demolish and install new hard and soft landscape along 17th Avenue, 4th Street, and partially along 15th Avenue
- existing building located at Site 38 to remain operational throughout phase 1, see sheet A103
- sidewalks to be reconstructed up to extents as indicated in plan

**VEHICULAR PARKING - PHASE 2**

PHASE 2 (refer to Phase 2 table on sheet A101)

- demolish existing surface parking lot up to Phase 3 line
- excavate, store, and construct Phase 2 parking entries below grade
- install new underground transformer vault for Phase 2 (and 3), initial water metering and utility connections for Phase 2
- partially construct and pave residential parking-cohere up to Phase 3 line, vehicular round-about to be fully functional and largely finished at end of Phase 2
- construct West City Homes and West Tower above grade
- demolish and install new hard and soft landscape partially along 15th Avenue
- existing building located at Site 38 to remain operational throughout Phase 2, see sheet A103
- sidewalks to be reconstructed up to extents as indicated in plan

**VEHICULAR PARKING - PHASE 1**

COMMODITY	REQUIRED IN PLAN (VEHICULAR PARKING COUNT - PHASE 1)	EXISTING	REQUIRED
COMMERCIAL	0	0	0
RESIDENT	0	0	0
TOTAL	0	0	0

EX-SURFACE	SHARED PARKING (BUILT OFF-SITE)	PHASE 1	PHASE 2	TOTAL
P1 P1.2	0	0	0	0
P1 P2.2	0	0	0	0
P1 P3.2	0	0	0	0
P1 P4.2	0	0	0	0
TOTAL	0	0	0	0

NOTE: EX-SURFACE STALLS TO BE FOR SHORT TERM USE

**VEHICULAR PARKING - PHASE 2**

COMMODITY	REQUIRED IN PLAN (VEHICULAR PARKING COUNT - PHASE 2)	EXISTING	REQUIRED
COMMERCIAL	0	0	0
RESIDENT	0	0	0
TOTAL	0	0	0

EX-SURFACE	SHARED PARKING (BUILT OFF-SITE)	PHASE 1	PHASE 2	TOTAL
P1 P1.2	0	0	0	0
P1 P2.2	0	0	0	0
P1 P3.2	0	0	0	0
P1 P4.2	0	0	0	0
TOTAL	0	0	0	0

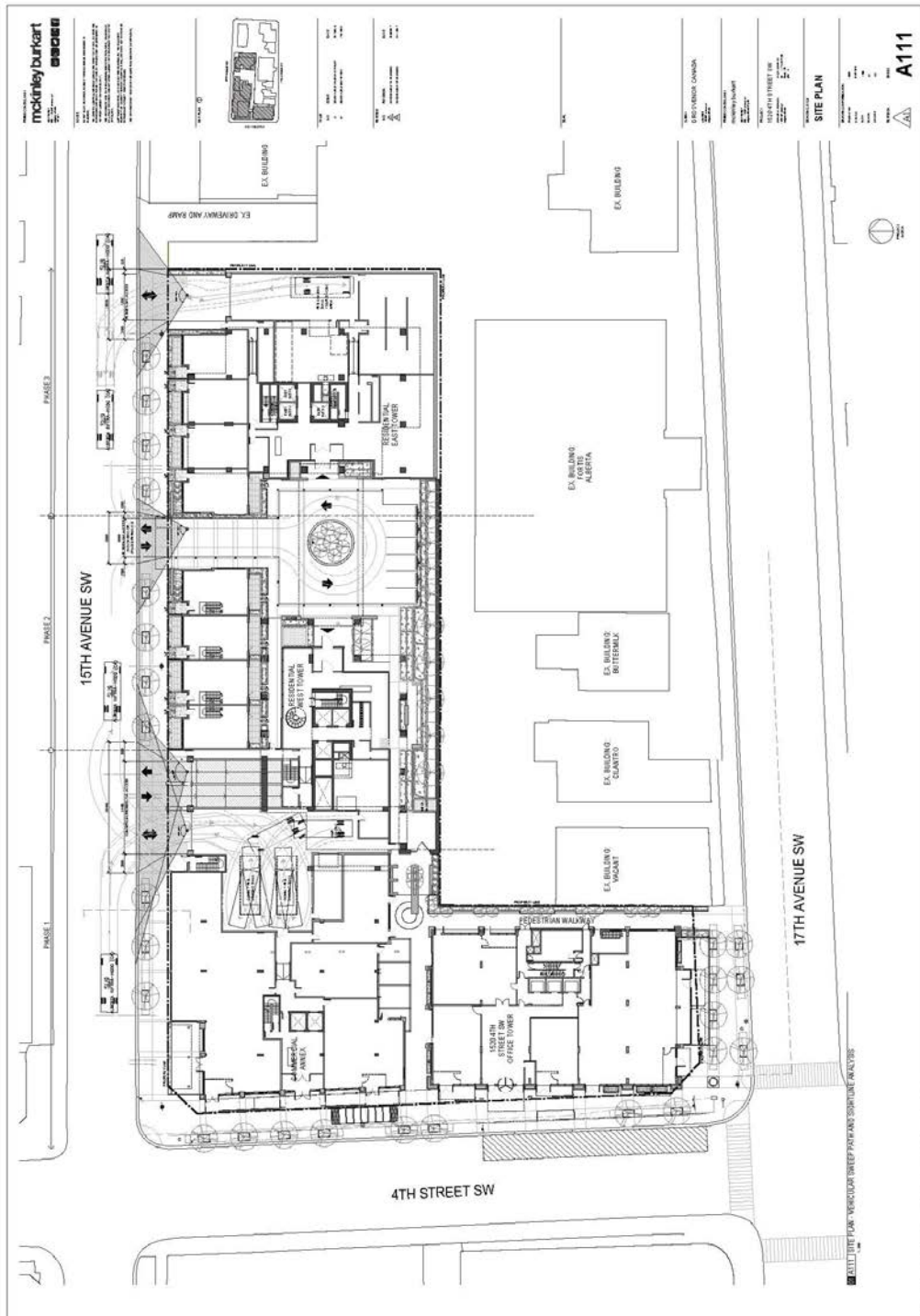
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BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C





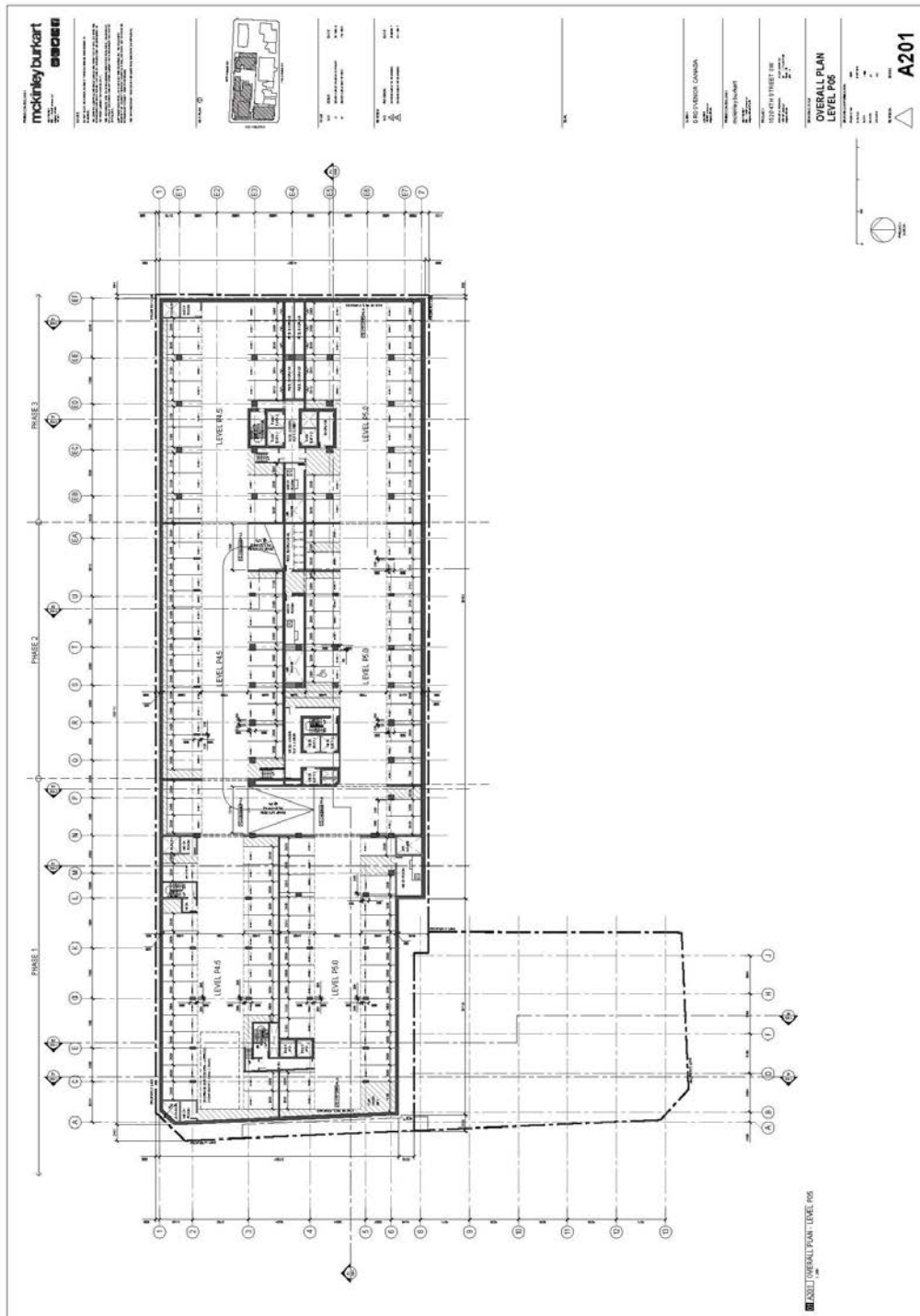
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MAP 15C

The image displays a site plan and a grid of photographs for a development project. The site plan, titled 'EXISTING SITE PLAN', shows a rectangular lot bounded by 4 Street SW, 17 Avenue SW, and 18 Avenue SW. It features a large building footprint with various internal divisions and parking spaces. Red callout boxes labeled 'a' through 'h' are placed around the site plan, corresponding to the photographs. The photographs show different views of the building and its surroundings, including street-level views, elevated views, and views from different angles. The building has a modern, multi-story design with a mix of brick and glass facades. The surrounding area includes other buildings, trees, and parking lots. The site plan also includes a legend for 'EXISTING SITE PLAN' and a scale bar. The project is identified as 'A113'.

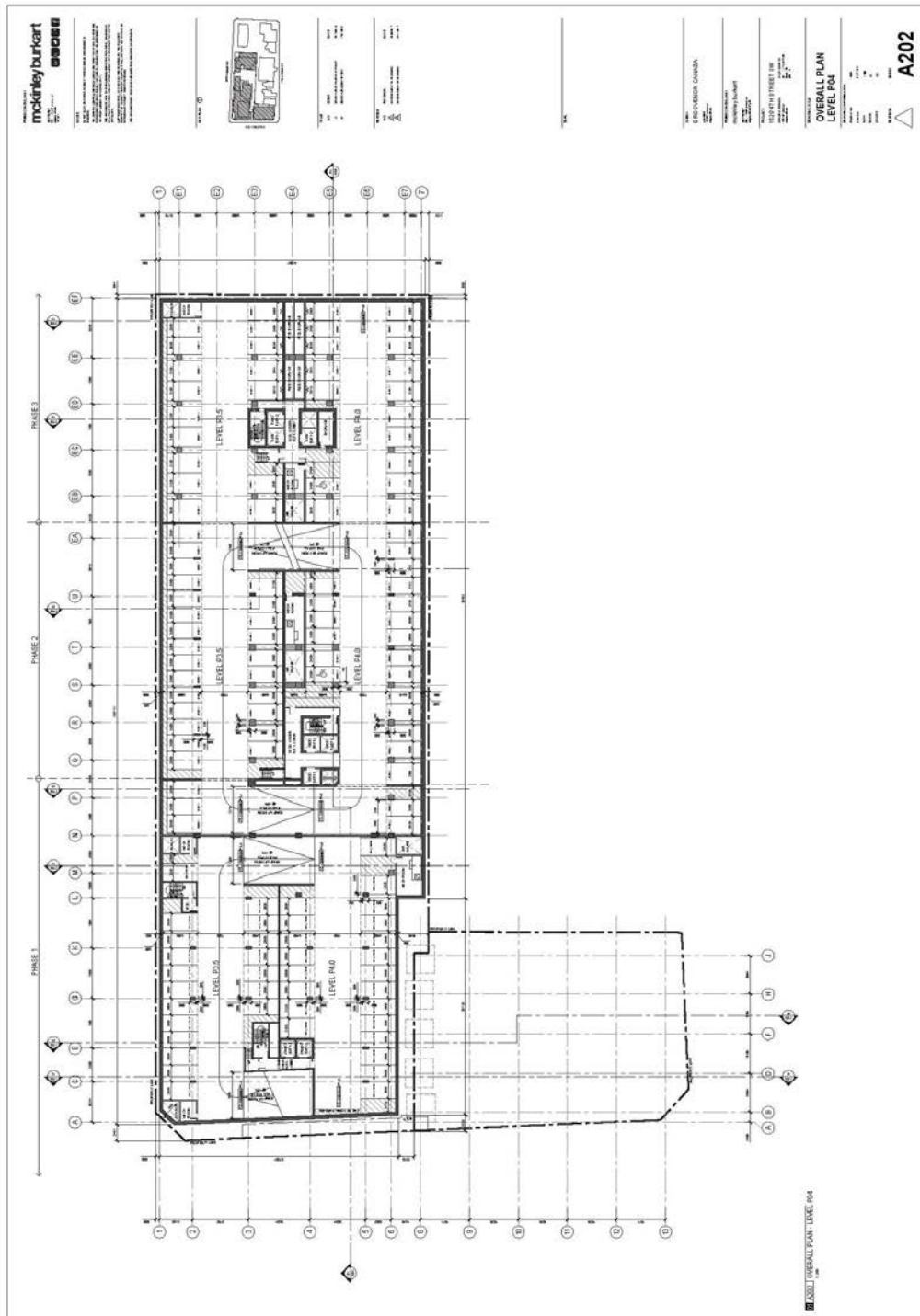
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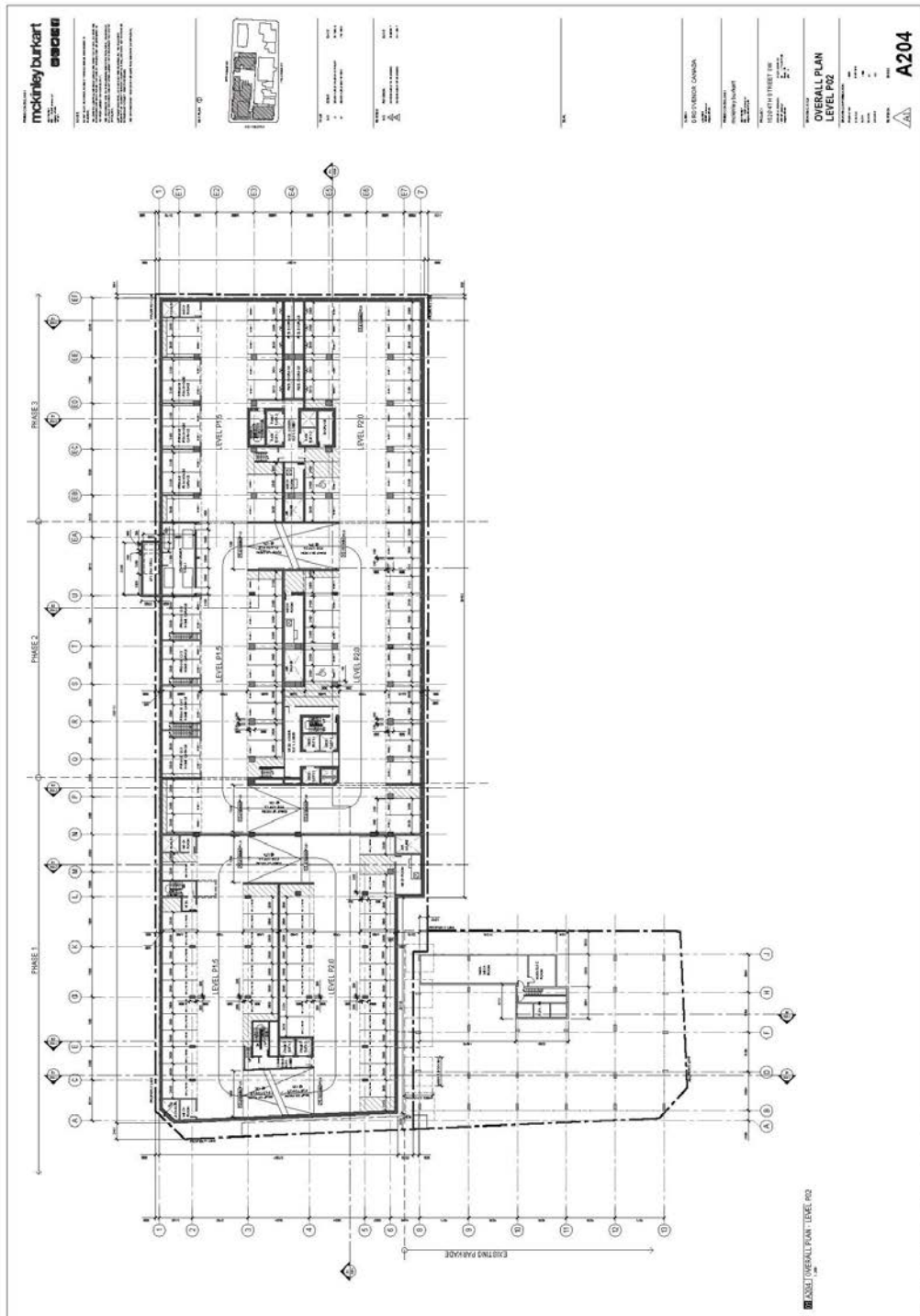
MAP 15C





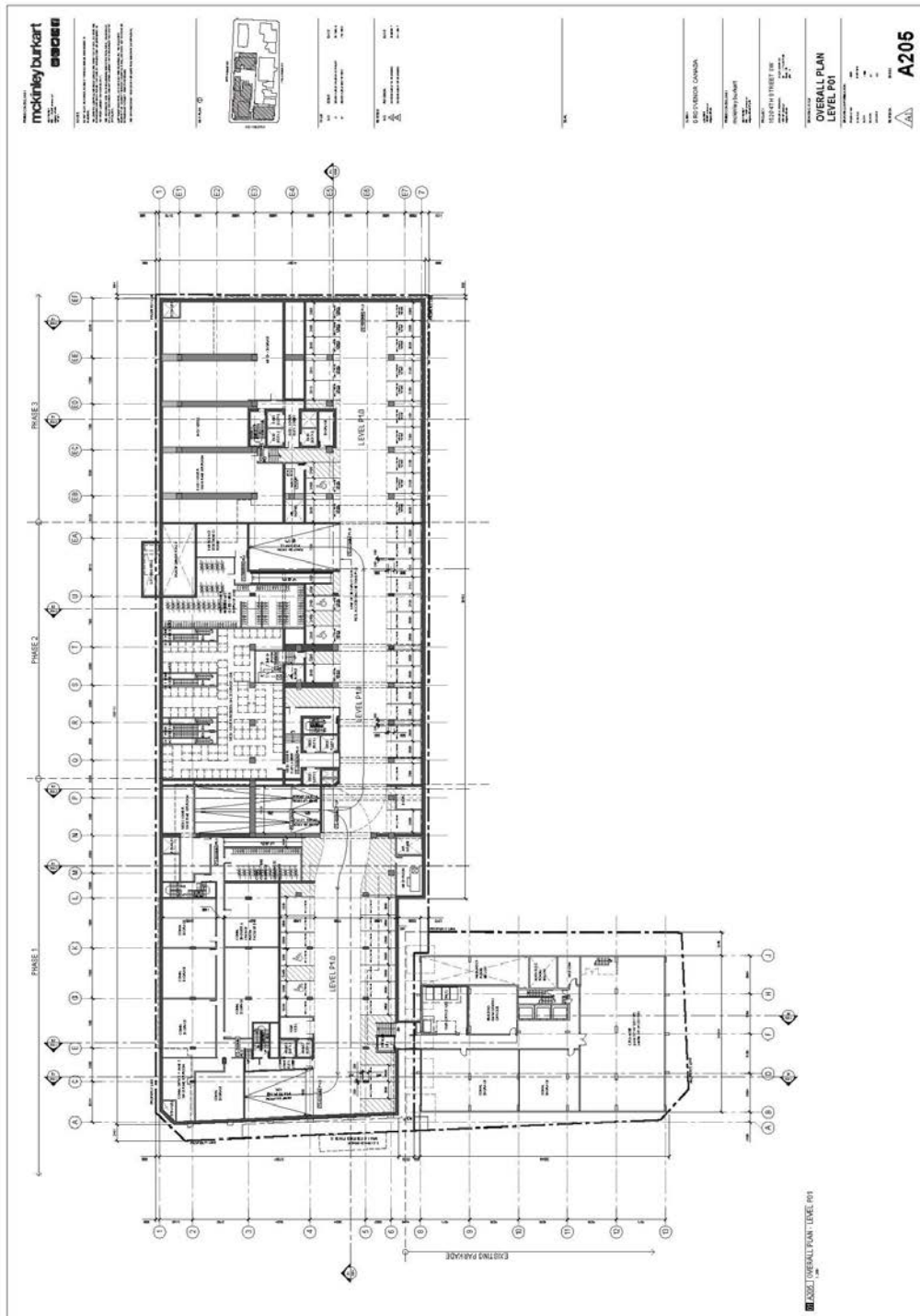
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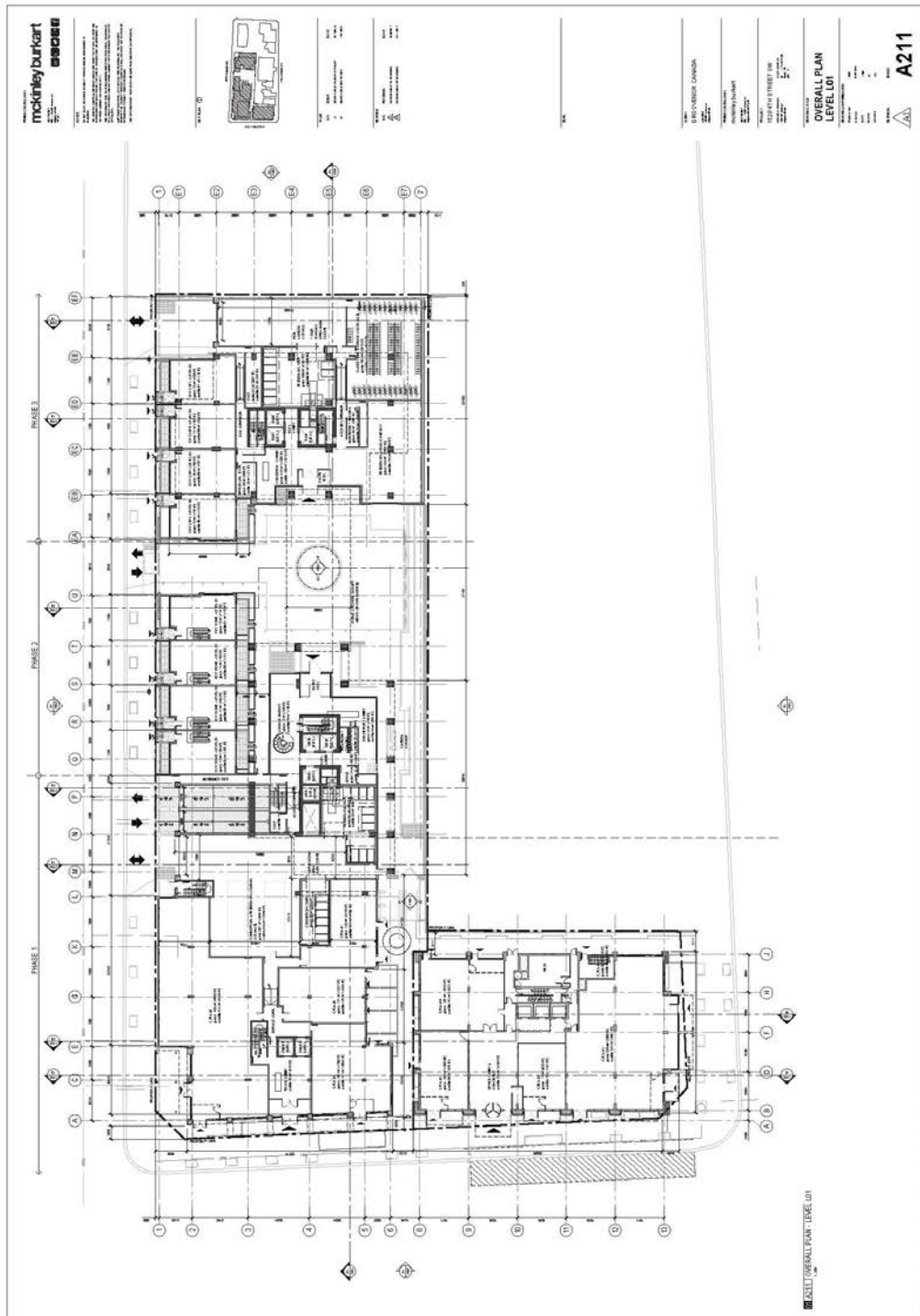
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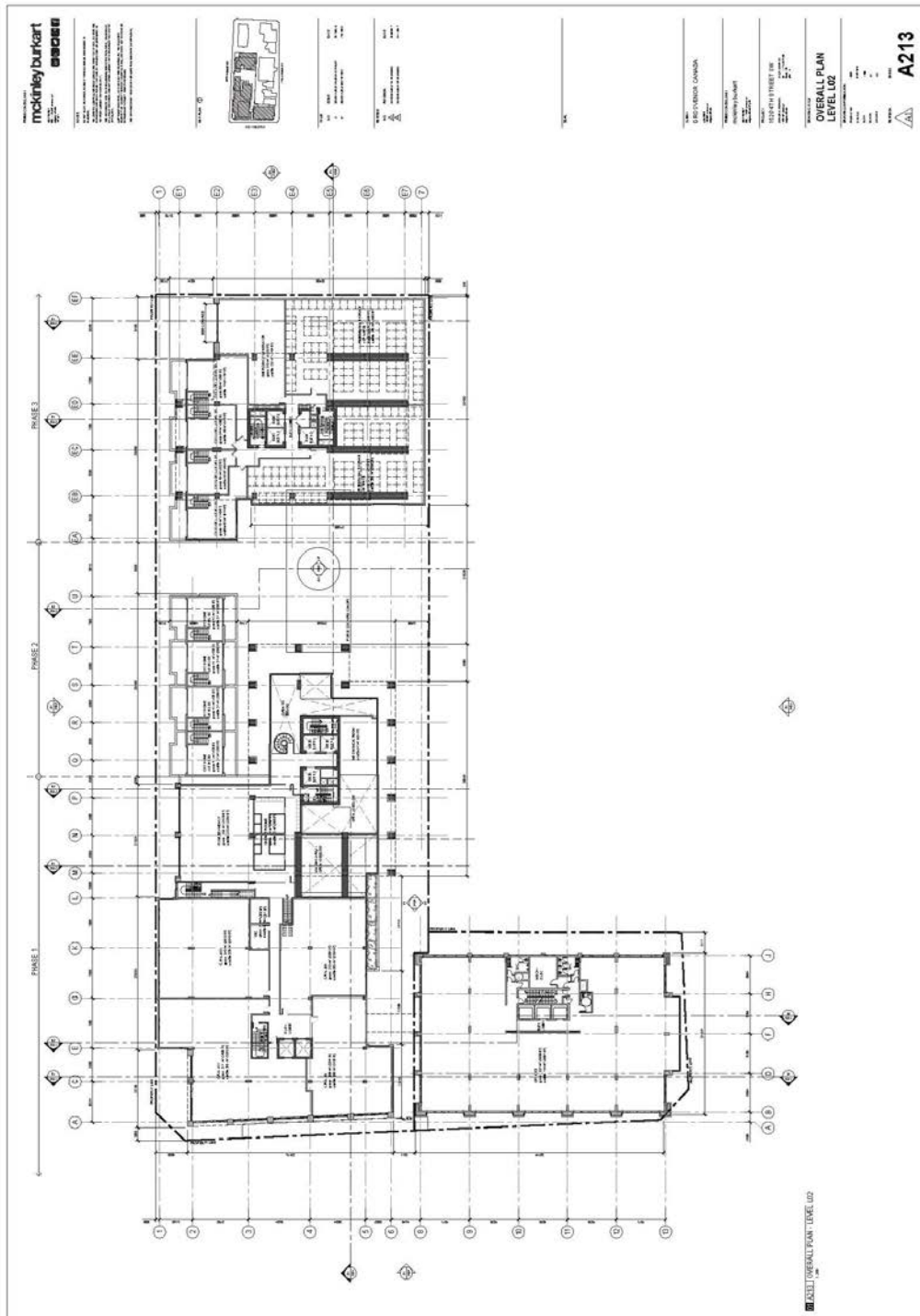
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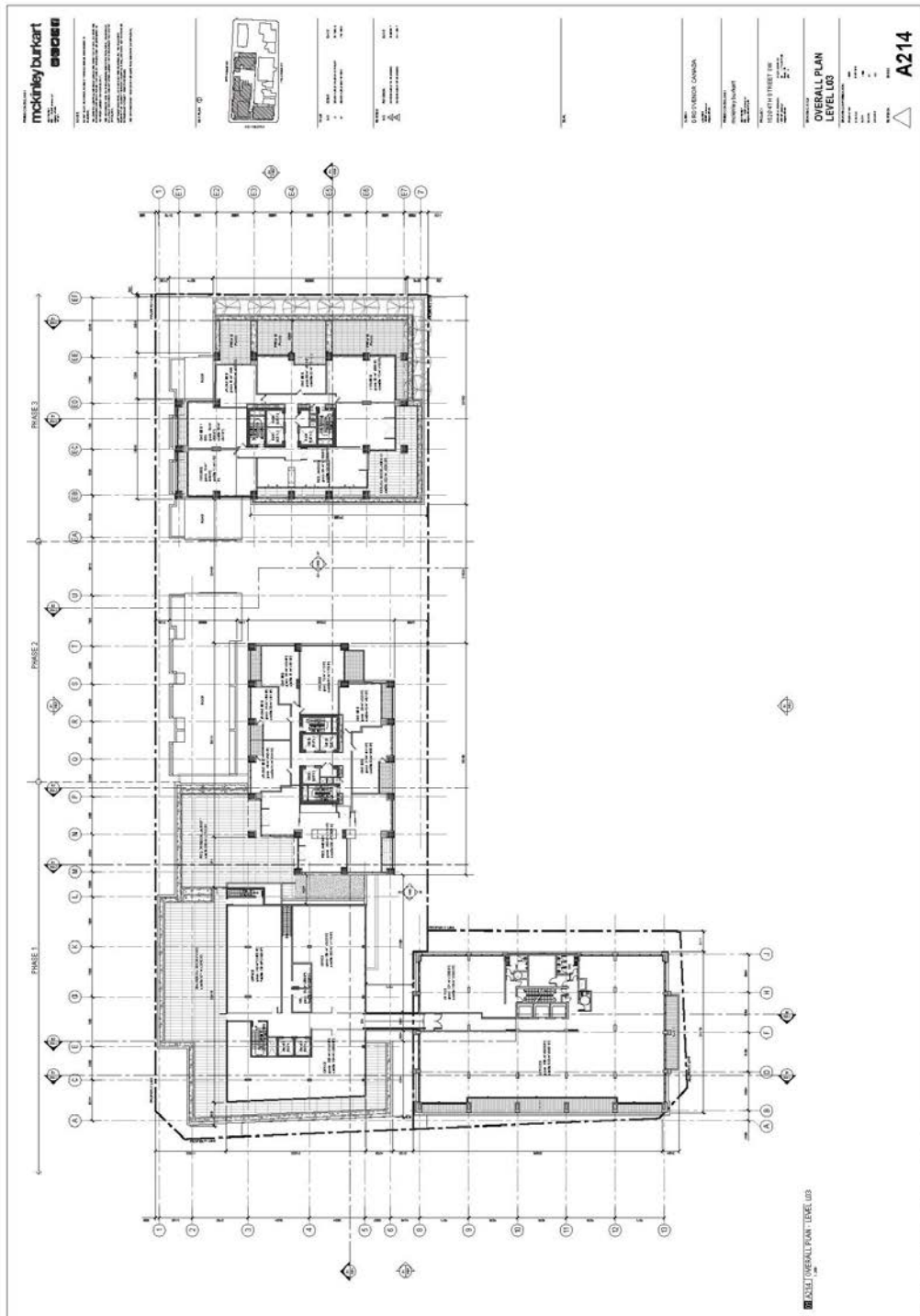
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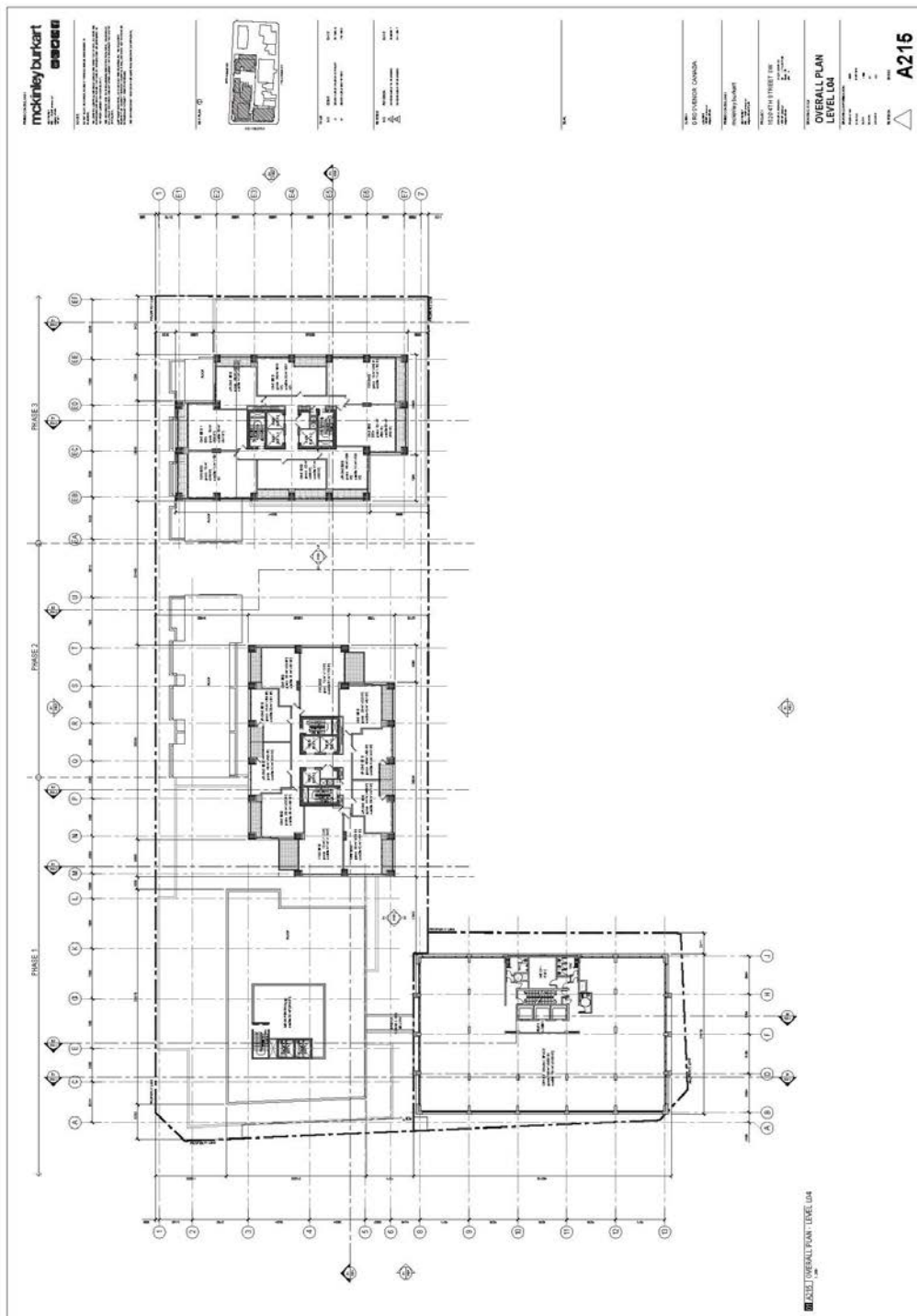
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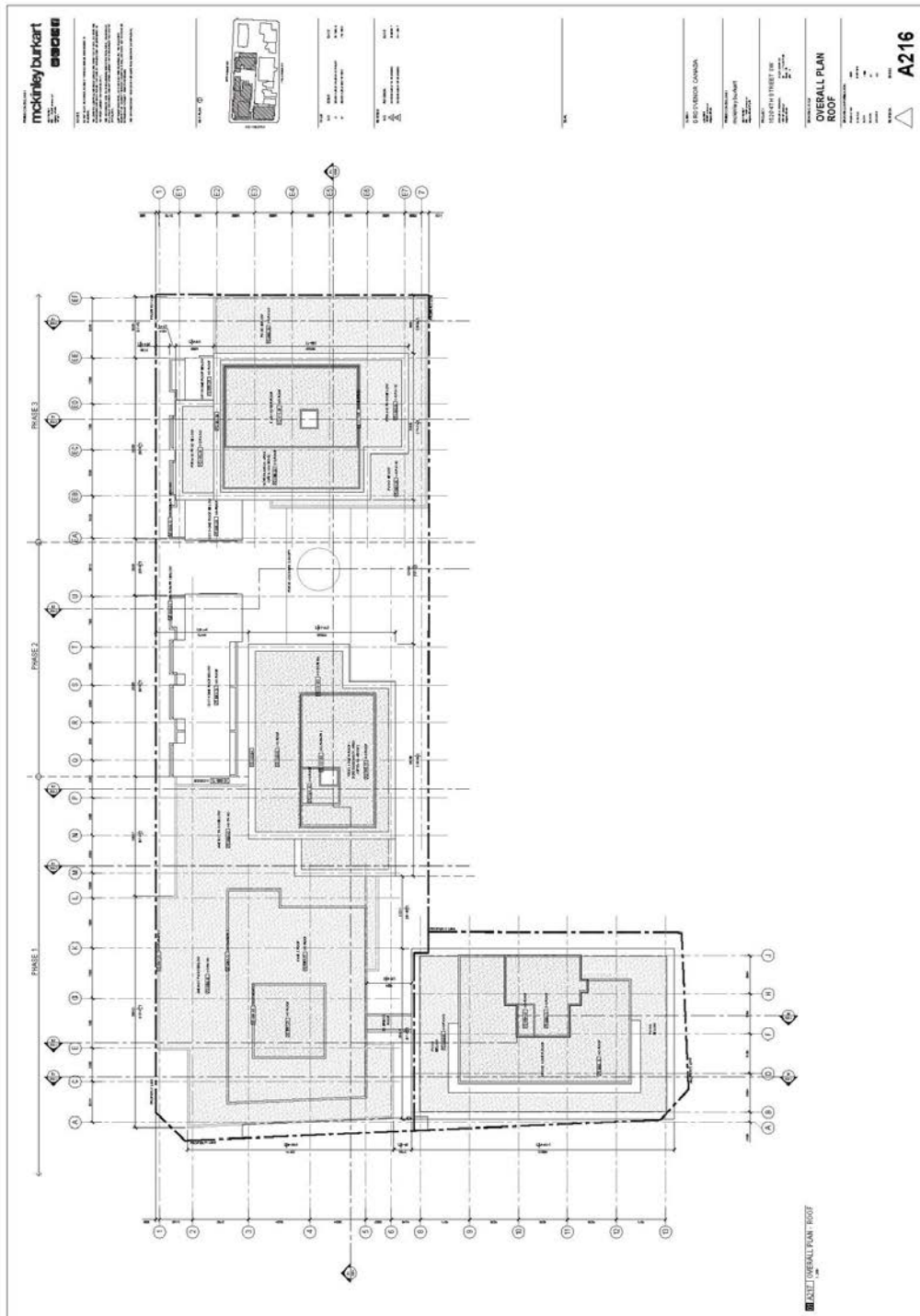
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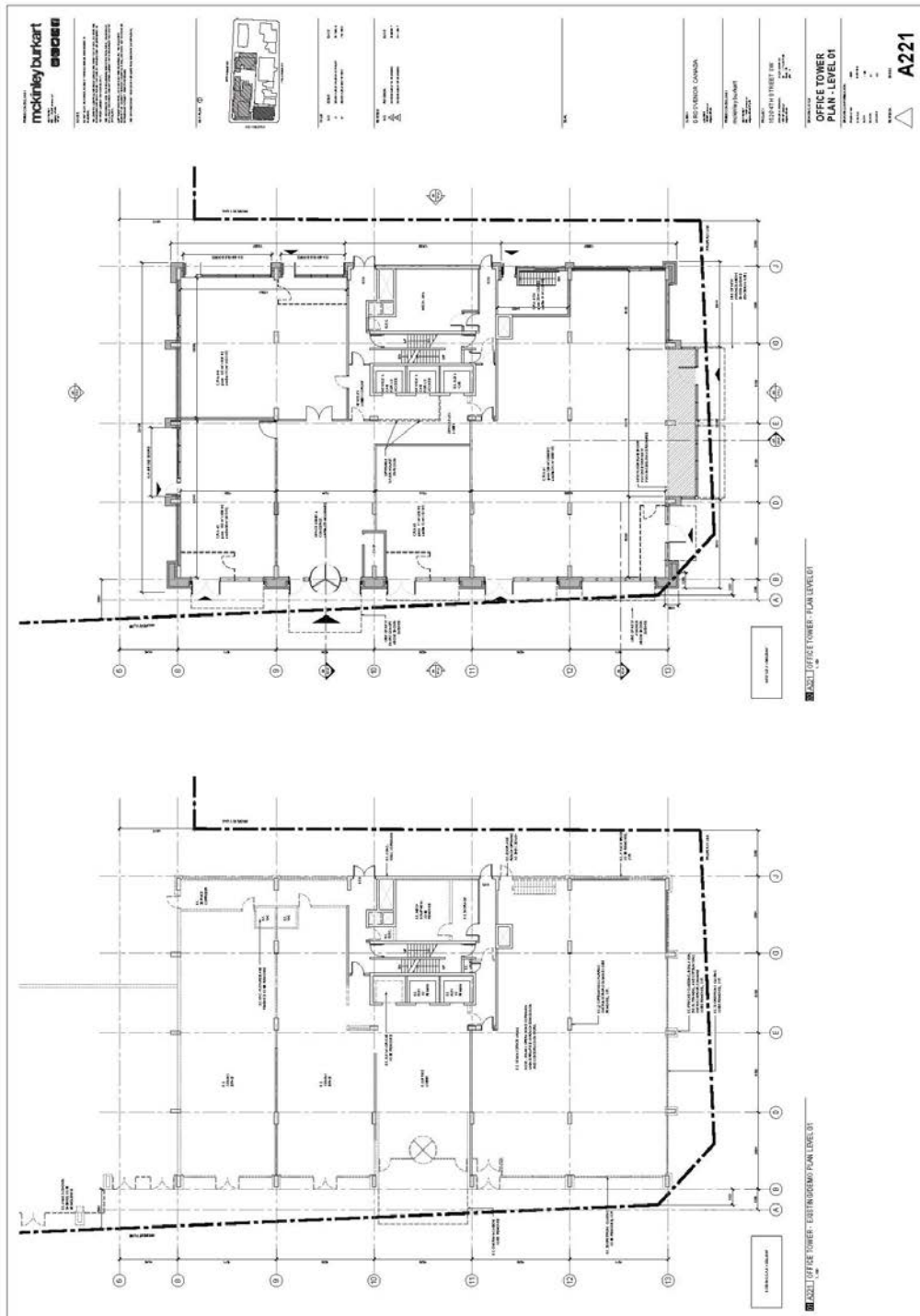
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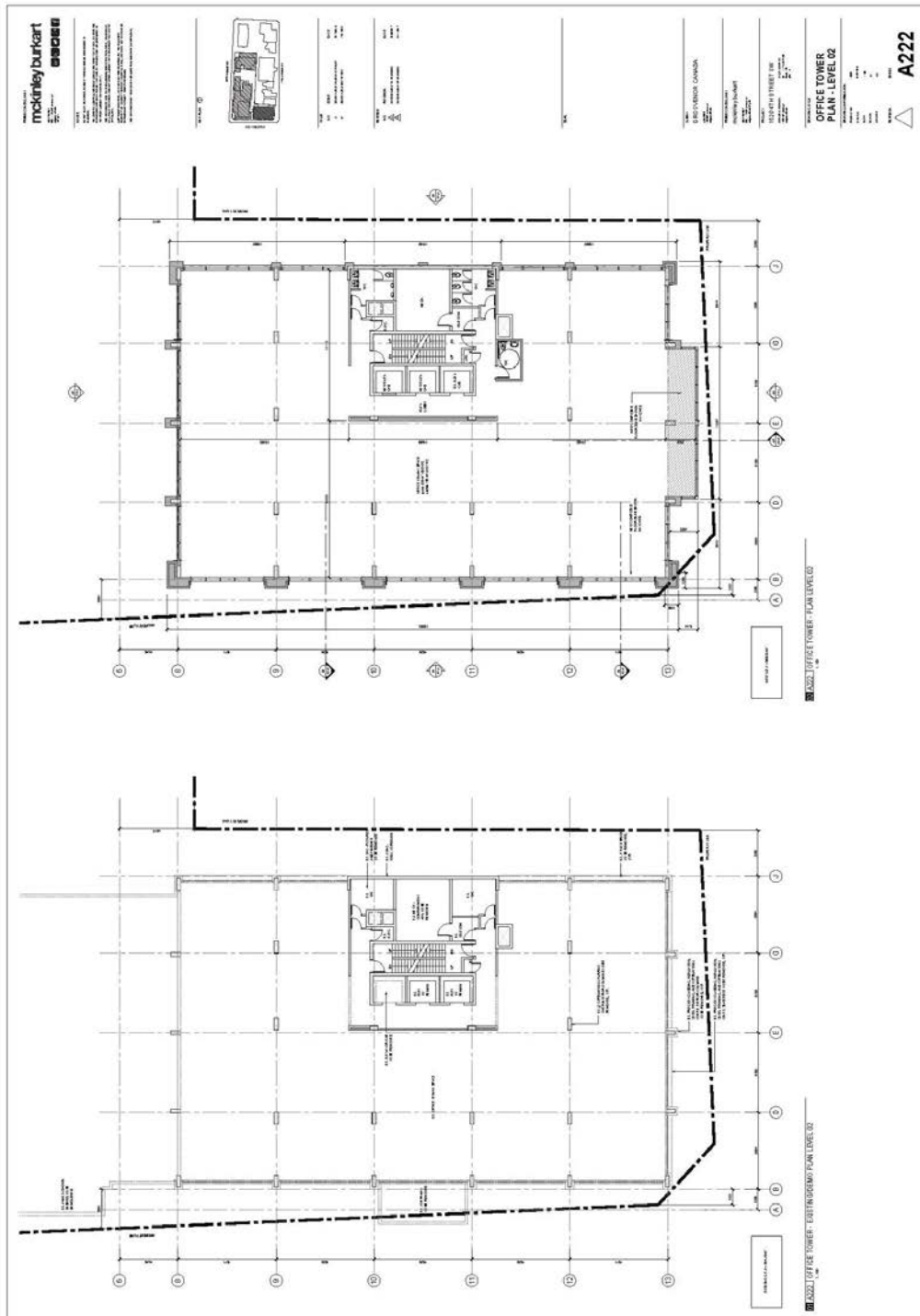
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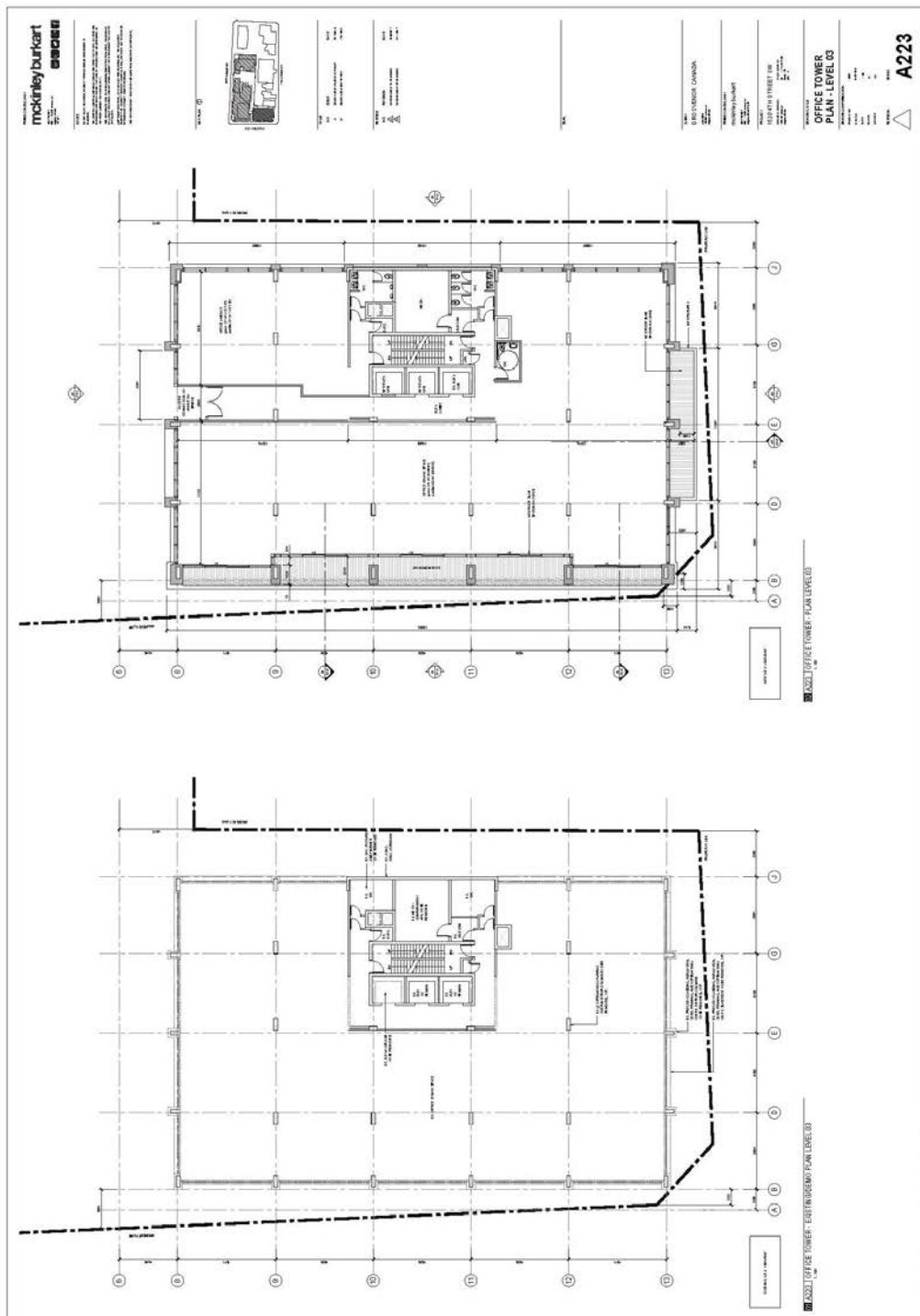
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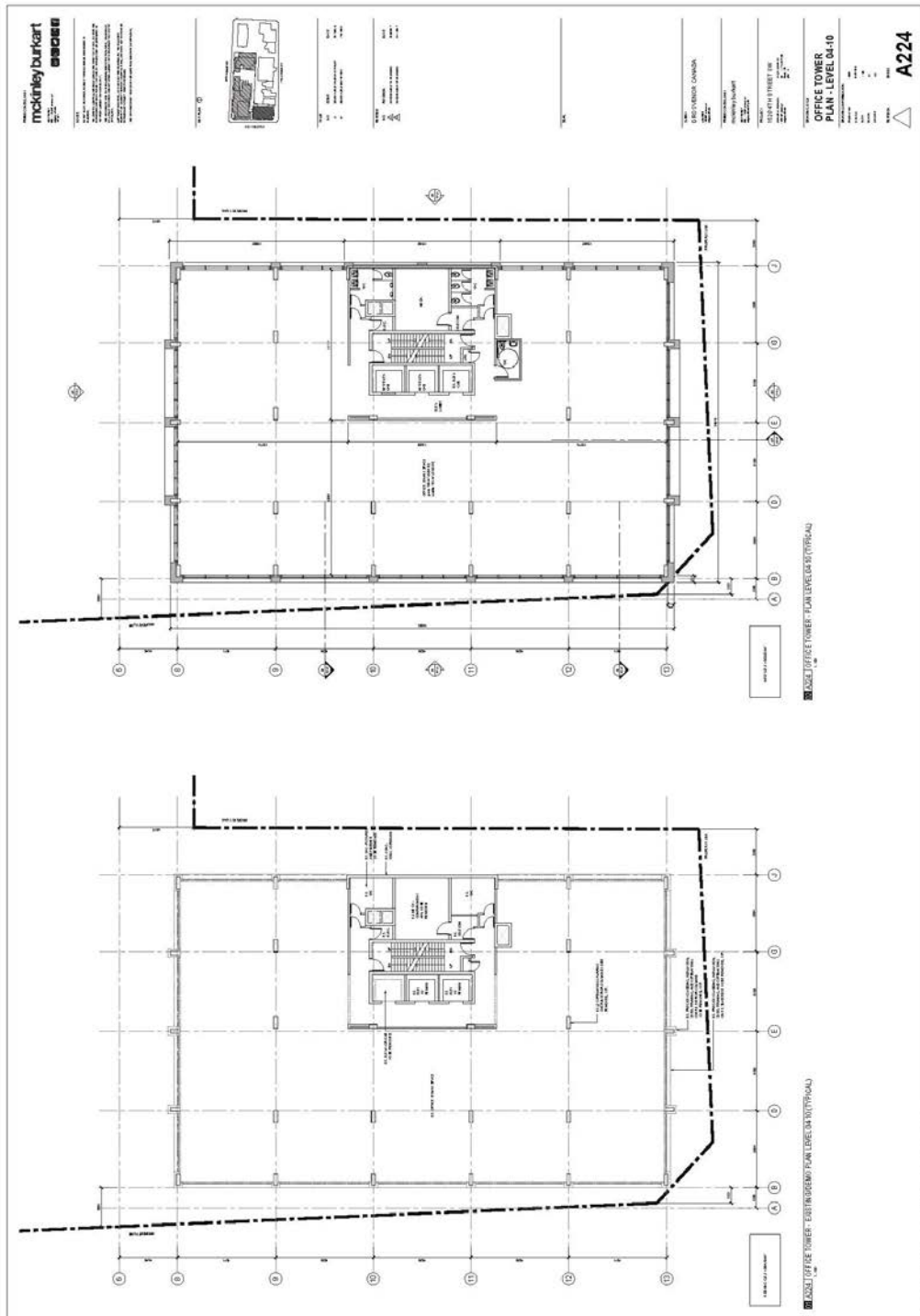
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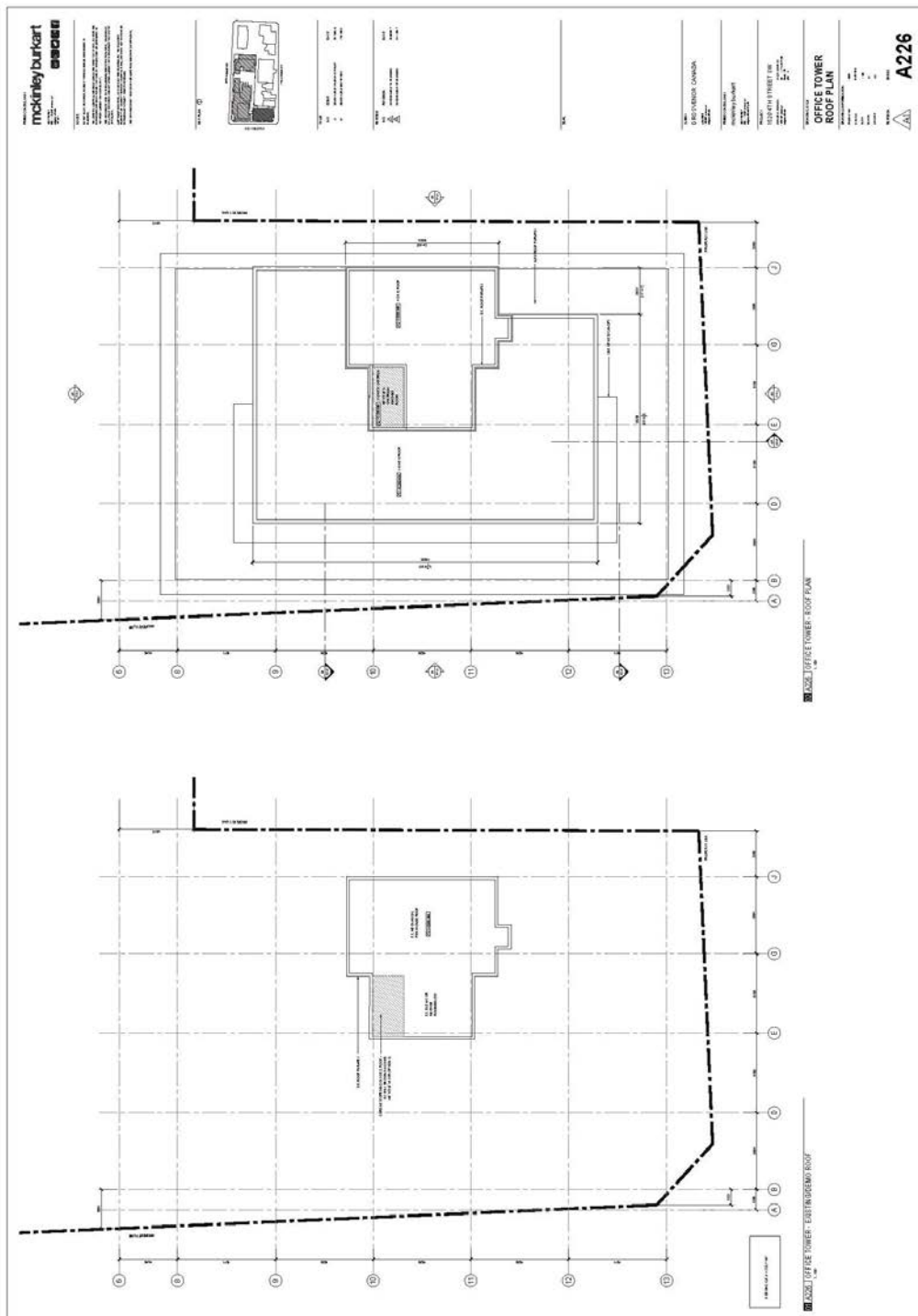
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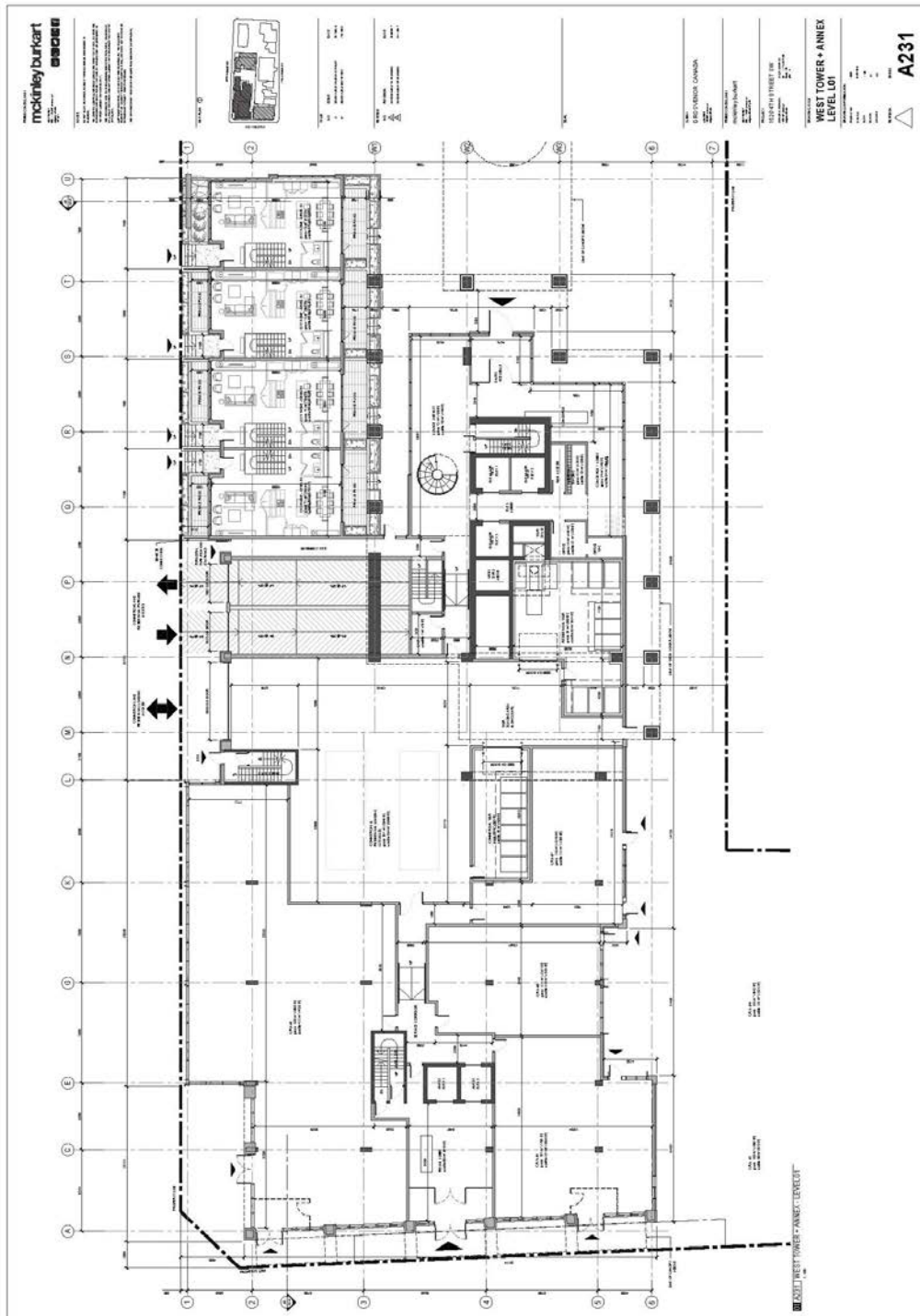
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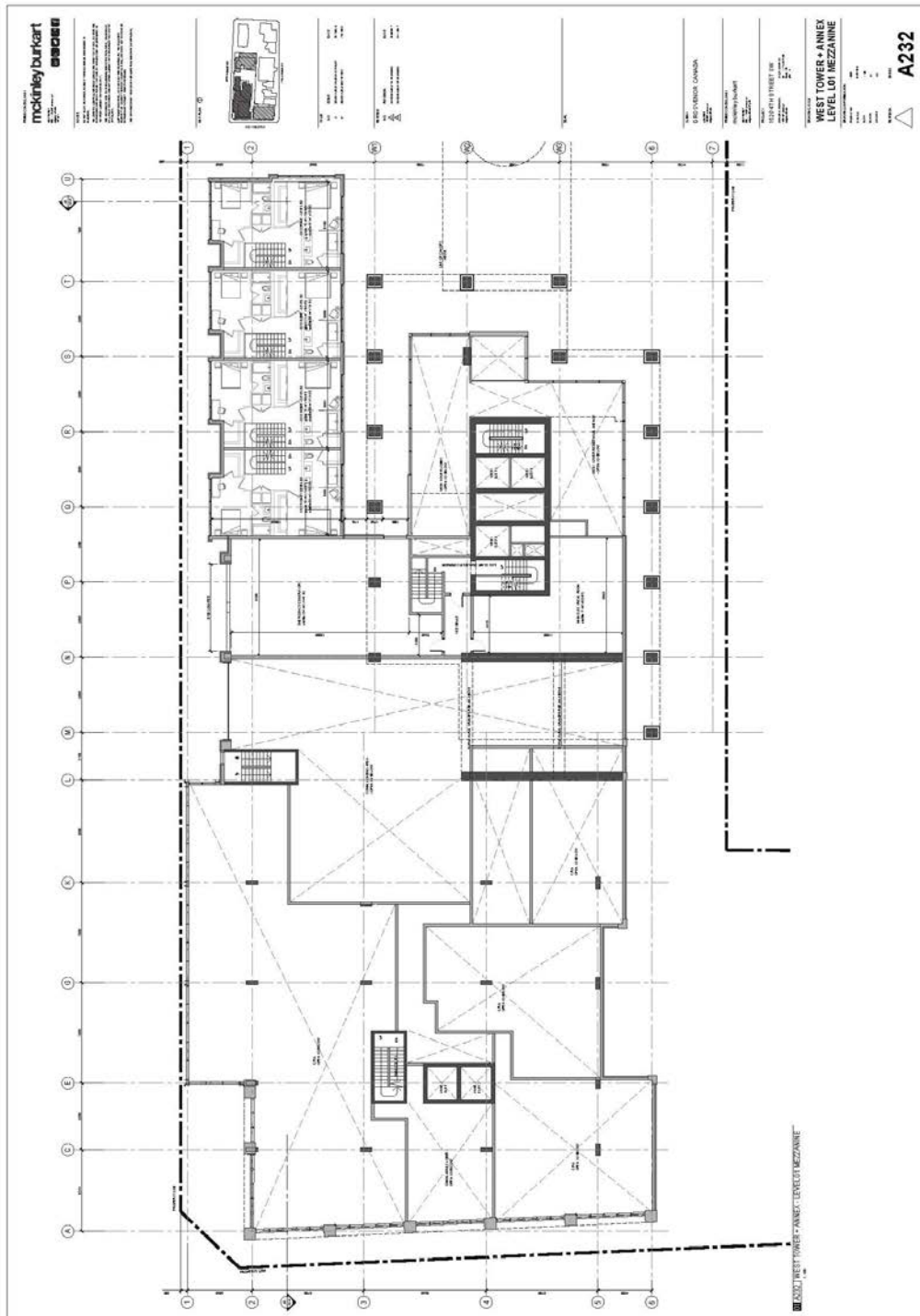
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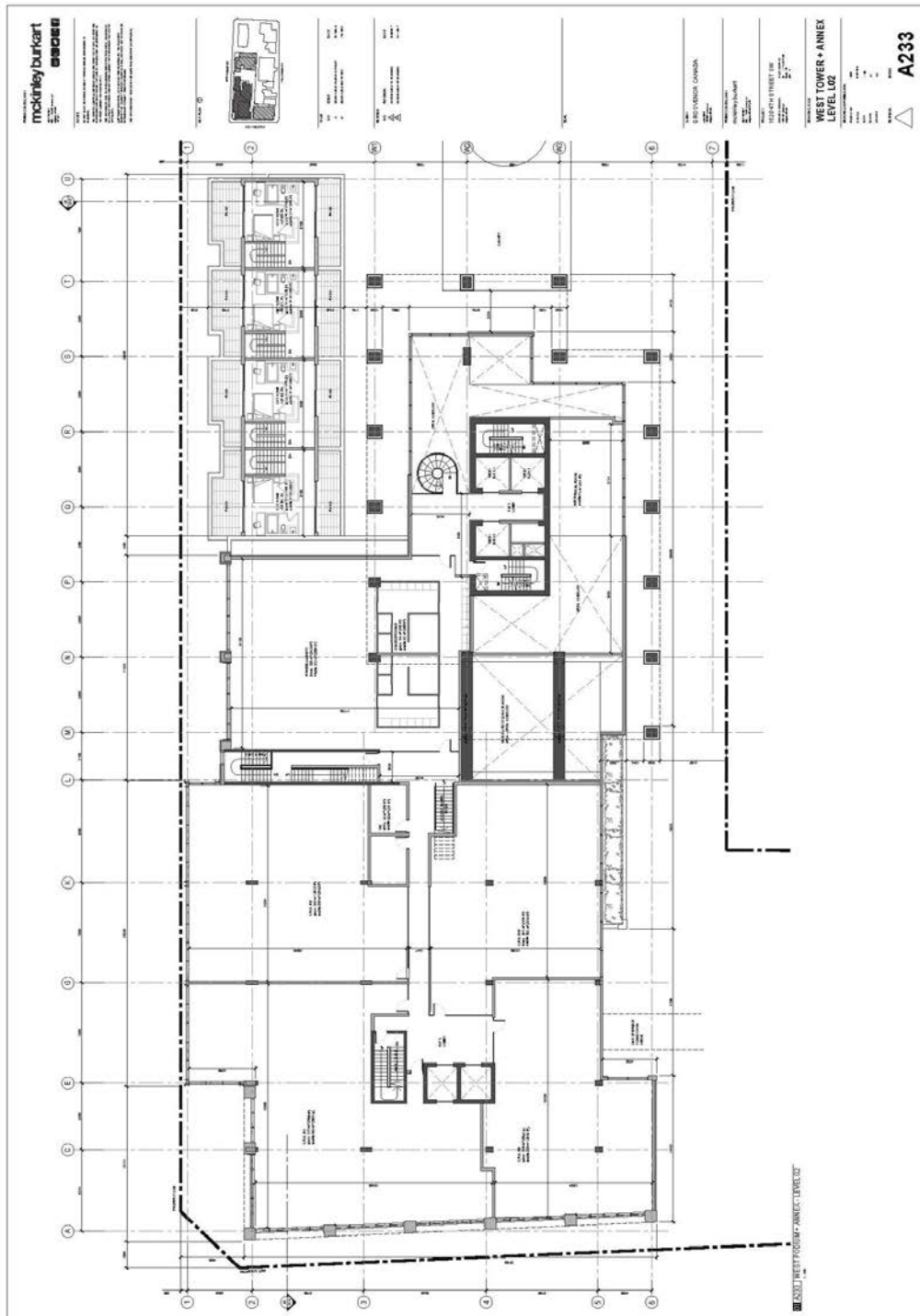
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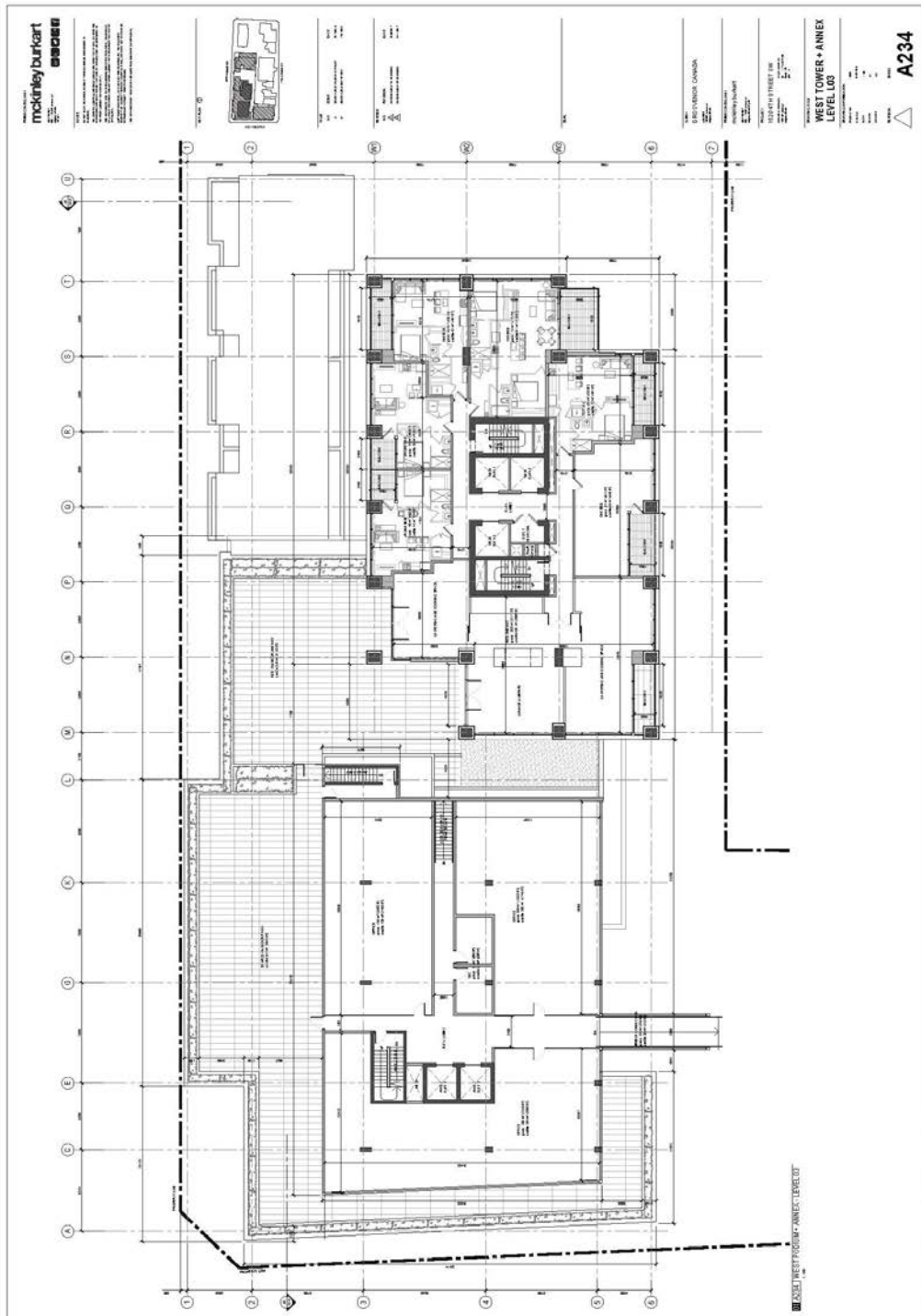
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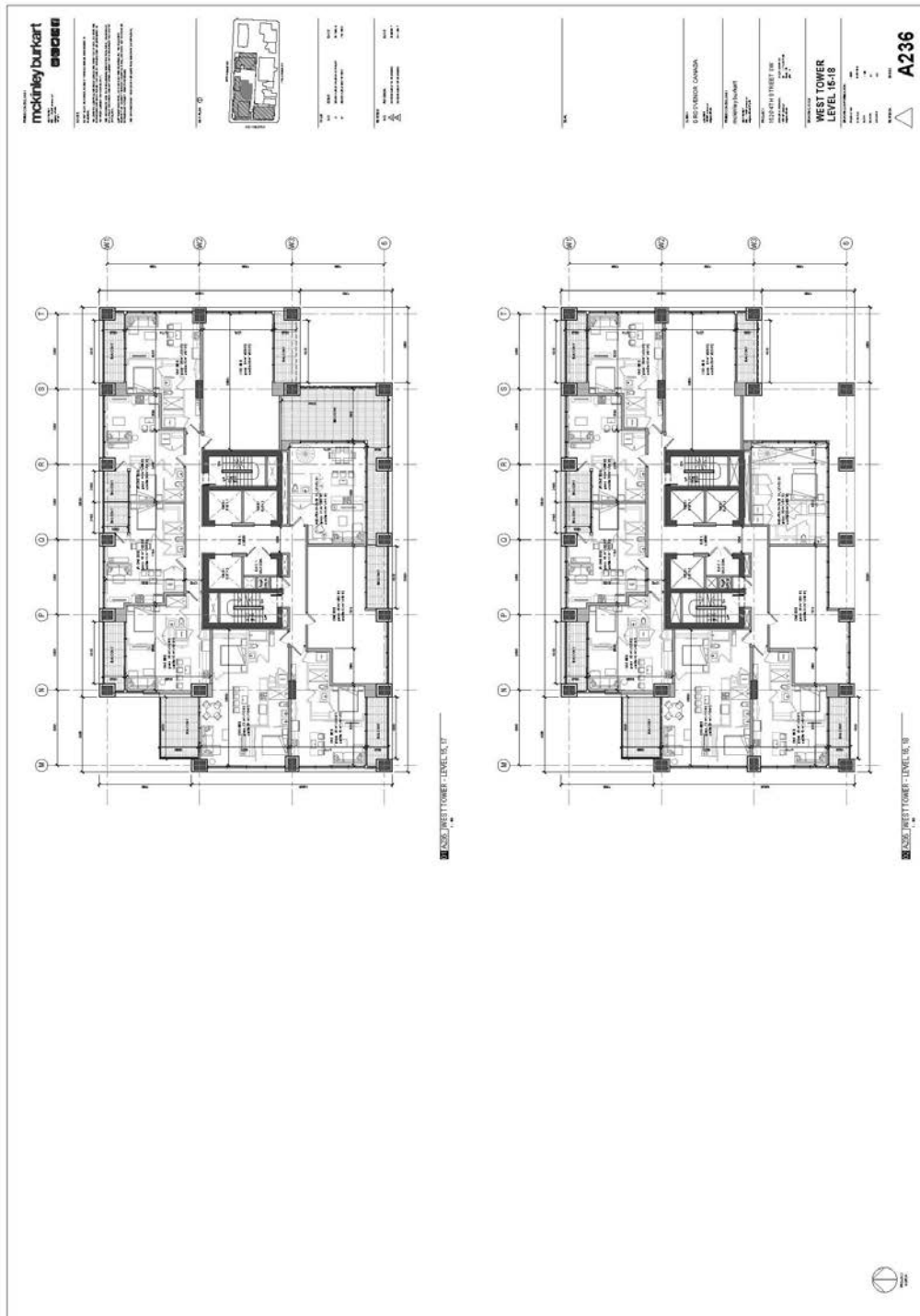
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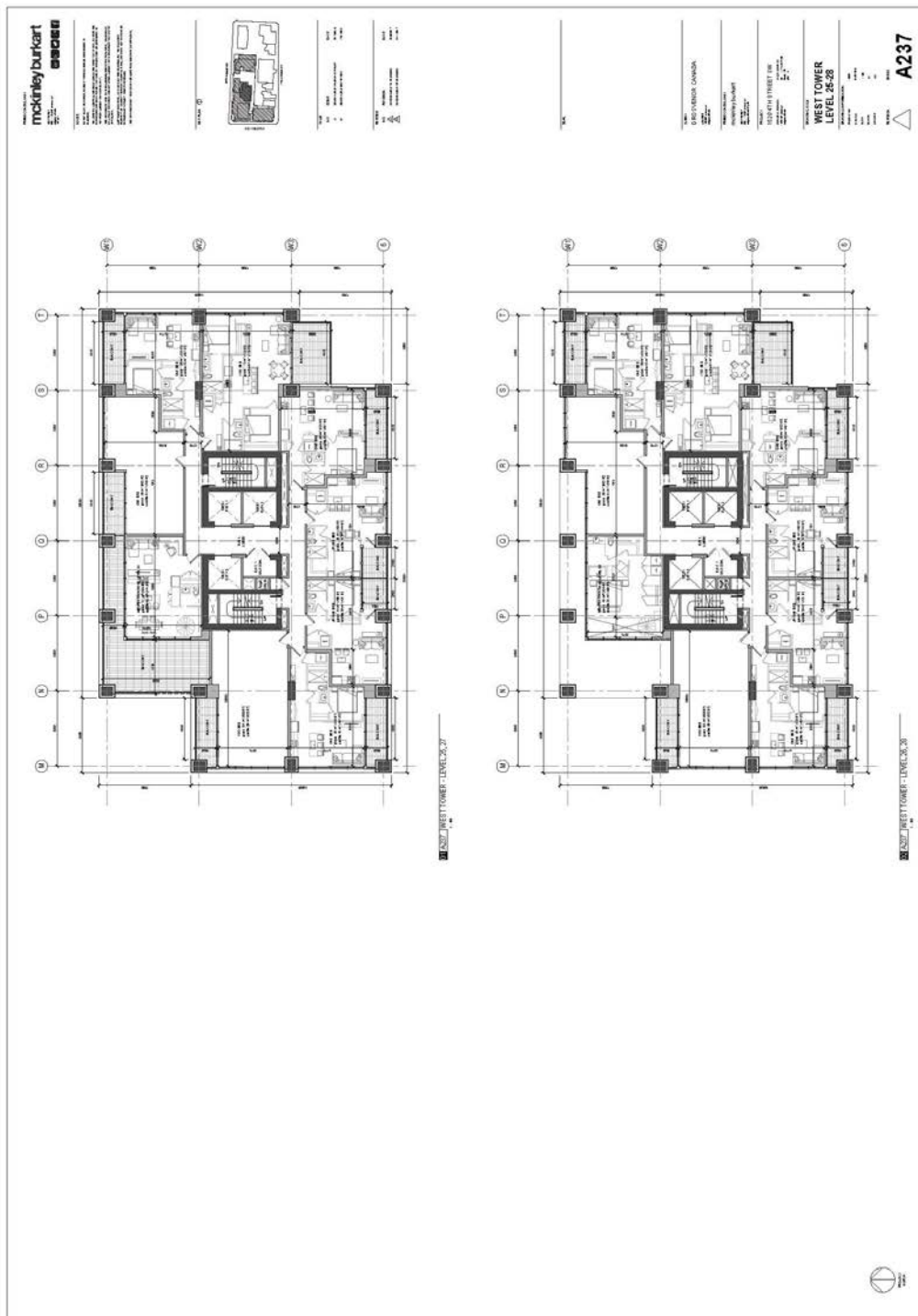
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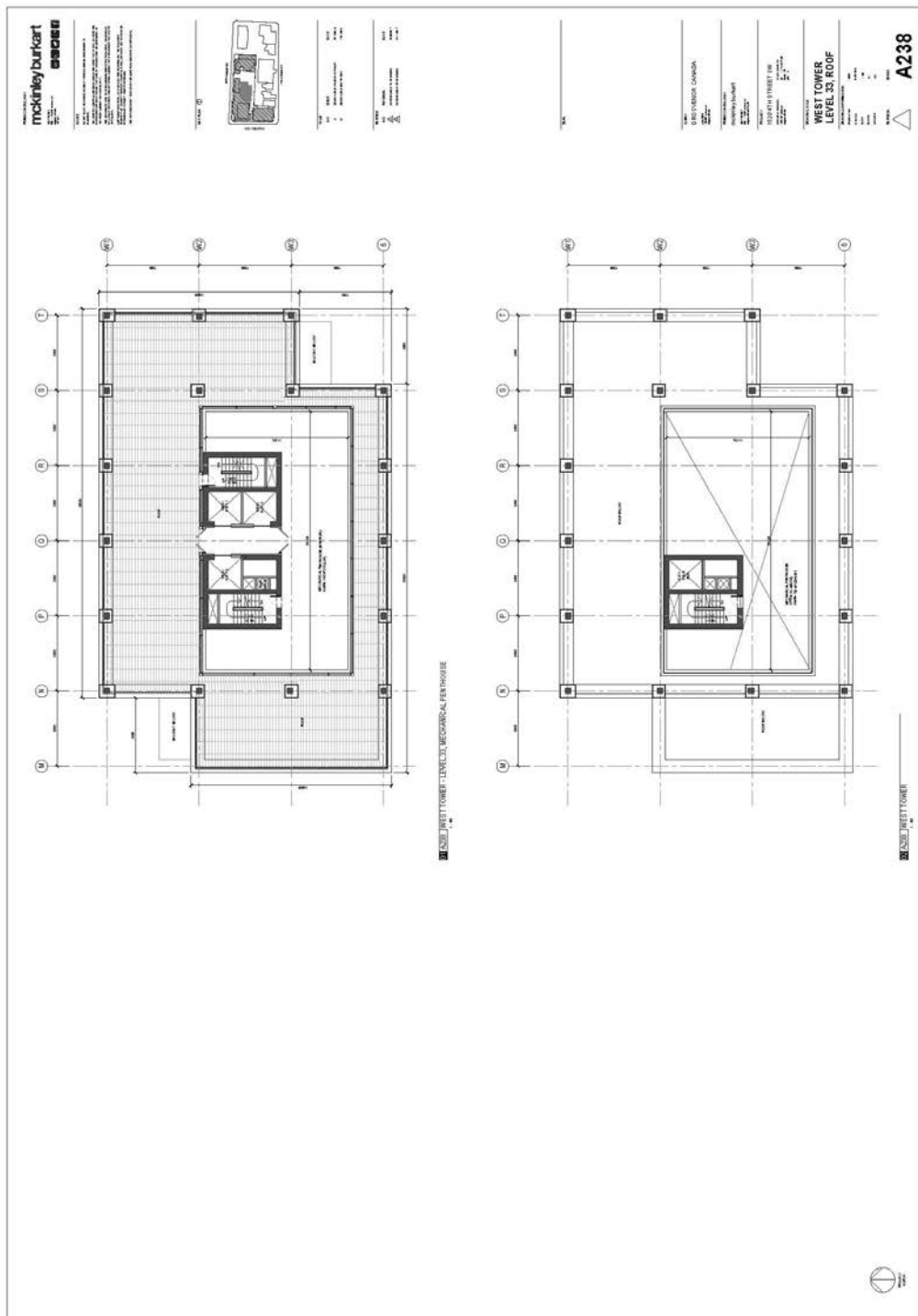
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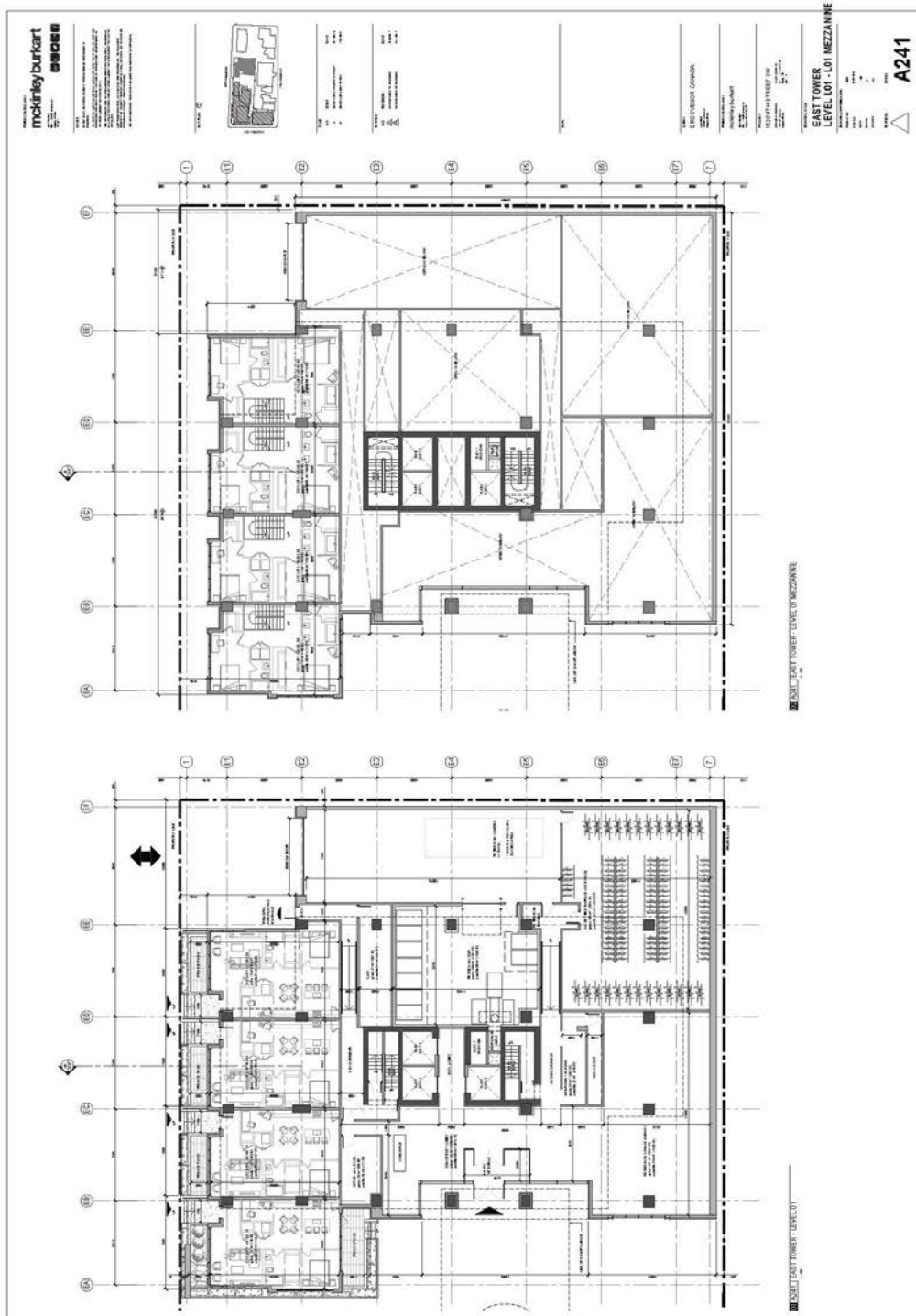
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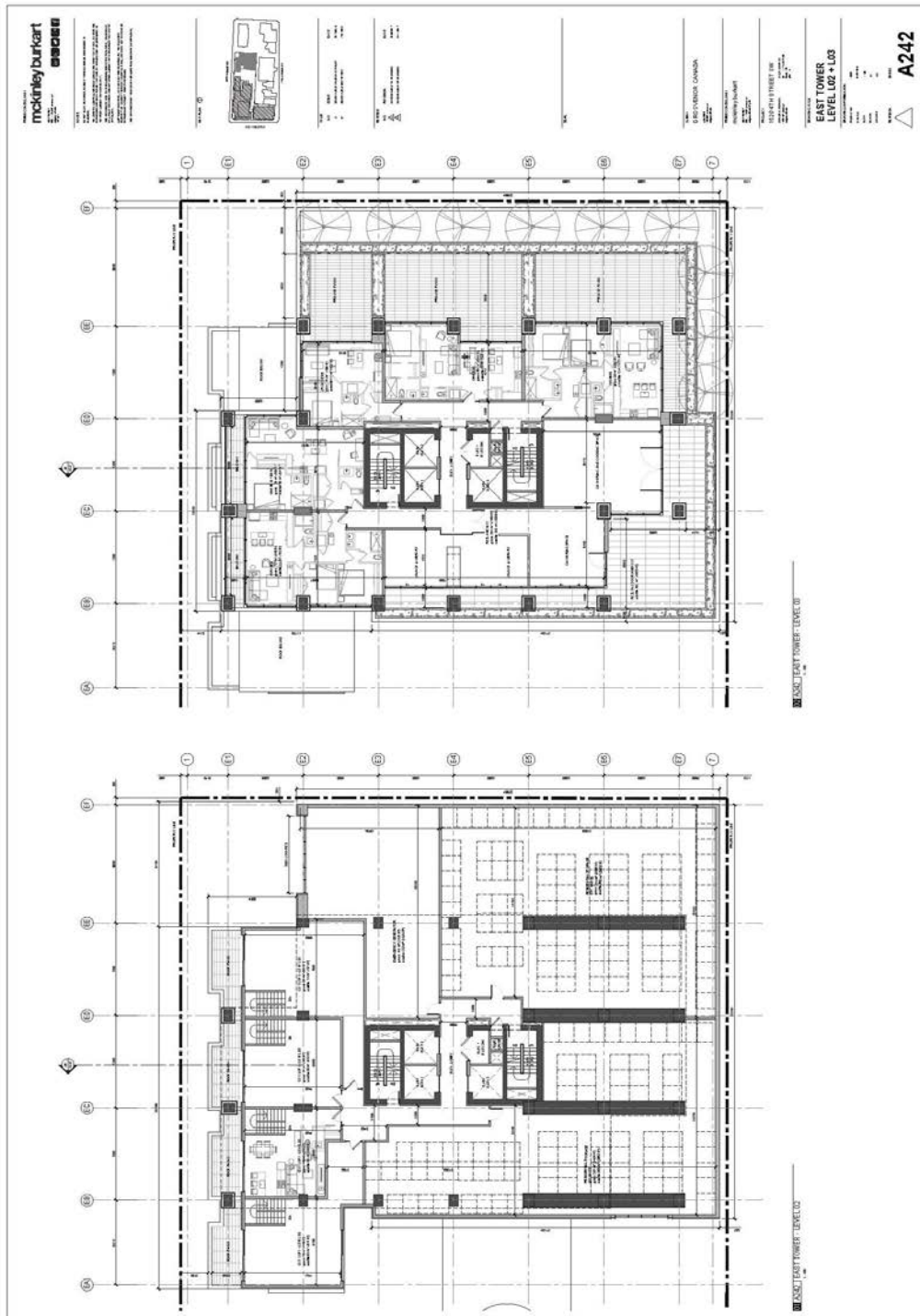
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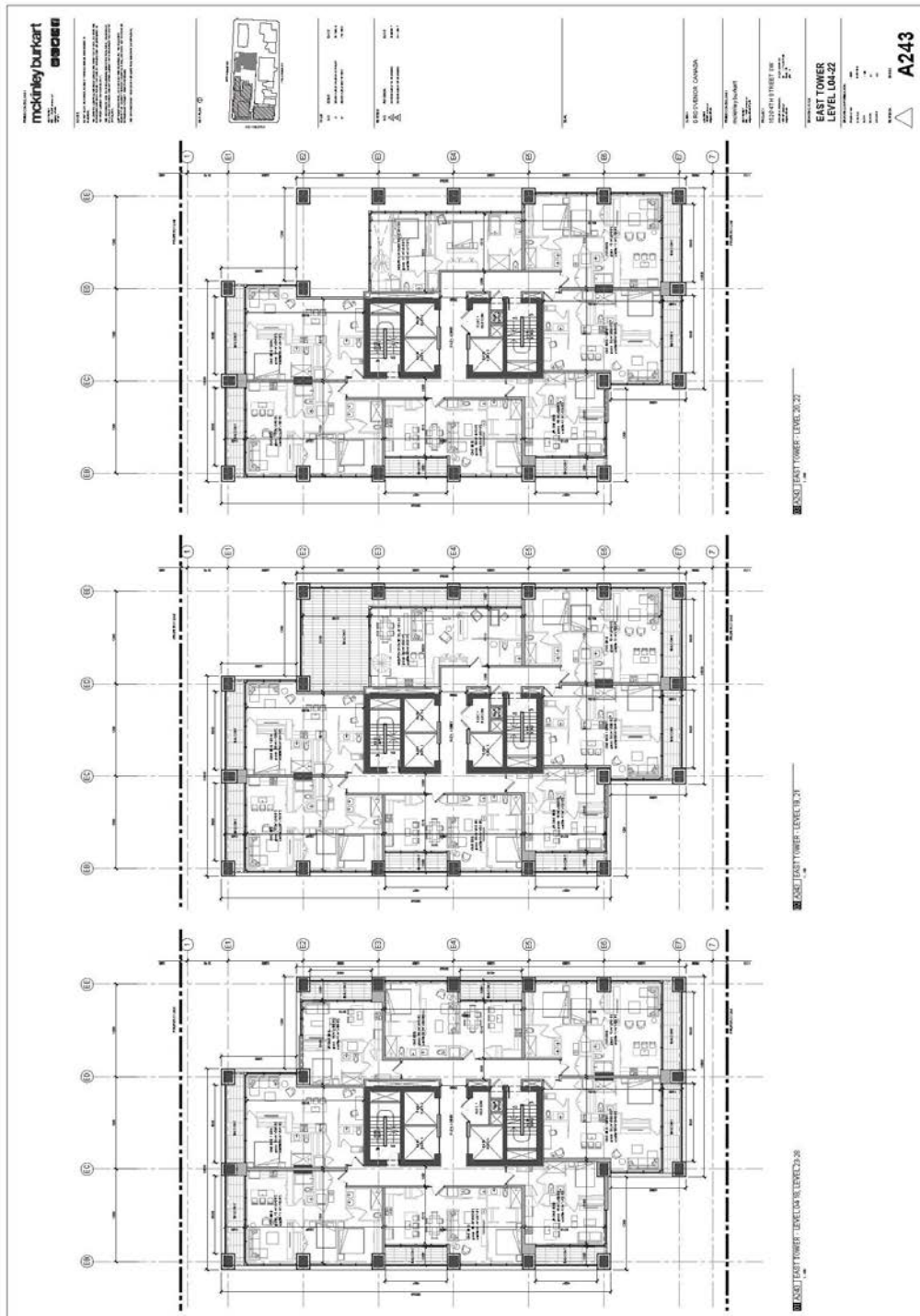
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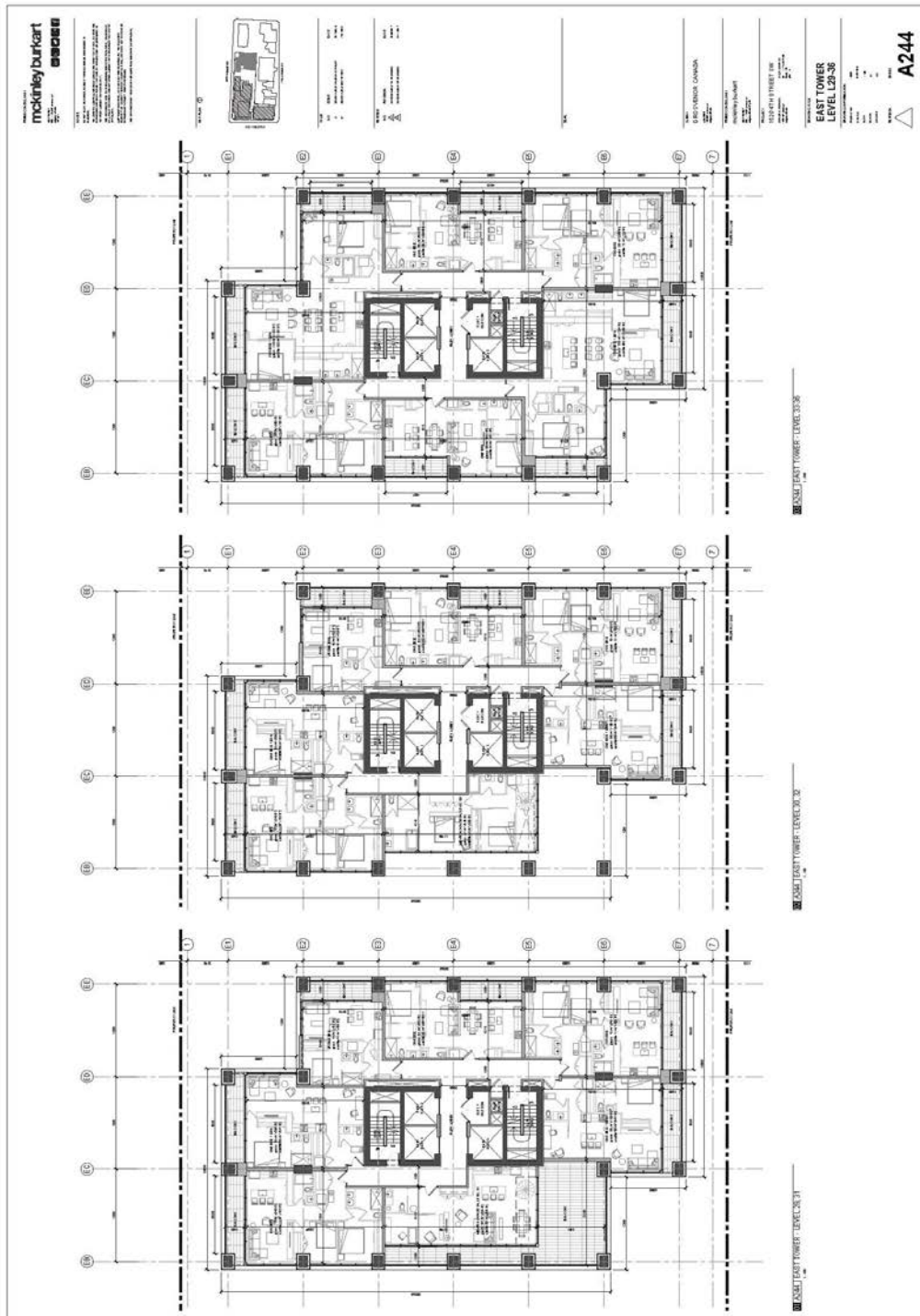
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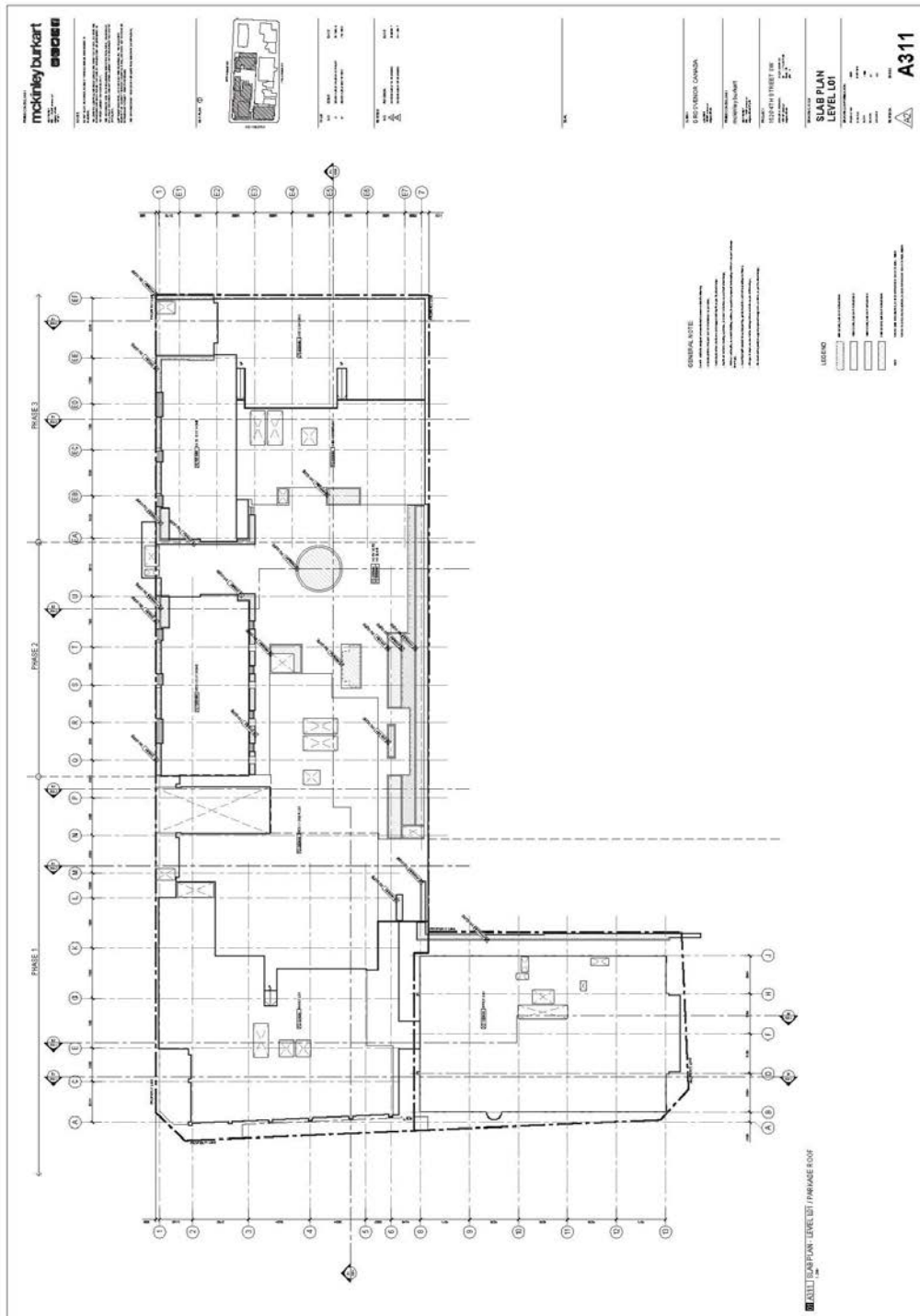
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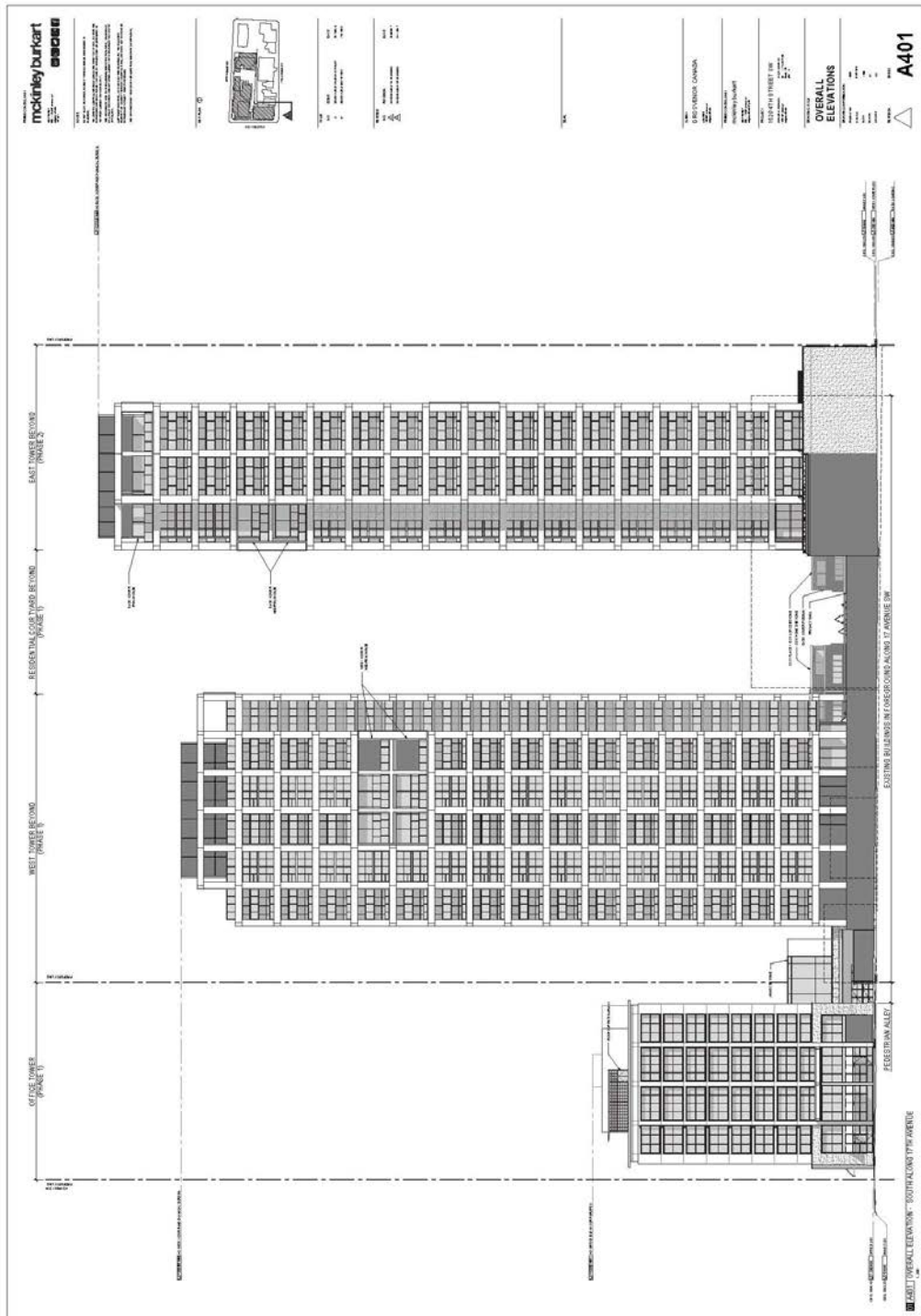
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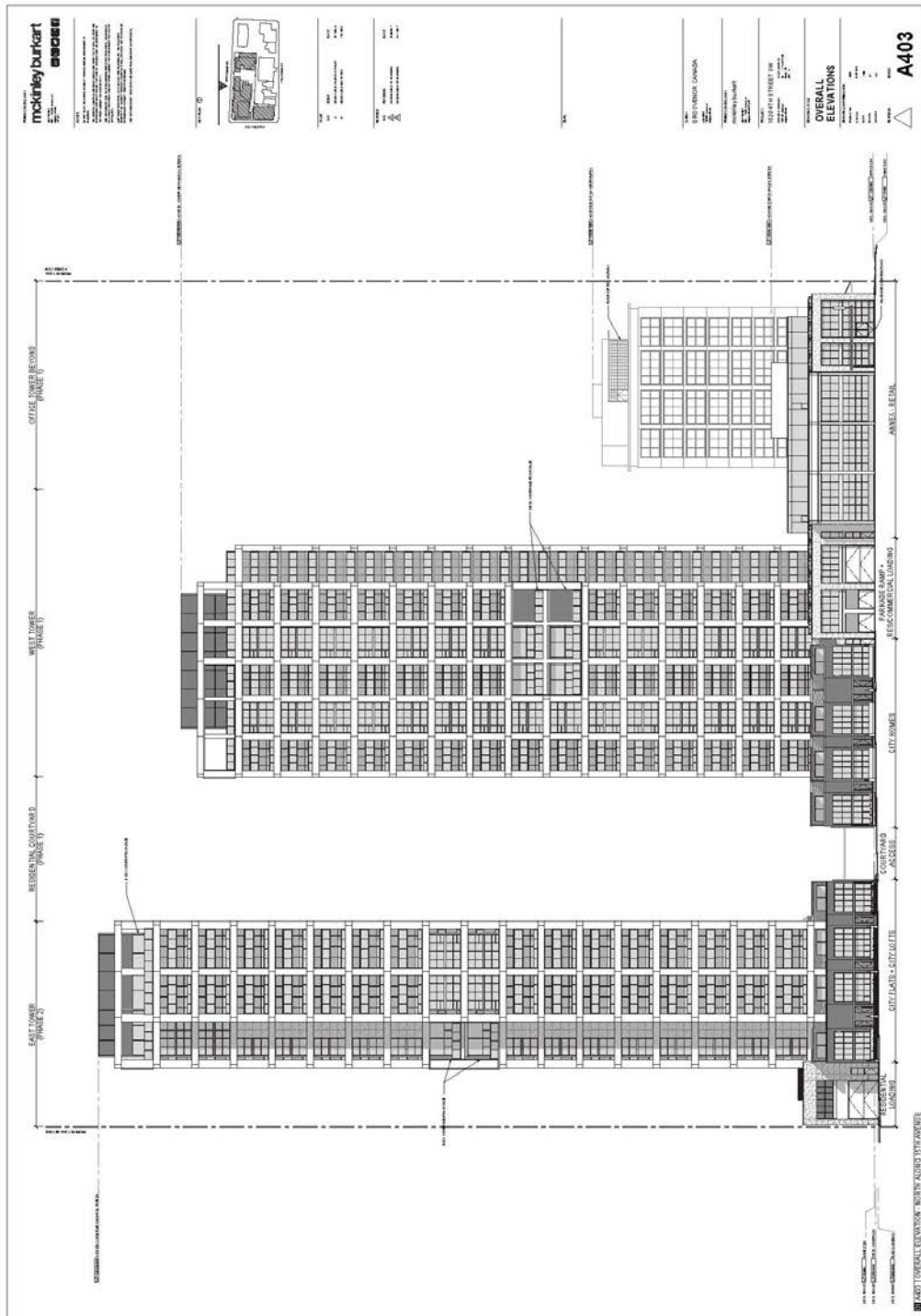
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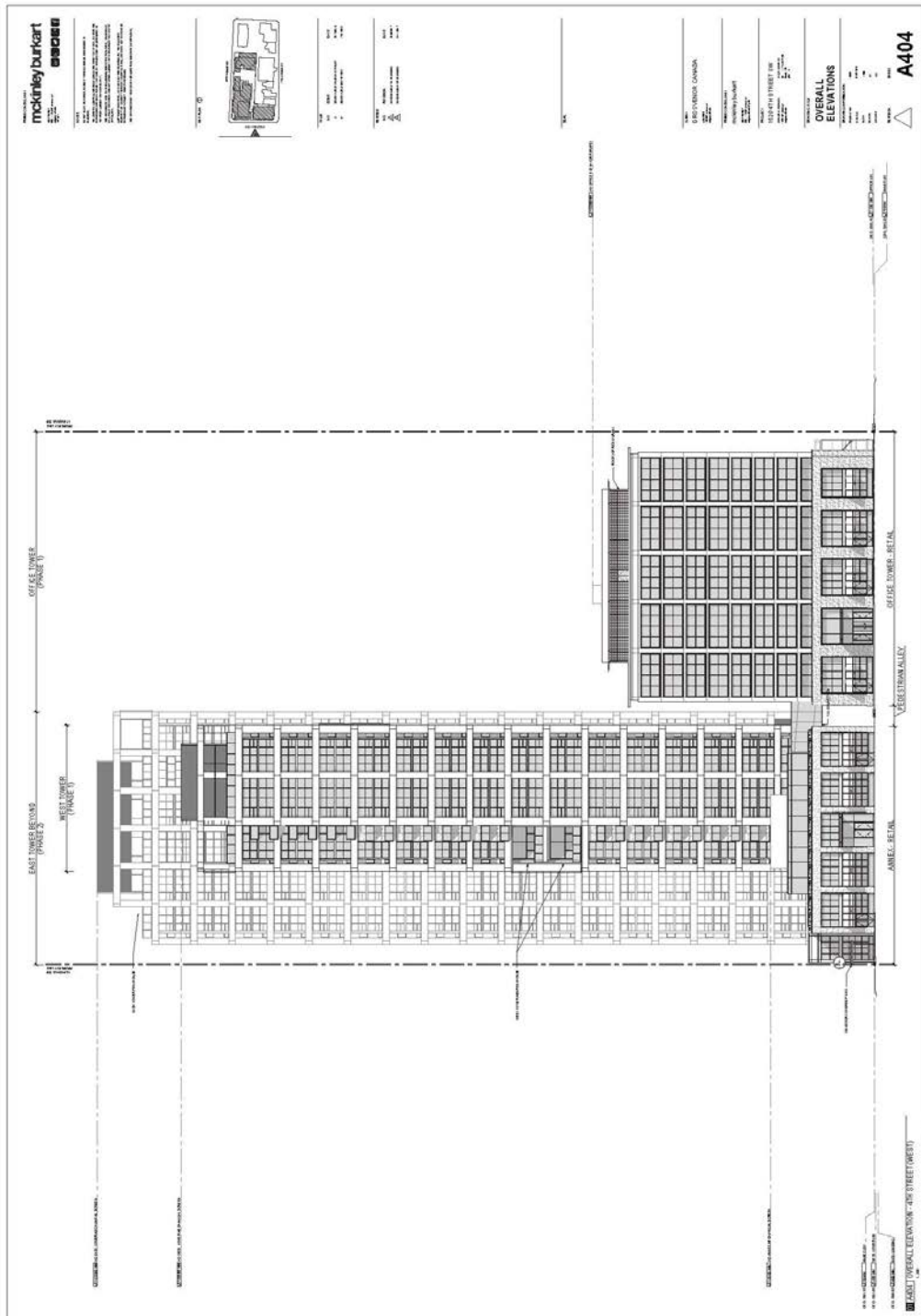
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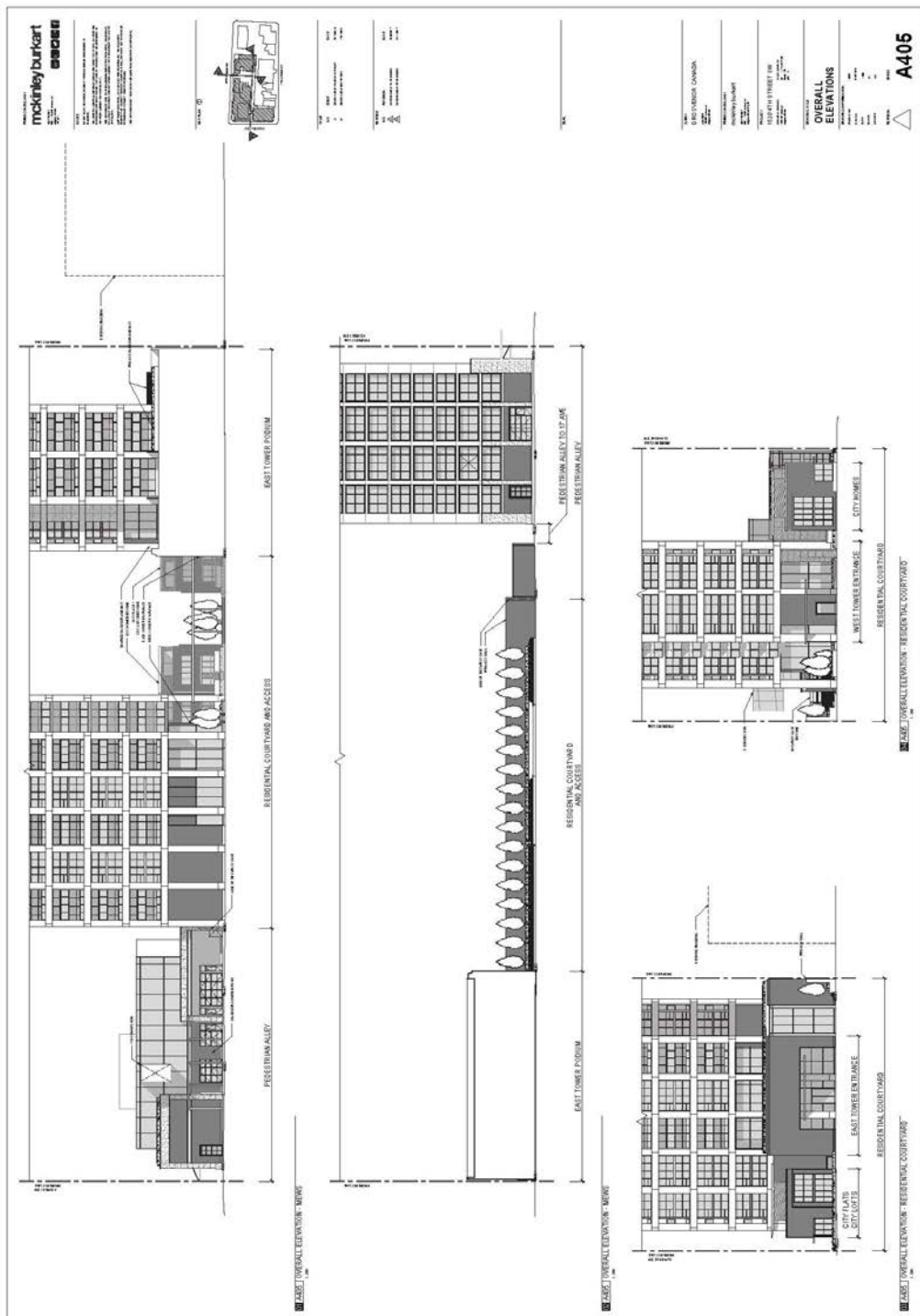
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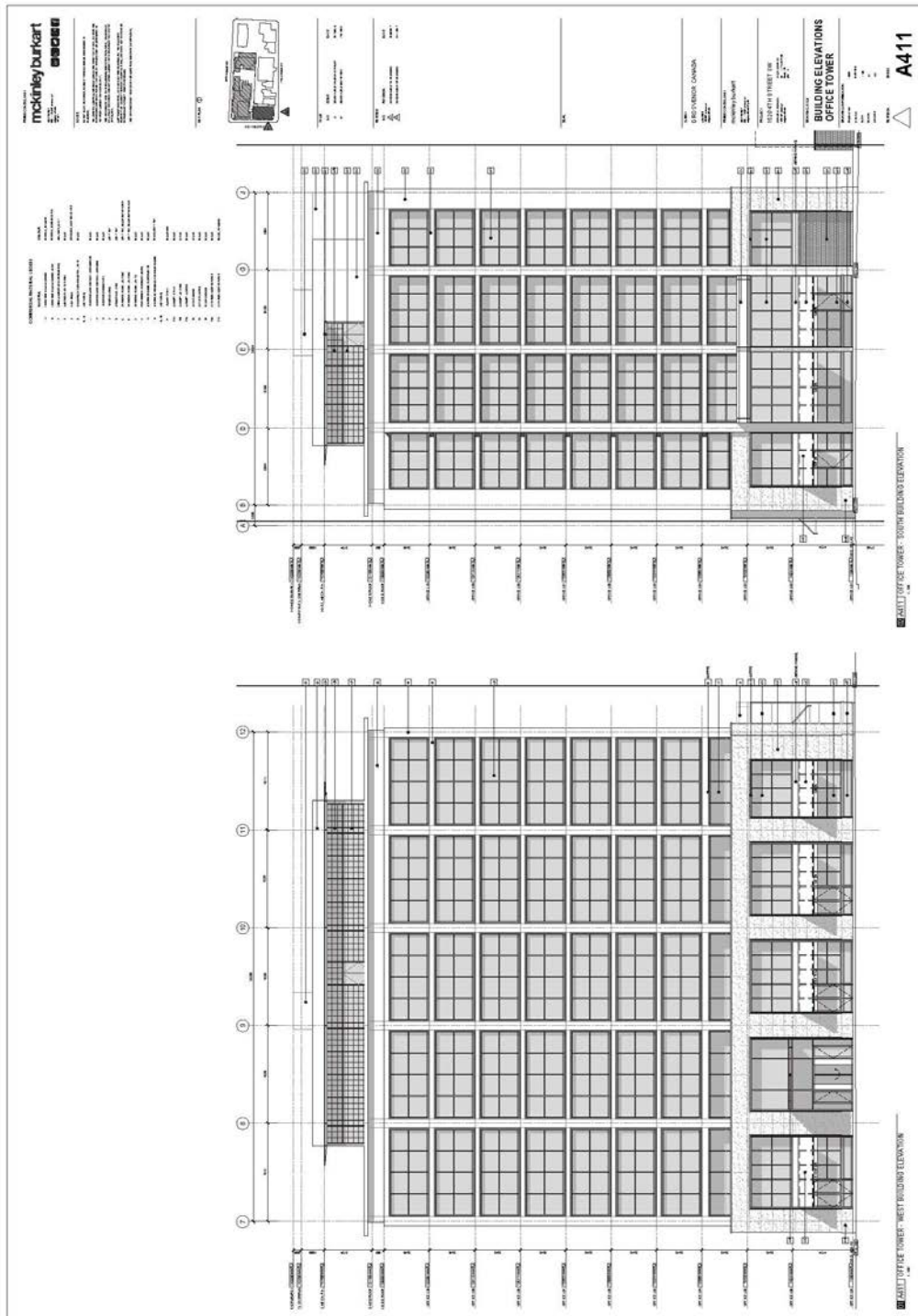
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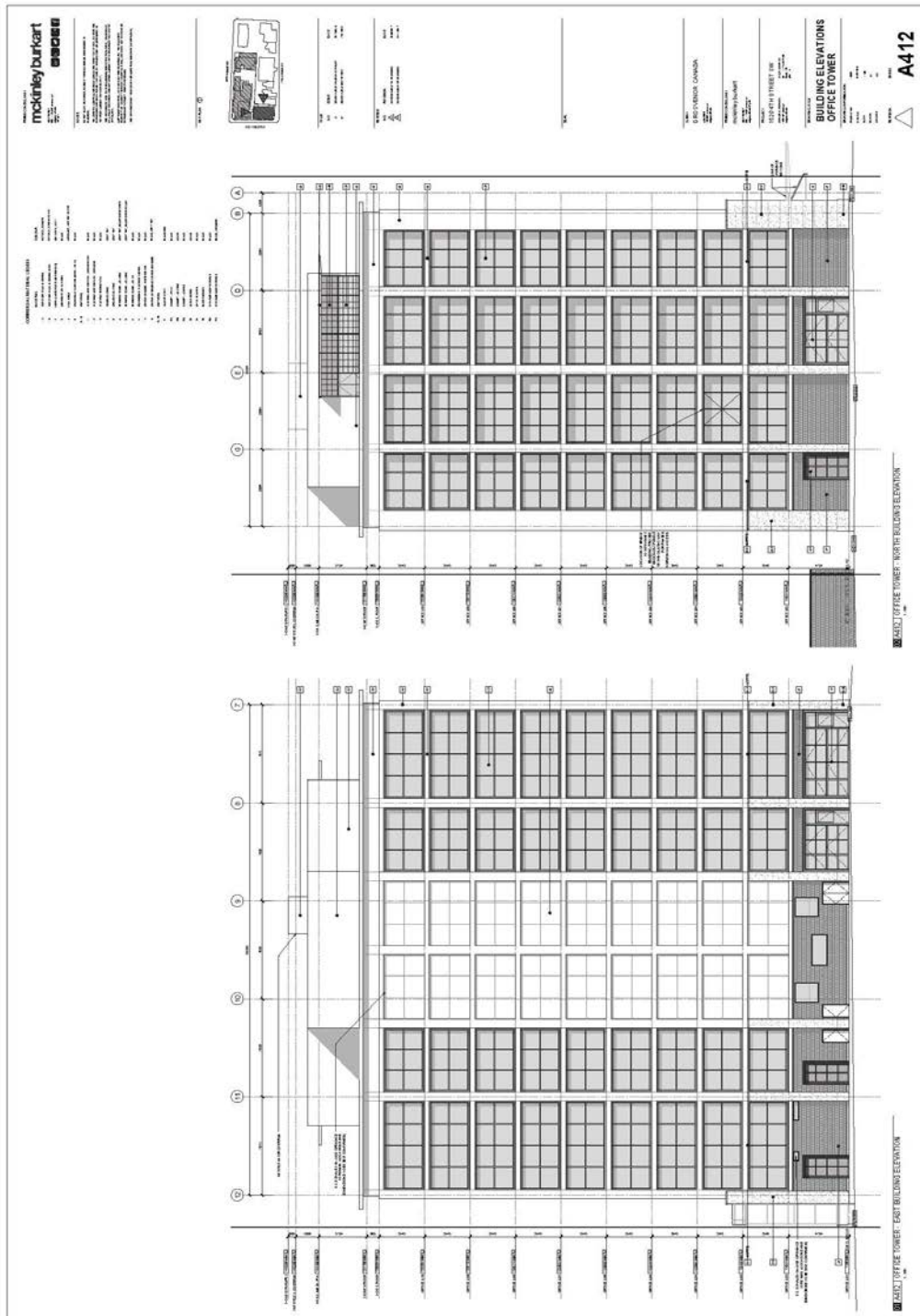
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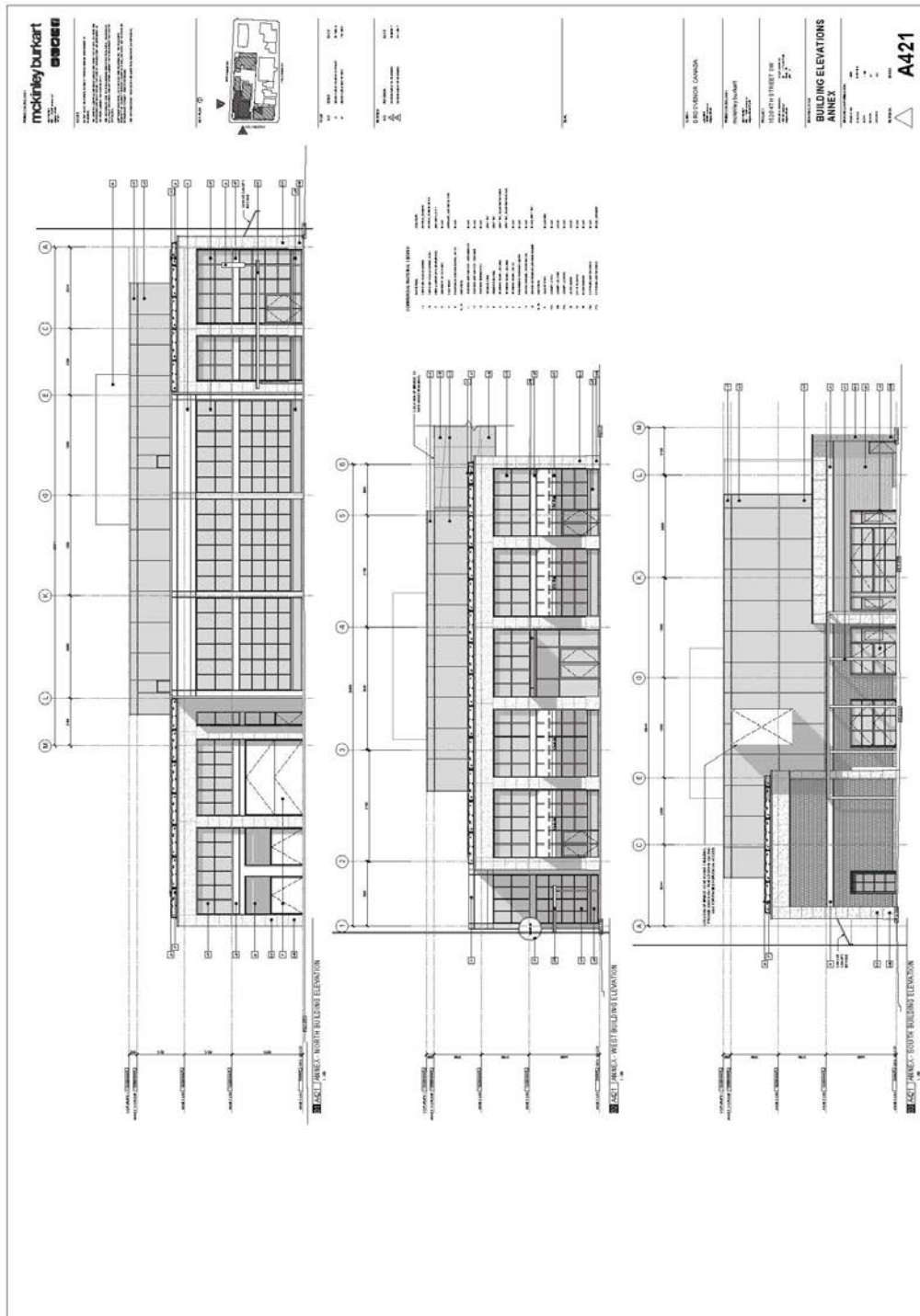
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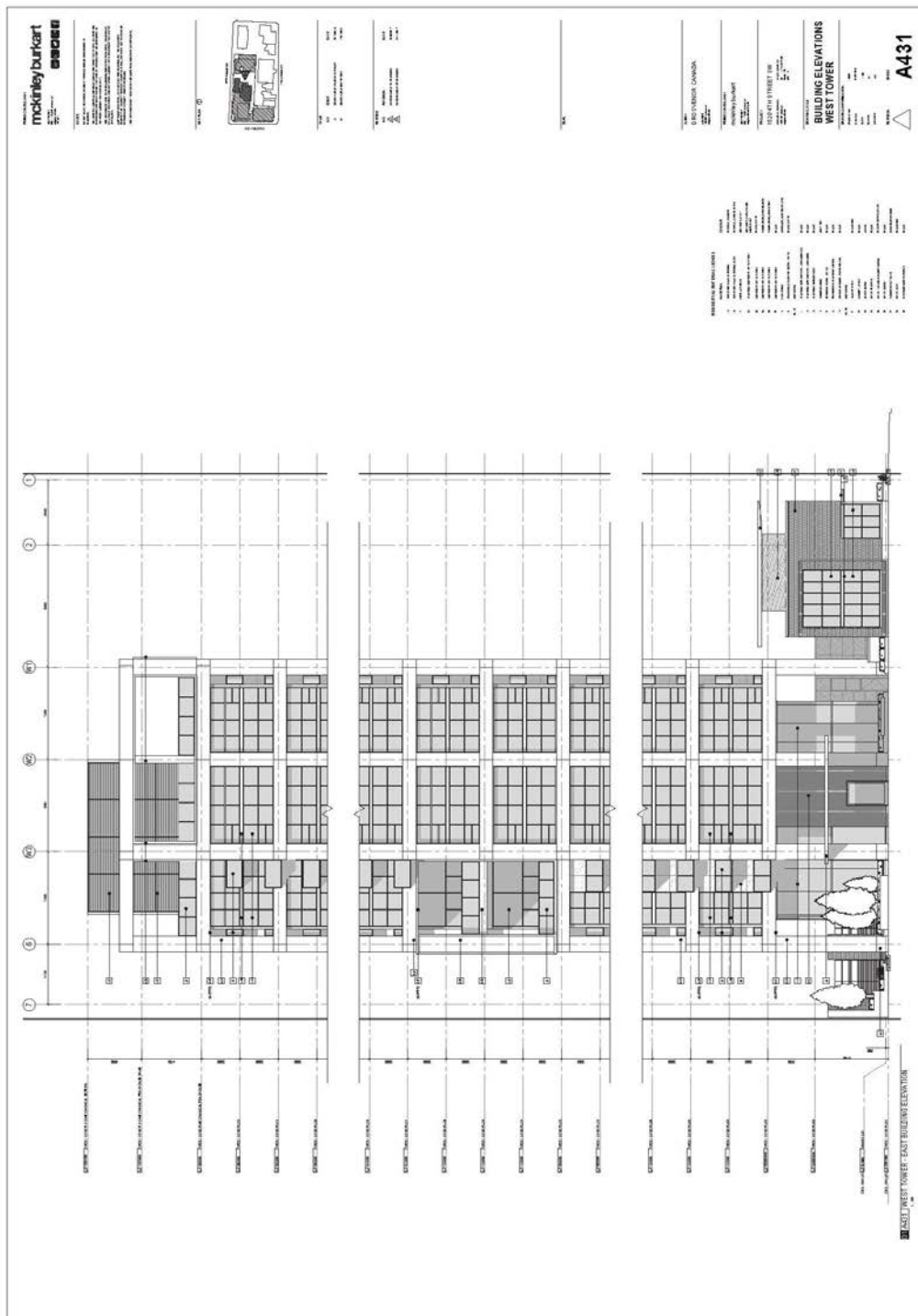
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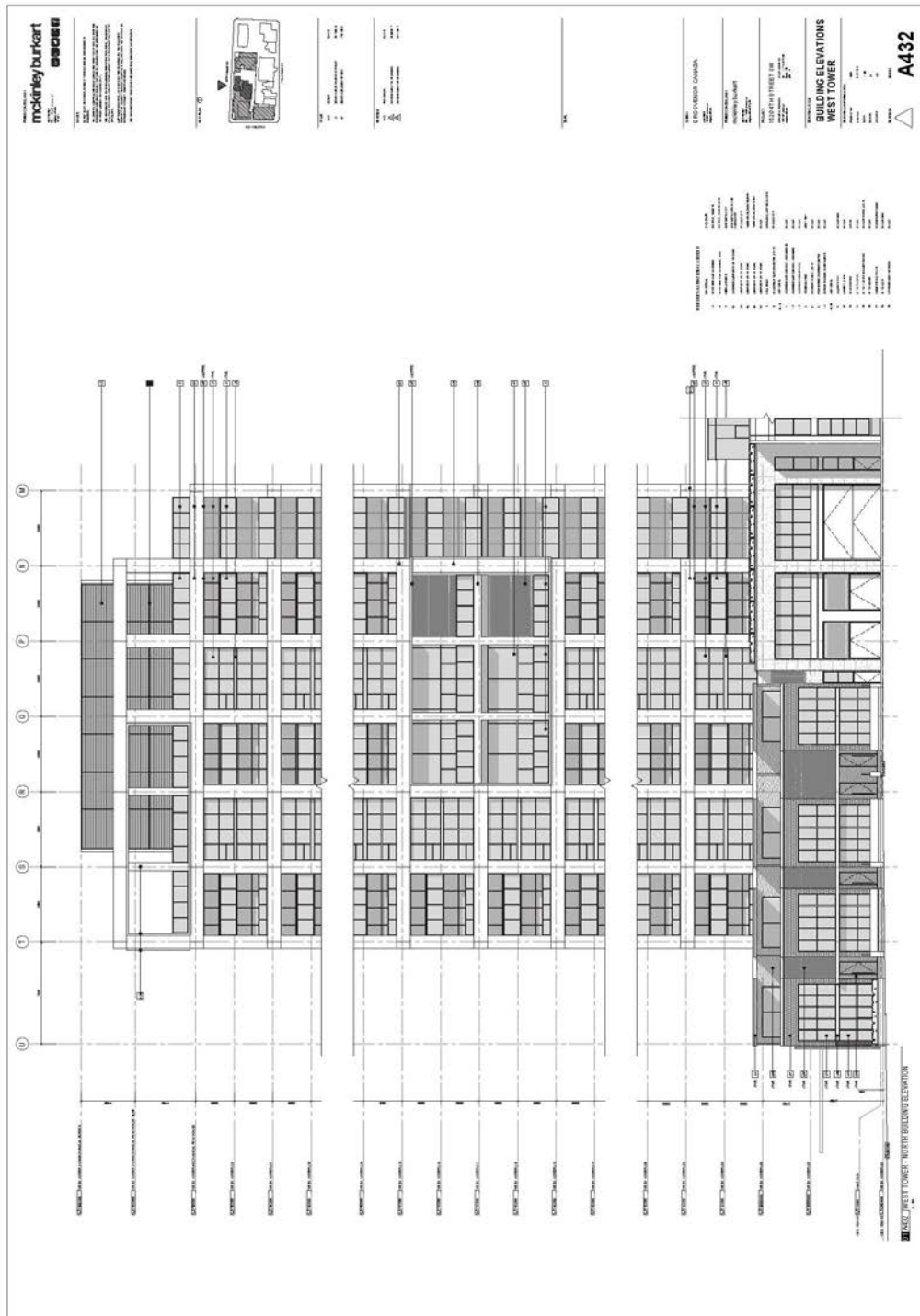
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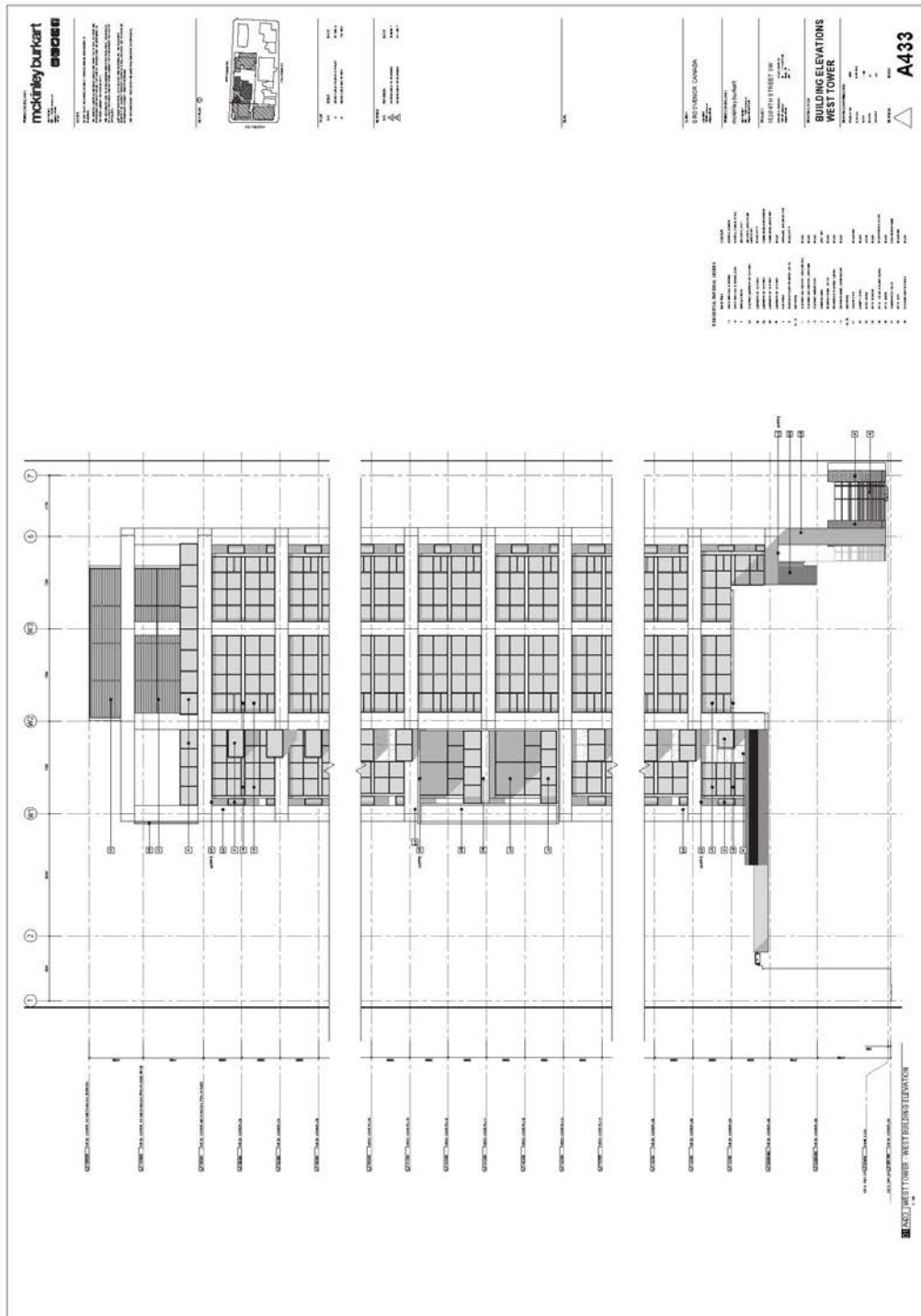
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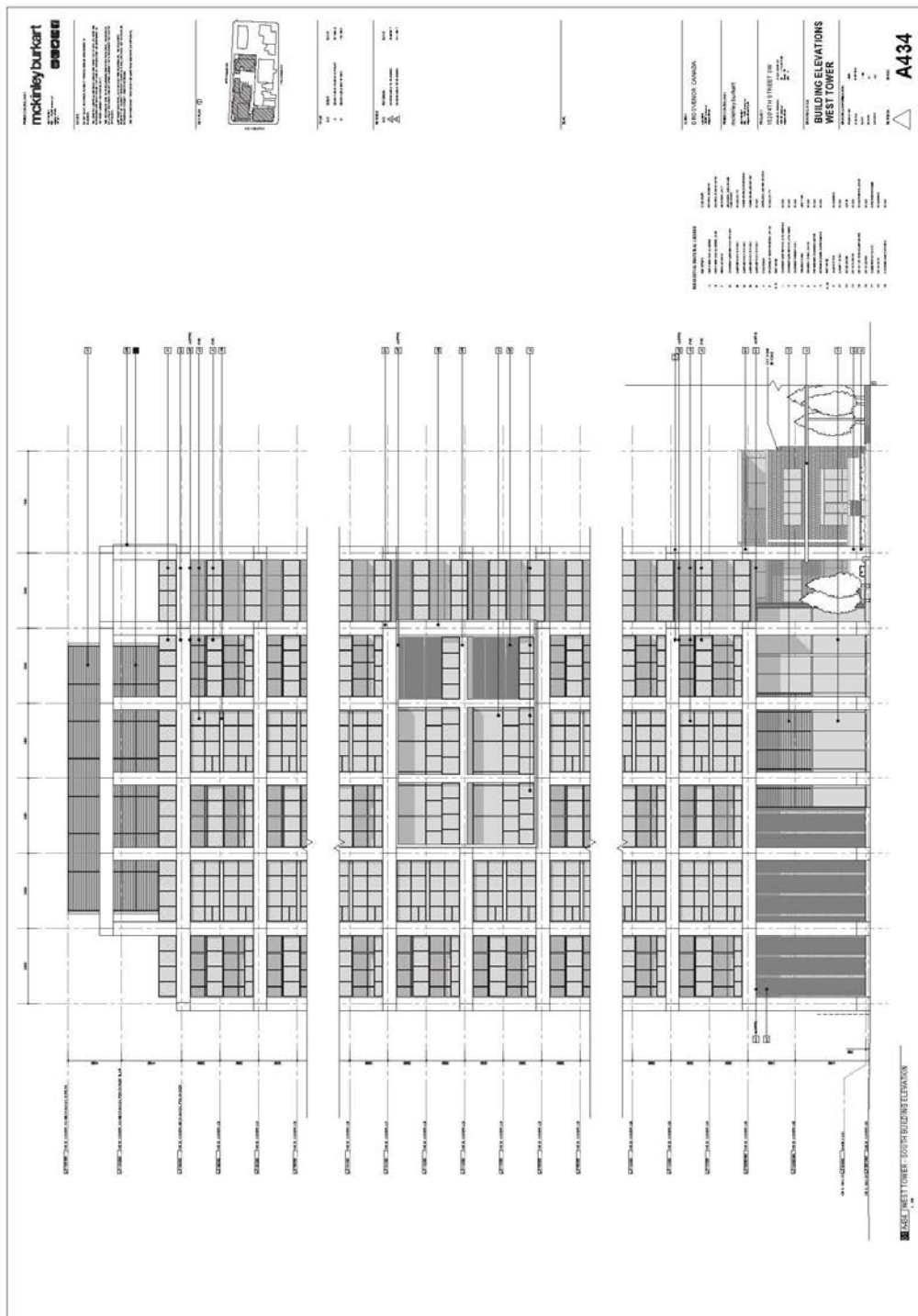
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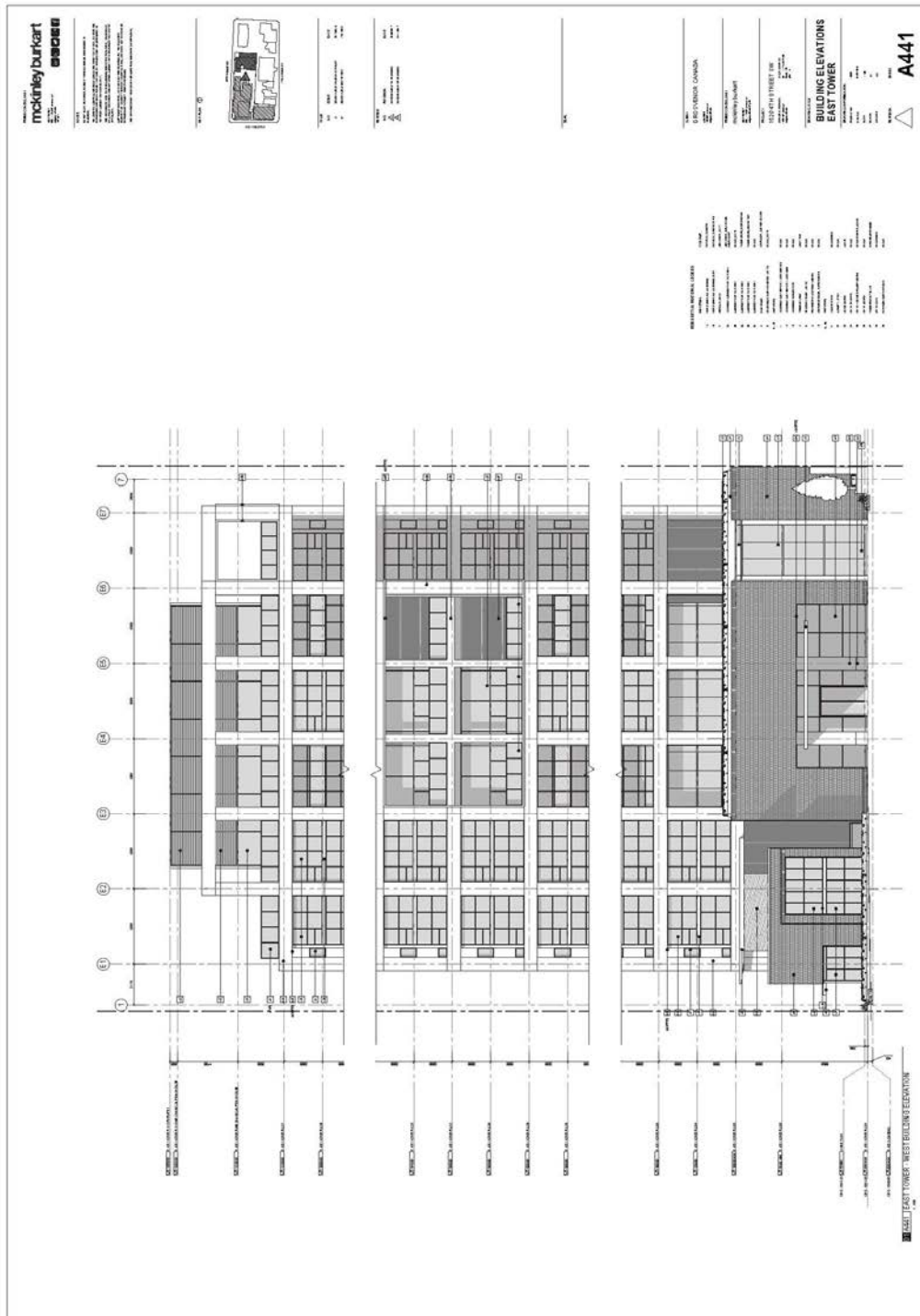
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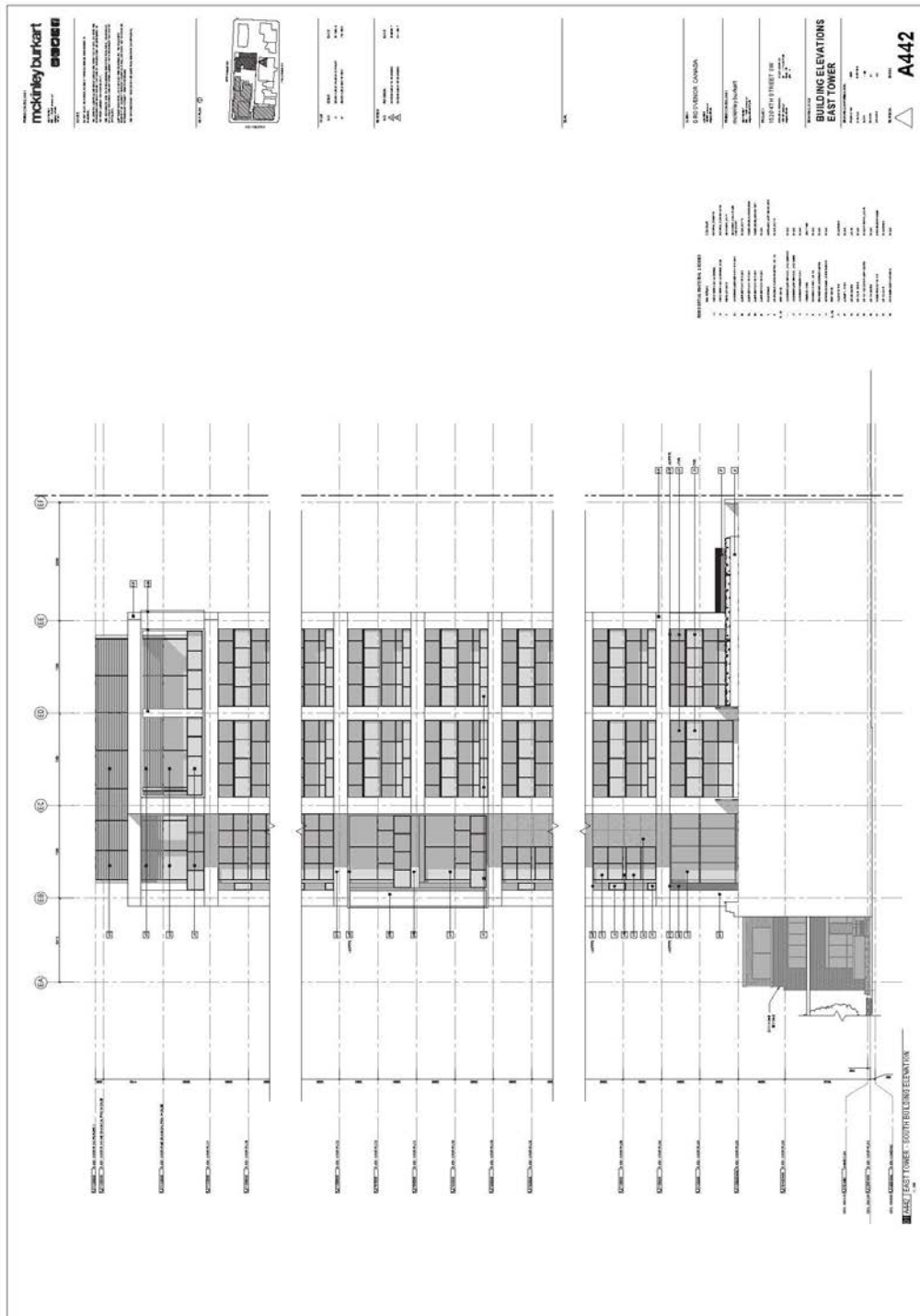
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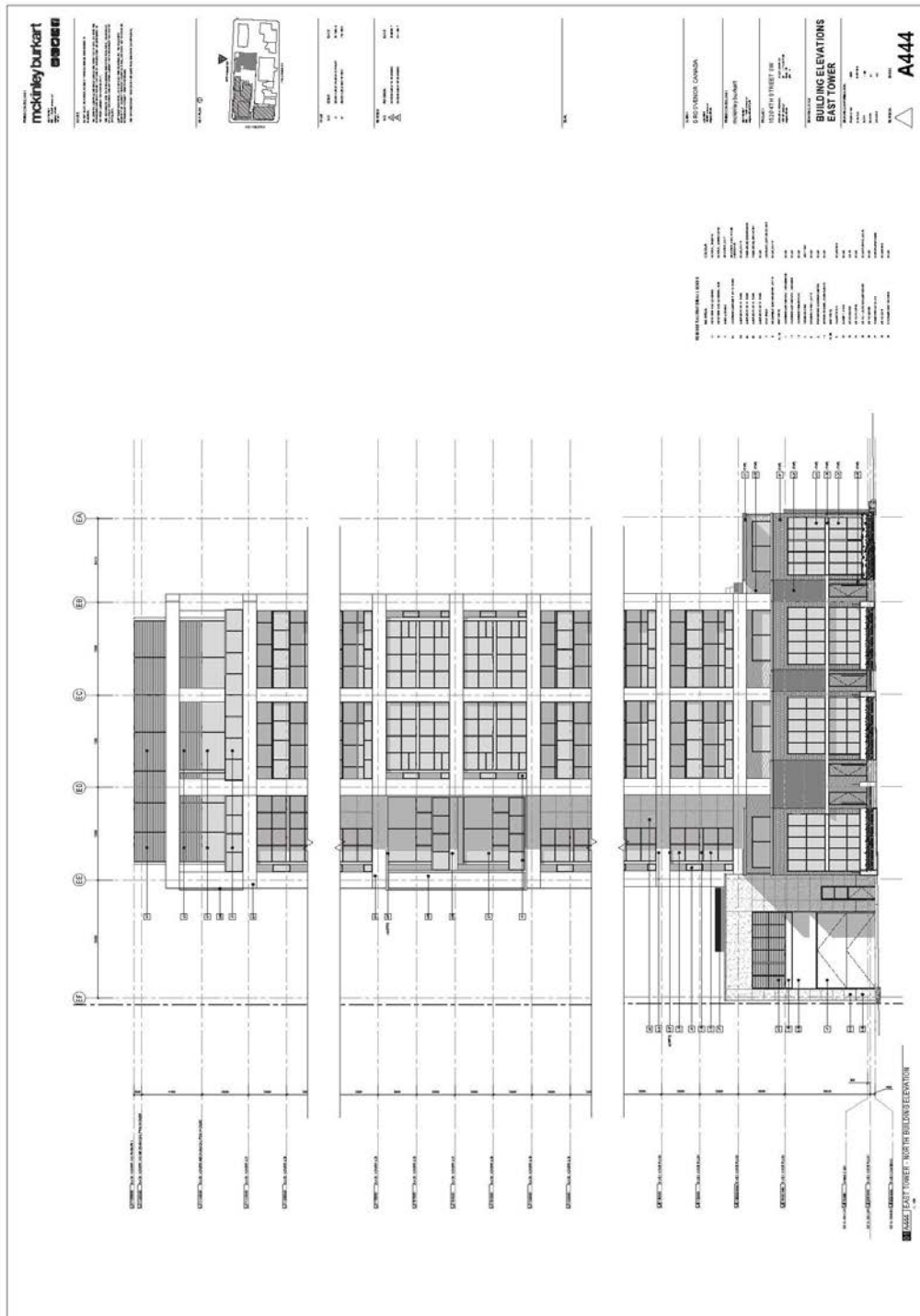
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MAP 15C

**COMMON MATERIALS**

- 1 PRECAST CONCRETE
- 2 COMPOSITE METAL PANEL MILANO GRIGIO
- 3 FACE BRICK CHARCOAL
- 4 COMPOSITE METAL PANEL GREY ASH
- 5 FIBRE REINFORCED CONCRETE IVORY
- 6 COMPOSITE METAL PANEL CHARRED WOOD
- 7 LIMESTONE SANDBLASTED
- 8 COMPOSITE METAL PANEL IVORY
- 9 LIMESTONE SMOOTH
- 10 COMPOSITE METAL PANEL BLACK
- 11 ALUMINUM GLAZING SYSTEM BLACK
- 12 CANVAS ACCENT BLACK
- 13 GLAZING GROOVED
- 14 GLAZING CLEAR
- 15 MECHANICAL LOUVRES BLACK
- 16 GLAZING GREY
- 17 STEEL ACCENT BLACKENED

**BUILDING MATERIALS**

**A451**

**McKinley Burkart**

**1504 17th Street SW**

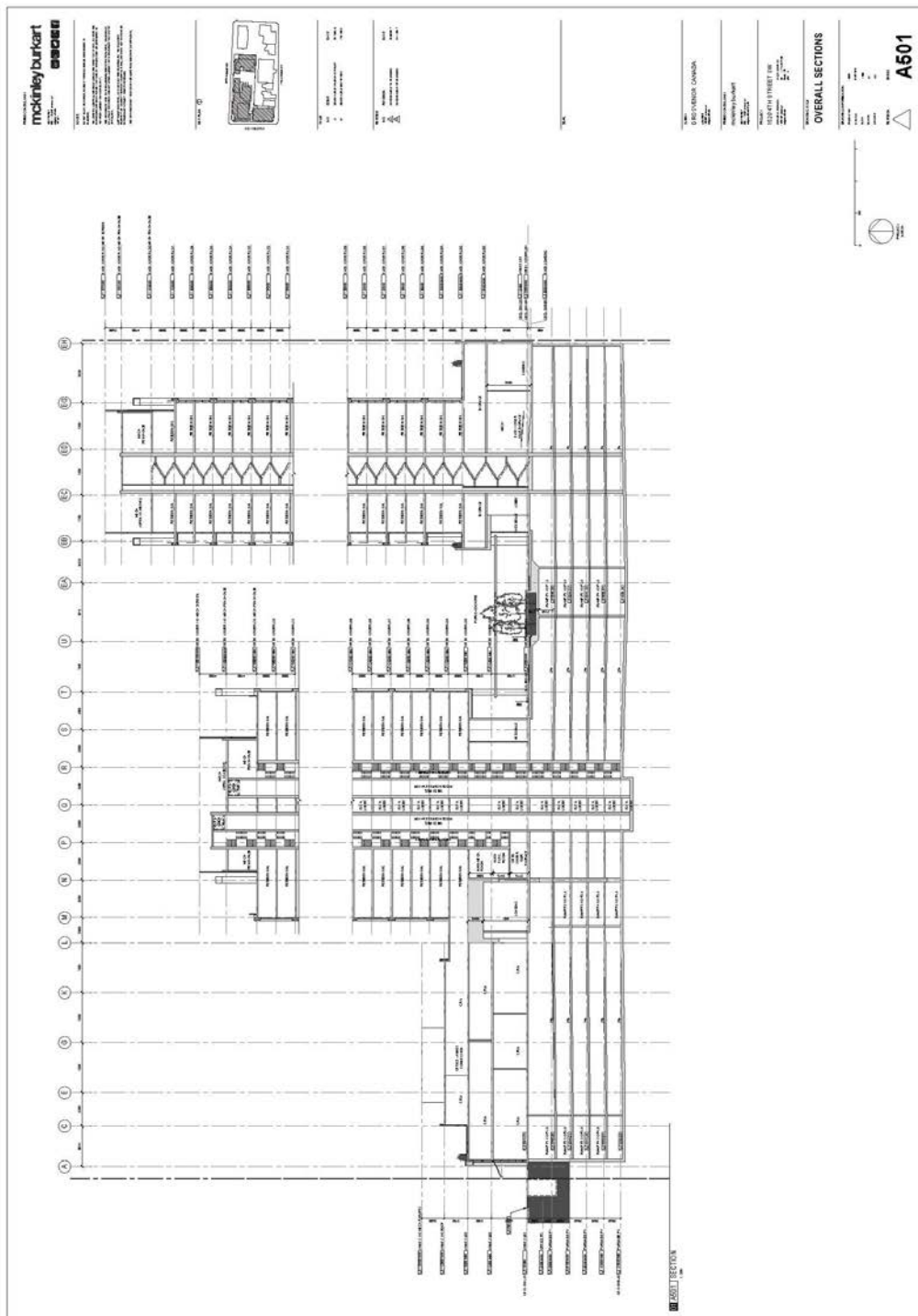
**Calgary, Alberta T2M 0K7**

**Phone: (403) 243-1111**

**www.mckinleyburkart.com**

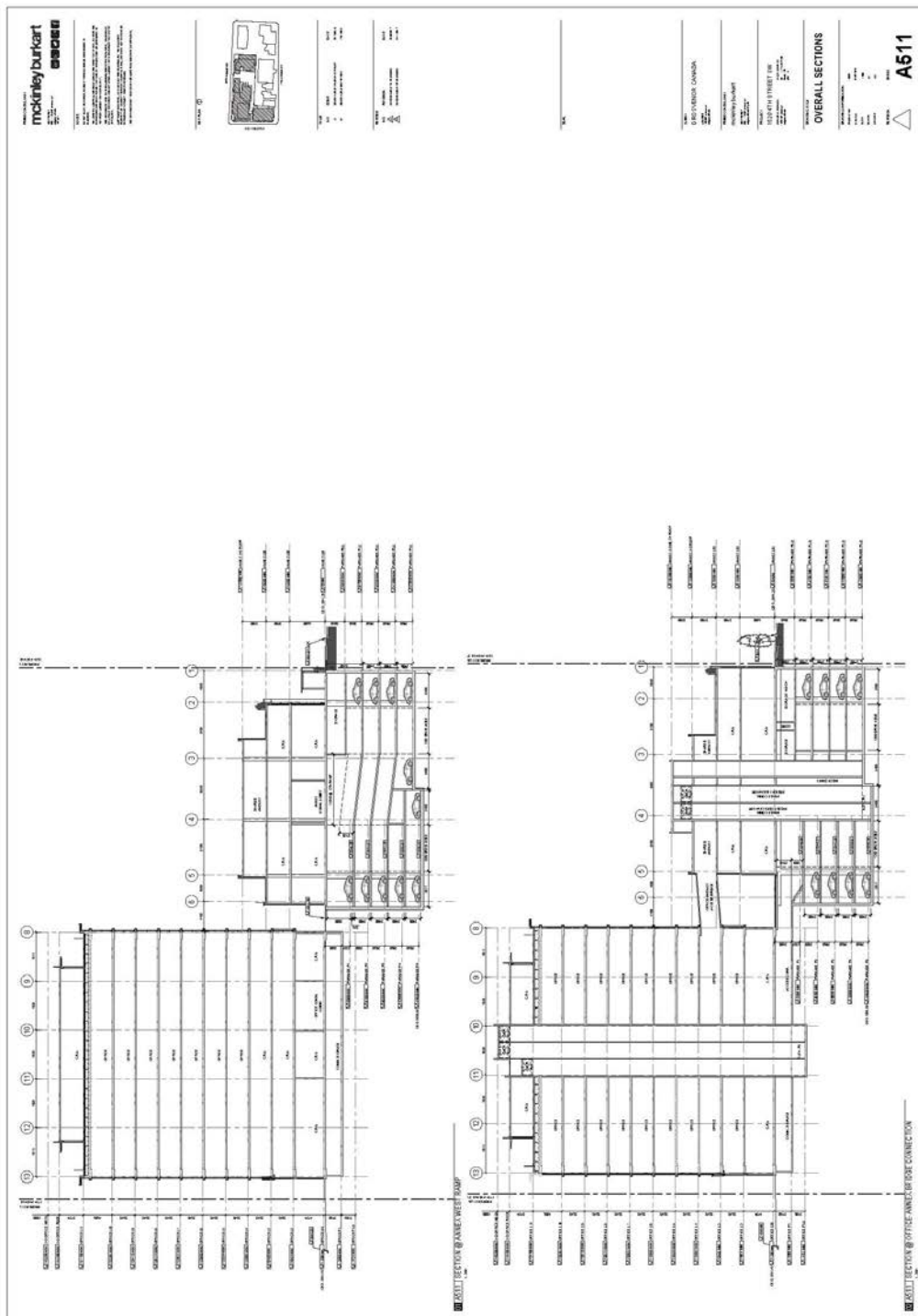
DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C



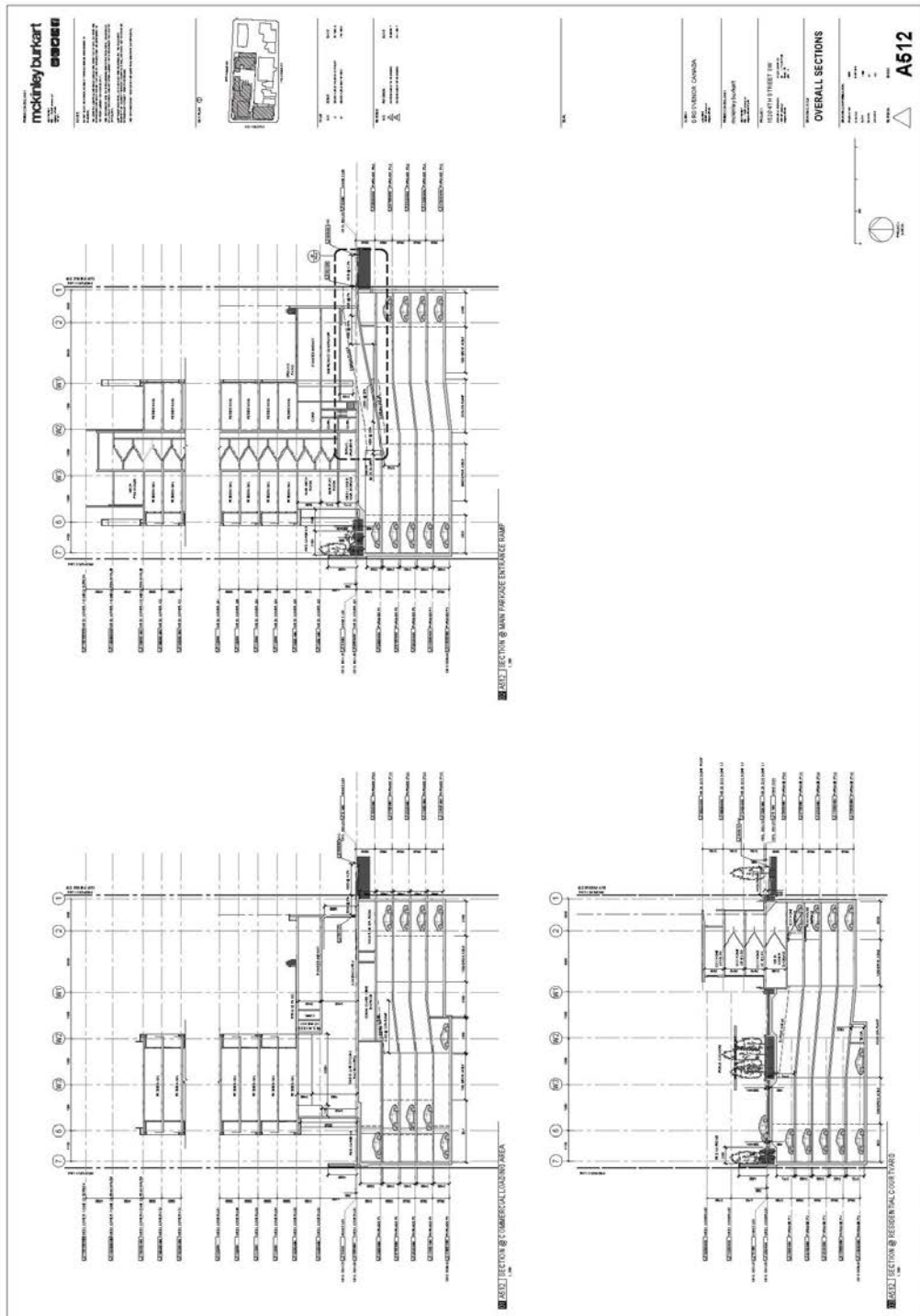
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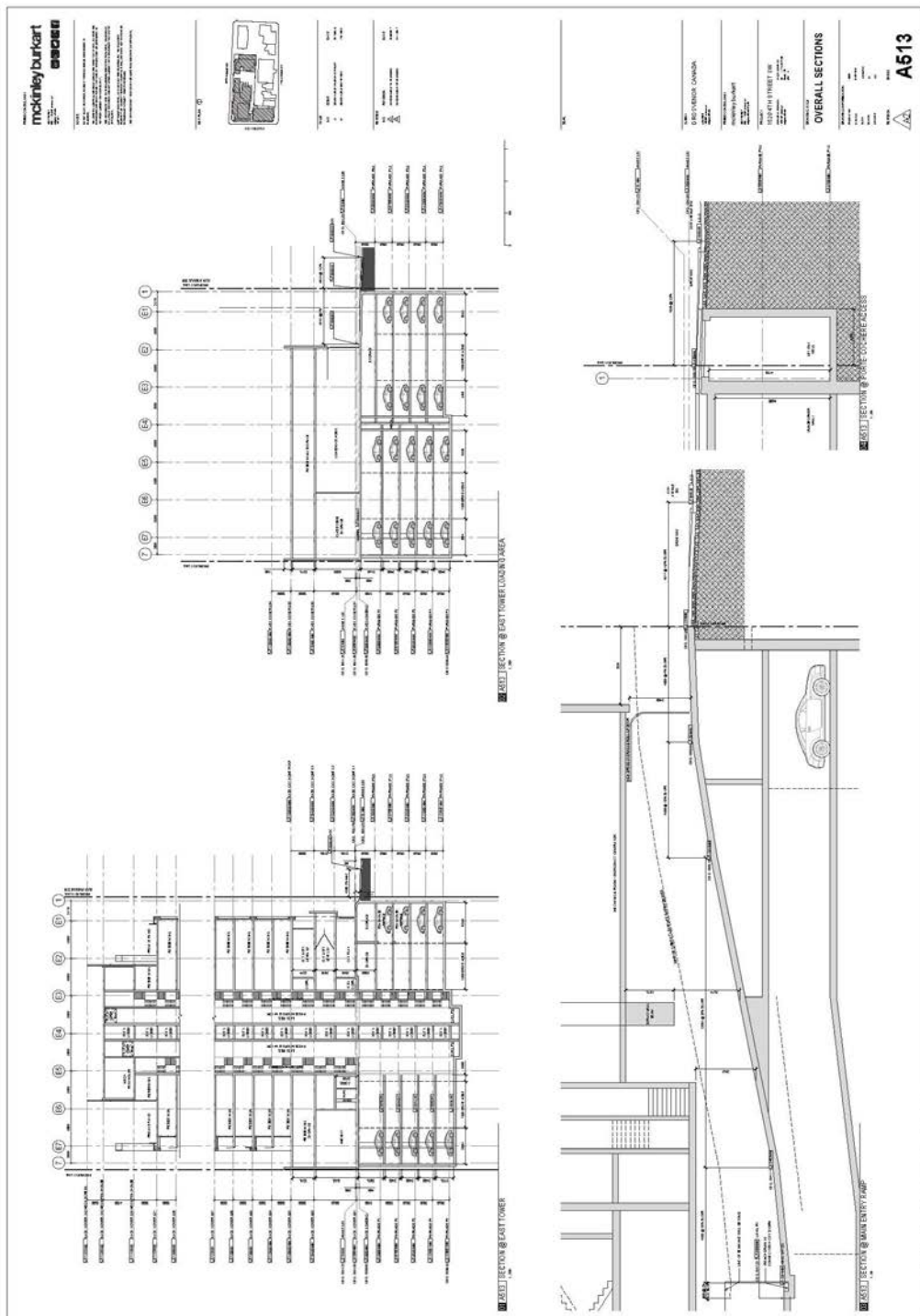
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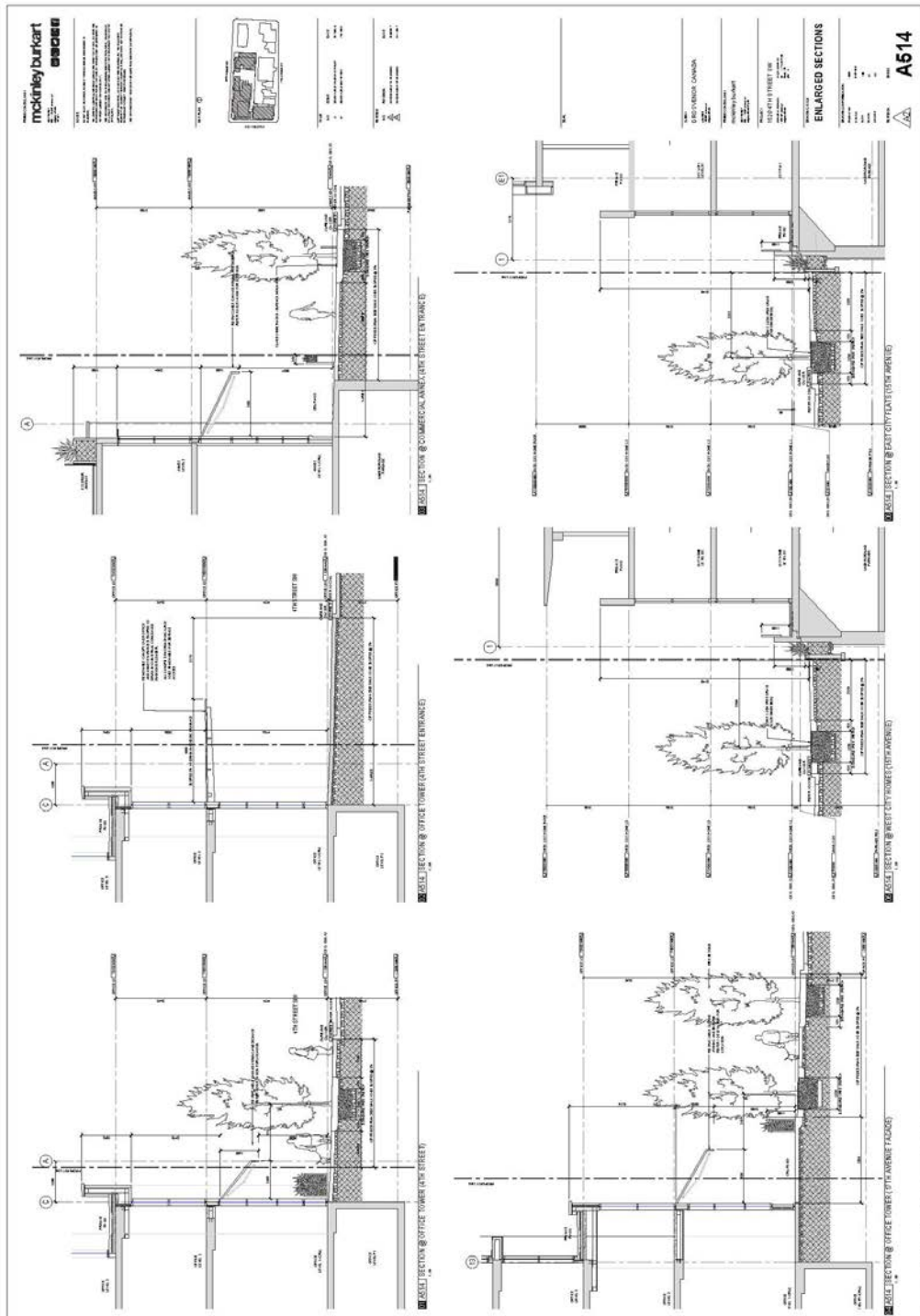
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 4 STREET SW AND 17 AVENUE SW

MAP 15C

Material Legend	Reference	Symbol	Material Name	Material Key	Material Notes
	27 - Concrete (see Appendix A for details on concrete types and finishes)		Concrete	Concrete	
	28 - Asphalt (see Appendix A for details on asphalt types and finishes)		Asphalt	Asphalt	
	29 - Brick (see Appendix A for details on brick types and finishes)		Brick	Brick	
	30 - Stone (see Appendix A for details on stone types and finishes)		Stone	Stone	
	31 - Glass (see Appendix A for details on glass types and finishes)		Glass	Glass	
	32 - Metal (see Appendix A for details on metal types and finishes)		Metal	Metal	
	33 - Wood (see Appendix A for details on wood types and finishes)		Wood	Wood	
	34 - Paint (see Appendix A for details on paint types and finishes)		Paint	Paint	
	35 - Landscaping (see Appendix A for details on landscaping types and finishes)		Landscaping	Landscaping	
	36 - Fencing (see Appendix A for details on fencing types and finishes)		Fencing	Fencing	
	37 - Signage (see Appendix A for details on signage types and finishes)		Signage	Signage	
	38 - Lighting (see Appendix A for details on lighting types and finishes)		Lighting	Lighting	
	39 - Other (see Appendix A for details on other material types and finishes)		Other	Other	

Plant List	STEM SET	BOTANICAL NAME	COMMON NAME	REMARKS	MATURE SIZE (Width x Height)
	H1	Salix purpurea	Willow		2.0m x 2.0m
	H2	Salix glauca	Willow		2.0m x 2.0m
	H3	Salix caprea	Willow		2.0m x 2.0m
	H4	Salix viminalis	Willow		2.0m x 2.0m
	H5	Salix alba	Willow		2.0m x 2.0m
	H6	Salix nigra	Willow		2.0m x 2.0m
	H7	Salix pyramidalis	Willow		2.0m x 2.0m
	H8	Salix discolor	Willow		2.0m x 2.0m
	H9	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H10	Salix humilis	Willow		2.0m x 2.0m
	H11	Salix glaberrima	Willow		2.0m x 2.0m
	H12	Salix caprea	Willow		2.0m x 2.0m
	H13	Salix viminalis	Willow		2.0m x 2.0m
	H14	Salix nigra	Willow		2.0m x 2.0m
	H15	Salix pyramidalis	Willow		2.0m x 2.0m
	H16	Salix discolor	Willow		2.0m x 2.0m
	H17	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H18	Salix humilis	Willow		2.0m x 2.0m
	H19	Salix glaberrima	Willow		2.0m x 2.0m
	H20	Salix caprea	Willow		2.0m x 2.0m
	H21	Salix viminalis	Willow		2.0m x 2.0m
	H22	Salix nigra	Willow		2.0m x 2.0m
	H23	Salix pyramidalis	Willow		2.0m x 2.0m
	H24	Salix discolor	Willow		2.0m x 2.0m
	H25	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H26	Salix humilis	Willow		2.0m x 2.0m
	H27	Salix glaberrima	Willow		2.0m x 2.0m
	H28	Salix caprea	Willow		2.0m x 2.0m
	H29	Salix viminalis	Willow		2.0m x 2.0m
	H30	Salix nigra	Willow		2.0m x 2.0m
	H31	Salix pyramidalis	Willow		2.0m x 2.0m
	H32	Salix discolor	Willow		2.0m x 2.0m
	H33	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H34	Salix humilis	Willow		2.0m x 2.0m
	H35	Salix glaberrima	Willow		2.0m x 2.0m
	H36	Salix caprea	Willow		2.0m x 2.0m
	H37	Salix viminalis	Willow		2.0m x 2.0m
	H38	Salix nigra	Willow		2.0m x 2.0m
	H39	Salix pyramidalis	Willow		2.0m x 2.0m
	H40	Salix discolor	Willow		2.0m x 2.0m
	H41	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H42	Salix humilis	Willow		2.0m x 2.0m
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	H44	Salix caprea	Willow		2.0m x 2.0m
	H45	Salix viminalis	Willow		2.0m x 2.0m
	H46	Salix nigra	Willow		2.0m x 2.0m
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	H51	Salix glaberrima	Willow		2.0m x 2.0m
	H52	Salix caprea	Willow		2.0m x 2.0m
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	H54	Salix nigra	Willow		2.0m x 2.0m
	H55	Salix pyramidalis	Willow		2.0m x 2.0m
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	H67	Salix glaberrima	Willow		2.0m x 2.0m
	H68	Salix caprea	Willow		2.0m x 2.0m
	H69	Salix viminalis	Willow		2.0m x 2.0m
	H70	Salix nigra	Willow		2.0m x 2.0m
	H71	Salix pyramidalis	Willow		2.0m x 2.0m
	H72	Salix discolor	Willow		2.0m x 2.0m
	H73	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H74	Salix humilis	Willow		2.0m x 2.0m
	H75	Salix glaberrima	Willow		2.0m x 2.0m
	H76	Salix caprea	Willow		2.0m x 2.0m
	H77	Salix viminalis	Willow		2.0m x 2.0m
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	H97	Salix elaeagnifolia	Willow		2.0m x 2.0m
	H98	Salix humilis	Willow		2.0m x 2.0m
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	H100	Salix caprea	Willow		2.0m x 2.0m

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	7 - Wood (see Appendix A for details on wood types and finishes)		Wood	Wood	
	8 - Paint (see Appendix A for details on paint types and finishes)		Paint	Paint	
	9 - Landscaping (see Appendix A for details on landscaping types and finishes)		Landscaping	Landscaping	
	10 - Fencing (see Appendix A for details on fencing types and finishes)		Fencing	Fencing	
	11 - Signage (see Appendix A for details on signage types and finishes)		Signage	Signage	
	12 - Lighting (see Appendix A for details on lighting types and finishes)		Lighting	Lighting	
	13 - Other (see Appendix A for details on other material types and finishes)		Other	Other	

General Notes
1. All materials shall be installed in accordance with the manufacturer's instructions.
2. All materials shall be installed in accordance with the applicable building codes.
3. All materials shall be installed in accordance with the applicable fire codes.
4. All materials shall be installed in accordance with the applicable accessibility codes.
5. All materials shall be installed in accordance with the applicable safety codes.
6. All materials shall be installed in accordance with the applicable environmental codes.
7. All materials shall be installed in accordance with the applicable health codes.
8. All materials shall be installed in accordance with the applicable social codes.
9. All materials shall be installed in accordance with the applicable cultural codes.
10. All materials shall be installed in accordance with the applicable spiritual codes.
11. All materials shall be installed in accordance with the applicable intellectual codes.
12. All materials shall be installed in accordance with the applicable emotional codes.
13. All materials shall be installed in accordance with the applicable physical codes.
14. All materials shall be installed in accordance with the applicable mental codes.
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BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C



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MAP 15C



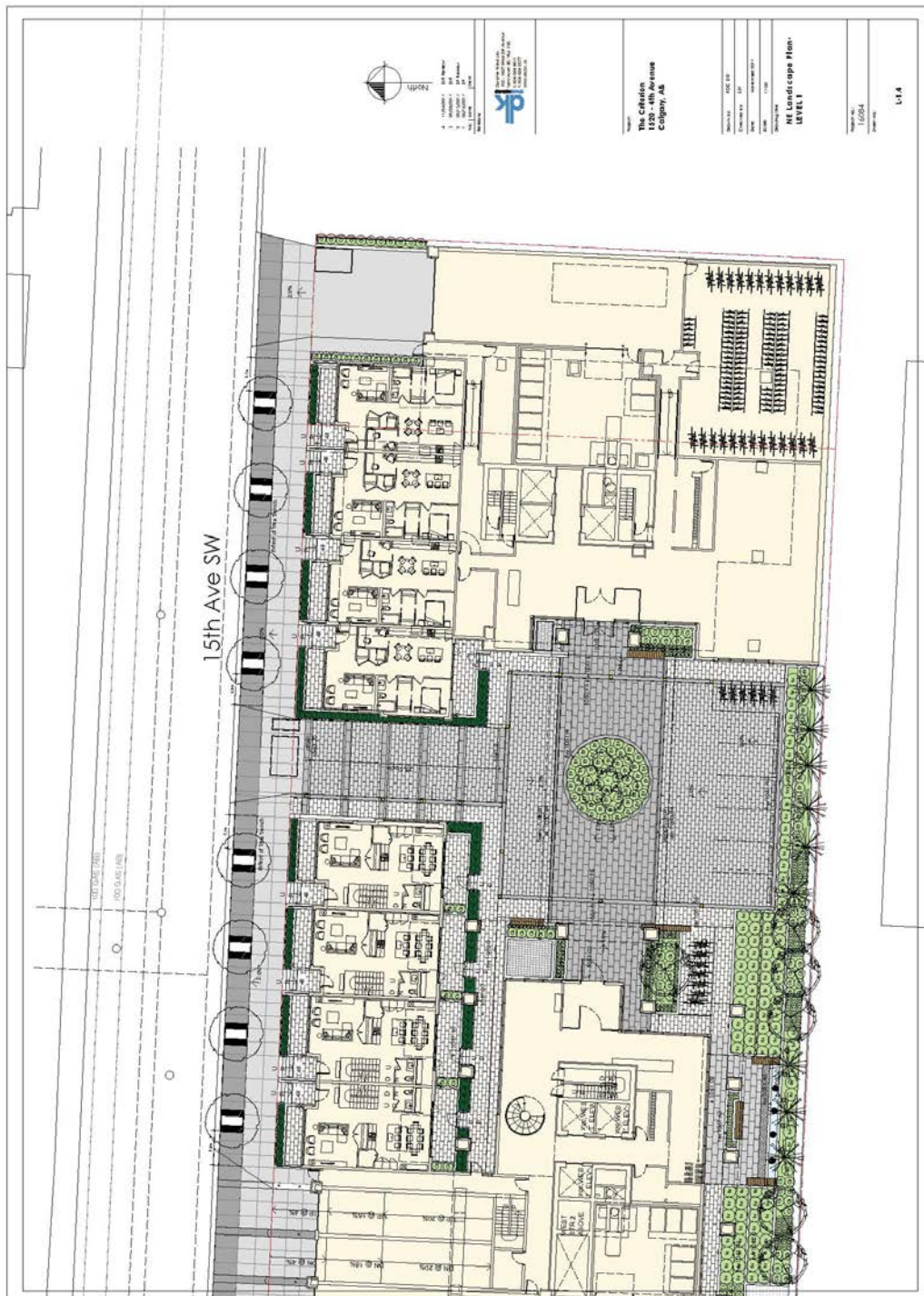
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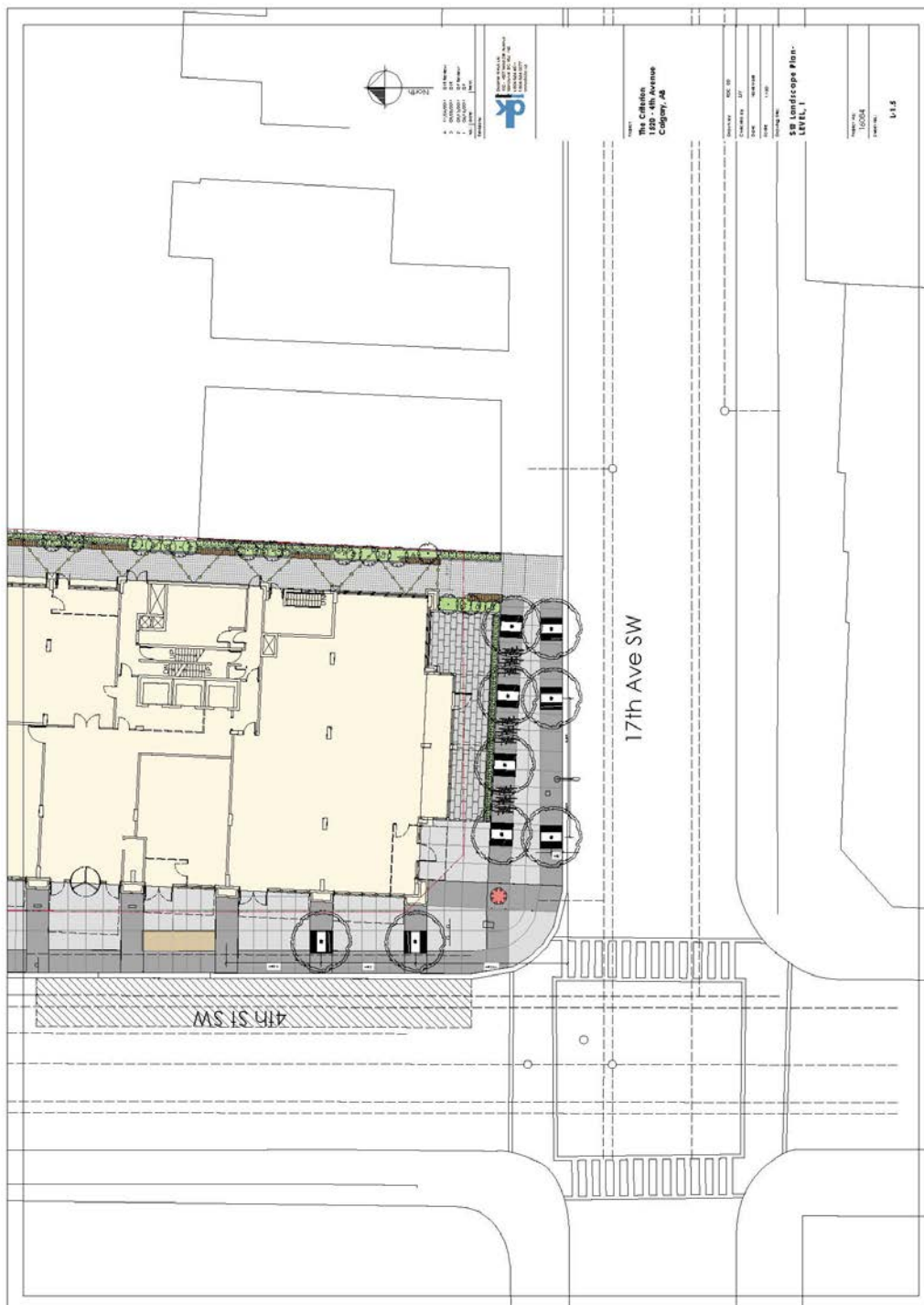
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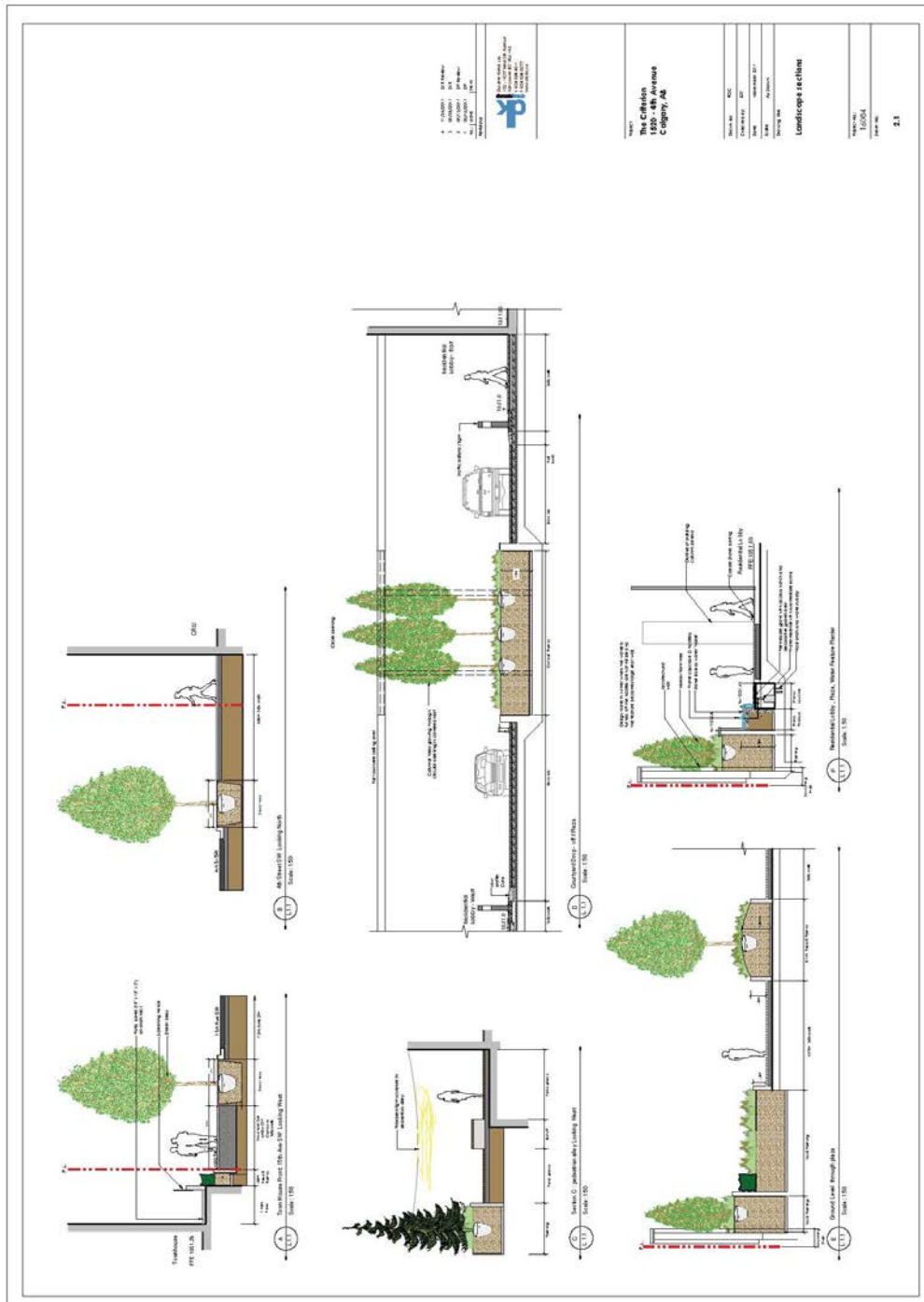
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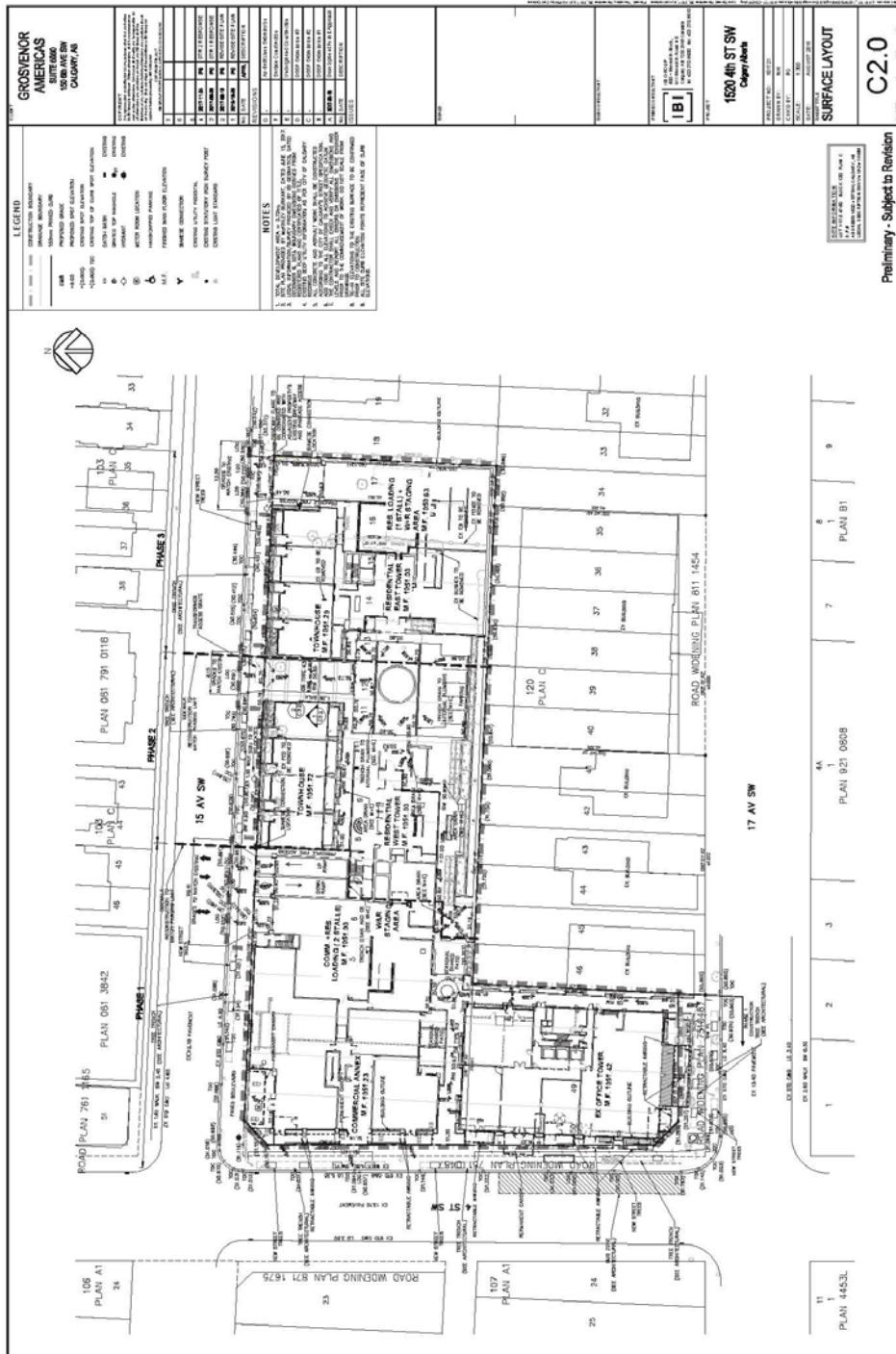






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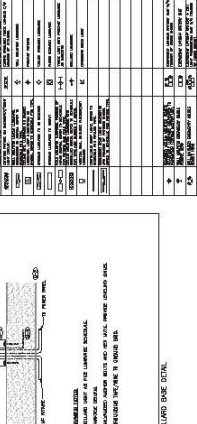
MAP 15C

**1520 - 4TH STREET SW**  
 ISSUED FOR DEVELOPMENT PERMIT  
 JUNE 16th, 2017  
 1520 4TH STREET SW  
 CALGARY, ALBERTA T2B 0A4



**E-000**

NO.	TYPE	NO. OF UNITS	CONSTRUCTION COST (\$)	LANDMARKS	DESCRIPTION	REMARKS	COMMENTS	IMAGE
11	NEW	1	1,000		CONCRETE WALL FENCE		REMOVE EXISTING WALL FENCE	
12	NEW	1	1,000		WALL FENCE		REMOVE EXISTING WALL FENCE	
13	NEW	1	1,000		RENOVATION WALL FENCE		REMOVE EXISTING WALL FENCE	
14	NEW	1	1,000		BOUNT FOOT LIGHT		REMOVE EXISTING BOUNT FOOT LIGHT	
15	NEW	1	1,000		ROUND HOUSE		REMOVE EXISTING ROUND HOUSE	
16	NEW	1	1,000		PARADE LIGHT		REMOVE EXISTING PARADE LIGHT	
17	NEW	1	1,000		PARADE LIGHT		REMOVE EXISTING PARADE LIGHT	



**TYPICAL ROUND DUCT DETAIL**

**LANDMARKS LEGEND**

NO.	DESCRIPTION	DATE
1	RENOVATION WALL FENCE	2017-06-16
2	CONCRETE WALL FENCE	2017-06-16
3	WALL FENCE	2017-06-16
4	RENOVATION WALL FENCE	2017-06-16
5	BOUNT FOOT LIGHT	2017-06-16
6	ROUND HOUSE	2017-06-16
7	PARADE LIGHT	2017-06-16
8	PARADE LIGHT	2017-06-16

**GENERAL NOTES**

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE CITY OF CALGARY DEVELOPMENT PERMITTING BY-LAW.
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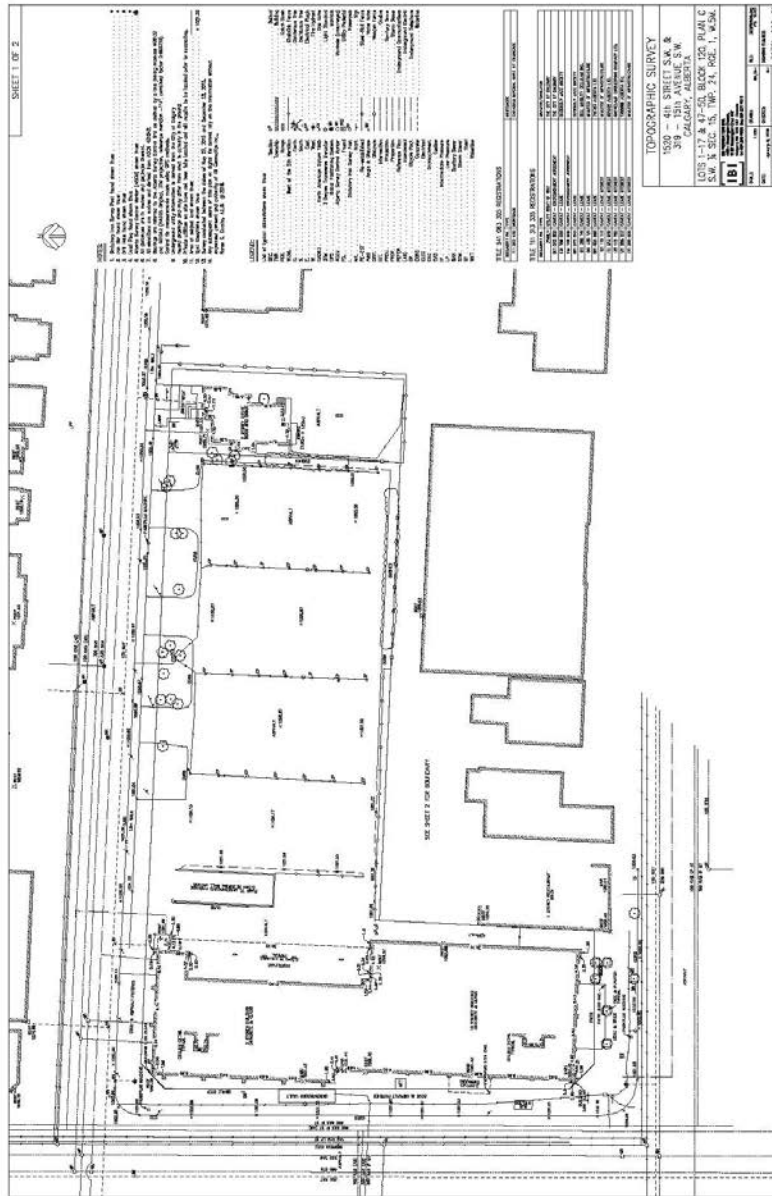
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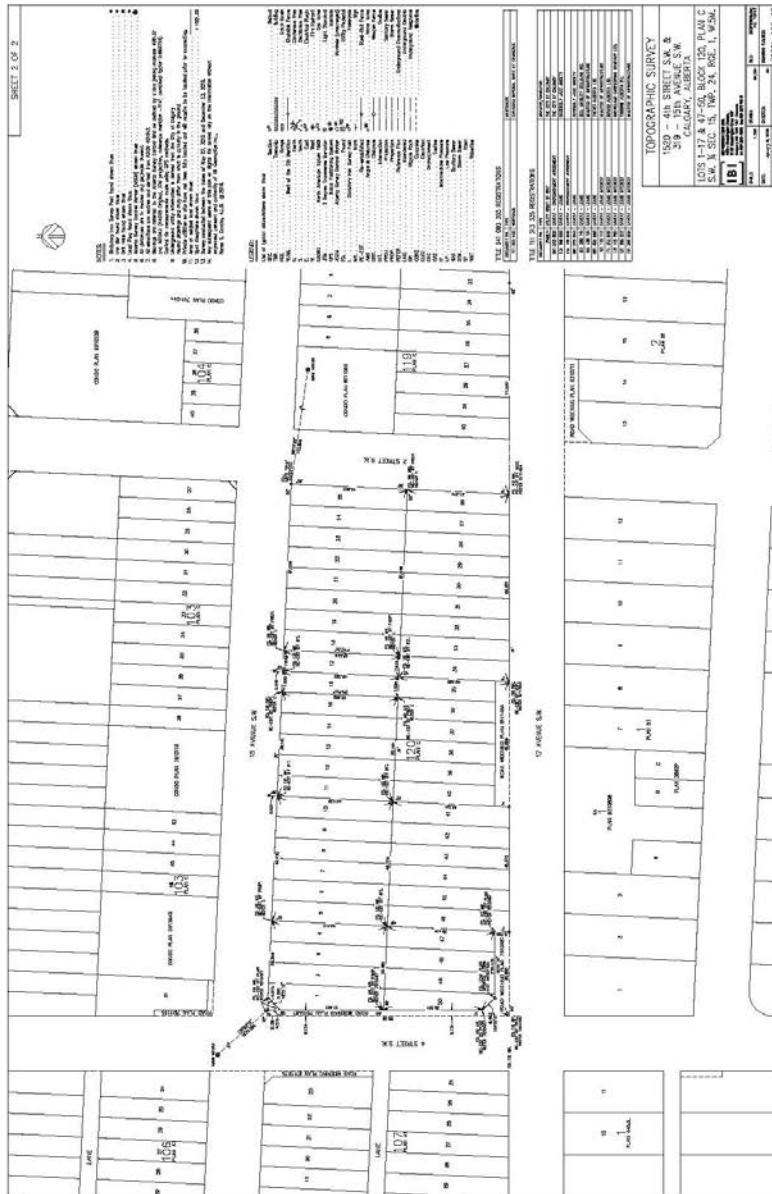
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MAP 15C

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**APPENDIX III**

**CONDITIONS OF APPROVAL**

**Prior to Release Requirements**

The following requirements shall be met prior to the release of the permit. All requirements shall be resolved to the satisfaction of the Approving Authority:

**Planning:**

1. Submit a total of six (6) complete sets of Amended Plans (file folded and collated) to the Planning Generalist that comprehensively address the Prior To Release conditions of all Departments as specified below.

In order to expedite the review of the Amended Plans, please include the following in your submission:

- a. **All of the plan set(s) shall highlight all of the amendments.**
- b. **Six (6) detailed written response(s) to the Conditions of Approval** document that provides a point by point explanation as to how each of the Prior to Release conditions were addressed and/or resolved.

Please ensure that all plans affected by the revisions are amended accordingly.

2. A Development Agreement shall be executed in conformity with all reports, plans and materials submitted to and approved by the Development Authority, to address:
  - Details of responsibilities for and construction of all improvements related to bonusing on the subject site including a registered public access easement over the bonus earning area (publicly-accessible private open space at grade);
  - All details of public art (valued at \$100,000) including location and format;
  - The delineation of public access easement areas, schedules and maintenance obligations.
3. Payment in the amount of \$1,620,000.00 to the Beltline Community Investment Fund (BCIF) in the form of a certified cheque payable to the City of Calgary, based on Approving Authority approval of the proposed bonus area (publicly accessible private open space) and Council approval of the proposed heritage density transfer through LOC2017-0175.

**Development Engineering:**

4. Provide a letter from Enmax, where Enmax confirms that they have no objections to the proposed development scope, including the trees and trenches proposed within the public right-of-way, removable canopy / awning proposed above / within the existing utility right-of-way or easement (registration #3188), etc.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

5. Amend the plans to:

Fire – Access Over an Underground Structure (Parkade)

- a. Provide height restricting bars OR an acceptable architecturally compatible alternative, which shall be to the satisfaction of the Fire Department, which shall prevent fire truck and heavy truck access into site (i.e. courtyard zone / on parkade structure).

6. Amend the plans to:

Fire – Lockbox Location

- a. Clearly indicate a “Calgary Fire Department approved lockbox” on the access route gateway.

Fire – Fire Department Connection (Siamese) Location(s)

Indicate and dimension the location of the Siamese location(s) such that they are (each):

- a. No closer than 3m and not more than 15m from the principal entrance(s).
- b. Does not obstruct egress from the building.
- c. Provides 2m operational clearance left and right of each port.
- d. Facing the street or access route, not blocked by columns, planters or landscaping etc.

**Notes:**

**-Some proposed fire department connections don't face the street or access route  
-Also, consider a “flush mount” style Siamese in scenarios where the Siamese potentially project into / within the pedestrian realm (as to limit possible conflict).**

7. Amend the plans to:

Fire – Alarm Panel Location

Indicate the location of the fire alarm panel such that:

- a. There is direct access from the principal entrance.
- b. An exterior strobe light is shown at the exterior door to the fire alarm panel location as there is no defined principal entrance.
- c. It is located in a Central Alarm Control Facility (CACF) room for buildings covered under ABC Div B 3.2.6 “Additional Requirements for High Buildings”.

8. Amend the plans to:

Waste & Recycling Services – Internal Storage Room

- a. Provide a minimum 3.0m wide unobstructed container passageway from the waste storage area to the collection area (2.0% maximum slope)

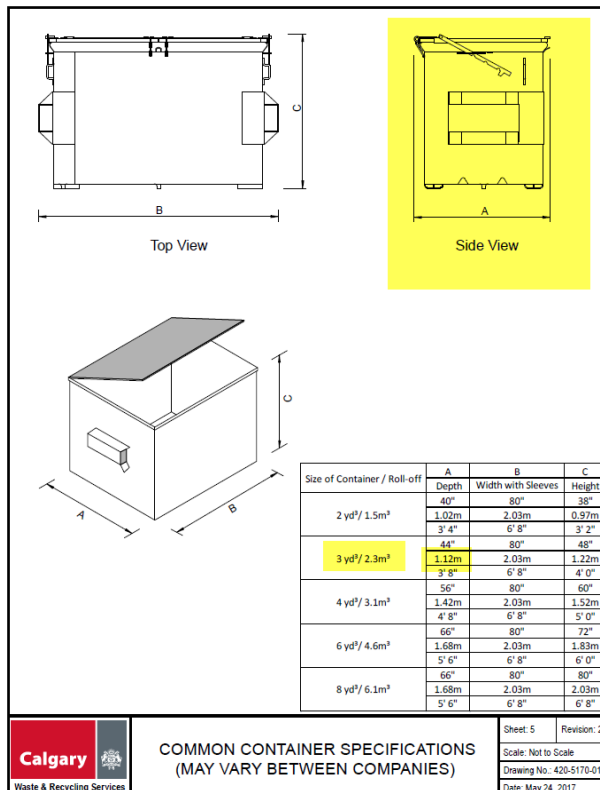
**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

MAP 15C

**Notes:**

- Applicant initially had initially only provided a minimum 2.0m passageway on the initial plans
  - Applicant has since increased the minimum width in most passageway zones to a minimum 2.2m, which could technically work for the containers anticipated / proposed
  - Extra horizontal clearance is recommended throughout, i.e. should there be any wall protection, etc.
- Applicant has proposed 3 yard waste and recycling containers, which (per spec) are roughly 1.12m in physical width at the narrowest face (see illustration below)
- Currently there is a ramp / transition area shown from office tower waste and recycling access corridor to anticipated staging area in P1 which is only 2.0m in width
  - This should be increased in order to achieve adequate width for movement of containers from storage to staging
- Show the slope throughout passageway and transition areas to ensure that full containers can be moved safely throughout

Illustration of Common Container Specifications:



COMMON CONTAINER SPECIFICATIONS  
 (MAY VARY BETWEEN COMPANIES)

Sheet: 5 Revision: 2  
 Scale: Not to Scale  
 Drawing No: 428-5170-013  
 Date: May 24, 2017

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

9. Follow the submission requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control (Erosion and Sediment Control Reports and Drawings: Technical Requirements) and either submit the required (2) copies of an Erosion and Sediment Control (ESC) Report and Drawings or the required (2) copies of a Written Notice to Development Engineering, for review and acceptance by Water Resources. The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: [www.calgary.ca/ud](http://www.calgary.ca/ud) (under publications).

Documents submitted shall conform to the requirements detailed in the current edition of The City of Calgary *Guidelines for Erosion and Sediment Control* and shall be prepared, signed and stamped by a qualified consultant specializing in erosion and sediment control, and holding current professional accreditation as a Professional Engineer (P. Eng.), Professional Licensee (P.L. Eng), Professional Agrologist (P. Ag.) or Certified Professional in Erosion and Sediment Control (CPESC). For each stage of work where soil is disturbed or exposed, documents must clearly specify the location, installation, inspection and maintenance details and requirements for all temporary and permanent controls and practices.

If you have any questions, contact 3-1-1. A Service Request (SR) will be created for the Erosion Control Team in Water Resources.

10. Consolidate the subject parcels. Submit a copy of the registered plan and certificate of title, confirming the consolidation of subject parcels onto a single titled parcel, to the Development Engineering Generalist.
11. Submit three (3) sets of the Development Site Servicing Plan details to Development Servicing, Inspections and Permits, for review and acceptance from Water Resources, as required by Section 5 (2) of the *Utility Site Servicing Bylaw 33M2005*. Contact [developmentservicing2@calgary.ca](mailto:developmentservicing2@calgary.ca) for additional details.

For further information, refer to the following:

**Design Guidelines for Development Site Servicing Plans**

[http://www.calgary.ca/PDA/pd/Documents/urban\\_development/publications/DSSP2015.pdf](http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/DSSP2015.pdf)

**Development Site Servicing Plans CARL (requirement list)**

<http://www.calgary.ca/PDA/pd/Documents/development/development-site-servicing-plan.pdf>

12. Submit a Fire Flow Letter, prepared by a qualified professional engineer under seal and permit to practice stamp to the satisfaction of Development Approvals Team Leader, Water Resources. The fire flow letter shall identify the type of the development, address of the development, and the fire flow required for the developing property.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

The available fire flow available in the adjacent City watermain is 15,000 Litres Per Minute (LPM).

**Note(s):**

**-A document was submitted to the City for review on December 6, 2017.  
-It has since been reviewed by Water Resources (December 11, 2017), where  
resubmission is deemed necessary.**

**For further details, contact Water Resources at 403-268-5605.**

13. Remit payment (**certified cheque**) for the infrastructure upgrades for the Centre City communities, in the amount of **\$760,109.22**, to Development Engineering. This levy includes both the Centre City Utility Levy approved under the **Centre City Utility Levy Bylaw 38M2009** and an amount approved by Council for community recreation, transportation, parks upgrading, and greenways. The amount identified above is determined by using **\$4,710.00 per meter of site frontage (on avenues only)** for the proposed development (**161.382m total lineal frontage length charged = 29.556m frontage along 17 AV S + 131.826m frontage along 15 AV S**).
14. **After the Development Permit is approved but prior to its release**, the landowner shall execute an Off-Site Levy Agreement for the payment of off-site levies pursuant to Bylaw 2M2016.

**Should payment be made prior to release of the Development Permit, an Off-Site Levy Agreement will not be required.**

To obtain the off-site levy agreement, contact the Public Infrastructure Coordinator, Calgary Approvals Coordination at 403-268-6739 or email [offsitelevy@calgary.ca](mailto:offsitelevy@calgary.ca).

**Transportation:**

15. Submit an application for an Encroachment Agreement, to the satisfaction of Real Estate and Development Services, for the proposed encroachments (re: proposed building cladding within the public road right of way / corner cut area at the intersection of 17 AV SW & 4 ST SW).

Provide proof that payment has been made for the Encroachment Agreement to the Development Engineering / Transportation Planning Generalist.

For further details, contact the Encroachment Administrator, Real Estate and Development Services, at 403-268-5034 or via [encroachments@calgary.ca](mailto:encroachments@calgary.ca)

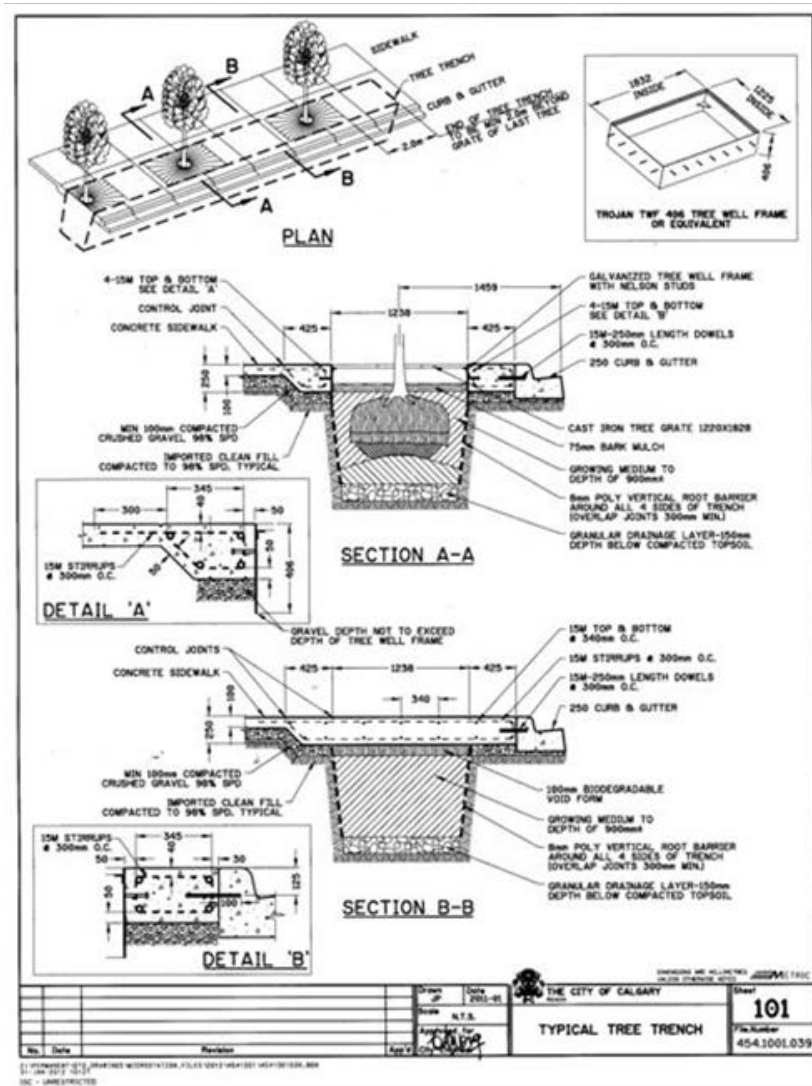
16. The proposed CRU patio onto 17 AV SW encroaches into the Road Right-of-Way (ROW). It is noted that a Development Permit does not give approval rights to build within the ROW, and that the patio can be shown for information purposes only. The Applicant is to be aware that a separate Licence of Occupation must be applied for through Transportation Roads.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

Amend the Plan to show that all items proposed within Road ROW are removable within 30 days and include a note that items shown are for information purposes only and require a separate approval through a Licence of Occupation. At the next submission, the Applicant is to indicate which sheets were updated

17. New boulevard trees (if solid concrete in boulevard) must be in tree trenches constructed to City standards, with 1.2m x 1.83m grates, as they can be replaced by the City in an emergency situation. Trees should not be planted in the middle of the sidewalk, as this would be an obstruction to the pedestrian realm. Street trees should be planted next to the curb (1459mm offset from a lip of gutter) as per Roads standards (See image below).



**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

**NOTE:** The applicant is to amend plans to use the standard City tree trenches as requested, and note that shrubs are not permitted within City boulevards, as they end up being garbage collector, which causes unwanted maintenance for Calgary Roads. Both Transportation Planning and Development Engineering have coordinated this with the applicable stakeholders (Utilities, and Utility Line Assignments), and the separations from existing utilities are acceptable. The applicant will have to coordinate with Roads Streetlighting (403) 268-5653 for relocation of the streetlighting cable/ conduit to be placed behind the tree trenches, at 2.08m back of the lip of gutter.

**NOTE 2:** This item remains outstanding and must be resolved prior to resubmission. Applicant is to amend the Southern grouping of trees on 4 ST SW to the City standard shown above, and in line with the proposed trees to the North. Also, amend plans to remove the inner row of trees on 17 AV SW, as they do not provide the minimum 3m of pedestrian realm between the edges of the tree grates. Amend the cross sections to show the new street lighting cable/ conduit at the above mentioned 2.08m back of the lip of gutter.

18. Amend plans to show all existing and proposed wheel chair ramps. Some corners may require two separate wheel chair ramps when done to City specs.

**NOTE:** This item is still outstanding, as new wheelchair ramps will be required for this development.

19. Amend plans to show that all proposed bike racks within the City boulevards /public sidewalk are to be the removable type.

**NOTE:** Plans do not show that the proposed class #2 bike racks are the removable type. Amend accordingly.

20. Execute and register on title an Access Agreement over lots 1-17 and 47-50, block 120, plan C (Servient Lands) in favour of public access and surface parking areas (Dominant Lands) for the purpose of parking access / vehicular access / pedestrian access. The agreement and registerable access right of way plan shall be to the satisfaction of the Director, Transportation Planning. A standard template for the agreement and an Instruction Document will be provided by the Transportation CPAG Generalist. Submit an original copy of the executed agreement and the certificate of title(s), indicating the agreement is registered on title, for all affected parcels.

21. Remit a performance security deposit (certified cheque, bank draft, letter of credit) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Unit. The amount of the deposit is calculated by Roads and is based on 100% of the estimated cost of construction.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

The developer is responsible to arrange for the construction of the infrastructure with their own forces and to enter into an Indemnification Agreement with Roads at the time of construction (the security deposit will be used to secure the work).

Roads

- a. Closure and rehabilitation of 7 existing driveway crossings (1 on 17th Avenue S.W., the rest on 15<sup>th</sup> Avenue S.W.),
  - b. Construction of 3 new driveway crossings,
  - c. Construction of new monolithic sidewalks adjacent to site frontages,
  - d. Construction of new wheelchair ramps,
  - e. Construction of new curb and gutter adjacent to site frontages,
  - f. Construction of tree trenches to City standards,
  - g. Rehabilitation of existing driveway crossings, sidewalks, curb and gutter, etc., should it be deemed necessary through a site inspection by Roads personnel.
22. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Roads

- a. Street lighting upgrading adjacent to site frontages (If required).
23. Remit payment (certified cheque, bank draft) for the proposed infrastructure listed below within the public right-of-way to address the requirements of the Business Units. The amount is calculated by the respective Business Unit and is based on 100% of the estimated cost of construction.

The developer is responsible to coordinate the timing of the construction by City forces. The payment is non-refundable.

Calgary Transit

- a. Providing funds to Calgary Transit for the purchase and installation of a Large Transit Shelter (dimensions are 5.9m L x 1.6m W). This is currently estimated at \$20,500.
24. Amend plans to provide the design details (heat tracing, etc.) for the proposed canopies to avoid the possibility of ice formation on perimeter edges that may fall onto pedestrians.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

NOTE: Although plans of the proposed canopies were provided, they are not very detailed, and make no mention of where possible ice formation, or rainwater would end up (instead of falling on to pedestrians). Typically this is to be routed within the building for drainage. Amend accordingly.

25. Driveway flares should not extend across the projection of the lot line (Easternmost driveway). Applicant has provided 1.0 metre flares which are currently not supported as loading vehicles track over the curb and gutter leading to degradation and maintenance issues.

Applicant is to maximize the flares (each flare does not have to be the same in this case) and provide sweep paths for both entrance and egress that shows the proposed design works. Label offset from top of curb and Amend the Plan accordingly.

**NOTE:** Applicant has the option to provide a copy of a written agreement with the adjacent landowner to have the proposed curb cut cross the lot line. If an agreement cannot be obtained, amend plans accordingly as per the above.

**Parks:**

26. Indicate on all relevant landscape plans the setbacks between the proposed boulevard trees and the existing or proposed utilities/servicing connections. Provide the minimum setback requirements as identified and required in **Section 4.1.3 of “Development Guidelines and Standard Specifications: Landscape Construction” (current edition)**:  
[http://www.calgary.ca/PDA/pd/Documents/urban\\_development/publications/Landscape-construction-2017.pdf](http://www.calgary.ca/PDA/pd/Documents/urban_development/publications/Landscape-construction-2017.pdf)

It appears several proposed boulevard trees along 15 Avenue SW will conflict with some existing or proposed utilities/servicing connections (see C1.0). In addition, two proposed boulevard trees along 4 Street will be too close to the existing transformer vault. A minimum 3.0m setback is required between boulevard trees and Enmax vault.

Adjust the tree spacing and/or remove some of the proposed boulevard trees. Amend all relevant plans accordingly.

27. Amend all relevant plans to remove the inner row of trees on 17 Avenue SW as per Transportation’s comment. Please note that tree spacing between the double rows of proposed Green Ash trees along 17 AV SW should be half of maximum spread (4.0m) as per Parks’ “Development Guidelines and Standard Specifications: Landscape Construction” to ensure healthy and sustainable tree growth. According to L-1.2, only 3.2m spacing between rows is provided.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

28. Amend L1.1 to provide the following accurate information concerning the existing public trees within 6.0m of the development site.

Tree Number	Species	DBH (m)	Height	Canopy
1		0.06		
2			3	2
3		0.18		7
4		0.07	5	2
5		0.07		2
6		0.07		2
7	American Elm / <i>Ulmus americana</i>	0.19	6	5

29. Indicate the soil depth and other details of the “Standard Tree Trench” on A514 as per Calgary Roads tree trench standard.

Also resolve the inconsistency concerning the width of the proposed tree trenches between Details 1, 3, 4, 5, and 6 on A514 and Landscape sections A and B/L1.1 on Plan 2.1.

**Permanent Conditions**

The following permanent conditions shall apply:

**Planning:**

30. The development shall be completed in its entirety, in accordance with the approved plans and conditions.
31. No changes to the approved plans shall take place unless authorized by the Development Authority.
32. A Development Completion Permit shall be issued for the development; **before the use is commenced or the development occupied**. A Development Completion Permit is independent from the requirements of Building Permit occupancy. Call Development Inspection Services at 403-268-5311 to request a site inspection for the Development Completion Permit.
33. This approval recognizes three phases on the approved plans which shall be completed in sequence. All the road works, landscaping and provisions for garbage collection shown within each phase shall be completed prior to the issuance of a Development Completion Permit for that phase. Call Development Inspection Services at 403-268-5311 to request site inspections for the Development Completion Permits.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

34. The properly executed Development Agreement referenced in Condition 4 shall remain in force and on title throughout the life of this development.
35. The walls, pillars and ceiling of the underground parkade shall be painted white or a comparable light colour.
36. The light fixtures in the parkade shall be positioned over the parking stalls (not the drive aisles).
37. All stairwell doors and elevator access areas shall be installed with a transparent panel for visibility.

**Development Engineering:**

38. If during construction of the development, the developer, the owner of the titled parcel, or any of their agents or contractors becomes aware of any contamination,
  - a. the person discovering such contamination shall immediately report the contamination to the appropriate regulatory agency including, but not limited to, Alberta Environment, Alberta Health Services and The City of Calgary (311).
  - b. on City of Calgary lands or utility corridors, The City of Calgary, Environmental and Safety Management division shall be immediately notified (311).
39. The subject parcels must remain on a single certificate of title for the duration of the development unless subdivision is approved by the subdivision authority.
40. The developer / project manager, and their site designates, shall ensure a timely and complete implementation, inspection and maintenance of all practices specified in erosion and sediment control report and/or drawing(s) which comply with Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control. Any amendments to the ESC documents must comply with the requirements outlined in Section 3.0 of The City of Calgary Guidelines for Erosion and Sediment Control.

For other projects where an erosion and sediment control report and/or drawings have not been required at the Prior to Release stage, the developer, or their designates, shall, as a minimum, develop an erosion and sediment control drawing and implement good housekeeping practices to protect onsite and offsite storm drains, and to prevent or mitigate the offsite transport of sediment by the forces of water, wind and construction traffic (mud-tracking) in accordance with the current edition of The City of Calgary Guidelines for Erosion and Sediment Control. Some examples of good housekeeping include stabilization of stockpiles, stabilized and designated construction entrances and exits, lot logs and perimeter controls, suitable storm inlet protection and dust control.

The City of Calgary Guidelines for Erosion and Sediment Control can be accessed at: [www.calgary.ca/ud](http://www.calgary.ca/ud) (under publications).

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

For **all soil disturbing projects**, the developer, or their representative, shall designate a person to inspect all erosion and sediment control practices a minimum of every seven (7) days and during, or within 24 hours of, the onset of significant precipitation (> 12 mm of rain in 24 hours, or rain on wet or thawing soils) or snowmelt events. Note that some practices may require daily or more frequent inspection. Erosion and sediment control practices shall be adjusted to meet changing site and winter conditions.

41. Contact the Erosion Control Inspector, Water Resources, with at least two business day's notice, to set up a pre-construction meeting prior to commencement of stripping and grading. Locations north of 17 Avenue S should contact 403-268-5271. Sites south of 17 Avenue S should contact 403-268-1847.
42. Stormwater runoff must be contained and managed in accordance with the "Stormwater Management & Design Manual" all to the satisfaction of the Director of Water Resources.
43. The grades indicated on the approved Development Site Servicing Plan(s) must match the grades on the approved Development Permit plans. Upon a request from the Development Authority, the developer or owner of the titled parcel must confirm under seal from a Consulting Engineer or Alberta Land Surveyor, that the development was constructed in accordance with the grades submitted on the Development Permit and Development Site Servicing Plan.
44. No trees, shrubs, buildings, permanent structures or unauthorized grade changes are permitted within utility rights-of-way.
45. Pursuant to Bylaw 2M2016, off-site levies are applicable.
46. **Prior to issuance of a Development Completion Permit or any occupancy of the building**, payment shall be made for off-site levies pursuant to Bylaw 2M2016. **To obtain a final estimate**, contact the Public Infrastructure Coordinator, Calgary Approvals Coordination at 403-268-6739 or email [offsitelevy@calgary.ca](mailto:offsitelevy@calgary.ca)

**Transportation:**

47. This development does not qualify for the residential Parking Permit Program.
48. No loading or stopping permitted on 17 Avenue S.W. or 4 Street S.W. adjacent to the subject site.
49. The developer shall be responsible for the cost of public work and any damage during construction in City road right-of-ways, as required by the Manager, Transportation Planning. All work performed on public property shall be done in accordance with City standards.

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

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50. Indemnification Agreements are required for any work to be undertaken adjacent to or within City rights-of-way, bylawed setbacks and corner cut areas for the purposes of crane operation, shoring, tie-backs, piles, surface improvements, lay-bys, utility work, +15 bridges, culverts, etc. All temporary shoring, etc., installed in the City rights-of-way, bylawed setbacks and corner cut areas must be removed to the satisfaction of the Manager of Transportation Planning, at the applicant's expense, upon completion of the foundation. **Prior to permission to construct, contact the Indemnification Agreement Coordinator, Roads at 403-268-3505.**

**Parks:**

51. Public trees located on the boulevard adjacent to the development site shall be retained and protected unless otherwise authorized by Urban Forestry. Prior to construction, install a temporary fence around the extent of the branches ("drip line") and ensure no construction materials are stored inside this fence.
52. Tree protection information given as per the approved Development Permit does not constitute Tree Protection Plan approval. Tree Protection Plan approval must be obtained separately through Urban Forestry. Visit [www.calgary.ca](http://www.calgary.ca), call 311, or email [tree.protection@calgary.ca](mailto:tree.protection@calgary.ca) for more information.
53. In order to ensure the integrity of existing public tree and roots, construction access is only permitted through the rear lane and outside the drip line of public tree.
54. The submitted plans indicate that the removal of existing public trees along property frontage. As per the City of Calgary Tree Protection By-law, a letter of authorization to remove public trees is required from Parks Urban Forestry. The applicant is to contact Urban Forestry at 311 to make arrangements for the letter and compensation.
55. Any tree planting in the City boulevard shall be performed and inspected in accordance with Parks Development Guidelines and Standard Specifications – Landscape Construction (current edition). Applicant is to contact the Parks Development Inspector at 403-620-3216 or at 403-268-5204 to arrange an inspection.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

**APPENDIX IV**

**URBAN DESIGN REVIEW PANEL COMMENTS**

	<b>Urban Design Review Panel: Review Template</b>		<b>Applicant:</b>	<b>Grosvenor - 1520 4th Street SW</b>
			<b>Ranking</b>	<i>Support with Comments</i>
	<b>Information for the use of this Review Template:</b>			
	<b>Topics</b>	<b>Best Practice Guidelines</b>	<b>Ranking</b>	<b>Commentary</b>
	<i>Topics are provided to facilitate consistent review practices, and to help define the scope of the review. Where the Panel feels it is appropriate - the "other" box can be used for additional topics.</i>	<i>Best Practices are included to guide discussion. Best Practices are based on approaches that promote urban vitality, liveability, urban connectivity, active use, sustainability, beauty and enjoyment of the urban realm by the public at large. They are not prescriptive, and are intended to provide desired outcomes. Applicants are encouraged to use innovative approaches to achieve the outcome where suitable.</i>	<i>Support Support with Conditions Do not Support Not Applicable TBD</i>	

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

	<b>General Comments</b>			
	<p>UDRP commends Grosvenor for proposing a project that will improve one of the most important intersections in central Calgary by providing more active retail frontages and an improved public realm. The panel is impressed with the concept for the proposed alley, but is concerned that without the proper curation of the retail uses and execution of the design to the level of detail demonstrated in the presentation that the alley's success could be compromised. Given this observation and as the applicant is seeking bonus density in exchange for the provision of the alley, the panel suggests that conditions be placed on the development permit to ensure that the vision of the alley is realised. UDRP suggests that a condition that prevents the consolidation of the retail units to less units than is shown in the package presented to UDRP. This condition will ensure that the space remains fine grained for perpetuity. UDRP also suggests conditions that will ensure that views into the retail units are maintained. Conditions would include not permitting window display boxes and a requirement that uses be retail or food and beverage to ensure active uses. UDRP supports the residential uses along 15 avenue but suggests that the applicant consider providing ground floor patios on the 15 avenue frontage. The addition of patios on this street will provide greater opportunity of social interaction between residents and an improved relationship between the units and the streetscape.</p>			
	<p>The laneway sun-trap, wind-protected environment is a unique microclimate opportunity for zone-distinctive plant material (trees, shrubs, groundcover) that don't typically grow in less protected spaces. The walk-around and walk-through public realm in the streetscape and laneway passage offers an intriguing change in scale and experience. The laneway artwalk qualities coupled with dynamic activation strategies and light design should evolve an authentic placemaking brand for the project.</p> <p>The panel also encourages the applicant to continue to pay close attention to the design of the retail, sidewalk, vegetation and terraces on the 17th Avenue and 4th Street edges - the alley is a unique and interesting space, but the street frontage will have a significant impact on the neighborhood and should remain of primary importance. The choice of vegetation in particular needs further consideration along 4th Street. Implementing smaller grain retail, terraces, well-designed public seating and other strategies are also key.</p>			
	<b>Urban Vitality Contribution</b>			
	<b>Topic</b>	<b>Best Practice</b>	<b>Ranking</b>	<b>Commentary</b>
1	Retail Street Diversity	Retail streets encourage pedestrians along sidewalk with a mix and diversity of smaller retail uses. Retail wraps corners of	Support	

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

		streets. Space for patios and cafe seating is provided.		
2	Retail Street Transparency and Porosity	Retail street maximizes extent of glazing - 70% and more. Maintains view into and out of retail - avoids display only windows.	Support with comment	The proposed retail streetscape is a significant improvement to the existing condition. UDRP suggests that glazing be required on all facades of the retail units. UDRP requests that glazing be added to the southern retail unit on the existing office building to create more porosity between the retail unit and the alley. This design detail will help draw pedestrians into the alley.
				<b>Plans and elevations amended to provide more vision glazing from the CRU's facing the entries to the Pedestrian Alley off 4 Street SW and off 17 Avenue SW. Refer to A102 for revised Site Plan.</b>
3	Pedestrian First Design	Sidewalks are continuous on all relevant edges. Materials span driveway entries and parking access points. No drop offs or lay-bys in the pedestrian realm. Street furnishings support the pedestrian experience.	Support with comment	UDRP suggests that the sidewalk paving material be continued along the entire length of the pedestrian zone on 15 Avenue SW. This gesture will signal to drivers accessing the building that they are crossing a pedestrian zone and provide a consistent pedestrian walkway.
				<b>Plans amended to provide continuity of pedestrian paving material as suggested across all curb cuts and driveway access points. Refer to L1.2 for revised Overall Landscape Plan.</b>

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

4	Entry Point Definition / Legibility	Entry points are clear and legible	Support with comment	UDRP suggests implementing strategies to the make the entrance to the alley from 17 Avenue SW more visible. For example to shorten the terrace and provide public furniture that invites the use of the area around the entrance by the public.
				<b>Plans amended to increase visibility of entrance from 17 Avenue SW as suggested by shifting street trees along 17 Avenue SW towards the West to strengthen sight lines from the street into the Alley. Refer to A102 for revised Site Plan.</b>
5	Residential Multi-Level Units at grade	Inclusion of two or three storey units are encouraged, particularly at street level. Private outdoor patios with access to the sidewalk are ideal. Patios are large enough to permit furnishing and active use.	Support with comment	See comment above regarding including patios along 15 Avenue SW.
				<b>Plans amended to reflect design revision to improve the interfaces between the residential frontages and 15th Avenue, with outdoor patios at grade facing the sidewalk as suggested. Refer to A102 for revised Site Plan.</b>
6	At grade Parking	At grade parking is concealed behind building frontages along public streets.	Support	
7	Parking Entrances	Ramps are concealed as much as possible. Entrances to parking	Support	

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

MAP 15C

		are located in discrete locations. Driveways to garage entries are minimized, place pedestrian environment and safety first.		
8	Other		Unused	
	<b>Urban Connectivity</b>	Provide visual and functional connectivity between buildings and places, ensure connection to existing and future networks. Promote walkability, cycle networks, transit use, pedestrian first environments		
	<b>Topic</b>	<b>Best Practice</b>	<b>Ranking</b>	<b>Commentary</b>
9	LRT Station Connections	Supports LRT use via legible, dedicated pedestrian pathways to stations with direct routes. Avoids desire lines / shortcutting through parking areas.	N/A	
10	Regional Pathway Connections	Supports walkability via intentional urban design connections to pathway systems	NA	
11	Cycle Path Connections	Supports cycling via intentional, safe urban design connections to pathway systems and ease of access to bicycle storage at grade.	N/A	
12	Walkability - connection to adjacent neighbourhoods / districts / key urban features	Extend existing and provide continuous pedestrian pathways. Extend pedestrian pathway materials across driveways and lanes to emphasize pedestrian use.	Support with comment	See comment above regarding extending the sidewalk paving material across the entire pedestrian zone on 15 avenue.
				<b>Plans amended to provide continuity of pedestrian paving material as suggested across all curb cuts and driveway access points.</b>

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

				Refer to L1.2 for revised Overall Landscape Plan.
13	Pathways through site	Provide pathways through the site along desire lines to connect amenities within and without the site boundaries.	Support	
14	Open Space Networks and Park Systems	Connects and extends existing systems and patterns.	Support	
15	Views and Vistas	Designed to enhance views to natural areas and urban landmarks.	Support	
16	Vehicular Interface		Support	
	<b>Contextual Response</b>	Optimize built form with respect to mass, spacing and placement on site in consideration to adjacent uses, heights and densities.		
	<b>Topic</b>	<b>Best Practice</b>	<b>Ranking</b>	<b>Commentary</b>
18	Massing relationship to context	Relationship to adjacent properties is sympathetic	Support	
19	Massing impacts on Sun Shade	Sun Shade impacts minimized on public realm and adjacent sites	Support	
20	Massing orientation to street edges	Building form relates / is oriented to the streets on which it fronts.	Support	

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

21	Massing Distribution on Site		Support	
22	Massing contribution to public realm at grade	Building form contributes to a comfortable pedestrian realm at grade	Support	
23	Other			
	<b>Safety and Diversity</b>	Promote design that accommodates the broadest range of users and uses. Achieve a sense of comfort and security at all times.		
	<b>Topic</b>	<b>Best Practice</b>	<b>Ranking</b>	<b>Commentary</b>
24	Safety and Security	CPTED principles are to be employed - good overlook, appropriate lighting, good view lines, glazing in lobbies and entrances.	Support	
25	Pedestrian Level Comfort - Wind	Incorporate strategies to block wind, particularly prevailing wind and downdrafts. Test assumptions and responses via Pedestrian Level Wind Analysis. Particular attention to winter conditions.	Support	UDRP requests a wind and snow study be prepared and submitted to demonstrate comfortable conditions in the alley.
				<b>Additional soft landscaping features have been added in the Pedestrian Alley and the outdoor rooftop amenities at Level 03 to provide mitigation from wind and weather elements. Refer to A102 for revised Site Plan and L1.3 Overall Landscape Plan.</b>

**DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS), ADDITION: RETAIL AND CONSUMER SERVICE BELTLINE (WARD 11) 4 STREET SW AND 17 AVENUE SW**

**MAP 15C**

26	Pedestrian Level Comfort - Snow	Incorporate strategies to prevent snow drifting. Test assumptions and responses via Snow Drifting Analysis. Particular attention to winter conditions.	Support	
27	Weather Protection	Weather protection is encouraged at principal entrances. Continuous weather protection is encouraged along retail / mixed used frontages.	Support	
28	Night Time Design		Support	
29	Barrier Free Design	Site access to be equal for able and disabled individuals. Provide sloped surfaces 5% grade or less vs ramps.	Support	
30	Winter City	Maximize exposure to sunshine for public areas through orientation, massing. Design public realm that supports winter activity.	Support with comment	UDRP requests that more information is provided regarding the public function of the alley during the winter months.
				<b>As there are several CRUs fronting off of the Pedestrian Alley, the Alley will continue to function with full pedestrian accessibility through all four seasons (no difference compared to the CRUs fronting along a major avenue or street). Resident access between the Alley and the residential porte-cochere will also be maintain throughout the year.</b>

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

				Maintenance (such as removal of snow) will be provided to maintain full access and desito the Alley.
31	Other		Unused	
	<b>Service / Utility Design</b>	Promote design that accommodates service uses in functional and unobtrusive manner. Place services uses away from and out of sight of pedestrian areas where possible. Screening elements to be substantive and sympathetic to the building architecture.		
	<b>Topic</b>	<b>Best Practice</b>	<b>Ranking</b>	<b>Commentary</b>
32	Waste / Recycling		Support	
33	Enmax (Power) / Atco (Gas)		Support	
34	Transformer / Switchgear		Support	
35	Exhaust / Intake		Support	
36	Electrical Vaults		Support	
37	Loading		Support	
38	Fire Truck Access		NA	

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
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ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

APPENDIX V

COMMUNITY ASSOCIATION LETTER



September 21, 2017

The City of Calgary  
Planning, Development & Assessment  
Municipal Building  
800 Macleod Trail SE  
Calgary, AB T2P 2M5

ATTN: Desmond Bliet, File Manager

RE: DP2017-2886 – 1520 4th Street SW – Phased Mixed-Use Development

Dear Desmond,

I am writing this letter on behalf of the Beltline Neighbourhoods Association (BNA) in response to the above-noted development application.

The BNA supports this application and believes the development will be a great addition to the neighbourhood. The increase in density to the block, along with the refurbishment of an existing building demonstrate a high regard for the goals set forth in the Beltline ARP.

The project has incorporated interesting elements like a "pedestrian alley" and a rooftop "outdoor cafe" to ensure the project improves the urban realm of the site. The higher FAR in the proposed new DC district aligns with nearby properties, and the distribution of the density on the site is logical.

The applicant has been engaged with the BNA Planning Group throughout their early design process, and presented the project to our group last winter, welcoming feedback.

In closing, the BNA supports this application as is, and is looking forward to seeing it take shape. Thank you for giving the BNA an opportunity to provide a comment.

Please note that the BNA's Director of Planning, Tyson Bolduc, has not been a part of this review process due to his prior professional involvement with this project.

Sincerely,

Peter Oliver  
President, Beltline Neighbourhoods Association

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

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**APPENDIX VI**

**PROPOSED DIRECT CONTROL GUIDELINES**

Note: these Direct Control guidelines are part of LOC2017-0175.

**Purpose**

1 This Direct Control District is intended to:

- (a) allow Heritage Density Transfer to a **DC receiving parcel** (site 2) from **DC source parcels** (sites 3 and 4) as allowed by Part 11, Division 7, of Land Use Bylaw 1P2007.
- (b) allow for additional **uses** on Sites 1 and 2.

**Compliance with Bylaw 1P2007**

2 Unless otherwise specified, the rules and provisions of Parts 1, 2, 3 and 4 of Bylaw 1P2007 apply to this Direct Control District.

**Reference to Bylaw 1P2007**

3 Within this Direct Control District, a reference to a section of Bylaw 1P2007 is deemed to be a reference to the section as amended from time to time.

**General Definitions**

4 In this Direct Control District:

- (a) “**DC receiving parcel**” means the **parcels** legally described as Lots 1-17, Block 120, Plan C with the municipal addresses, 1506 and 1508 4 Street SW and 319, 323, 327, 333, and 337 15 Avenue SW which are the **parcels** receiving an increase in **density** of 14,032.7 square metres from the **DC source parcels**.
- (b) “**DC source parcel**” means the **parcels** legally described as as Lots 21-23, Block 72, Plan A1 with the municipal address 1121 6 Street SW and Lots 38-40, Block 100, Plan C with the municipal address 1324 1 Street SW which are the **parcels** from which the transfer of **density** is being made.

**Site 1**

0.12 hectares (± 0.30 acres)

**Application**

5 The provisions in sections 6 through 9 apply only to Site 1.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

**Permitted Uses**

6 The **permitted uses** of the Centre City Mixed Use District (CC-X) of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

**Discretionary Uses**

7 The **discretionary uses** of the Centre City Mixed Use District (CC-X) of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of:

- (a) **Drinking Establishment – Large;**
- (b) **Restaurant: Food Service Only – Large;**
- (c) **Restaurant: Licensed – Large; and**
- (d) **Restaurant: Neighbourhood.**

**Bylaw 1P2007 District Rules**

8 Unless otherwise specified, the rules of the Centre City Mixed Use (CC-X) District of Bylaw 1P2007 apply in this Direct Control District.

**Floor Area Ratio**

- 9 (1) The maximum **floor area ratio** is 5.0.
- (2) The maximum **floor area ratio** may be increased from 5.0 to 8.2 in accordance with the bonus provisions contained in Part 11, Division 7 of Bylaw 1P2007.

**Site 2**

0.55 hectares (± 1.36 acres)

**Application**

10 The provisions in sections 11 through 15 apply only to Site 2.

**Permitted Uses**

11 The **permitted uses** of the Centre City Multi-Residential High Rise Support Commercial District (CC-MHX) of Bylaw 1P2007 are the **permitted uses** in this Direct Control District.

**Discretionary Uses**

12 The **discretionary uses** of the Centre City Multi-Residential High Rise Support Commercial District (CC-MHX) of Bylaw 1P2007 are the **discretionary uses** in this Direct Control District with the addition of:

- (a) **Dinner Theatre;**
- (b) **Drinking Establishment – Large;**
- (c) **Drinking Establishment – Medium;**
- (d) **Food Kiosk;**
- (e) **Night Club;**
- (f) **Restaurant: Food Service Only – Large;**

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

- (g) Restaurant: Food Service Only – Medium;
- (h) Restaurant: Licensed – Large;
- (i) Restaurant: Licensed – Medium; and
- (j) Restaurant: Neighbourhood.

**Bylaw 1P2007 District Rules**

- 13 Unless otherwise specified, the rules of the Centre City Mixed Use District (CC-MHX) of Bylaw 1P2007 apply in this Direct Control District.

**Floor Area Ratio**

- 14 (1) The maximum ***floor area ratio*** is 7.55, which includes the 14,032.7 square metres which has been transferred from Sites 3 and 4.
- (2) The maximum ***floor area ratio*** may be increased from 7.55 to 9.0 in accordance with the bonus provisions contained in Part 11, Division 7 of Bylaw 1P2007.

**Location of Uses**

- 15 The following ***uses*** must not front onto 15 Avenue SW:

- (a) Dinner Theatre;
- (b) Drinking Establishment – Large;
- (c) Drinking Establishment – Medium;
- (d) Food Kiosk;
- (e) Night Club;
- (f) Restaurant: Food Service Only – Large;
- (g) Restaurant: Food Service Only – Medium;
- (h) Restaurant: Licensed – Large;
- (i) Restaurant: Licensed – Medium; and
- (j) Restaurant: Neighbourhood.

**Site 3**

0.08 hectares (± 0.21 acres)

**Application**

- 16 The provisions in sections 17 through 20 apply only to Site 3.

**Permitted Uses**

- 17 The ***permitted uses*** of the Centre City Mixed Use District (CC-X) of Bylaw 1P2007 are the ***permitted uses*** in this Direct Control District.

**Discretionary Uses**

- 18 The ***discretionary uses*** of the Centre City Mixed Use District (CC-X) of Bylaw 1P2007 are the ***discretionary uses*** in this Direct Control District.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

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**Bylaw 1P2007 District Rules**

19 Unless otherwise specified, the rules of the Centre City Mixed Use District (CC-X) of Bylaw 1P2007 apply in this Direct Control District.

**Floor Area Ratio**

20 (1) The maximum *floor area ratio* is 1.42

(2) The bonus provisions contained in Part 11, Division 7 of Bylaw 1P2007 do not apply to site 3 of this Direct Control District.

**Site 4**

0.05 hectares (± 0.12 acres)

**Application**

21 The provisions in sections 22 through 25 apply only to Site 4.

**Permitted Uses**

22 The *permitted uses* of the Centre City Commercial Corridor District (CC-COR) of Bylaw 1P2007 are the *permitted uses* in this Direct Control District.

**Discretionary Uses**

23 The *discretionary uses* of the Centre City Commercial Corridor District (CC-COR) of Bylaw 1P2007 are the *discretionary uses* in this Direct Control District.

**Bylaw 1P2007 District Rules**

24 Unless otherwise specified, the rules of the Centre City Commercial Corridor District (CC-COR) of Bylaw 1P2007 apply in this Direct Control District.

**Floor Area Ratio**

25 (1) The maximum *floor area ratio* is 1.55

(2) The bonus provisions contained in Part 11, Division 7 of Bylaw 1P2007 do not apply to site 4 of this Direct Control District.

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

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**APPENDIX VII**

**PROPOSED AMENDMENTS TO THE BELTLINE  
AREA REDEVELOPMENT PLAN**

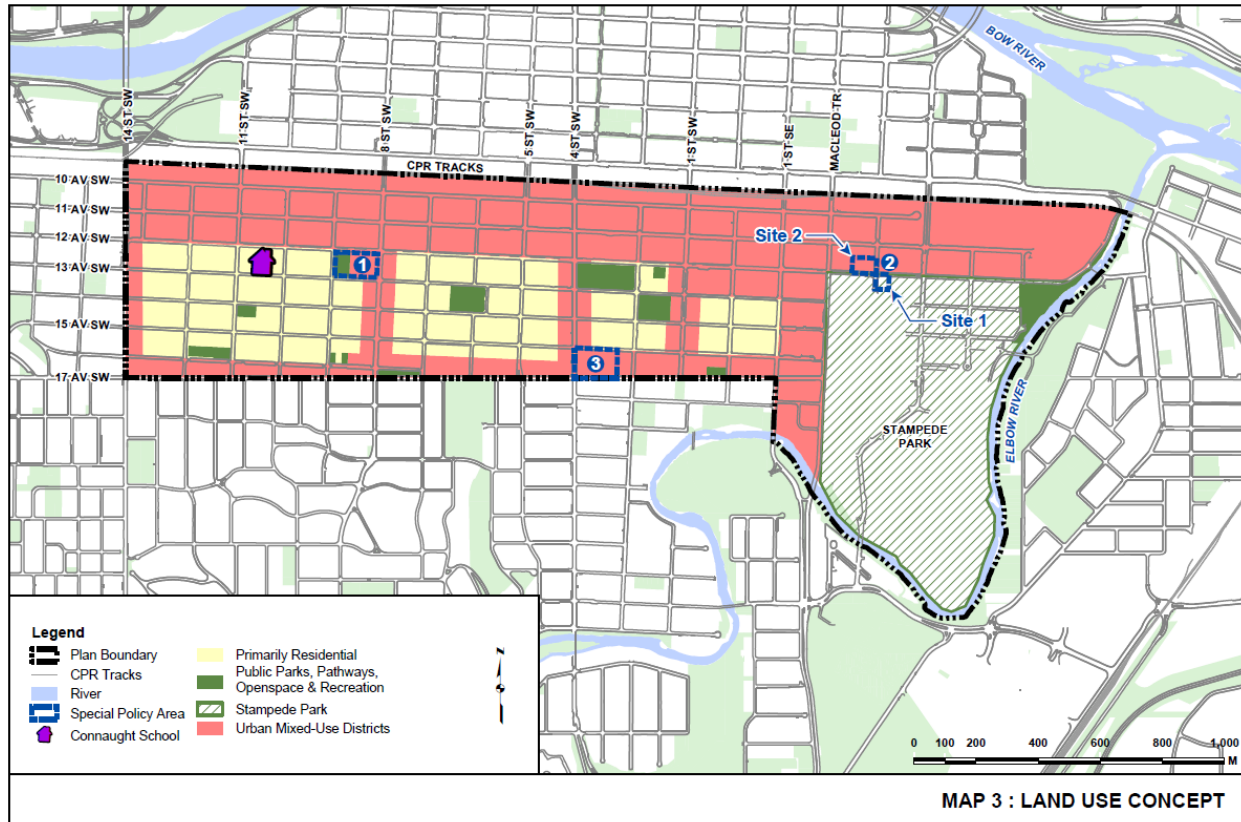
Note: these proposed amendments are part of LOC2017-0175.

- (a) Delete the existing Map 3 entitled “Land Use Concept” and replace with new Map 3 entitled “Land Use Concept” (APPENDIX VIII)
- (b) In Section 4.0, subsection 4.6, insert:
  - “4.6.3 Special Policy Area 3
  - General Policies
  - a) Notwithstanding Section 5, subsection 5.2 of this plan, applications for increased density for sites within Density Area D may be considered within Special Policy Area 3 provided that they contribute to the animation of a pedestrian connector linking 17 Avenue and 4 Street SW, support a pedestrian-oriented experience along 17 Avenue SW, and demonstrate a transition in massing and height from high density development in the Beltline to lower density development south of 17 Avenue SW.”
- (c) Delete the existing Map 4 entitled “Density Areas” and replace with new Map 4 entitled “Density Areas” (APPENDIX IX).

DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
 SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
 ADDITION: RETAIL AND CONSUMER SERVICE  
 BELTLINE (WARD 11)  
 4 STREET SW AND 17 AVENUE SW

MAP 15C

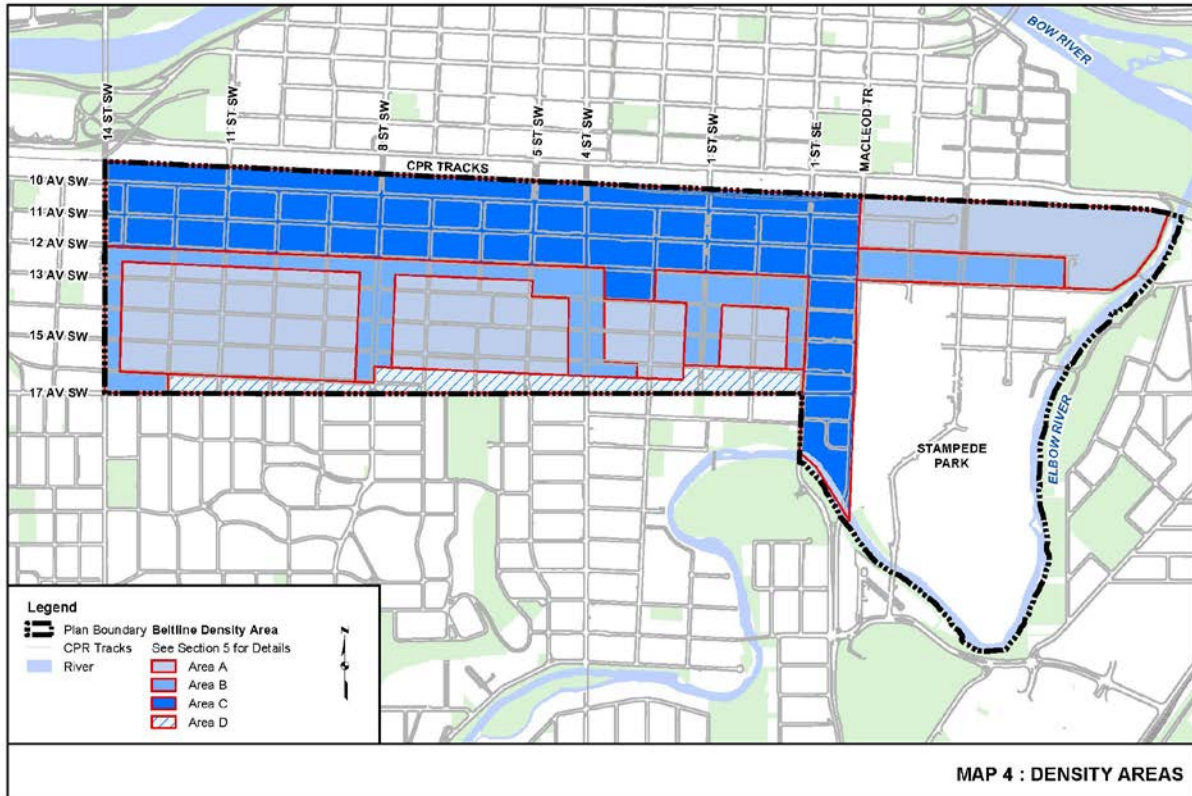
**APPENDIX VIII**



DEVELOPMENT PERMIT – NEW: RETAIL AND CONSUMER  
SERVICE, DWELLING UNIT (4 BUILDINGS, 548 UNITS),  
ADDITION: RETAIL AND CONSUMER SERVICE  
BELTLINE (WARD 11)  
4 STREET SW AND 17 AVENUE SW

MAP 15C

**APPENDIX IX**



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through a transit instrument mounted on a tripod. The surveyor is holding a pencil in their right hand. The background is white.

**Calgary Planning Commission 2018 February 22**

**Item 5.07**



**POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW**

**MAP 34W**

**EXECUTIVE SUMMARY**

This Land Use Amendment application seeks to redesignate a single residential parcel from Residential – Contextual One / Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District to allow for the development of rowhouses. A development permit application has also been submitted and is currently under review.

An amendment to the Bowness Area Redevelopment Plan (ARP) is required to accommodate this Land Use Amendment application.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment.

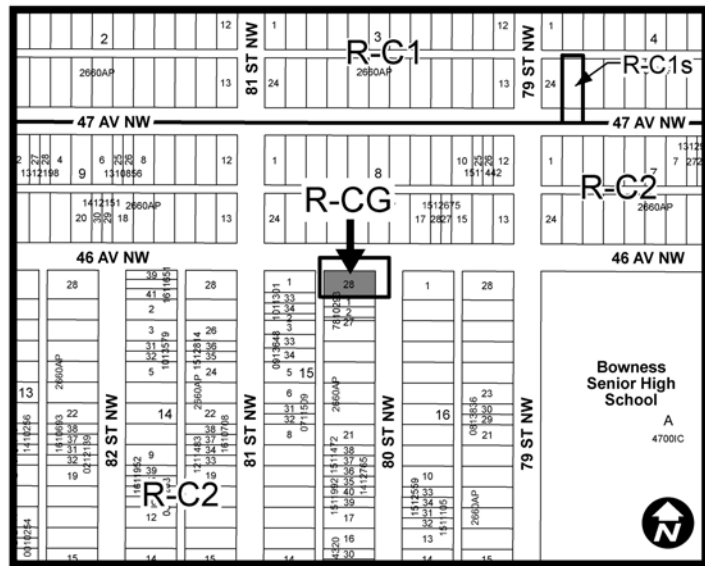
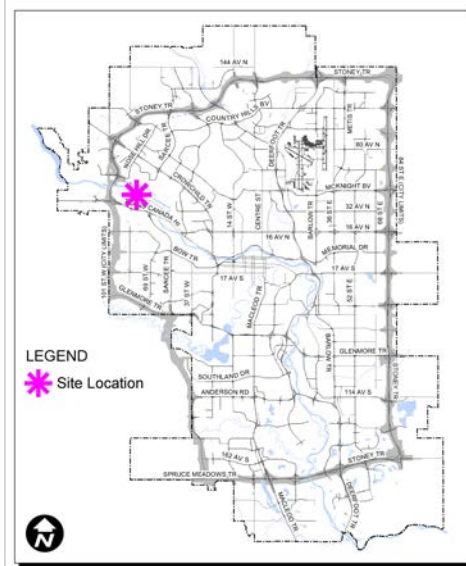
**REASON(S) FOR RECOMMENDATION:**

This proposal is in conformance with the applicable policies of the Municipal Development Plan and the Local Area Plan as amended. The proposed land use district was designed to be implemented in proximity to, or directly adjacent to, low density residential development. The proposal represents a modest increase in density and allows for a development that has the ability to be compatible with the character of the existing neighbourhood.

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

LOCATION MAPS



POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendment to the Bowness Area Redevelopment Plan (APPENDIX III).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.07 hectares  $\pm$  (0.17 acres  $\pm$ ) located at 4655 – 80 Street NW (Plan 2660AP, Block 15, Lot 28) from Residential – Contextual One/Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District.

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

**Applicant:**

Lasting Legacies

**Landowner:**

1620719 Alberta Ltd (Cory Fell)

**PLANNING EVALUATION**

**SITE CONTEXT**

The subject parcel is located in the community of Bowness, on the corner of 46 Avenue NW and 80 Street NW. The site is currently developed with a single detached dwelling and has access from the rear lane. The existing R-C2 district allows for a maximum of two dwelling units on the site. Surrounding development consists of low density residential dwellings to the north, east, west and south.

<b>Bowness</b>	
Peak Population Year	1982
Peak Population	13,134
2017 Current Population	10,895
Difference in Population (Number)	- 2,239
Difference in Population (Percent)	-17%

**LAND USE DISTRICTS**

The proposed R-CG district is intended to accommodate grade-oriented development in the form of rowhouses in close proximity or directly adjacent to low density residential development. The district offers flexibility with regards to building setbacks and building height that facilitate integration of a diversity of grade-oriented housing types over time. At the maximum permitted density of 75 units per hectare, this site could potentially accommodate 4 units.

**LEGISLATION & POLICY**

**South Saskatchewan Regional Plan (SSRP)**

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan (SSRP).

**POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW**

**MAP 34W**

Municipal Development Plan (MDP)

The subject parcel is located within the Developed Residential Area - Inner City typology of the MDP, which supports moderate intensification that respects the community context. In general, these policies encourage redevelopment of the Inner City area in a way that is similar in scale and built form to existing development and contributes to a greater variety of housing types overall. The MDP also encourages higher residential densities in areas that are well serviced by existing infrastructure, public amenities, and transit.

The proposed R-CG District allows for development that is sensitive to the existing context and conforms to the relevant policies of the MDP. The subject site is adjacent to low density residential development, is on a corner parcel, and will allow for a greater variety of housing forms in the area.

Bowness Area Redevelopment Plan (ARP)

The Bowness ARP supports compatible infill development. In order to bring the proposal into alignment with the ARP, due to the density and use proposed on these parcels, it is necessary to make a minor amendment to Map 2 – Land Use Policy of the ARP to change the designation of the parcels from Conservation/Infill to Low Density Residential. This amendment would allow for grade-oriented development in the form of Rowhouses, Duplex Dwellings, Semi-detached Dwellings and/or Cottage Housing Clusters that are already allowed in certain locations by the ARP.

Location Criteria for Multi-residential Infill

The proposed land use generally aligns with several of the location criteria for multi-residential infill development in low density residential areas. The following chart provides a summary of these criteria. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP, to assist in determining the appropriateness of an application in the local context.

<b>LOCATION CRITERIA</b>	<b>MEETS CRITERIA?</b>
1. On a corner parcel	Yes
2. Within 400 metres of a transit stop	Yes
3. Within 600 metres of an existing or planned primary transit stop	No
4. On a collector or higher standard roadway on at least one frontage	Yes
5. Adjacent to existing or planned non-residential development or multi-unit development	No

**POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW**

**MAP 34W**

6. Adjacent to or across from an existing or planned open space, park or community amenity	No
7. Along or in close proximity to an existing or planned corridor or activity centre	No
8. Direct Lane Access	Yes

**TRANSPORTATION NETWORKS**

Pedestrian and vehicular access to the site is available from 46 Avenue NW and the rear lane. The area is served by Calgary Transit's primary transit network Bus Rapid Transit (BRT) route 305 with a bus stop location within approximately 900 metres walking distance of the site on Bowness Road NW. The site is 32 metres walking distance from the Route 40 bus stop servicing the Crowfoot and Lions Park LRT stations. On-street parking adjacent to the site is unregulated through the Calgary Parking Authority's residential parking permit system.

**UTILITIES & SERVICING**

Water is available to service the site which is within the Glenmore pressure zone. The existing water system is able to support the proposed development. Sanitary sewer is available. No storm sewer is immediately available to service the site. At the developer's expense, a drywell or a storm extension can be provided. The storm can be extended from the rear lane or 46 Avenue NW.

**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required.

**ENVIRONMENTAL SUSTAINABILITY**

Sustainability measures will be evaluated at the development permit stage.

**GROWTH MANAGEMENT**

The proposed amendment does not trigger capital infrastructure investment and therefore there are no growth management concerns at this time.

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

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## PUBLIC ENGAGEMENT

### Community Association Comments

Administration received a letter in opposition from the Bowness Community Association (APPENDIX II).

The comments received are summarized as follows:

#### Locations for infill & densification within community

Concern expressed claims that other areas within the community would better accommodate proposed development density.

#### Loss of affordable single family homes

Concern that a land use amendment to accommodate higher density would result in loss of affordable single detached homes in the city.

#### Row housing contextually inappropriate for subject site

Concern that the proposed rowhouse development is contextually inappropriate and would negatively affect the neighbourhood.

### Citizen Comments

Administration received two letters in support of the proposed redesignation.

Administration received 11 letters and a petition signed by 51 people in opposition to the proposed redesignation.

Reasons stated in opposition were focused on the following themes:

#### Increased traffic and parking concerns

Concern was expressed over increased traffic and reduced on-street parking. Administration established that 46 Avenue NW is classified as a Collector road, and on-site parking and visitor parking will be required as part of a future Development Permit application.

#### Loss of property value

Concern that surrounding property values would be negatively affected.

#### Contextually inappropriate

Row housing is contextually inconsistent with all existing housing in this area of Bowness. The review of the Development Permit by Administration will ensure it is contextually appropriate and conforms to the Land Use Bylaw.

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

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Privacy concerns

Row housing will lead to overlooking into neighbouring yards and homes and loss of privacy. The review of the Development Permit by Administration will ensure privacy of adjacent homes is appropriately maintained.

Precedent for future applications

Redesignation to R-CG would be the first in Bowness and set precedent in the whole community which should be consulted prior to change of this magnitude.

Administration advised the applicant to engage the community early in the application process. However, the applicant has chosen not to engage community stakeholders to respond to their concerns either during or after the Administration's review of the subject application.

**Public Meetings**

No public meetings were held.

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

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**APPENDIX I**

**APPLICANT'S SUBMISSION**

This Bowness community corner lot is definitely a prime candidate for redevelopment and an increased densification, as there are examples of this found throughout this community. Corner lots are typically prime candidates for redevelopment, and the developer is looking to right-size the number and size of housing units. as a wider lot than normal, a semi-detached would still under-use the land, and a triplex or fourplex would be more suitable for a density increase.

The developer is proposing a low two-storey 3-bedroom low density design - change of use from the Current zoning of R-C2 to the relatively new zoning of R-CG. The project is located on a bus route. The developer focuses on Bowness, and especially this part of Bowness, having built numerous projects within blocks of this location.

The proposed development is planned to be respectful to its' neighbors, and contextually designed and built with quality materials. The development will allow a more affordable option for new families to move into this section of the neighborhood, as there are schools, shopping, and Bowness Park and the river within walking distance of this development, which provides incentive for young families to move here. By building 4 housing units where there is now one, and normally 2 under R-C2 zoning, it will allow for a drop in land price, which will result in a more affordable home purchase.

The R-CG rezoning request recognizes the balance between higher density and lower density housing in a redeveloping community . It allows for larger private amenity space, and more green space in general, than its' counterpart M-CG.

R-CG accommodates existing residential development.

R-CG accommodates grade-oriented development in the form of Rowhouse Buildings.

R-CG provides flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time.

R-CG accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

This proposed redesignation will feature:

1. A quality-built 4 Unit Single building that is dual-street orientated
2. All Units will be 3-Bedroom with the possibility of a fourth bedroom in the lower level
3. Grade-Orientated
4. Single parking stall/per unit in indoor garage
5. Visitor parking stall
6. Waste and recycling respectful of division of waste, recyclable, and compostable materials
7. Larger % of greenspace and amenity space than found in higher densification changes
8. Low-profile building
9. Design to respect neighboring home outdoor space privacy

With the City of Calgary looking to increase innercity density and right-size new development, we believe that this lot is suitable for redesignation to R-CG. This will also be another project to assist in revitalizing Bowness, in its' own small way.

Thank you for your consideration and time in this matter,

The Legacies Team

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

APPENDIX II

COMMUNITY ASSOCIATION LETTER

Circulation Control  
Planning and Development  
P.O. Boxx 2100 Station M  
IMC 8201

Attn: [CPAG.circ@calgary.ca](mailto:CPAG.circ@calgary.ca)  
Re: LOC2017 – 0281 (4655 80 St NW)  
Cc: Felix Ochieng ([felix.ochieng@calgary.ca](mailto:felix.ochieng@calgary.ca))

October 20, 2017

To Whom It May Concern:

Thank you for the opportunity to comment with respect to the Land Use Amendment for a property located at 4655 80 St NW.

The review of this Land Use Application by the Bowness Planning and Development Committee, which proposes an amendment of the existing land use R-C2 to R-CG involved notification to neighbours adjacent to the subject parcel as well as discussion at the Oct 4 Planning and Development Committee Meeting. The comments below regarding this LOC application are those of both committee members and neighbours who have provided comments to us. In addition to this letter you should be receiving comments from several residents who will be impacted by this proposal.

The application is seeking a new land use designation of R-CG from the existing R-C2. We are **opposed to this redesignation** for the following reasons:

1. The Community of Bowness is not adverse to densification and over the past few years we have seen many new developments approved from secondary suites through to multi-unit developments. Attainable Homes, Habitat for Humanity and Homespace have multifamily projects located throughout the community and, most recently a 17 unit development was approved in the community. We will be embarking on a Main Street program next summer which will provide opportunity for densification along and adjacent to the designated main street corridor. As well, there is a parcel of land located along Bowness Road between 30 – 33 Avenue that has been rezoned M-C1 which will allow for development of higher density. Greenwich Village/Greenwood Briar a new community located on the west portion of the community is designed with significantly higher densities. The first development permit for this area was submitted early in 2017 and construction has commenced.
2. We believe that the community is in support of meaningful increased density.
3. The redesignation of this individual parcel is opposed as there exists a range of Land Use Districts in our community that would better accommodate a development such as the one proposed. There are areas within the community that consist of multifamily units and could use an injection of newer more modern development to assist in the rejuvenation of the area. We see this creep of increased density through rezoning as part of a troubling trend –

POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

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we struggle with applications for redesignations where land use districts that allow for meaningful gains are abundant in our community; districts or areas that if rezoned could accommodate the kind of density contemplated by the R-CG applications.

4. There is a loss of affordable single family homes which, if left unchecked, will be a loss for the entire city as Bowness has always been able to provide good, safe entry level housing for those young families who wish to purchase a single family home. If developers see the potential to build 4 units instead of 2 or 3 the entire community will be impacted.
5. From a contextual perspective this proposal for a 4 unit row house located on a corner lot has the potential to negatively affect neighbours to a great degree. A city of Calgary document entitled "Low Density Residential Infill Housing Guidelines for Established Communities" states "A strong emphasis is placed on encouraging development to respect and enhance the overall quality and character of the street and community in which it takes place." This proposed development in this location violates this principle.
6. The Federation of Calgary Communities is working with the City Planning department to discuss the communities concerns on the whole infill and density issue. Again, it is imperative to state that we are not against densification but, for certain classifications, we would like to be able to plan where they would fit in OUR COMMUNITY.

Please feel free to contact the Bowness Planning and Development Committee to discuss this matter further.

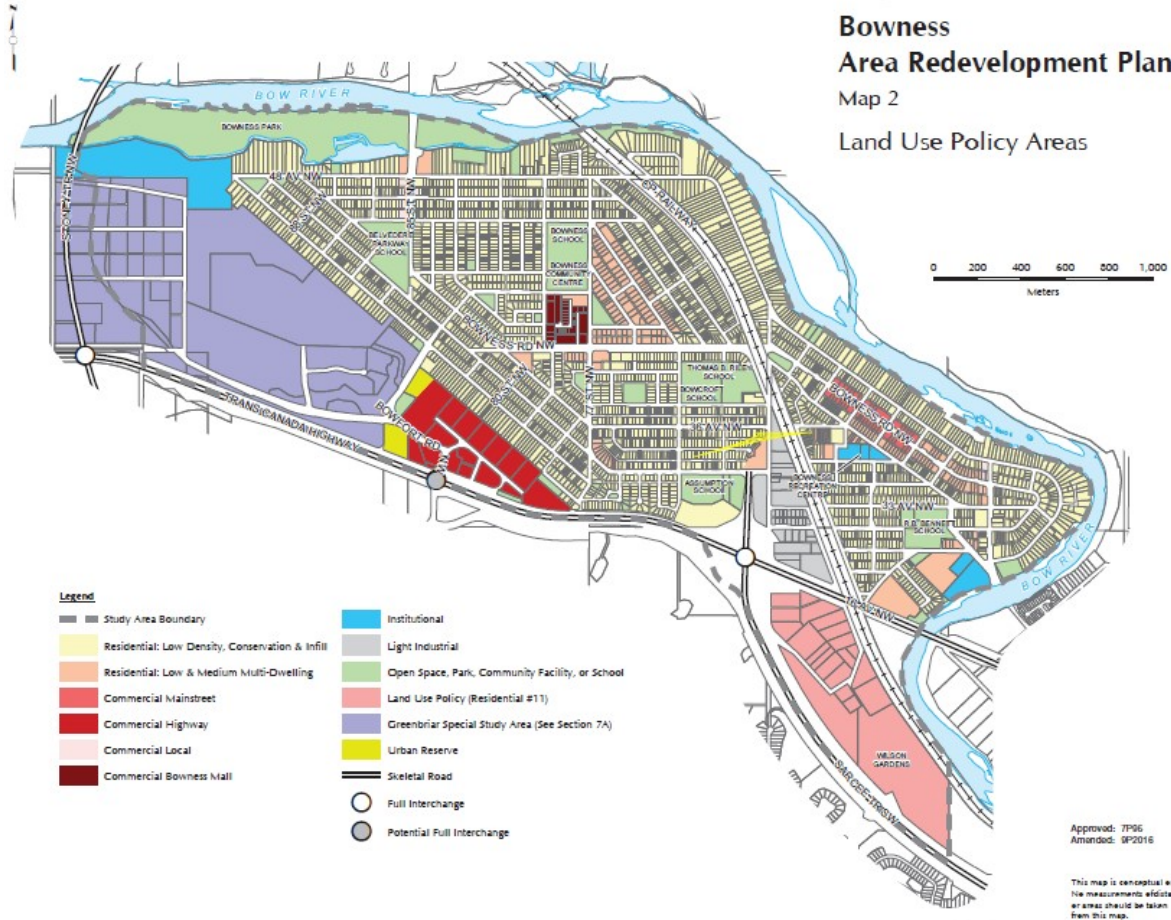
POLICY AMENDMENT AND LAND USE AMENDMENT  
BOWNESS (WARD 1)  
46 AVENUE NW AND 80 STREET NW

MAP 34W

APPENDIX III

PROPOSED AMENDMENT TO THE BOWNESS  
AREA REDEVELOPMENT PLAN

- (a) Delete the existing Map 2 entitled “Land Use Policy Areas” and replace with the revised Map 2 entitled “Land Use Policy Areas”, as follows:



A grayscale silhouette of a surveyor wearing a wide-brimmed hat and a jacket, looking through the eyepiece of a theodolite. The theodolite is mounted on a tripod. The surveyor's right hand is on the instrument, and a pencil is visible in their left hand. The background is plain white.

**Calgary Planning Commission 2018 February 22**

**Item 5.08**



**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

**EXECUTIVE SUMMARY**

This Land Use Amendment application proposes the redesignation of a corner parcel from Residential-Contextual Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District in the community of Richmond. The application is intended to change the designation to allow for a new rowhouse development featuring four units.

An amendment to the Richmond Area Redevelopment Plan (ARP) is required to accommodate the proposed redesignation. The ARP amendment seeks to reclassify this site from the current “Conservation and Infill” typology to “Low Density Residential”.

Administration has received a development permit application DP2017-5627 for a new four (4) unit rowhouse building and corresponding four (4) car garage. This application does not include proposals for secondary suites. The application is currently under review and its approval is contingent upon the adoption of the Local Area Plan and Land Use Bylaw amendment by Council.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Policy Amendment and Land Use Amendment

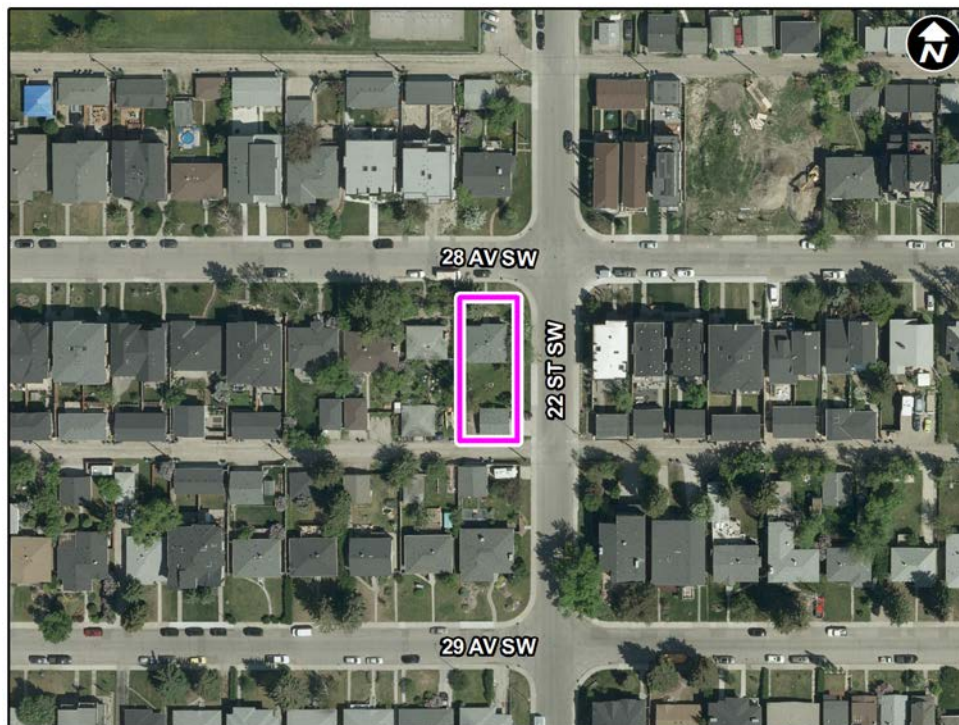
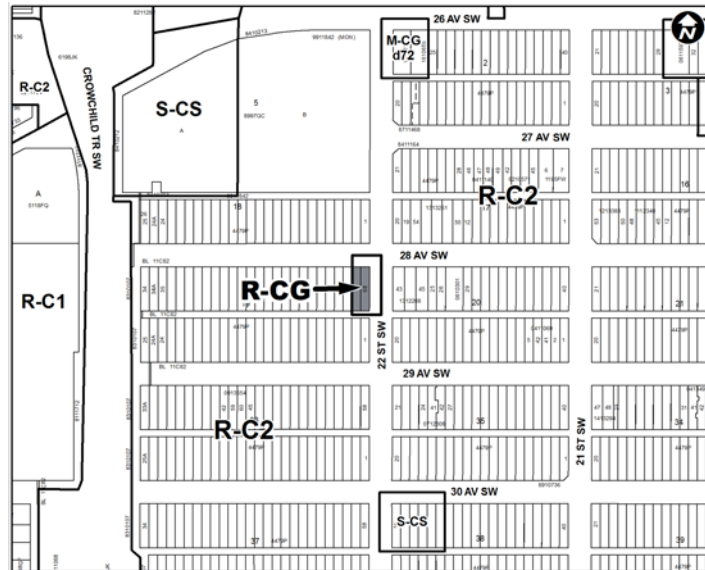
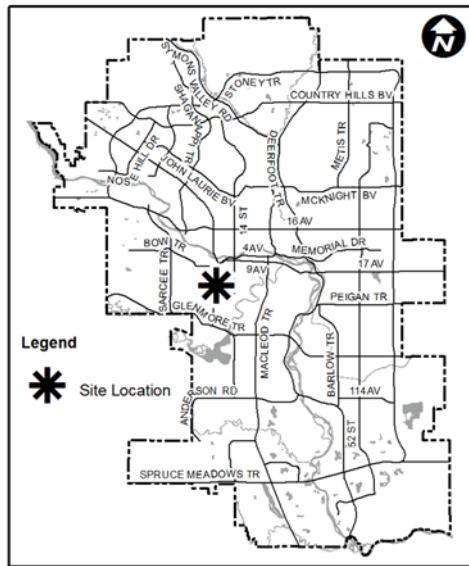
**REASON(S) FOR RECOMMENDATION:**

The proposed Local Area Plan amendment and Land Use amendment are in keeping with the relevant goals and policies of the Municipal Development Plan and serve to accommodate future development that enables a moderate increase in density while maximizing the use of existing infrastructure. Moreover, the proposed redesignation seeks to accommodate a development that meets the City’s “Location Criteria for Multi-Residential Infill” as outlined in report to Council PUD2016-0405.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

LOCATION MAPS



POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

1. Recommend that Council **ADOPT**, by bylaw, the proposed amendment to the Richmond Area Redevelopment Plan (APPENDIX II).
2. Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.06 hectares  $\pm$  (0.14 acres  $\pm$ ) located at municipal address 2403 - 28 Avenue SW (Plan 4479P, Block 19, Lots 57 and 58) from Residential-Contextual Two Dwelling (R-C2) District **to** Residential – Grade-Oriented Infill (R-CG) District.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

**Applicant:**

Civicworks Planning + Design

**Landowner:**

South Calgary 24 Ltd

**PLANNING EVALUATION**

**SITE CONTEXT**

The subject site is located at the southwest corner of 22 Street SW and 28 Avenue SW, in the community of Richmond. An existing single detached dwelling occupies the site with a back lane providing access to a detached garage. The immediate surrounding area consist entirely of single detached dwellings. The Richmond Elementary School and the Richmond Community Hall buildings along with associated green spaces are located a block to the north. The ARP amendment and redesignation of the subject site is expected to result in a moderate increase to density for the area.

Data from The City of Calgary 2017 Census is provided below showing the peak population and differences between the peak and 2017 population figures.

<b>Richmond</b>	
Peak Population Year	1968
Peak Population	5,080
2017 Population	4,781
Difference in Population (Number)	-299
Difference in Population (Percent)	-6%

**LAND USE DISTRICTS**

**Current Land Use District**

The Residential – Contextual Two Dwelling (R-C2) District is intended to accommodate existing residential development in the form of duplex, semi-detached and single detached dwellings in developed areas of the City to a maximum of two units.

**Proposed Land Use District**

The Residential – Grade-Oriented Infill (R-CG) District is intended to:

- accommodate existing residential development;
- accommodate grade-oriented development in the form of rowhouse, duplex, semi-detached and cottage housing clusters;
- accommodate secondary and backyard suites within new and existing residential development;

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

- provide flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing forms over time; and
- accommodates site and building design that is adaptable to the functional requirements of evolving household needs.

The Residential – Grade Oriented infill (R-CG) District allows for a maximum residential density of 75 units per hectare, and can accommodate infill housing forms that include secondary suites. As per the applicant's submission, no secondary suites are being proposed as part of the proposed 4 unit rowhouse development. The proposed density for this parcel with the four units will be 69 units per hectare – below the maximum of the district.

## **LEGISLATION & POLICY**

### South Saskatchewan Regional Plan (SSRP)

This provincial plan establishes a vision for the region using a cumulative effects management approach that requires alignment of local land use decisions with Alberta's long-term economic, environmental and social goals. The proposed redesignation complies with the overall goals of the provincial plan including the Land Use Patterns policies (subsection 8.14).

### Municipal Development Plan (MDP)

The site is located within a "Residential Developed – Established Area" on the Urban Structure Map (Map 1) in the *Municipal Development Plan* (MDP). While the MDP makes no specific reference to this site, this land use proposal is consistent with MDP policies including the Developed Residential Areas policies (subsection 3.5.1), the Neighbourhood Infill and Redevelopment policies (subsection 2.2.5) and the Housing Diversity and Choice policies (subsection 2.3.1).

The proposed Residential – Grade-Oriented infill (R-CG) District allows for development that is sensitive to the existing context and conforms to the relevant policies of the MDP. The subject site is located on a corner parcel, and will allow for a greater variety of housing forms in the area. For this reason, the proposed redesignation is found to comply with the general goals of the MDP while providing sound rationale to accommodate the minor amendment to the Richmond Area Redevelopment Plan.

### Richmond Area Redevelopment Plan (ARP)

This application was reviewed against the applicable policies of the Richmond Area Redevelopment Plan, being the applicable Local Area Plan. The Land Use Policy map of the ARP identifies the parcel as being "Conservation and Infill." The intent of the Conservation and Infill typology is to improve existing neighbourhood quality and character while permitting low profile infill development that is compatible with the surrounding dwellings.

The redesignation of the parcel to Residential – Grade-Oriented Infill (R-CG) District requires a site specific minor amendment to Map 2, see Appendix II. The ARP amendment to change the classification to "Low Density Residential" ensures alignment with the policy framework. The proposed classification is intended to provide for low profile family-oriented redevelopment that includes multi-dwelling infill projects comprised of townhousing or stacked townhousing.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

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Location Criteria for Multi-Residential Infill

The “Location Criteria for Multi-Residential Infill” are a set of guidelines that were adopted by Council on 2014 March 31. These guidelines provide measures to assess the appropriateness of multi-residential infill within its immediate context. While the guidelines were intended to be used in the evaluation of proposed M-CG, M-C1 and M-C2 districts, using the same guidelines in evaluating the proposed R-CG district is found to be appropriate for the proposed redesignation. The list of criteria and assessment is summarized in APPENDIX III.

The subject parcel meets five (5) of the eight (8) location criteria for multi-residential infill. The three criteria which the subject parcel does not meet are not deemed to be critical in appropriately accommodating infill residential in the scale that can be accommodated by the proposed redesignation. For such reasons, the redesignation from Residential-Contextual Two Dwelling (R-C2) District to Residential – Grade-Oriented Infill (R-CG) District generally complies with the Location Criteria guidelines.

**TRANSPORTATION NETWORKS**

Subject parcel is located on a corner lot with vehicular access from the lane. On street parking is available on adjacent streets. Nearest transit stop is 250m from the parcel and is located along 26 Avenue SW (served by Transit Route 6). Additional transit service is available along Crowchild Trail with the South West BRT route also serving the area in the near future. The transit stops on Crowchild Trail SW are almost 300 meters from the parcel.

**UTILITIES & SERVICING**

Sanitary and stormwater mains are available to service the site. A 150 mm water main and 250 mm sanitary sewer are both available from 28 Avenue SW. No upgrades are required for either.

A 375 millimetre storm sewer line is available off of 22 Street SW. Further comments including allowable release rate will be provided to the developer at the time of development permit review.

**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required at the time of review.

**ENVIRONMENTAL SUSTAINABILITY**

Sustainability measures will be evaluated at the Development Permit review stage.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

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## **GROWTH MANAGEMENT**

The proposed amendments do not trigger capital infrastructure investment, and therefore there are no growth management concerns at this time.

## **PUBLIC ENGAGEMENT**

### **Citizen Comments**

During Administration's review of the proposed amendments the application was circulated to the adjacent neighbours. Letters from the community and adjacent properties resulted in fourteen (14) letters of opposition. A summary of opposition issues are provided below:

- The proposed application and any subsequent development cannot be supported because it deviates from the community plan and applicable ARP policies for Richmond.
- The proposed 3 storey high development will overshadow adjacent lot(s).
- Proposal to proceed with 4 units and building orientation will lead to more units backing onto the adjacent lot and affect privacy. Also, unclear on how many upper storey windows will overlook into the existing homes.
- The reduced building separation (setbacks) between the proposed redevelopment and adjacent existing dwellings is of concern due to privacy.
- With provisions for Secondary Suites added into the development, it is likely more than 4 dwelling units that will be developed, which will lead to parking shortage and additional traffic in the area amounting to congestion on roadways for local traffic. Traffic in this area is already a problem during peak and school hours introducing more vehicles to the area may compromise safety.
- The popularity of the adjacent Marda Loop commercial areas often leads visitors to this part of the city to "spill over" and park on close by residential streets. The proposed rezoning needs to consider the cumulative impacts from off-site parking and 4 units (or more) being developed.
- The area in general is lacking an overall transportation/traffic/mobility plan to ensure that quality of life for the area is not reduced by piecemeal approval of higher density land uses.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

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- A proposed increase in density will significantly and adversely affect numerous adjacent residents and will detract from the character of the neighbourhood, which did not anticipate this amount of density to be tagged at the end of a lower density residential block. Should zoning for row housing be allowed in this locale, a terrible precedent will be set; opening the door for considerable additional density not anticipated or supported by community residents.

### **Community Association Comments**

The Richmond/Knob Hill Community Association provided formal comment outlining its opposition to the application, refer to APPENDIX IV. The Community Association comments have been attached to this document for your review. The following reasons were given for the opposition:

- MDP policies encourage the provision of a broader range of affordable housing to all income levels, and strategies to densify should be determined through a community planning process that involves a broader community engagement on where density is appropriate.
- The current land use typology within the applicable ARP identifies the site as “Conservation and Infill”, and the amendment of the ARP map is not supported by the Community Association.
- The site is not located on a collector or higher standard roadway and does not meet several other requirements as outlined in the document titled “Location Criteria for Multi-Residential Infill” set by Council in 2014.

Administration reviewed all comments and concerns raised by area residents and the Community Association and forwarded them on to the applicant to address. In response, the applicant prepared a comprehensive response that addressed all the concerns. Refer to APPENDIX VI for the applicant’s perspective.

Administration’s perspective to the community opposition is informed by Council’s recent direction, found in the policies and objectives of the Municipal Development Plan. Considering the age of the outdated Richmond ARP, Administration’s assessment is that the Local Area Plan policies and direction have not kept up, and misalign with the more recent corporate objectives and priorities around infill redevelopment. Administration is satisfied with the applicant’s response and supports this application, which meets several overarching objectives of the Municipal Development Plan and enables a moderate increase in density while maximizing the use of existing infrastructure.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

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**Applicant-Initiated Public Engagement**

The applicants have provided a summary of their public engagement with the community, see APPENDIX V. The applicant used various strategies to communicate its intent to develop the parcel for a four (4) unit rowhouse prior to the submission.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

APPENDIX I

APPLICANT'S SUBMISSION



460 - 5119 Elbow Drive SW P 403 201 5305  
Calgary, Alberta T2V 1H2 F 403 201 5344

OCTOBER 30, 2017

City of Calgary  
Planning and Building  
800 MacLeod Trail SW  
Calgary AB T2P 2M5

RE: Land Use Redesignation from R-C2 to R-CG: 2403 - 28 Avenue SW | Lots 57 & 58, Block 19, Plan 4479P

The subject parcel is located in the community of Richmond / Knob Hill and consists of 0.05795 ha of privately owned land. RNDSQL Inc. has retained CivicWorks Planning + Design Inc. to undertake a land use redesignation process to facilitate the construction a four-unit Rowhouse Building with front doors facing 22 Street and 28 Avenue SW. The proposed use is well-suited to the site, given its surrounding context, lot characteristics and location.

In support of the proposed development, this application seeks to amend the existing R-C2 (Residential – Contextual One / Two Dwelling) District to a R-CG (Residential – Grade-Oriented Infill) District. A supporting Minor ARP Amendment to the Richmond ARP will also be required. The site's current R-C2 (Residential – Contextual One / Two Dwelling) District allows for duplex, single and semi-detached dwellings. Secondary suites are permitted and Backyard suites are discretionary, though only on parcels 9m and 13m wide respectively, thus limiting this parcel to two households regardless of configuration.

Like R-C2, the R-CG District is a Low Density Residential District intended for grade oriented development and does not support multi-residential uses. The intent of the R-CG District is to accommodate grade-oriented development in the form of Rowhouse Buildings, Duplex Dwellings, Semi-detached Dwellings and Cottage Housing Clusters, allow Secondary Suites and Backyard Suites with new and existing residential development, provide flexible parcel dimensions and building setbacks that facilitate integration of a diversity of grade-oriented housing over time, and accommodate site and building design that is adaptable to the functional requirements of evolving household needs.

**LOCATION CRITERIA FOR MULTI-RESIDENTIAL INFILL**

The proposed R-CG land use and associated rowhouse development aligns with many of the City of Calgary location criteria for multi-unit infill development in low density residential areas. The following chart is a summary of the preferred conditions that support multi-unit infill development like rowhousing:

CRITERIA	SITE ALIGNMENT
On a corner parcel	> YES - The site occupies a corner parcel.
Within 400m of a transit stop	> YES - The site is ~225m from a transit stop.
Within 600m of an existing or planned primary transit stop	> YES - The site is ~400m from a primary transit stop / planned Southwest BRT stop.
On a collector or higher standard roadway on at least one frontage	> YES - The site fronts onto a local collector road and is less than 225m from 26 Avenue SW.
Adjacent to existing or planned non-residential development or multi-unit development	> NO - However, the site is proximate to a number of recently built semi-detached developments.
Adjacent to or across from an existing or planned open space, park or community amenity	> YES - The site is located ~125m from Richmond school, and ~325 from Richmond / Knob Hill Community Centre.
Along or in close proximity to an existing or planned corridor or activity centre	> YES - The site fronts onto a local collector road and is within 500m (5-10min walk) of 33 Avenue SW Main Street.
Direct lane access	> YES - The site has direct lane access.



POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Project Website | [www.engageRNSQR.com](http://www.engageRNSQR.com)

To ensure that all stakeholders have consistent and easy access to information about future RNSQR projects, the project team has developed and launched a website portal for projects that are in the planning and approvals process. The engageRNSQR website portal includes:

- Information about RNSQR and their vision;
- Project-specific pages that include site location, the proposed development vision and associated land use change;
- Answers to frequently asked questions;
- Opportunities to contact the project team with general or site-specific questions and comments; and
- Contact information for The City File Manager, along with LOC & DP reference numbers (once filed and assigned).



POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Surrounding Area Postcard Drop | ~100 surrounding area neighbours

Paired with on-site signage, hand delivered postcards are delivered to neighbours on surrounding blocks to ensure that adjacent property owners are aware of the proposed land use change.

The postcards outline RND5QR's vision for introducing innovative new housing options to Calgary's inner-city communities and direct interested parties to get in touch with the project team. A corresponding project website includes additional site-specific information and provides answers to commonly asked questions.

**HEY NEIGHBOUR!**  
An exciting change is happening near you!

**We are proposing a land use change at 2403 - 28 Avenue SW to give more Calgarians an opportunity to:**

- FIND NEW INNER-CITY LIVING OPTIONS**  
The inner city is our domain. This is where we connect, where we work, and where we play. It is full of amazing talent and we want to help grow our community by giving more Calgarians the opportunity to see what makes inner city living great! For inner city families looking for something different, we build homes that fit in the missing middle - not typical condos or infills. In everything we do, we remain committed to being good neighbours and working with the community where we build. We look forward to working with you to realize our vision!
- HAVE MORE CHOICES FOR GETTING AROUND**  
Over the last 100 years, cities have been shaped by the needs of vehicles. Planning transportation around the automobile has allowed people to live further away from their everyday destinations and given rise to how most cities look today. We want to create homes for those who dread the thought of the daily commute. We choose to build in walkable neighbourhoods with great access to transit, car-sharing services and cycling amenities. We want to give more Calgarians this option - to live in communities where having or using a vehicle is a choice and not a necessity.
- FIT WHERE YOU LIVE**  
It's where you live that's about finding balance. We all want to get more out of our lives, and for some, that means less commuting and more living. It means appreciating that inner-city life is more - a philosophy that embodies connection and sustainable living. At RND5QR, we focus on living, connecting, and creating spaces where people can make it all fit. We build homes for people who understand the joys of walking around the corner for groceries, a fresh coffee, and enjoying local meals. We build for people who love the vibrant mix of the inner city. We also value quality craftsmanship, and believe we can fit that in too.

Want to know more or let us know what you think?  
Visit [www.engageRND5QR.com](http://www.engageRND5QR.com)

**fit where you live.**

The inner-city is our domain. This is where we connect, where we work, and where we play. It is full of amazing talent and we want to help grow our community by sharing it with you.

We craft inspired, affordable, modern spaces for people who want to spend less time commuting and more time living. We believe you should love where you live, and we are passionate about the details that make each project unique.

CONCLUSION

The proposed land use will facilitate a development that address the "missing middle" - a form of housing that meets the needs of those looking for inner-city housing options that lie somewhere between a traditional condominium and a single-family home or duplex. For the reasons outlined above, we respectfully request that Administration, Calgary Planning Commission and Council support this application.

Should you have any questions, comments or concerns, please contact me at 403.889.4434 or [boris@civicworks.ca](mailto:boris@civicworks.ca).

Sincerely,

Boris Karn, Planner  
B.H.Sc., M.Plan.



POLICY AMENDMENT AND LAND USE AMENDMENT  
 RICHMOND (WARD 8)  
 2403 28 AVENUE SW  
 22 STREET SW AND 28 AVENUE SW

MAP 8C

APPENDIX II

PROPOSED AMENDMENT TO THE RICHMOND  
 AREA REDEVELOPMENT PLAN

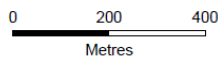
- (a) Delete the existing Map 2 entitled "Land Use Policy" and replace with the revised Map 2 entitled "Land Use Policy", as follows:

Map 2

Land Use Policy

**Legend**

- Study Area Boundary
- Main Street Area Developed Areas Guidebook
- Future Comprehensive Plan
- Neighbourhood - Low Rise
- Neighbourhood Limited
- Community Mid Rise
- Conservation/ Infill
- Low Density Residential
- Medium Density Residential
- High Density
- Open Space
- Institutional
- General Commercial
- Local Commercial
- Restricted Local Commercial
- Pedestrian Bridge
- Transition Policy Applies



POLICY AMENDMENT AND LAND USE AMENDMENT  
 RICHMOND (WARD 8)  
 2403 28 AVENUE SW  
 22 STREET SW AND 28 AVENUE SW

MAP 8C

**APPENDIX III**

**LOCATION CRITERIA FOR MULTI-RESIDENTIAL INFILL**

<b>Criteria</b>	<b>Assessment</b>
Located on a Corner Parcel.	Site is located at the southwest corner of 22 Street SW and 28 Avenue SW
Within 400 metres of a transit stop	Site is approximately 225 metres from the nearest transit stop.
Within 600 metres of an existing or planned primary transit stop or station.	Site is approximately 400 metres from the planned South West Bus Rapid Transit line.
On a collector or higher standard roadway on at least one frontage.	The site is not located along a collector roadway.
Adjacent to existing or planned non-residential development or multi-dwelling development.	The site is not adjacent to planned non-residential or multi-unit development.
Adjacent to or across from existing or planned open space or park or community amenity.	Site is not located across an open space or park. However, the site offers easy access for pedestrians to a green space located 125 metres from the site.
Along or in close proximity to an existing or planned corridor or activity centre.	The 33 <sup>rd</sup> Avenue SW Main Street corridor is approximately 500 metres (5-7 minute walk) from the site.
Direct lane access.	Site has direct lane access.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

APPENDIX IV

COMMUNITY ASSOCIATION LETTER



**Richmond/Knob Hill Community Association**

November 29, 2017

Attention: Mr. Fazeel Elahi  
Planning & Development  
City of Calgary  
PO Box 2100 Station M  
Calgary AB T2C 1M2

Re: Community Association Comments – LOC2017-0325 – 2403 28 AV SW

We understand that you are the File Manager for the captioned application to change the land use designation of a corner parcel located at 2403 28 Avenue SW (the "Subject Parcel") from R-C2 Residential - Contextual One/Two Dwelling to R-CG Residential - Grade-Oriented Infill, to allow for a 4-unit row house development to be constructed thereon (the "Application"). The Development Committee for the Richmond/Knob Hill Community Association (the "Association") has reviewed the Application and advises that it opposes the Application for the following reasons:

- 1) Although Richmond/Knob Hill ("RKH") falls within the Developed Residential Area – Inner City, being an area in which the Municipal Development Plan ("MDP") generally supports moderate intensification that respects the community context and contributes to a greater variety of housing types overall, and encourages higher residential densities in areas that are well serviced by existing infrastructure, public amenities and transit, it should be noted that the MDP also provides that such intensification is to take place in accordance with Local Area Plans established through community planning processes. In this regard we refer you to:
  - a) MDP Section 2.3.1.b.iii -- Promote a broader range of housing choice for all ages, income groups, family types and lifestyles by including supportive land use policies and development strategies in the Implementation Guidebooks and/or in Local Area Plans that encourage the provision of a broader range of housing affordable to all income levels (emphasis added);
  - b) MDP Section 2.3.1.d -- Promote methods to efficiently use or adapt the city's existing housing stock to enable changing households to remain in the same home or neighbourhood for many years. Strategies may include allowing accessory units in low-density areas and other methods determined through community planning processes (emphasis added); and

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

- 2 -

- c) MDP Section 2.3.2.d -- Ensure that the preparation of Local Area Plans includes community engagement early in the decision making process that identifies and addresses local character, community needs and appropriate development transitions with existing neighbourhoods.
- 2) The Local Area Plan applicable to the Subject Parcel is the Richmond Area Redevelopment Plan (the "Richmond ARP"), which provides for the following residential land use policies:
  - a) Conservation and Infill, which allows for single detached dwelling, semi-detached dwelling and duplex developments;
  - b) Low Density, which allows multi-dwelling infill developments, such as townhouses and stacked townhouses, not exceeding 75 units per hectare;
  - c) Medium Density, which allows townhouse, stacked townhouse and apartment developments not exceeding 210 units per hectare; and
  - d) High Density, which includes apartment developments not exceeding 321 units per hectare.

As the Application seeks a redesignation from R-C2 to R-CG to allow the construction of a 4-unit row house development, to be supported by the Richmond ARP the Subject Parcel would need to be located in an area designated as Low Density or Medium Density. However, the Subject Parcel is located in an area designated as Conservation and Infill (see attached ARP map – Attachment 1), and the Application is therefore not supported by the Richmond ARP.

As the Richmond ARP dates back to 1986, and has not been materially updated since then, consideration should be given to whether its designation of the Subject Parcel as Conservation and Infill is still appropriate, or whether a different designation would better address "local character, community needs and appropriate development transitions within" RKH.

- 3) The "Location Criteria for Multi-Residential Infill" implemented by City Council in 2014 set out certain criteria for assessing the appropriateness of applications seeking redesignation to allow multi-residential infill development in a low density area. Those criteria, and their application to the Subject Parcel, are as follows:

Is the Subject Parcel:

- a) A corner parcel – YES, the Subject Parcel is a corner parcel;
- b) Within 400m of a transit stop – YES, the Subject Parcel is located within 400m of transit stops on 26 Avenue SW and Crowchild Trail S;
  - i) In this regard, with transit routes on 17 Avenue SW, 26 Avenue SW, Crowchild Trail S and portions of 33 Avenue SW, it should be noted that there are transit stops within 400m of every low density residential parcel ("LDR Parcel") in RKH (see attached map – Attachment 2;

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

- 3 -

- c) Within 600m of a transit stop on the Primary Transit Network – YES, the Subject Parcel is within 600m of transit stops on Crowchild Trail S, which is part of the Primary Transit Network;
- i) In this regard, as 17 Avenue SW, 33 Avenue SW and Crowchild Trail S are all part of the Primary Transit Network, it should be noted that there are Primary Transit Network transit stops within 600m of every LDR Parcel in RKH (see attached map – Attachment 3);
- d) On a collector or higher standard roadway on at least one frontage – NO, the Subject Parcel is not located on a collector or higher standard roadway;
- i) The Subject Parcel has frontages on 22 Street SW and 28 Avenue SW, neither of which is classified as a collector or higher standard roadway (see attached map – Attachment 4), and the City has even taken steps to reduce traffic on 22 Street SW by installing a NB restrictor at 33 Avenue SW;
- ii) The only roads within or bordering onto RKH that are classified as collectors are:
- (1) 19 Street SW north of 19 Avenue SW;
  - (2) 19 Avenue SW (between 24 Street SW and 25A Street SW);
  - (3) 19/20 Avenue SW (between 19 Street SW and Richmond Road SW)
  - (4) 20 Street SW (south of 26 Avenue SW);
  - (5) 24 Street SW (between 17 Avenue SW and Crowchild Trail);
  - (6) 25 Street SW (between 26 Avenue SW and Richmond Road SW);
  - (7) 26 Avenue SW; and
  - (8) Richmond Road SW;
- e) Adjacent to existing or planned non-residential development or multi-unit development – NO, the Subject Site is surrounded by other R-C2 parcels and developments which are consistent with that designation;
- f) Adjacent to or across from an existing or planned open space, park or community amenity – NO, the Subject Site is surrounded by other R-C2 parcels and developments which are consistent with that designation;
- g) Along or in close proximity to an existing or planned corridor or activity centre – NO, the Subject Site is surrounded by other R-C2 parcels and developments which are consistent with that designation, and is 5 blocks away from the nearest neighbourhood corridor, being 33 Avenue SW; and
- h) Served by direct lane access – YES, the Subject Parcel is served by direct lane access
- i) In this regard, as RKH is a laned community, it should be noted that virtually all of its LDR Parcels are served by direct lane access.

The Subject Parcel therefore satisfies only 4 of the 8 criteria, as would virtually every other corner LDR Parcel in RKH, and 3 of those 4 “satisfied” criteria would be satisfied by virtually every LDR Parcel in RKH. Accordingly, if these results are considered sufficient to conclude that the Subject Parcel is an appropriate location for multi-residential infill development, and therefore that its designation under the Richmond ARP should be upgraded from

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

- 4 -

Conservation and Infill to Low Density, then that would suggest that every corner LDR Parcel in RKH, and possibly every LDR Parcel in RKH, should be similarly upgraded. If a change is to be made to the Richmond ARP that has the potential to have ramifications of that magnitude, then it should only be done through a comprehensive community engagement process that determines not only the need for such a change, but also widespread community support for such a change, as was recently done to update the Area Redevelopment Plans for the NW communities of Banff Trail and Capitol Hill.

- 4) It is not apparent to the Association that there is either a need to open up RKH's non-collector LDR Parcels, whether corner or interior, to multi-residential infill development, nor widespread community support for such a change. With respect to the issue of need, RKH's population stopped declining back in 1988 and since then has grown by 31%, and is expected to surpass its previous 1968 peak within the next year or two (see attached chart – Attachment 5). Over the same period the number of residential units in RKH has increased by 33%. Much of this increase in population and number of units has taken place within the R-C2 portions of RKH's Conservation and Infill areas, where older wide-lot bungalows have been subdivided and replaced with 2 narrow lot detached or semi-detached infills. Opportunities exist for significantly more population and unit count growth:
- a) Within the Richmond ARP's existing Conservation and Infill areas, as to date only around half of RKH's R-C2 parcels have been redeveloped;
  - b) Within the Richmond ARP's existing Low Density and Medium Density areas, which to date have also only been partially redeveloped;
  - c) Along and in the vicinity of RKH's two "Main Streets", being:
    - i) 17 Avenue SW, the westmost portion of which recently underwent a major Main Streets community engagement process that culminated in City-initiated upzoning, including of R-C2 parcels to R-CG, and amendments to that portion of the Richmond ARP – we anticipate a similar process being initiated for RKH's remaining portion of 17 Avenue SW in the near future; and
    - ii) 33 Avenue SW, for which a new "activity centre" Area Redevelopment Plan was enacted in 2014 and a Main Streets community engagement process is just getting underway, and where over 200 new apartment-style units are either currently under construction or in the planning approval process;
- In this regard, and in anticipation of the City-initiated upzoning that is likely to result from the 33 Avenue SW Main Streets community engagement process, the Association recently responded favourably to an R-C2 to R-CG upzoning application for the corner parcel at 2403 32 Avenue SW, which backs onto a Marda Loop business district property on the north side of 33 Avenue SW (LOC2017-0238);
- d) Along RKH's collector roads, where to date the Association has responded favourably to all applications to upzone LDR Parcels, including:
    - i) in 2014, applications to upzone:

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

- 5 -

- (1) each of the interior parcel at 2235 26 Avenue SW and the adjacent corner parcel at 2239 26 Avenue SW (both of which are located along the 26 Avenue SW collector road) from R-C2 to M-CG (LOC2014-0096); and
  - (2) the corner parcel at 2104 Richmond Road SW (located at the intersection of the 20 Avenue SW and Richmond Road SW collector roads) from R-C2 to R-CG (LOC2014-0154);
- ii) in 2015, applications to upzone:
- (1) the corner parcel at 2840 25A Street SW (located along the Richmond Road SW collector road) from DC (based on R-2) to M-CG (building height modifier was requested -- LOC2015-0166); and
  - (2) the interior parcel at 2220 26 Avenue SW (located along the 26 Avenue SW collector road) from R-C2 to M-CG (LOC2015-0182); and
- iii) In 2017, applications to upzone:
- (1) the corner parcel at 2803 25 Street SW (located along the 26 Avenue SW collector road) from DC (based on R-2) to R-CG (LOC2017-0125);
  - (2) the corner parcel at 2804 25A Street SW (located along the 26 Avenue SW collector road) from DC (based on R-2) to R-CG (LOC2017-0172); and
  - (3) the interior parcel at 2224 26 Avenue SW (located along the 26 Avenue SW collector road) from R-C2 to M-CG (LOC2017-0252).

In this regard it should be noted that on several occasions the Association has also taken the position that a DP application for a low density development on an R-C2 parcel located along one of RKH's collectors should be denied on the basis that a higher density development would be more appropriate in that location, in one case even going so far as to include that argument in an appeal of an approved DP to the Subdivision and Development Appeal Board (SDAB2017-0017).

- e) On the Viscount Bennett school site, which the Association understands is likely to become available for redevelopment in the near future.

Based on the above, there would appear to be plenty of opportunities for both further population growth and increased "missing middle" development within RKH without having to open up its non-collector LDR Parcels, whether corner or interior, to multi-residential infill development.

- 5) With respect to the issue of community support, based on the feedback that the Association has received from RKH residents over the last few years, including through:
- a) a 2010 community engagement process as part of the development of the Association's Residential Development Design Guidelines, which referenced a willingness to consider upzoning applications for parcels along RKH's collectors/corridors;
  - b) a 2014 Winterfest community engagement event;
  - c) a 2015 Community Visioning project in which the Association partnered with the Federation of Calgary Communities, the University of Calgary Urban Studies group and the Killarney Glengarry Community Association; and

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

- 6 -

d) regular interaction with RKH residents both generally and in relation to specific development proposals,

it has become quite clear to the Association that there is reasonably widespread support among RKH residents for further intensification, but only to the extent either contemplated by the Richmond ARP or located along our collectors and Main Streets. We have not detected any groundswell of support for having our non-collector LDR Parcels, whether corner or interior, opened up to multi-residential infill development.

Based on the above, the Association requests that the Application be denied, and that the applicant be encouraged to redevelop the Subject Parcel in accordance with its existing R-C2 land use designation.

Thank you.

Richmond/Knob Hill Community Association  
c/o 2126 28 Avenue SW  
Calgary AB T2T 1K5  
development@richmondknobhill.ca

  
Dennis Cant  
Director, Development

  
Doug Roberts  
Development Committee Chair

Attachments (5)

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

APPENDIX V

SUMMARY OF APPLICANT-LED COMMUNITY ENGAGEMENT

COMMUNITY ENGAGEMENT

RNDSQR and the project team have designed and undertaken a comprehensive community engagement process in support of this application and others like it to ensure a clear and transparent application process for all stakeholders. Our process is focused on informative and fact-based engagement and communications. The project team is committed to working with our neighbours throughout the application process. Key elements of our engagement strategy include:

On-site Signage | To be installed on-site at time of submission

To supplement the usual City of Calgary notice signage that is associated with Land Use Redesignation and Development Permit Applications, RNDSQR and the project team have designed and custom built on-site signage that notifies neighbours and surrounding community members of a proposed land use change.

The signage outlines RNDSQR's vision for introducing innovative new housing options to Calgary's inner-city communities and directs interested parties to get in touch with the project team. A corresponding project website includes additional site-specific project information and provides answers to commonly asked questions about the proposed land use district and development outcome.



SIGNAGE INSTALLED ON-SITE DURING R-CG APPLICATION PROCESS

**HEY NEIGHBOUR!**  
An exciting change is happening here!

**We are proposing a land use change here to give more Calgarians an opportunity to:**

**FIND NEW INNER-CITY LIVING OPTIONS**  
The inner city is our domain. This is where we connect, where we work, and where we play. It is full of amazing talent and we want to help grow our community by sharing it with you.  
We want to give more Calgarians the opportunity to see what makes inner city living great! For inner city families looking for something different, we build homes that fit in the missing middle - in other words, not typical condos or townhomes.  
In everything we do, we remain committed to being good neighbours and working with the communities where we build. We look forward to working with you to realize our vision!

**HAVE MORE CHOICES FOR GETTING AROUND**  
For over 100 years, cities have been shaped by the needs of vehicles. Planning transportation around the automobile has allowed people to live further away from their everyday destinations and has given rise to most cities look today.  
We want to create homes for those who dread the thought of the daily commute. We choose to build in walkable neighborhoods, with great access to transit, car sharing services and cycling amenities making car ownership optional for more and more people living in the inner city. We want to give more Calgarians this option - to live in communities where having a vehicle is a choice and not a necessity.

**FIT WHERE YOU LIVE**  
"It's where you live" is about finding balance. We all want to get more out of our lives, and for some, that means less commuting and more living. It means appreciating that sometimes less is more - a philosophy that embodies conscious and sustainable living. At RNDSQR, we focus on living, connecting, and creating spaces where people can make it all fit.  
We build homes for people who understand the joys of walking around the corner for groceries, a fresh coffee, and amazing local meals. We build for people who love the vibrant mix of the inner city. And we also value quality craftsmanship, and believe we can fit that in too.

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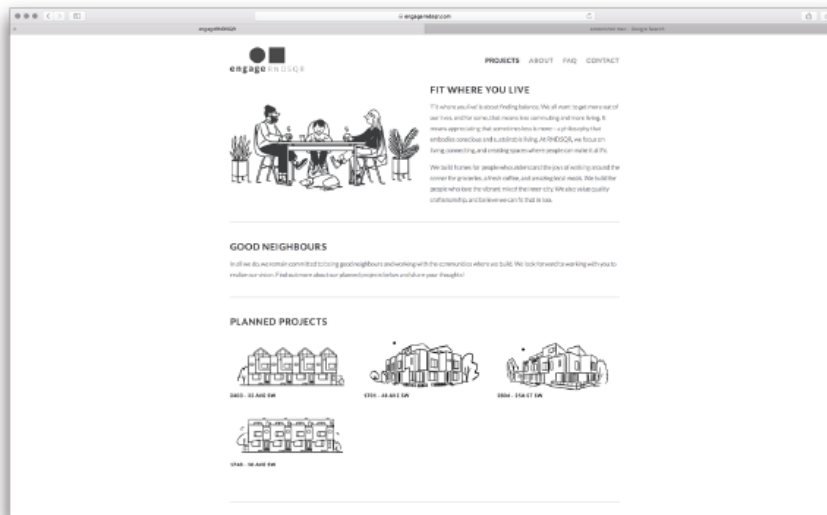
POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

Project Website | [www.engageRNDSSQR.com](http://www.engageRNDSSQR.com)

To ensure that all stakeholders have consistent and easy access to information about future RNDSSQR projects, the project team has developed and launched a website portal for projects that are in the planning and approvals process. The engageRNDSSQR website portal includes:

- Information about RNDSSQR and their vision;
- Project-specific pages that include site location, the proposed development vision and associated land use change;
- Answers to frequently asked questions;
- Opportunities to contact the project team with general or site-specific questions and comments; and
- Contact information for The City File Manager, along with LOC & DP reference numbers (once filed and assigned).



POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

Surrounding Area Postcard Drop | ~100 surrounding area neighbours

Paired with on-site signage, hand delivered postcards are delivered to neighbours on surrounding blocks to ensure that adjacent property owners are aware of the proposed land use change.

The postcards outline RNDSSQR's vision for introducing innovative new housing options to Calgary's inner-city communities and direct interested parties to get in touch with the project team. A corresponding project website includes additional site-specific information and provides answers to commonly asked questions.



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POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

APPENDIX VI

APPLICANT PREPARED RESPONSE TO COMMUNITY OPPOSITION



**PLANNING + DESIGN RATIONALE**

**2403 28 Avenue SW**

R-CG Land Use Redesignation (LOC2017-0325)

Development Permit (DP2017-5627)

JANUARY  
2018

RND SQR.ca

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



202 1680 40 Ave SW OFFICE  
Calgary, AB T2T 6T8  
403 444 9000 PHONE

**RE:**

Planning + Design Rationale (LOC2017-0325):

City of Calgary CPAG Detailed Team Review #1 Comments & Richmond / Knob Hill Community Association Correspondence

**TO:**

Email Delivery:

Mr. Fazeel Elahi | City of Calgary File Manager  
Councillor Evan Woolley | Ward 8 Councillor  
Mr. Dennis Cant | Richmond / Knob Hill Community Association, Director - Development  
Mr. Doug Roberts | Richmond / Knob Hill Community Association, Development Committee Chair

JANUARY 2018

In light of the comments and correspondence received by Administration regarding the appropriateness of the proposed R-CG land use redesignation at 2403 28 Avenue SW (LOC2017-0325), the project team has prepared the below responses, key insights and commentary on the suitability of the R-CG District in developed inner-city communities like Richmond / Knob Hill.

This document aims to address both application-specific concerns, as well as those generally heard about the implementation of the new R-CG District in established inner-city communities where Local Area Plans are no longer aligned with current planning best practices and broader city-wide policies and Bylaws.

**ABOUT RNDSQR**

RNDSQR is an active, experienced and well-regarded inner-city builder/ developer with multiple holdings and emerging applications in many of Calgary's inner-city communities. RNDSQR is committed to implementing and building on the success of the City's new R-CG District by delivering high-quality, innovative housing options for Calgarians looking for the "missing middle" - affordable and flexible inner-city homes that lie somewhere between a traditional condominium and a single-family home or duplex. We build homes for people who understand the joys of walking around the corner for groceries, a fresh coffee, and amazing local meals. We build for people who love the vibrant mix of the inner-city. We choose to build in walkable inner-city neighbourhoods, with great access to transit, car sharing services and cycling amenities. We want to give more Calgarians this option - to live in communities where having or using a vehicle is a choice and not a necessity. We also value quality craftsmanship, and believe we can fit that in too.

**MEETING MDP GOALS: ABOUT THE RESIDENTIAL - GRADE-ORIENTED INFILL DISTRICT (R-CG)**

In order to support greater housing choice and reinforce more complete and resilient residential neighbourhoods, the *Municipal Development Plan (MDP)* identifies ground-oriented housing as a key component of complete communities (Policy 2.3.1[a]) and encourages growth and change in low density residential neighbourhoods through the addition of a diverse mix of ground-oriented housing options (Policy 2.2.5[a]). In response to this city-wide policy direction, Administration worked with a wide range of City, community and industry stakeholders to develop and implement the Residential - Grade-Oriented Infill District (R-CG) District.

The R-CG District was specifically designed to facilitate a wide range of low density ground-oriented housing, including rowhouses, semi-detached dwellings, single-family dwellings and secondary suites within Calgary's developed areas. The rules and regulations of the R-CG District are responsive to typical infill conditions and were designed to support the sensitive integration of new and innovative site configurations within existing low density neighbourhoods and facilitate redevelopment that was adaptable to the functional requirements of evolving household needs. The R-CG District also provides communities with more certainty at the Land Use Redesignation stage of redevelopment, since the District rules feature contextual considerations and limit the form and scale of housing, relative to Multi-Residential Districts.

The new District received praise from members of Calgary Planning Commission and was carried unanimously. Following Calgary Planning Commission's recommendation, Council unanimously adopted the District into the Land Use Bylaw in September of 2014.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Planning + Design Rationale (LOC2017-0325)

**ENGAGEMENT AND COMMUNITY ASSOCIATION SUPPORT FOR THE R-CG DISTRICT**

In developing the R-CG District, Administration undertook a community engagement process that included meetings held with numerous stakeholders, including Community Associations. Representatives from the following Community Associations attended one or more of these City-hosted meetings: Bankview, Capitol Hill, Chinook Park, Kelvin Grove / Eagle Ridge, Renfrew, Richmond / Knob Hill, Rosedale and Triwood,

Of note, and specific to the community context of the subject application, the Richmond / Knob Hill Community Association's Development Committee submitted a letter of general support for the R-CG District, dated May 25, 2014 (See Appendix 1). The letter begins optimistically, noting the real potential of the R-CG District to have a concrete positive impact on the diversity and availability of housing in the community:

*"The RKHCA remains generally supportive of the R-CG Amendments, as they have the potential to create more diversity in housing options for established communities such as Richmond/Knob Hill. Richmond/Knob Hill was originally developed in the 1950s as a bungalow community and its parcels are primarily R-C2, with some R-C1 parcels and even fewer multi-residential parcels. We estimate that our community is approximately 50% redeveloped, and to date that redevelopment has consisted almost exclusively of older bungalows on R-C2 parcels being demolished and replaced with a pair of 2-storey or 3-storey detached or semi-detached dwelling units. Our concern is that if this development pattern continues Richmond/Knob Hill will end up being a slightly denser but still excessively homogeneous residential community, with too little demographic or economic diversity (ie. fewer rental, starter or senior-friendly housing options). Our window of opportunity to "change our stars" and avoid this result is closing rapidly."*

**RICHMOND / KNOB HILL COMMUNITY ASSOCIATION DEVELOPMENT COMMITTEE CORRESPONDENCE**

Contrary to the previously expressed optimism about the positive potential of the R-CG District, the Richmond / Knob Hill Community Association (RKHCA) submitted correspondence dated November 29, 2017 (Appendix 2) to Administration expressing their concern with the subject application. The main thrust of their opposition focused on the following key topic areas:

- Alignment with the Local Area Plan -Richmond Area Redevelopment Plan (1986)
- Location Criteria for Multi-Residential Infill
- Implications of Secondary Suites

With respect to the above areas of concern, RNDSQR and the project team offers the following responses:

**Alignment with the Local Area Plan - Richmond Area Redevelopment Plan (1986):** As the RKHCA's November 29, 2017 letter (Appendix 2) notes, the subject application seeks to redesignate the subject corner parcel (2403 28 Avenue SW) from R-C2 to R-CG to facilitate the construction of a four unit rowhome. To ensure policy alignment, the project team is also seeking a minor amendment to the *Richmond Area Redevelopment Plan (ARP)*, to change the subject parcel's Policy Area from Conservation / Infill to Low Density.

It is critically important to note that the RKHCA's November 29, 2017 letter (Appendix 2) regarding the subject application states: *"As the Richmond ARP dates back to 1986, and has not been materially updated since then, consideration should be given to whether its designation of the Subject Parcel as Conservation / Infill is still appropriate, or whether a different designation would better address "local character, community need and appropriate development transition within RKH (sic Richmond Knob Hill)."*

This observation highlights a key issue with the relevance of outdated inner-city Local Area Plans like the 32-year old *Richmond ARP* – a Plan with a stated shelf life of 10-15 years, based on the planning goals and principles of the 1979's *Inner City Plan*. Unsurprisingly, Calgary's (and arguably much of North America's) planning realities, policies and goals have drastically changed since these plans were first developed and written. Today, a much greater priority is placed on building complete and resilient communities that make sustainable and efficient use of limited resources like land, energy, infrastructure, services and municipal funding capital. These contemporary realities and their implications are directly reflected in the city-wide policies of the *Municipal Development Plan (MDP)*, the *Calgary Transportation Plan (CTP)* and associated Implementation Plans like the *Developed Areas Guidebook*. Together, these city-wide plans actively facilitate and encourage the development of more innovative and affordable housing options in established communities; more efficient use of services and infrastructure; and more compact built forms in locations with direct and easy access to transit, shopping, schools, amenities, open spaces and other community services.

The goals, policies and priorities of these contemporary city-wide plans are directly reflected in the R-CG District – specifically designed and developed by Administration and eventually adopted by Council to allow more innovative and affordable housing options to reside among and next to other forms of low density residential development within Calgary's developed areas. Given its allowable forms and context sensitive rules, the *Land Use Bylaw* does not consider the R-CG District to be a Multi-Residential use, grouping and categorizing it among other Low Density Residential Districts like R-C1 and R-C2.

This categorization is further reflected by the policies and directions of the *Developed Areas Guidebook (DAG)*, which:

- Translates *MDP* objectives into community level policy.
- Provides general policy to shape more complete communities that are well connected and supported by amenities and services that meet the daily needs of all people.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C

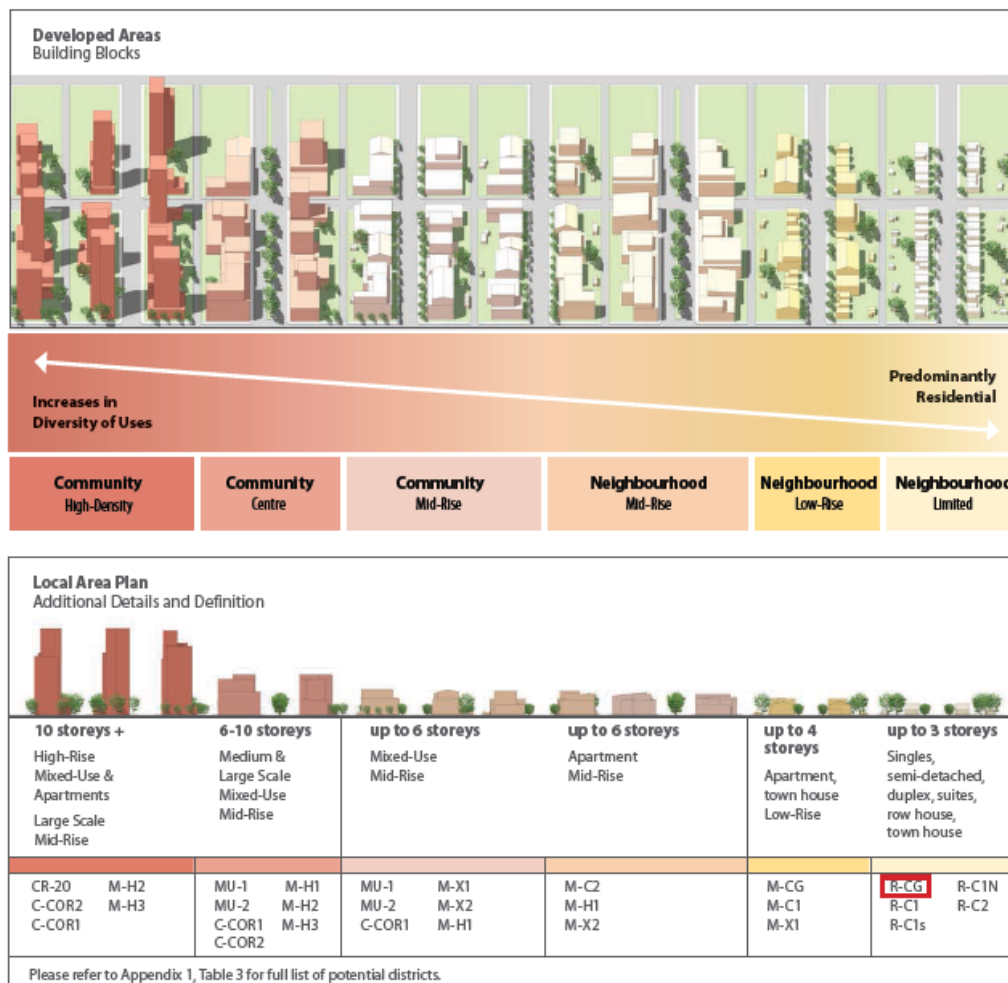


Planning + Design Rationale (LOC2017-0325)

- Provides a common community framework, consisting of land use, urban form and policy direction, for how the Developed Areas are planned and developed today and into the future.
- Establishes a consistent approach for undertaking a Local Area Plan or significant Local Area Plan amendments and implementing the *Land Use Bylaw*.

The DAG provides the following illustrative policy 'Building Blocks' for established communities (Fig.1) – each associated with particular forms and building heights that are typical of specific land use districts. Note that the R-CG District is categorized as 'Neighbourhood Limited' use, among other low density districts like R-C1, R-C1s, R-C1N, and R-C2.

FIG.1 DEVELOPED AREAS: BUILDING BLOCKS  
Source: Developed Areas Guidebook (2017)



POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Planning + Design Rationale (LOC2017-0325)

Ultimately, the fundamental misalignment of *Richmond ARP* policy with the R-CG District lies with the ARP's most predominant Residential Policy Area: Conservation / Infill. This Policy Area is directly informed by the goals and policies of the patently outdated *Inner City Plan (1979)* – written in a time of very different planning priorities, values and principles. This is stated directly in the Policy Area purpose statement: “*The conservation policy of the Inner City Plan is reaffirmed through a conservation and infill policy, the intent of which is to improve existing neighbourhood quality and character while permitting low profile infill development that is compatible with surrounding dwellings.*”

Interestingly, the above statement is very similar to the purpose and intent of the Low Density Policy Area, which reads: “*A low density policy is intended to improve existing neighbourhood residential quality and character, as described in the conservation and infill policy, while providing for low profile family-oriented redevelopment. Acceptable redevelopment under the RM-2 district would include single and two-family dwellings and multi-dwelling infill projects comprised of townhousing or stacked townhousing. Maximum density should not exceed 75 units per hectare (30 units per acre).*”

Despite its age, the language and allowable forms/densities of the Low Density Policy Area are far better aligned with current city-wide policy and allow for the sensitive addition of the new and innovative ground-oriented housing options contemplated by the MDP and DAG. Yet today, the outdated Conservation / Infill Policy Area accounts ~165 acres, or ~8.7% of the total community area zoned for residential uses in Richmond / Knob Hill. By contrast, the far more flexible and relevant Low Density Policy Area accounts for a mere 4.7 acres, or ~2%. Any future evaluation of the *Richmond ARP* and other outdated Local Area Plans should seriously consider the practical policy implications of this imbalance, along with relevance of 1970s-era conservation policy.

As noted above, the proposed development vision, land use redesignation and supporting minor amendment to the *Richmond ARP* are directly aligned with both current city-wide policy and planning best principles. The proposed change also meets the spirit and intent of the *Richmond ARP*, whose residential land use policies seek to improve existing neighbourhood quality and character, provide family-oriented housing options, and allow compatible and sensitive infill development.

**Location Criteria for Multi-Residential Infill:** In evaluating Land Use Redesignation applications, Administration currently draws on a number location criteria that highlight preferred conditions to support land use amendments in low density residential areas. The *Location Criteria for Multi-Residential Infill* were initially developed and adopted in 2014 to support the Land Use Redesignation application review process for the following Multi-Residential Districts (see Appendix 3):

- Multi-residential – Contextual Grade-Oriented (M-CG) District
- Multi-residential – Contextual Low Profile (M-C1) District
- Multi-residential – Contextual Medium Profile (M-C2) District

As a result of a Motion Arising and subsequent Council direction, Administration would go on to further expand the *Criteria* to include more Districts (including R-CG), be more graphically illustrative, and include supporting rationale and references to MDP policy. As part of this process, Administration consulted with a number of stakeholders, including members of Calgary Planning Commission (CPC). It is important to note that even at that time, members of CPC expressed their concern that the guiding criteria “would become the rule and that community members would use the checklist to add up the checks and refuse an application.” The comments outlined in the RKHCA correspondence demonstrate the plausibility of this concern. Given that many inner-city corner lots meet some of the *Location Criteria*, community members feel that “truly appropriate” sites should meet most, if not all of the criteria, regardless of proposed use. This stems from a concern that otherwise, approvals could happen in an indiscriminate way, without due process or the consideration of good planning principles by Administration and the Approving Authority.

The *Criteria* are not (and have never been) intended to be treated as a set of requirements, checklist or scorecard for appropriateness, but rather as an additional tool for Administration to highlight some of the preferred site characteristics that may make a site more suitable for redevelopment. There are no essential *Criteria*, nor is there a specific “appropriateness threshold” for the number of *Criteria* that a site can or must meet. As stated by the *Location Criteria* purpose statement: “*These criteria are meant to be used as a guideline and are to be used in the review and evaluation of land use amendment applications that support multi-residential, rowhouse and cottage housing cluster developments. These criteria are not meant to be applied in an absolute sense to determine whether or not a site should be recommended for approval. In general, the more criteria an application can meet, the more appropriate the site may be considered for multi-residential infill development. In some cases, there may be applications that are appropriate but meet only a few criteria, or may meet multiple criteria but are determined not to be appropriate. These will need to be considered based on the scale and type of development proposed in relation to the local context.*”

A central concern raised by the RKHCA in their correspondence was that the site did not front onto a Collector class road, implying that this was the one key criteria that would make the site appropriate for an R-CG redesignation. Given the modest scale of development proposed – 4 units with 4 private garages and no secondary suites – the resultant traffic generation and parking demand is likely to be minimal. Whether built on a local or collector class road, the distinctly residential form and appearance created by the R-CG District’s steady rhythm of doors and porches adds visual interest to the neighbourhood streetscape, calming local traffic and enhancing pedestrian safety and experience along adjacent sidewalks. Meanwhile, location along a Collector or higher order road may be especially well-suited to higher intensity Multi-residential districts like M-CG, M-C1 and M-C2, which have a higher order of traffic generation and parking demand.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Planning + Design Rationale (LOC2017-0325)

Given the breadth of site characteristics covered in the *Criteria*, meeting many or all of the criteria may actually indicate to Administration that a site is especially well-suited for more intense forms of redevelopment – forms typically seen in the Multi-Residential of Mixed Use class of land use districts. Given the modest scale and intensity of change, the R-CG District does not need to meet all of the *Criteria* to be able to sensitively integrate with the existing fabric of low density communities and still achieve the policy goals and directions of the MDP. In the case of 2403 28 Avenue SW, the subject lands provide an opportunity to add innovative and affordable inner-city housing options for Calgarians in a location that benefits from:

**Corner Location:** The subject site occupies a corner parcel, allowing the proposed development to contribute to the neighbourhood streetscape and reflect existing residential design patterns, with units facing both 28 Avenue and 22 Street SW. Grade-oriented entrances on both streets and building façade articulation provide the appearance of distinct houses, adding to the residential appearance of the street, calming traffic and enhancing pedestrian safety and experience along adjacent sidewalks.

**Direct Lane Access:** The subject site has direct lane access, facilitating a development that orients vehicle access to the rear lane, creating an uninterrupted, pedestrian-friendly streetscape interface along 28 Avenue and 22 Street SW. Sites with direct lane access minimize the impact of vehicles on adjacent streets and sidewalks, increase parking options with limited impact on the existing neighbourhood, and provide safer, more pedestrian-oriented streetscapes with fewer driveways crossing sidewalks.

**Proximity To Primary & Local Transit:** The subject site is ~225m from a Local Transit Stop (Route 6), and ~400m from a Primary Transit Stop (Routes 20 & 112), situated along the Crowchild Trail Primary Transit Network corridor. The Primary Transit Network provide communities with daily reliable public transit service, with a frequency of every 10 minutes or less, 15 hours a day, 7 days a week.

**Proximity To An Existing Open Space, Park or Community Amenity:** The subject site allows future residents direct and easy access to a variety of community resources. The site is only ~65m from an integrated community destination that includes a large park and open space, the Richmond School, the Richmond / Knob Hill Community Association Hall and an outdoor rink.

**Proximity to a Main Street:** The subject site is ~500m (5-10min walk) from 33 Avenue SW, one of the City-identified Neighbourhood Main Streets – active corridors that attract Calgarians to socialize, work, live, shop, dine, and celebrate. These streets also act as important transportation routes for those who walk, cycle, drive or use transit. City-wide policy encourages both population and job growth around these key corridors.

**Implications of Secondary Suites:** A critical area of concern for the Richmond / Knob Hill Community Association and surrounding area residents is the uncertainty posed by the inclusion of basement and accessory suites in the permitted uses of the R-CG District. This concern was also heard during the engagement process that informed the development of the District, and continues to be a common theme heard from community members who comment on proposed R-CG land use redesignations. RND SQR and the project team acknowledge that the uncertainty surrounding secondary suites is a credible concern, since it appears to allow for the proposed unit count to effectively double.

In considering this common concern, it is important to note that to meet building and fire code requirements, secondary suites must be specifically considered and integrated into the architectural design of a rowhome. The inclusion of secondary suites in rowhome buildings requires the inclusion of additional fire prevention measures and fire rated party walls. These requirements cannot be retrofitted into a rowhome building after it has been constructed. This means that a proposed rowhome development with four dwelling units cannot accommodate the future addition of secondary suites. Retrofitting a secondary suite into an existing rowhome is both technically impracticable and cost-prohibitive.

RND SQR is committed to a transparent and information focused engagement approach that leads to bricks and mortar outcomes. As part of this approach, the team pursues Development Permit applications in step with Land Use Redesignation applications to demonstrate both their development vision and intent. This also ensures that stakeholders have a chance see why a Land Use Redesignation is being sought and the detailed design being proposed.

The proposed development vision does not include any basement or secondary suites, nor does it include the necessary fire prevention/mitigation measures that would allow for the development of suites in the future. This is demonstrated in the detailed design drawings of the associated Development Permit application (DP2017-5627).

#### COMMON COMMUNITY QUESTIONS & CONCERNS

As part of the Land Use Redesignation application review process, Administration actively informs surrounding area residents of an application having been made and accepts comments from interested citizens and surrounding area neighbours. The File Manager for the subject application received a total of fourteen (14) letters from citizens and neighbours expressing their comments and concerns. The correspondence may have been further encouraged by an anonymous letter distributed by an unknown party to surrounding area residents (Appendix 4). The below provides a response to the key community questions, comments and concerns:

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Planning + Design Rationale (LOC2017-0325)

**Application & Approvals Process:** RND SQR is committed to a transparent and information focused engagement approach that leads to bricks and mortar outcomes. As part of this approach, the team pursues Development Permit applications in step with Land Use applications to demonstrate both their development vision and intent. This also ensures that stakeholders have a chance see why a Land Use Redesignation is being sought and the detailed design being proposed. Appendix 5 provides a photo-realistic visualization of the proposed design, as outlined by the project team's Development Permit application (DP2017-5627).

RND SQR and the project team place great importance on being a good neighbour and working with surrounding community members and stakeholders throughout the application process. As part of that commitment, RND SQR and the project team have designed and undertaken a comprehensive community engagement process in support of this application and others like it to ensure a clear and transparent application process for all stakeholders. Stakeholders like the Community Association and Ward Councillor's office are actively invited to participate in our process, which focuses on informative and fact-based engagement and communications.

**Deviation from the Local Area Plan:** Please refer to the previously provided commentary on the challenges of older Local Area Plans like the *Richmond Area Redevelopment Plan (ARP)*.

**Height & Shadow Concerns:** The rules of the R-CG District have been designed to be sensitive to surrounding area context and adjacent landowners. Height rules for Rowhouse buildings allow up to an 11m height in the middle of a building, but maintain contextual height rules for parts of the building directly adjacent to other low density districts. Regardless of building form, the R-CG District limits the maximum height within 4.2 metres of the adjacent property line to 8.6 metres, similarly to other Contextual Districts that have rules governing the relationship to adjacent properties. Furthermore, any floor above 8.6 metres must be reduced by 25% to prevent the building from being large and imposing. As a result, the maximum 11m height for a rowhome is typically only achieved at a building's roof peak. District rules for building coverage and amenity space also ensure that appropriate yard space is still provided.

Prior to the rules of the R-CG District coming into effect, rowhouse forms built in Calgary tended to be large and blocky, having been built under the rules of the M-C1 District. The R-CG District was created specifically to prevent such uniform massing and be contextually similar to other 2 and 3-storey forms allowed by low density districts like R-C2.

**Privacy:** R-CG rules dictate that rooftop amenity spaces located on the top storey must be oriented towards the street, in order to minimize potential overlooking and privacy concerns. All outdoor amenity spaces and the glazing of all units has been designed, sized and oriented to minimize potential overlooking and privacy concerns. When compared with allowable R-C2 semi-detached building forms, R-CG built forms do not result in additional overlooking or material compromises in privacy for adjacent landowners.

**Setbacks:** The setback, site coverage, amenity space and height rules of the R-CG District all culminate to orient built form towards the street and away from adjacent neighbouring properties, resulting in more substantial side setbacks than seen in typical R-C2 detached or semi-detached development scenarios. Additionally, Administration generally encourage applicants to seek a reduced front setback to move buildings towards the corner and away from neighbours.

**Secondary Suites:** Please refer to the previously provided commentary regarding the building and fire code requirements for including secondary or accessory suites in new R-CG rowhomes, as well as the feasibility of adding or retrofitting accessory suites into built R-CG rowhouse forms.

**Traffic & Parking:** Given current trends toward decreased car ownership in the inner-city and the modest scale of development proposed – 4 units with 4 private garages and no potential for secondary suites – the resultant traffic generation and additional parking demand will be minimal and unlikely to have a material detrimental effect on local traffic or the supply of on-street parking in the neighbourhood. The distinctly residential appearance created by the R-CG District's steady rhythm of doors and porches adds visual interest to the neighbourhood streetscape, calming local traffic and enhancing pedestrian safety and experience along adjacent sidewalks. Today, the lack of on-street parking restrictions in the neighbourhood indicates a healthy and readily available on-street parking supply. Should this change in the future, residents can choose to apply to the Calgary Parking Authority to implement on-street parking restrictions and permits, given sufficient neighbour support.

RND SQR specifically chooses to build in walkable inner-city neighbourhoods, with excellent access to transit, car sharing services and cycling amenities. This gives future residents multiple mobility options and further reduces the potential traffic impacts associated with new development. Those looking for more attainable housing options in inner-city communities are also less likely to opt for multiple vehicle ownership, given the additional costs and considerations associated with car ownership and maintenance.

POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW

MAP 8C



Planning + Design Rationale (LOC2017-0325)

**Density & Neighbourhood Character:** RND SQ R is committed to creating homes that are considerate of their surrounding context, offer a high standard of architectural quality and contribute to the fabric of inner-city neighbourhoods. The proposed land use change and associated development vision represent a modest increase in density, while introducing more flexible and affordable inner-city housing options for Calgarians looking to live in communities with direct and easy access to transit, shopping, schools, amenities, open spaces and other community services.

Today, available forms of housing in Calgary's inner-city communities can be prohibitively expensive for young professional and families. The gap in the affordability of detached and semi-detached housing in inner-city communities like Richmond / Knob Hill is highlighted by the most recent City Centre statistics from CBRE (Fig.2 & 3).

FIG.2 CBRE® CITY CENTRE HOUSING BENCHMARK PRICE STATISTICS  
Source: CREB Monthly Statistics Package - City of Calgary (December 2017)

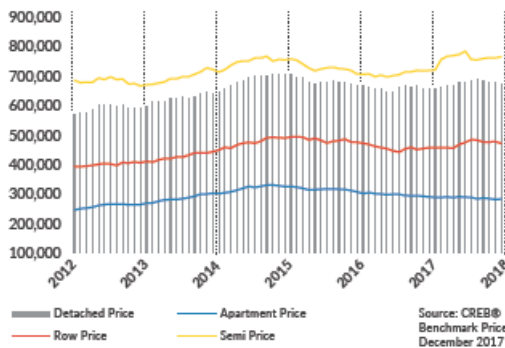


FIG.3 CBRE® CITY CENTRE STATISTICS AREA  
Source: CREB Monthly Statistics Package - City of Calgary (December 2017)



The historic data shows that semi-detached housing prices in particular have continued to climb, making newly built duplexes some of the most expensive housing options available. The benchmark price for an inner-city duplex is now roughly ~\$750,000, putting this housing option out of reach of many families and households who value inner-city living. With a far more affordable benchmark price of ~\$450,000, inner-city rowhomes address the crucial "missing middle" of housing in Calgary- attainable homes that meets the needs of those looking for inner-city housing options that lie somewhere between a traditional condominium and a single-family home or duplex.

Given the diverse range of neighbourhoods represented by the City Centre statistics data, the benchmark housing prices will naturally vary by community. However, the significant affordability gap between the cost of a detached or semi-detached home as compared to a rowhome remains constant. For example, the 2017 MLS sold statistics for Richmond / Knob Hill show that the average sold price for a semi-detached home was ~\$912,000, while the average rowhome sold for ~\$553,000 – a dramatic gap, especially considering the upcoming mortgage rule changes and recent interest rate hikes that will make the costs of home ownership even more challenging for those looking to live in Calgary's inner-city.

The risk of simply maintaining the land use status quo is highlighted in RKHCA's letter of support for the R-CG District, dated May 25, 2014 (Appendix 1): "We estimate that our community is approximately 50% redeveloped, and to date that redevelopment has consisted almost exclusively of older bungalows on R-C2 parcels being demolished and replaced with a pair of 2-storey or 3-storey detached or semi-detached dwelling units. Our concern is that if this development pattern continues Richmond/ Knob Hill will end up being a slightly denser but still excessively homogeneous residential community, with too little demographic or economic diversity (ie. fewer rental, starter or senior-friendly housing options)."

The sensitive addition of rowhomes to neighbourhoods like Richmond / Knob Hill represents a vital opportunity to contribute to the evolving character of Calgary's inner-city and foster complete, resilient and vibrant communities that welcome a diversity of Calgarians.

**POLICY AMENDMENT AND LAND USE AMENDMENT  
RICHMOND (WARD 8)  
2403 28 AVENUE SW  
22 STREET SW AND 28 AVENUE SW**

**MAP 8C**

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Planning + Design Rationale (LOC2017-0325)

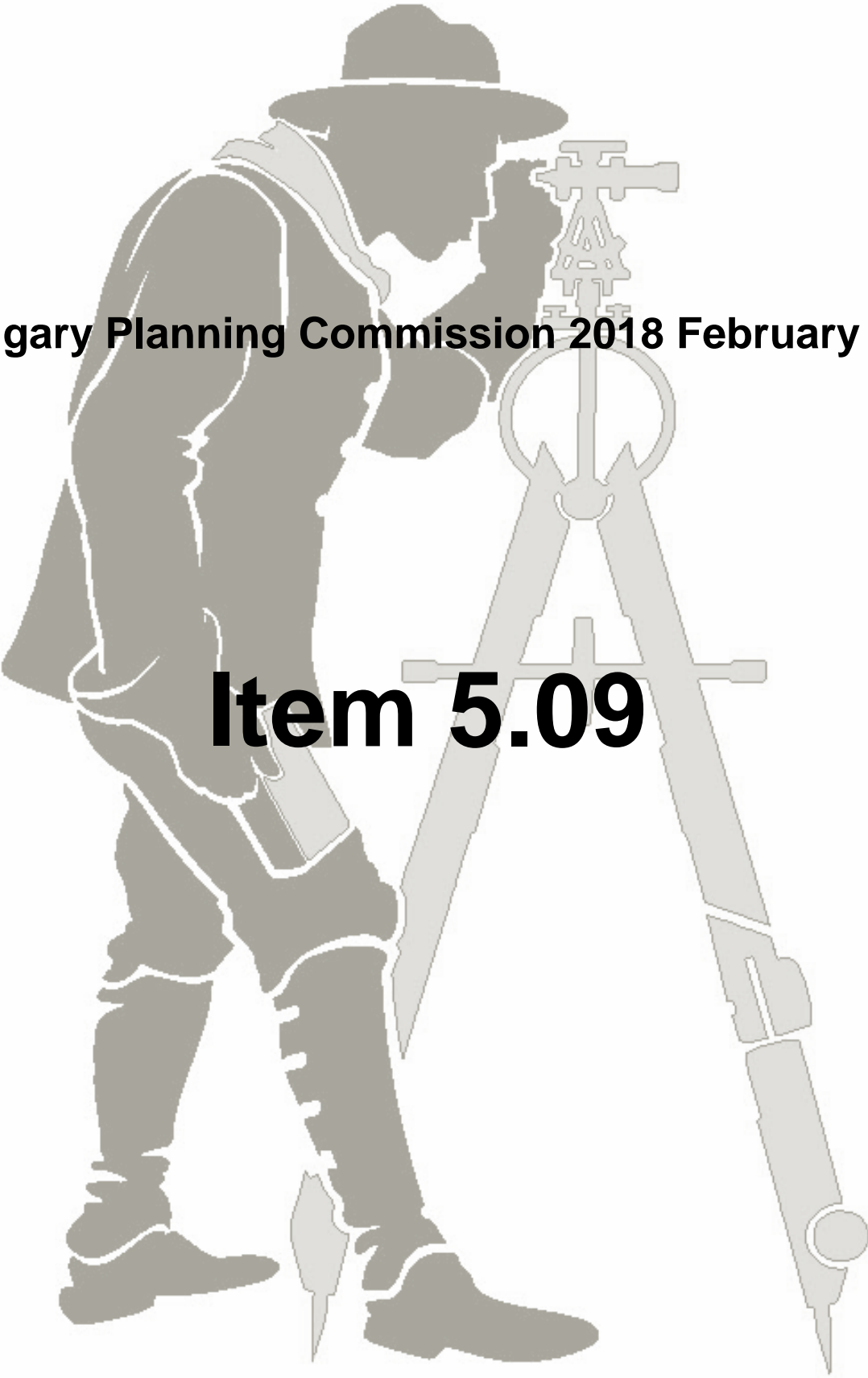
**CONCLUSION**

The proposed land use redesignation at 2403 28 Avenue SW will contribute to the continued vitality of Calgary's inner-city neighbourhoods and facilitate a development vision that addresses the "missing middle" - a form of housing that meets the needs of those looking for inner-city housing options that lie somewhere between a traditional condominium and a single-family home or duplex. For the reasons outlined above, we respectfully request that Administration, Calgary Planning Commission and Council support this application.



**Calgary Planning Commission 2018 February 22**

**Item 5.09**





LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

**EXECUTIVE SUMMARY**

This land use amendment application seeks to redesignate four adjacent residential parcels located at 1403, 1407, 1411 and 1415 – 26A Street SW in the community of Shaganappi from a Residential – Contextual One / Two Dwelling (R-C2) District to a Multi-Residential – Contextual Medium Profile (M-C2) District to allow for a Multi-Residential Development. The site is approximately 100 metre walking distance to the Shaganappi Point LRT Station.

An amendment to the Shaganappi Point Area Redevelopment Plan (ARP) is not required to accommodate this land use amendment application.

A development permit (DP2017-2492) has also been submitted and is currently under review by Administration.

**PREVIOUS COUNCIL DIRECTION**

None.

**ADMINISTRATION RECOMMENDATION(S)**

2018 February 22

That Calgary Planning Commission recommends **APPROVAL** of the proposed Land Use Amendment.

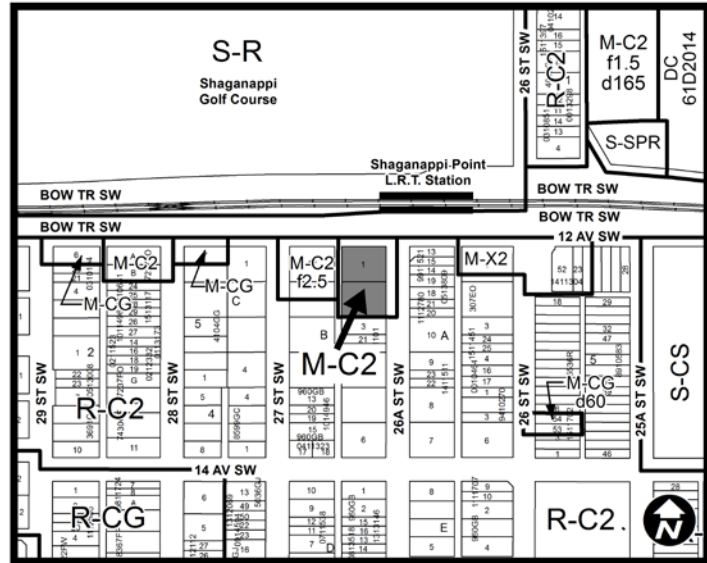
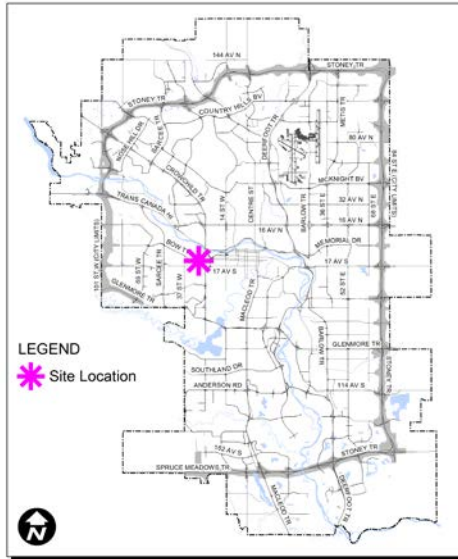
**REASON(S) FOR RECOMMENDATION:**

The proposed Multi-Residential – Contextual Medium Profile (M-C2) District supports an increase in density in close proximity to an existing Transit Centre (Shaganappi Point LRT Station). The proposal is consistent with the relevant policies of the Municipal Development Plan (MDP) and the Shaganappi Point Area Redevelopment Plan (ARP). The proposed land use district allows for a form and uses that are compatible and sensitive to the surrounding community.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

LOCATION MAPS



LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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**ADMINISTRATIONS RECOMMENDATION TO CALGARY PLANNING COMMISSION**

Recommend that Council **ADOPT**, by bylaw, the proposed redesignation of 0.23 hectares  $\pm$  (0.57 acres  $\pm$ ) located at 1403, 1407, 1411 and 1415 – 26A Street SW (Plan 307EO, Block B, Lots 1 and 2) from Residential – Contextual One / Two Dwelling (R-C2) District **to** Multi-Residential – Contextual Medium Profile (M-C2) District.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

**Applicant:**

Casola Koppe

**Landowner:**

Fatima Borhot  
Mohamed Borhot  
Said Borhot  
David Brown  
Jennifer Brown  
Dimsdale Ranching Co Ltd  
Jason Johnston  
Laura Pasacreta

**PLANNING EVALUATION**

**SITE CONTEXT**

The subject site is comprised of four parcels in the community of Shaganappi. The site is approximately 60 metres by 38 metres and 0.23 hectares (0.56 acres) in size and is developed with four single storey single detached dwellings and detached garages that are accessed from the rear lane. The site is surrounded by low density residential use to the east, south and west. However, a development permit has been issued and is pending release for a 60 unit Multi-Residential Development across the rear lane to the west (1404, 1408, 1410 26 Street SW). The site is approximately 100 metre walking distance to the Shaganappi Point LRT Station, 250 metre walking distance to Alexander Ferguson Elementary School, and 350 metre walking distance to Shaganappi Park.

**LAND USE DISTRICTS**

The proposed M-C2 district allows for medium height and medium density multi-residential development in a variety of forms in close proximity to, and adjacent to, low density residential development, and is ideally located near transit nodes. Transitional heights and contextual setback requirements ensure that redevelopment reflects the immediate context and is sensitive to the surrounding community.

The application proposes to increase density to allow for approximately 73 dwelling units in a three to five storey building, as proposed in the development permit application (DP2017-2492) currently under review. The existing land use (R-C2) allows for up to eight single or semi-detached dwelling units (plus potential secondary suites in the single detached dwellings).

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

According to data from The City of Calgary 2017 Census, the following table identifies Shaganappi's peak population and year, current 2017 population and the population amount and percentage difference between the peak and current populations.

Shaganappi	
Peak Population Year	1969
Peak Population	2,132
2017 Current Population	1,693
Difference in Population (Number)	-439
Difference in Population (Percent)	-20.6%

## LEGISLATION & POLICY

### South Saskatchewan Regional Plan (2014)

The recommendation by Administration in this report has considered and is aligned with the policy direction of the South Saskatchewan Regional Plan.

### Municipal Development Plan (2009)

The subject site is classified as Residential Developed – Inner City on the Urban Structure Map (Map 1) in the *Municipal Development Plan* (MDP). This proposal is consistent with MDP policies, including Transit-supportive Land Use Framework policies (subsection 2.2.2), Neighbourhood Infill and Redevelopment policies (subsection 2.2.5), Housing Diversity and Choice policies (subsection 2.3.1), General – Developed Residential Areas policies (subsection 3.5.1), and Inner City Area policies (subsection 3.5.2).

### Shaganappi Point Area Redevelopment Plan (2014)

The City, along with the community developed the Shaganappi Point Area Redevelopment Plan (ARP) in order to guide redevelopment pressures in this area as a result of the completion of the West LRT and specifically the Shaganappi Point LRT Station, and redevelopment interest in the area. The ARP provides clear policy direction that encourages higher density development along 12 Avenue SW and near the LRT station, while ensuring redevelopment respects and enhances the existing community.

The subject property is located within the Medium Density Residential area on Map 2.1: Land Use Policy Areas (APPENDIX III) of the ARP. The intent of the Medium Density Residential area is to provide the opportunity for increase in density, while ensuring redevelopment is sympathetic with the surrounding community. The ARP envisions new development within this area should be limited to medium-density multi-residential developments and include townhouses, apartments, and live/work units.

**LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW**

**MAP 18C**

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The proposed M-C2 district aligns with the intent of the Medium Density Residential area, as it provides for multi-residential developments in a variety of forms, and allows for varied building heights and front setback areas that reflect the context of the adjacent low density residential development.

Location Criteria for Multi-Residential Infill

In 2014, Council implemented 'Location Criteria for Multi-Residential Infill' to provide a more specific tool for the review of land use amendment applications in the Developed Areas and associated amendments to local area plans. The proposed land use generally aligns with several of the location criteria for multi-residential infill development in low density residential areas. The table in APPENDIX V provides a summary of alignment with the criteria. The criteria are not meant to be applied in an absolute sense, but are used in conjunction with other relevant planning policy, such as the MDP, to assist in determining the appropriateness of an application in the local context.

**TRANSPORTATION NETWORKS**

The site is located approximately 100 metres walking distance from the intersection of Bow Trail SW and 26 Street SW, giving direct connection to the Shaganappi Point LRT station and transit stops for the circle route (72/73). On-street parking adjacent to the site is regulated through the Calgary Parking Authority's residential parking permit system. Multi-family sites must provide all required parking on-site and residents are not issued residential parking permits.

The applicant has been advised that the development permit currently under review shall meet all bylaw requirements, including: required parking, parking stall size, loading stall, visitor parking, and bike parking. Vehicular access to the site is available from the rear lane.

**UTILITIES & SERVICING**

A Sanitary Servicing Study and Fire Flow Calculations will be required through the development permit application currently under review. Any upgrades required to support the proposed development will be at the developer's expense. Servicing arrangements shall be to the satisfaction of the Manager Infrastructure Planning, Water Resources.

**ENVIRONMENTAL ISSUES**

An Environmental Site Assessment was not required for this application. An Environmental Site Assessment may be required through the development permit process.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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## ENVIRONMENTAL SUSTAINABILITY

Close proximity to the Shaganappi Point LRT station provides opportunities for alternative modes of transportation, other than single occupancy vehicles. The increased maximum density of the site allows for a more efficient use of resources. Environmentally sustainable design can be achieved through the proposed land uses. Building and site design will be assessed at the development permit application review stage.

## GROWTH MANAGEMENT

This land use proposal does not require additional capital infrastructure investment and therefore, no growth management concerns have been identified at this time. The proposal is in alignment with the MDP references associated with growth management matters.

## PUBLIC ENGAGEMENT

### Community Association Comments

Administration received a letter of opposition to the application from the Shaganappi Community Association (APPENDIX II). The community association contends that the application proposes medium density redevelopment in the Low Density Residential area identified on Map 2.1: Land Use Policy Areas (APPENDIX III). Due to the encroachment of medium density development into the low density area of the neighbourhood, the community association raised concerns with buffering and protecting the substantial amount of redevelopment that has occurred on the east side of 26A Street SW under existing R-C2 land use.

Based on a review of the Shaganappi Point ARP, Administration identified pertinent references to interpreting the boundaries separating different land uses in Map 2.1, including the footnote on Map 2.1 and item 6.1.1. The references identify the boundaries as conceptual only and are to be determined at the land use amendment or development permit application stage. Administration is satisfied that the proposed land use application aligns with land use area boundaries in light of the overall objectives of the Shaganappi Point ARP.

### Citizen Comments

Administration initially received eleven (11) letters of opposition to the application from citizens. Reasons stated for opposition focused on the following themes:

#### Increased Traffic

Concerns were expressed about increased traffic volume generated by the development that would use local streets and lanes (26A Street SW, 27 Avenue SW, and lane between 26A Street SW and 27 Avenue SW in particular) to access Bow Trail SW.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

Administration will ensure traffic volume is reviewed as part of the Development Permit application to ensure residents' concerns are appropriately evaluated.

Parking

Concerns were expressed about increased competition for public on-street parking, particularly on 26A Street SW. However, on-street parking adjacent to the site is regulated through the Calgary Parking Authority's residential parking permit system and the applicant has been advised that no parking permits will be issued for a Multi-Residential Development on the subject site.

Building Height and Transition to Surrounding Development

Concerns were expressed about the size and height of the building, and sensitivity to surrounding low density development. Administration reviewed the purpose and regulations of the M-C2 district and building form and site design policies in the Shaganappi Point ARP (Section 3.0) and concluded that the district in conjunction with ARP policy ensures medium density development is compatible with the surrounding low density development.

Neighbourhood Character

Concerns were expressed about the compatibility of multi-residential development, in particular of buildings in excess of three storeys with the context of new homes and buildings on the east side of 26A Street SW. In addition, many comments were received regarding including the fourth parcel from the north end of the block (1415 – 26A Street SW) in the application. Administration reviewed the ARP and concluded that boundaries shown on Map 2.1 are intended to be conceptual only (see Section 6.1.1 – Interpretation of Map Boundaries), and the proposed M-C2 district in conjunction with ARP policy support medium density development in close proximity to low density development.

**Public Meetings**

A public meeting led by the applicant was held 2017 November 20, which included a presentation of the project and a question-and-answer period during which the applicant (Casola Koppe), developer (Chander & Associates) and Administration were present and responded to stakeholder questions. Representatives of the Shaganappi Community Association and the Ward 8 Councillor's office were also present. As a result of this meeting, the applicant, developer and The City committed to additional communication once residents had an opportunity to formulate a formal response.

Administration received a formal submission from area residents on 2017 December 4 (APPENDIX IV). Administration, applicant, developer and appointed community representatives met on 2017 December 6 to discuss the following concerns raised by the residents in their submission:

- inclusion of the fourth lot (1415 – 26A Street SW) in the land use and development permit applications;

**LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW**

**MAP 18C**

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- compatibility of the proposed development with the existing character of the neighbourhood;
- traffic and noise generation;
- erosion of community character due to a lack of ownership of the units in the proposed development; and
- infrastructure (sewer) capacity.

All parties agreed to a subsequent information session after the applicant had an opportunity consider feedback received and amend the development application to support the proposed land use application.

A subsequent information session was held 2018 January 11. The applicant provided an overview of changes to the design of the proposed development to address community comments. These changes included addressing the compatibility of the proposed development with the existing character of the neighbourhood (fifth storey stepback along 26A Street SW, enhanced landscaping, refined building entry, and finer grain facade treatment) and inclusion of the fourth lot (1415 26A Street SW) in the land use and development applications. Administration's Corporate Planning Applications Group also attended the meeting and responded to concerns about increased competition for on-street parking, increased traffic and infrastructure capacity.

Following the applicant's resubmission of amended development permit drawings on 2018 January 23, Administration received a letter of support to the application from the Shaganappi Community Association (SCA) (APPENDIX VI) and a letter of conditional support from area residents (APPENDIX VII).

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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APPENDIX I

APPLICANT'S SUBMISSION



Tony Casola Architect Ltd. Hans Koppe Architect Ltd.  
Suite 300, 1410 1st Street SW, Calgary, Alberta, T2R 2R2  
Ph: 403 287-9960 Ext. 2 Fax: 403 287-9962

Friday, December 22, 2017

City of Calgary Planning and Building Department  
Re: Land Use Redesignation, 1403, 1407, 1411 and 1415 26a Street SW, Shaganappi

Existing Zoning – RC-2  
Proposed Zoning – MC2

Applicable Community Policies – Shaganappi Point Area Redevelopment Plan

Upon review of existing policy and meetings with the City Planning Department, Ward 8 Councillor Woolley and Community Planning Representative Ron Goodfellow, our client has asked us to formally apply for a Land Use Redesignation for parcels mentioned above.

Current policies related to Transit Oriented Development, and general intent of Municipal Development Plan could inspire a developer to aim for higher density developments, however the recently approved Shaganappi Point Area Redevelopment Plan (SPARP) clearly limits development to Medium Density Development and a maximum height of 16m.

Originally we had looked at proceeding with an MH1 application with FAR and height restrictions, however, the limitations of this zoning means further stepbacks and setbacks related to the general purpose of the MH1 designation, which is a small point tower on a mixed use podium. MH1 also comes with the stigma of High Density Residential.

The stepbacks at 10 m height would make it very difficult and costly to transfer loads in a wood framed building. The lack of an appropriate land use which bridges the requirements of MC2 to MH1 is an ongoing issue, which has been exacerbated by the 2014 ABC's provision to now allow for 6 storey wood framed buildings.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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As an MC2 zoning would appear to provide an adequate solution, the requirements as outlined in LUB 604.4 pertaining to reduced upper floor plate of 40% maximum, make it difficult to provide an adequate upper floor which would provide enough revenue to qualify the costs related to a 5 storey building. The new classification in the 2014 ABC, 3.2.2.50, has the following requirements which are above and beyond those of 4 storey combustible;

- More expensive Sprinkler System
- Non-Combustible Cladding requirements
- Robust framing required at lower floors including steel connections for lateral loads.
- 

We are effectively asking for a contextually sensitive MC2 with a relaxation for FAR from max 2.5 to max 2.54 (as determined by current DP stage) a negligible number, and to increase the maximum floor area of upper storey (above 12m) from 40 % to 77%. This will provide opportunities to minimize shadowing impacts to adjacent low density parcels located to the South. We have provided a setback along most of the façade facing 26a Street to minimize visual impact to residents across the street.

Sincerely Yours,



Hans Koppe, Principal  
Architect, AAA, MAA, SAA, LEED® AP  
Casola Koppe Architects

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

APPENDIX II

SHAGANAPPI COMMUNITY ASSOCIATION FIRST SUBMISSION – 2017 JUNE 12



June 12, 2017

Adam Sheahan  
Planner | Planning & Development #8201  
The City of Calgary  
700 Macleod Trail South  
Calgary AB  
T2G 2M3

Dear Sir:

**Re: LOC2017 -2021 – 1403 26A St SW**

The Shaganappi Community Association is in support of the many affected residents who have written letters of objection to you on the above land use amendment.

Notwithstanding the footnote on Map 2.1 and item 6.1.1 which suggests the three lot (150 feet) strip of land along 12th Ave SW is conceptual only, we are of the view that the ARP specifically addresses our concerns with respect to buffering and protecting the substantial amount of redevelopment that has already been made under the existing R-C2 land use. From our perspective, the line is not conceptual as it was specifically intended to support the single family residential uses throughout the core of our very narrow (often only two blocks wide) community.

The same issues were addressed along 17th Avenue when the Mainstreets program was recently passed.

During the many discussions and open houses that addressed the ARP, it was agreed by the Community that the first three lots south of 12th Ave SW was an appropriate border for new multi-family housing. However, there is also a requirement for this multifamily development to be sensitively stepped down in height and to have setbacks that respect the existing low-density development.

This application does not respect these criteria and as such, we cannot support it.

Yours truly,

Ron Goodfellow, FRAIC (retired)  
Director of Community Development  
Shaganappi Community Association  
403 233 0179

Mike Wilhelm  
President  
Shaganappi Community Association

Cc: Evan Woolley  
Beth Tingle, V.P., Shaganappi Community Association  
David Hawthorn, Shaganappi Community Association

A. Sheahan

LAND USE AMENDMENT  
 SHAGANAPPI (WARD 8)  
 12 AVENUE SW AND 26A STREET SW

MAP 18C

APPENDIX III

MAP 2.1: LAND USE POLICY MAP  
SHAGANAPPI POINT AREA REVELOPMENT PLAN

*Map 2.1: Land Use Policy Areas*



**Legend**

- Plan Area Boundary
- LRT Station
- Low Density Residential
- Low Density Multi-Residential
- Medium Density Residential
- Residential Commercial
- Seniors Mixed-Use
- Park/ Open Space

This map is conceptual only. No measurements of distances or areas should be taken from this map.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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**APPENDIX IV**

**AREA RESIDENTS' FIRST SUBMISSION – 2017 DECEMBER 4**

December 4, 2017

Regarding the Proposed Development 1403 to 1415 26A ST SW

The residents of Shaganappi have organized themselves to express their concerns about the proposed development at 1403 to 1415 26A ST SW. The Community Association worked in good faith with the City to meet the desire for Transit-Oriented Development while respecting the character of the community and the interests of the current residents. This is a narrow community bordered by Bow Trail and 17th Ave SW, and the community felt that up-zoning should be limited to land on those major corridors and major North-South streets. This proposal would clearly require up-zoning in an area not intended by the community in the Plan. In fact, the community has already made concessions that will quintuple the population of this small neighbourhood without deviating from the conceptual map in the ARP.

The concerned residents are willing and prepared to work with the City planners, the Architect and developer to address the following concerns to maintain and improve the character of the Shaganappi community.

**Prioritized list of concerns raised by the residents:**

- 1. The fourth lot (1415 26A St SW) should not be included in the proposed development:**
  - i. The expansion of the proposed development to include lot 1415 26A St SW is beyond what was envisioned in the ARP and will materially interfere with or affect the use, enjoyment or value of neighbouring parcels of land specifically those properties (1400, 1402, 1404, 1406 & 1410) facing West on 26A St SW and 1419 26A St SW facing East.
  - ii. The contextualization transitioning as prescribed in the ARP should be incorporated into the end of the building occupying 1411 26A St SW to minimize the impact on the residences occupying lot 1415 26A St SW.
  
- 2. The facade of the building facing 26A St SW is inconsistent with the character of the neighbourhood:**
  - i. The facade proposed for the 5-storey modernistic building faces a group of newer single-family homes that are likely to remain there for the foreseeable future, and it would be completely incongruous in the existing streetscape.
  - ii. The fifth floor does not comply with the ARP requirements for 40% of the floor plate. The upper floor should be set back from 26A St SW and the residence at 1415 26A St SW to minimize the impact on the use, enjoyment or value of neighbouring parcels of land.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

- iii. The height of the building, the public space located on the South end of the building and the balconies facing East will erode the privacy for existing landowners when the elevation creates sight lines into the existing residents front verandas, windows and backyards.
- iv. The height of the building and large rectangular profile will restrict sight lines and views for the existing residents.
- v. The ARP describes creating a walkable community: the North facing wall of the proposed development coupled with the noise of the Bow Trail will further dissuade walking traffic on 12th avenue as residents will continue to avoid the noise tunnel that this combination will create.
- vi. The residents have concerns about the owners' ability to manage the upkeep of the building, what assurances can be given that the balconies will not be used as storage lockers, etc. that is inconsistent with the otherwise clean and orderly existing neighbourhood.
- vii. Possible solutions suggested include:
  - a. Where possible the profile of the building should be softened, e.g. better colour selection, less dramatic facade, etc.
  - b. Step back the 4th & 5th floor; or reduce the height to a 4-storey building
  - c. Reduce the imposing profile facing 26A St SW by removing balcony roofs on the upper floor d. Re-design the South West units on floors 2 and 3 so that their balconies do not look down into the back yard of 1415 26A St SW
  - d. Consider a solid or opaque deck guard rail to minimize the impact of elevated balconies on the properties in the community.

**3. Traffic and noise:**

- a. The back alley between 26A St & 27 St will become the main access route for over 78 parking spots in the proposed development which will overload this narrow and congested alley. As a result 26A St SW will become the thoroughfare for the residents of the proposed development.
- b. The traffic from 12th Ave travels at excessive speeds on 26A ST SW and regularly ignores the traffic rules and signs at the 12th ave/26A St Intersection. We ask for the sake of the children on our street and in the area that some means of traffic calming technology be installed. iii) The sidewalk on the North side of 14th Ave at the south end of the back alley between 27th St and 26A St SW has a significant amount of foot traffic, many of whom are kids enroute to Alex Ferguson.
- c. Possible solutions:
  - i. Install some form of traffic calming technology on 26A St SW and additional signs to show "No access to bow trail"
  - ii. Install some form of traffic calming technology at the South end of the back alley where it empties into 14th Ave.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

- iii. Install a 'Right Turn Only' exit sign and a 'Left Turn Only' entry sign at the parkade entry ramp to keep the additional traffic to the north end of the alley (d) A possible solution could include parking garage access off of 12th Ave to dissuade use of the back alley to access the parking garage.
  - d. We understand that the provision of sound walls on the South side of the Bow Trail in conjunction with the West LRT was restricted by utility issues on the north outside perimeter of 12th Avenue, in light of that, the city might be squandering an opportunity to improve an already challenging pedestrian and bike environment, from a sound and visual impacts perspective, through thoughtful sound attenuation on the south side.
- 4. The proposed development is inconsistent with the character of the community:**
- a. Shaganappi is a family-friendly community with a highly-ranked elementary school and an active community association – a building comprised of rental apartments is inconsistent with the current community comprised of residents who own and live in their houses.
    - i. The community is concerned that the lack of ownership of the units in the proposed development will erode the current character of our neighbourhood.
- 5. The aging utility infrastructure on 26A ST SW has created a number of water-related issues:**
- a. What actions are planned or have been taken to address these proactively?
    - i. The water table may be very high, is the developer aware, and do they have plans to address this issue? Has a geotechnical analysis been done?
    - ii. 1403, 1407, 1402, and 1404 26A ST SW had sewer backups in Feb 2016. 1402 and 1404 had flooding in a previous year and the prior owner believed water runs beneath these properties.

The residents of Shaganappi are approaching Cllr Woolley, City planners, the Architect and developer to address these concerns in an effort to maintain and improve the character of the Shaganappi community. This letter summarizes the concerns and will be supported in person by the residents when and where required to ensure that the impact is understood by the proponents of the development.

The residents have agreed as a group that we are prepared to work with the City and developers to sustain the character of the Shaganappi community while the City densifies the core.

Concerned Residents

LAND USE AMENDMENT  
 SHAGANAPPI (WARD 8)  
 12 AVENUE SW AND 26A STREET SW

MAP 18C

APPENDIX V

LOCATION CRITERIA FOR MULTI-RESIDENTIAL INFILL

Location Criteria for Multi-Residential Infill	Meets Criteria?
1. On a corner parcel.	Yes
2. Within 400 metres of a transit stop: <ul style="list-style-type: none"> <li>• Approximately 100 metres to route 72/73 stop</li> </ul>	Yes
3. Within 600 metres of a transit stop on the Primary Transit Network approximately: <ul style="list-style-type: none"> <li>• Approximately 100 metres to Shaganappi Point LRT Station</li> </ul>	Yes
4. On a collector or higher standard roadway on at least one frontage: <ul style="list-style-type: none"> <li>• Site fronts 12 Avenue SW, which is a frontage road for Bow Trail</li> </ul>	Yes
5. Adjacent to existing or planned non-residential development or multi-unit Development <ul style="list-style-type: none"> <li>• Development permit (DP2015-5350) has been issued and is pending release for a Multi-Residential Development on three parcels to the west</li> </ul>	Yes
6. Adjacent to or across from an existing or planned open space, park or community amenity: <ul style="list-style-type: none"> <li>• Site is approximately 350 metre walking distance to Shaganappi Park</li> </ul>	No
7. Along or in close proximity to an existing or planned corridor or activity centre: <ul style="list-style-type: none"> <li>• 17 Avenue SW is identified as a neighbourhood corridor and is located approximately 350 metres south</li> </ul>	Yes
8. Served by direct lane access <ul style="list-style-type: none"> <li>• Single access to underground parkade from rear lane maintains sidewalk continuity and available on-street parking</li> </ul>	Yes

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

APPENDIX II

SHAGANAPPI COMMUNITY ASSOCIATION SECOND SUBMISSION – 2018 FEBRUARY 7



February 7, 2018

Adam Sheahan  
Planner II | Centre West Community Planning  
The City of Calgary  
5<sup>th</sup> Floor, 800 Macleod Trail S.E.  
Calgary AB  
T2G 2M3

Dear Adam:

**Re: DP2017-2379/LOC 2017-0121 @ 1403 to 1415 26-A St SW – THE “GIORDANO”**

Further to the recent consultations with residents, the Shaganappi Community Association, Koppe Casola Architects and the Developer’s representative, the Shaganappi Community Association is pleased to confirm our support for the rental apartment project, now known as the Giordano, located at the above address.

Except for the matter outlined in the next paragraph, the consultations have resolved the broader issues with respect to the new development’s integration into the community. It is understood that the remaining issues will be worked on as the development proceeds through the planning process. In that regard, we would ask you to refer to the January 24, 2018 letter addressed to Evan Woolley from Bill Sweet and Geoff Bowerman on behalf of the adjoining residents.

We have one community wide concern that does remain. This is in regard to the interpretation of clauses in the 12th Ave ARP which suggests (contrary to the mapping) that the multi-family interface with the RC-2 lands designations south of 12th Ave SW were deemed more flexible than we originally thought. The Community Association (“CA”) and our residents believed that the multi-family precinct along 12th Ave was three lots, or 150 feet wide. The CA would like further City Planning clarification on this issue to provide future clarity as to the ARP’s intent.

In this case, the Developer was able to modify his plans in a manner that continued to use the fourth lot and helpfully mitigated the impacts on the RC-2 lot directly south of their project through setbacks and transitional massing. Consequently, we are in a better position to support the incorporation of the fourth lot into this particular development if discussions with the adjoining residents continue to be positive.

We also understand that vehicular traffic, pedestrian and stormwater issues require further review and that the appropriate City Departments will be addressing same.

Shaganappi Community Association  
2516 – 14 Avenue SW  
Calgary AB T3C 3V2

A. Sheahan

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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Adam Sheahan, Planner II

DP2017-2379/LOC 2017-0121 @ 1403 to 1415 26-A St SW – THE “GIORDANO”

Page 2 of 2

In conclusion, an application that could have very well become fractious evolved into one of honest negotiation and cooperation between all the parties. The willingness of the Developer to work with the Community and the adjoining residents and the guidance of various City officials involved in the process is exemplary.

Yours truly,



Ron Goodfellow, FRAIC  
Lead, Planning and Development  
Shaganappi Community Association  
403 233 0179



Michael Wilhelm  
President  
Shaganappi Community Association

Cc: Evan Woolley, Brienne Biblow, Ward 8 Office, City of Calgary  
Development Committee, Shaganappi CA

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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## APPENDIX VII

### AREA RESIDENTS' SECOND SUBMISSION – 2017 JANUARY 24

#### **Regarding the Proposed Development 1403 to 1415 26A ST SW:**

Our concerns and suggested changes and actions:

As a follow-up to the letter addressed to the developers on Dec 4, 2017, the concerned residents of Shaganappi would like to thank Hans Koppe & Roshan Chander for the changes made to the proposed development.

Notwithstanding our ongoing concerns regarding the height and length of this proposed building, with the latest version presented to the residents on Jan 11th, 2018, the major concerns with the appearance and the contextualization of the building design have been corrected to our satisfaction.

The residents have a few additional concerns that have arisen out of the Jan 11th meeting and a separate meeting held by the residents on Jan 18th. These are as follows:

1. The residents of 26A St SW wish for the developer to formally acknowledge that they will forgo applying for tenant parking passes on 26A ST SW as verbally committed during the meeting.
2. The residents do not wish to have a 9.1 m loading zone in front of the building on 26A ST SW as this will further reduce the available parking spots on the street and ask that the developer also formally acknowledge that they will forgo applying for a loading zone on 26A ST SW as verbally committed during the meeting.
3. We understand the developer to say that a resident manager and or full time janitor will be employed by the owners of the building and the residents would appreciate receiving confirmation of this.
4. We understand that the owner will be creating residency rules regarding use of the property including a small dog policy (ten lbs or less), the use of balconies overlooking properties, in particular the use of the balcony to store the owners possessions, etc.. We would appreciate receiving confirmation of these rules and that the owners will enforce these.
5. Confirmation that the developer will install, maintain and use an irrigation system for the property improvements facing 26A St SW to promote growth of the grasses, trees and shrubs that the developer has committed to plant.

LAND USE AMENDMENT  
SHAGANAPPI (WARD 8)  
12 AVENUE SW AND 26A STREET SW

MAP 18C

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6. Confirmation that the developer will install a 6 ft high wooden fence on top of the new retaining wall to be constructed between the properties of 1415 & 1419 26A St SW.
7. Confirmation that the landscaping will consist of:
  - a) Full hedge planting to block the planters along 12th Ave and 26A St SW,
  - b) A Fescue lawn,
  - c) Creepers and vines to soften the building image,
  - d) The trees as indicated in the information provided will be approximately 2 - 3m in height with a 75mm trunk,The developer has committed to installing direction signs to promote traffic flow in the alley between 26A St SW and 27 St SW.

**New requests:**

1. The residents also request that large boulders be used as fill between the new 2m high fence and the building south facing wall to discourage traffic and temporary use by vagrants.
2. The residents would like to have input into the times that the garbage from the building will be removed. Our concerns relate to blocking traffic on the alley and the noise associated with the large trucks used for garbage collection.

The concerned residents would appreciate a written response to the concerns raised in the previous letter dated Dec 4th, 2017 and the requests made in this letter.

The concerned residents of Shaganappi will also reply separately to the letter from Cllr Woolley and will elaborate further our concerns that this development presents to our community to the City planners in an effort to maintain and improve the character of the Shaganappi community.

Concerned Residents